

central subway

Laying down the rails

Track has been placed and most rail foundations have been built inside the northbound tunnel up to Chinatown Station.



Progress Report

November 2018

HAPPY

Thanksgiving



SFMTA

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Cover photo: A crew cleans up at the end of a shift installing rails and rebar for track plinths in the northbound tunnel as it approaches Chinatown Station. Once rail and plinth installation reaches the track crossover cavern, rail will be installed inside the southbound tunnel through to where it is already placed just south of Union Square/ Market Street Station. **More photos can be found starting on page 36.**

Above photos: Workers discuss operations inside the cross-cut cavern, where the Chinatown Station platform cavern opens up into the headhouse, directly under the Stockton and Washington intersection.

See the Appendix E final page for CS websites hyperlinks and public outreach on line resources. The Project main web site is at: <http://www.centrlsubwaysf.com/>



A crew removes materials from the soil as part of regrading work where Clementina Alley approaches 4th Street, adjacent to the YBM headhouse.

Executive Summary

Chinatown Station - Completed construction of track slab at Crossover Cavern. Completed contact grouting for Crossover Cavern arches. Continued construction of guideway stem walls at South Platform Cavern. Began construction of guideway stem walls at North Platform Cavern. Continued placing concrete for Crosscut Cavern headwall. Continued street work (minor), ongoing monitoring and surveying.

Union Square/Market Street Station - O'Farrell and Stockton Street: Completed installation of water main/laterals and constructed new sidewalks/asphalt pavement roadway. Platform Station: Continued construction of stairs and elevators. Installed CMU walls, placed concrete for metal decks and wale encasements at multiple levels. North Concourse: Continued installation of fire sprinklers on Concourse and Fan levels. South Concourse: Continued installation of fire sprinklers and MEP. Stockton Street: Implemented 2018 Holiday Moratorium Winter Walk.

Yerba Buena/Moscone Station - Continued re-establishing utilities at 4th & Howard. Installed catch basin and culvert at 4th and Clementina. Backfilled PG&E #7 boxes at Folsom. Completed installing Stair 4. Continued installing Stair 6 & 7. Continued installing Headhouse Vent Shaft. Continued F/R/P of Headhouse Mezzanine walls Headhouse Undersurface stairs. Continued electrical work in Headhouse Concourse. Continued installing granite art panels and crystallized glass panels in Station Concourse. Continued F/R/P of seismic joints at Station Platform.

Surface, Track and Systems– Continued traction power conduit installation inside tunnel. Continued installing street light pole foundations on Townsend. Continued sidewalk and curb ramp pavement renovation at 4th/Bryant. Ongoing strut bracket installation in both tubes working South to North. Continued plinth construction in tunnel. Completed track slab construction at CTS Crosscut Cavern.

Tunnel - Contract administrative closeout is ongoing.

Total project costs to date are \$1,277.25 million, an increase of \$7.51 million over last month. The total cost to date equals 80.93% of the total project budget of \$1.578 billion. The Master Project Schedule forecasts a Revenue Service Date of December 2019.

The Stations Contractors' Safety Reports should show one recordable accident took place this month. The rates of work site accident incidents by the man hours worked continue to be below industry standards - see tables on page 32.

Key Milestones

Installing rails and track plinths in the northbound tunnel



1. Workers install rebar and rails atop a steel framework



2 Concrete is poured, walkways are added, and utility brackets are installed

MILESTONE	DATE EXPECTED
General	
Revenue Service	December 2019
Contract 1300 Stations, Surface, Track, Systems	
Notice to Proceed (NTP 1)	June 17, 2013 (A)
Notice to Proceed (NTP 2)	January 12, 2014 (A)
Substantial Completion	June 30, 2018



3 Once track installation reaches the track crossover cavern it will switch over to the southbound tunnel.

Costs and Schedule

Costs (See Appendix A for Details)

The Current Cost Estimate (CCE) for the Central Subway Project is **\$1.578 billion** in year of expenditure dollars (\$YOE). This total project cost is shown at the top of Report 7.1, Program Project Budget. This capital cost projection incorporates allocated and unallocated contingencies to cover the risks associated with the project completion.

Total net incurred costs for the project are \$1,277.25 million, a \$7.51 million increase over last month. The cost to date figure reflects expenditures through FAMIS 786 Report (\$1,238.55 million) plus the utilities joint trench Form B Reimbursement payment (\$11.27 million), invoices currently being processed (\$23.87 million) and estimates of outstanding pay requests (\$3.59 million). This incurred amount equals 80.93% of the total project budget of \$1.578 billion.

The current funding level to date is \$1,517.03 and includes FTA \$23.02 million FY2018/2019 New Starts Grant appropriated in November 2018. This represents 96% of the total project budget.

Earned Value Analysis

In November 2018 Report, the Preliminary Earned Value Analysis reports is based on the SFMTA November Schedule Update. The Planned Value, Earned Value, Actual Cost, Percent Complete and resulting indexes as follows:

Preliminary November Earned Value

Overall Budgeted Cost:	\$1,578,300,000
Planned Value:	\$1,537,943,252
Earned Value:	\$1,276,990,338
Actual Cost:	\$1,277,249,638
Schedule Performance Index (SPI):	0.83
Cost Performance Index (CPI):	1.00
Percent Complete:	80.8%

*November 2018 Notice: The City continues to transition from FAMIS to Financial System Project (FSP). During the transition, we are unable to provide accurate financial updates. Once FSP is updated and validated, we will reconcile our reports accordingly. We will be projecting current expenditures and anticipate the reconciled updates will be available early next year.

Please note that we continue to experience problems that are being caused by error and inaccuracy from the transition. The sources of error include errors resulting from mapping index codes in the old FAMIS system to translating chartfield accounts in the new Financial System Project as well as errors arising through processing such as abatements and adjustments being assigned to the incorrect account. As we continue to address these data quality problems through data cleaning, you will continue to see monthly fluctuations as we detect and remove errors and inconsistencies from data in order to improve on the quality of data so that we will be able to report accurate data.

Schedule Highlights

The Master Project Schedule (MPS) below includes progress through November 2018. The November 2018 Schedule Update submittal from Contract 1300 Contractor was not submitted as the CN1300 Contractor has not provided the updated corrections to their June, through May 2018 Schedule Updates. The Contract 1300 schedule represented in this report is based on the SFMTA November 2018 Schedule Update.

The MPS shows a forecast Revenue Service Date of December 2019.

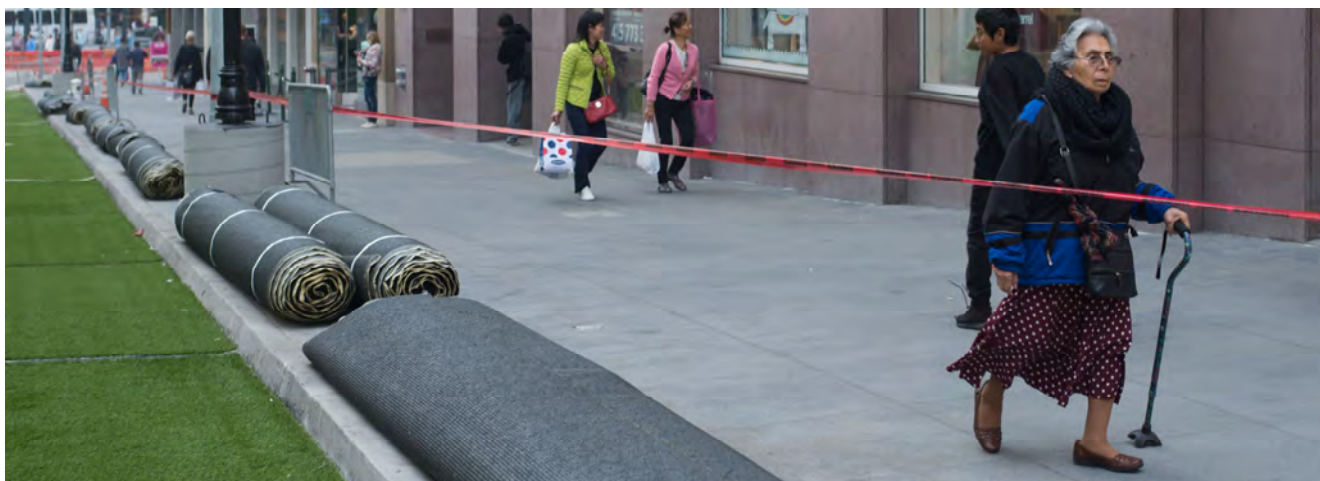
The controlling critical (longest) path of the MPS runs through the tunnel Concrete work, followed by Electrical activities, STS Startup & Testing, Commissioning and Pre-Revenue Activities to the Baseline Finish and Revenue Service Date. See Appendix B – Longest Path. The latest schedule shows the longest path running through the Chinatown Station (CTS). Contractor is required to implement a Recovery Schedule to put the Project back on schedule.

Schedule Contingency is fully utilized on the critical path of the MPS, which is below the Minimum Schedule Contingency level of 6 months. A schedule re-evaluation will be performed, utilizing the updated Contract 1300 Schedule. Recovery options are being implemented in key areas as work proceeds. SFMTA continues to meet with Contractor to discuss all schedule concerns and comments. Contract 1300 Schedule shows a delay in November due to Low productivity on electrical work within the tunnels caused by reduced number of resources on site has delayed the project with a new forecasted Revenue Service Date of 26 December 2019.

Contract 1300 Contractor submitted forty five (45) Schedule Updates from December 2014 to October 2018. SFMTA rejected twenty seven (27) Schedule Updates from January 2016 to April 2016 and June 2016 to May 2018 due to multiple and repetitive issues that vary from incorrect working sequences to unrealistic forecasted completion dates to artificially steering the schedule longest path through certain portions of the project. SFMTA approved as noted December 2014 through December 2015, and May 2016 Schedule Updates. Contractor has been directed to develop a Recovery Schedule as required by Contract to mitigate the current forecasted project delay. The 18 month “gap” of missing Schedule Updates at the beginning of the job has interfered with efficient resolution of Contractor’s assertions of Unavoidable Delay to the project-wide Substantial Completion date, which is additionally impacting the Contractor’s review of options for schedule recovery.

Contract 1300 - WP1253 UMS / WP1254R CTS / WP1255 YBM / WP1256 STS:

The Contractor, Tutor Perini Corporation’s (TPC) baseline schedule is incorporated into the master program schedule. The preliminary SFMTA Contract 1300 November 2018 schedule is used within the November Report. The SFMTA Contract 1300 November 2018 schedule is based on the approved baseline schedule logic with adjustments made for fixing retained logic and lags. The SFMTA will continue to use the SFMTA Contract 1300 schedule update as a forecasting tool going forward until the Contract 1300 Contractor submits an acceptable schedule that addresses all of SFMTA scheduling concerns.



Pedestrians pass by rolls of turf used as part of the 2018 Winter Walk, currently being unrolled along Stockton north of Ellis.

Schedule Highlights - Continued

Work Package P-1254R (CTS) has performed the following work this month:

- Completed construction of track slab at Crossover Cavern
- Completed contact grouting for Crossover Cavern arches
- Continue construction of guideway stem walls at South Platform Cavern
- Began construction of guideway stem walls at North Platform Cavern
- Continued placing concrete for Crosscut Cavern headwall
- Continued installing rebar for North Egress shaft
- Continued installing electrical and plumbing for Concourse slab at Headhouse
- Continued placing concrete for Concourse slab at Headhouse
- Continued installing rebar and formwork for Intermediate slab at Headhouse
- Continued removing temp level 5 struts and wales
- Began prep work for initial shotcrete layer at Slurry Walls for Underplatform Level at Headhouse
- Began construction of track slab in Platform Cavern
- Continued street work (minor), ongoing monitoring and surveying

Work Package P-1253 (UMS) has performed the following work this month:

- Ellis St to Geary St on Stockton Street: Completed installation of water main/laterals. Constructed new sidewalks and asphalt pavement roadway. Constructed new and temporary lightings. Constructed temporary traffic signals.
- Platform Station: Continued construction of stairs and elevators. Continued installation of stem walls and platform deck. Installed CMU walls, placed concrete for metal decks and wale encasements at multiple levels.
- North Concourse: Continued installation of CMU walls. Continued installation of fire sprinklers and MEP on Concourse and Fan levels.
- South Concourse: Continued installation of fire sprinklers and MEP
- Winter Walk Stockton Street: Implemented 2018 Holiday Moratorium Winter Walk

Work Package P-1255 (YBM) has performed the following work this month:

- Continued re-establishing utilities at 4th & Howard.
- Installed catch basin and culvert at 4th and Clementina
- Backfilled PG&E #7 boxes at Folsom
- Completed installing Stair 4
- Continued installing Stair 6
- Continued installing Stair 7
- Continued F/R/P of Headhouse Undersurface stairs

Schedule Highlights - Continued

- Continued installing Headhouse Vent Shaft
- Continued F/R/P of Headhouse Mezzanine walls
- Continued electrical work in Headhouse Concourse
- Continued installing granite art panels in Station Concourse
- Continued installing crystallized glass panels in Station Concourse
- Continued F/R/P of seismic joints (Station Platform)

Work Package P-1256 (STS) has performed the following work this month:

- Continued traction power conduit installation inside tunnel
- Continued installing street light pole foundations on Townsend
- Continued sidewalk and curb ramp pavement renovation at 4th/Bryant
- Continued 4th/Brannan platform construction
- Ongoing strut bracket installation in both tubes working South to North
- Continued plinth construction in tunnel
- Completed track slab construction at CTS Crossover Cavern



A concrete truck is positioned adjacent to a large area where concrete will be poured to rebuild a large section of sidewalk along the east side of Stockton south of Geary.

Master Project Schedule

Activity ID	Activity Name	Original Duration	Start	Finish	2019				2020			
					Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3
CENTRAL SUBWAY PROJECT												
Program Level Milestones												
PJ01000	Central subway Project Start	4377	04-Jun-03 A	28-Jan-21								
M80004A	Tunnel Excavation Complete - Project Milestone #4A	0	03-Jun-03 A	25-Dec-18								
M80019	Baseline Finish Date: 12-28-2018	0		28-Dec-18*								
M80009	CSP Revenue Service Date	0		28-Dec-18*								
Preliminary Engineering Phase												
Final Design												
	Light Rail Vehicles	1871	03-Jun-10 A	17-Jun-13 A								
	Real Estate	2482	16-Apr-13 A	11-Dec-18								
Construction Phase												
	Construction Support and Costs	3130	01-Aug-08 A	03-Jan-14 A								
	Construction Utility Contract #1- MOS & Portal CN-1250	2887	04-Jan-10 A	25-Jan-21								
	Construction Utility Contract #2 - UMS CN-1251	3388	04-Jan-10 A	28-Jan-21								
	Construction Tunnels CN-1252	605	04-Jan-10 A	23-May-11 A								
	Construction Tunnels CN-1300	843	12-Jan-11 A	16-Oct-12 A								
	CN- 1300 Milestone	1618	08-Jun-11 A	28-Nov-18								
	Construction UMS Station P-1253	1704	03-Jun-13 A	13-Oct-18								
	Construction CTS station P-1254R	1848	17-Jun-13 A	19-Oct-19								
	Construction YBM station P-1255	1704	17-Jun-13 A	03-Aug-18								
	Construction STS P-1255	1586	17-Jun-13 A	18-Jul-18								
	Project Start Up	1861	10-Jun-13 A	18-Jul-19								
	Unallocated Contingency	1842	03-Jun-13 A	03-Oct-19								
	Cost Activity Unallocated Contingency (LOE) - 1.7,600.00	186	15-Jul-19	27-Dec-19								
	Program Level Milestones	272	28-Nov-18	28-Dec-18								
	Cost Activity Unallocated Contingency (LOE) - 1.7,600.00	272	28-Nov-18	28-Dec-18								

<p>SFMTA Central Subway Project Master Project Schedule Summary Schedule - November 2018</p>												
<p>Required Revenue Service Date: 28-Dec-18 Data Date: 28-Nov-18</p>												



Steel reinforcing for future track foundations marks the X-shaped crossover inside the Chinatown Station track crossover cavern.

Contracts & Construction

Construction Contracts In Progress

Contract 1300: Combined Work Packages 1253, 1254, 1255, 1256

- Contractor: Tutor - Perini Corporation
- Amount: \$854.89 million
- Contract Status: 76.19% complete construction

Contracts Completed

[See Appendix D](#)

Contract 1250: Moscone Station and Portal Utilities Relocation

Contract 1251: Union Square/Market Street Station Utility Relocation

Contract 1277: Pagoda Theater Site Demolition (Funded separately from the CS Project budget)

Contract 1252: Central Subway Tunneling

Contract SBE Participation (Updated Quarterly) [See Appendix E](#)

Stations, Surface, Track and Systems

Contract 1300 Contractor: Tutor-Perini Corporation

Description of Work

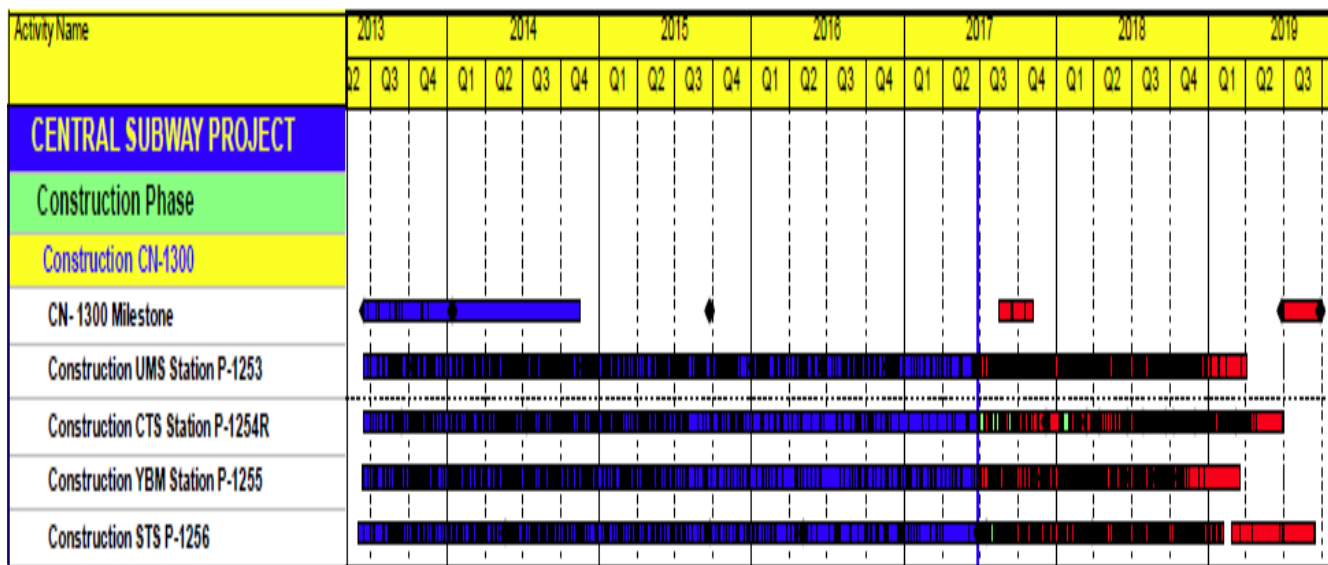
The Contract 1300 scope is to construct the Central Subway's three subway stations, one surface station, construct the 2,000 feet of surface track, and install track and operating systems throughout the new alignment. The separate station and systems work packages are presented in the following pages.

Work includes station finishes, AC and DC substations, elevators, escalators, lighting, emergency ventilation fans, HVAC fire alarm/ suppression/ protection, Cutter Soil Mixing, secant pile bottom up and Sequential Excavation Method construction, settlement monitoring, building protection, connecting to and modifying the BART Powell Street Station, PA, CCTV, signage, installation of fare collection equipment and station start-up and commissioning.

Contract Details	
Contract Awarded:	May 21, 2013
Notice to Proceed:	June 17, 2013
Substantial Completion:	June 30, 2018
Contract Award Value:	\$839,676,400
Modifications to Date (\$):	\$15,218,064
Modifications to Date (Days):	140
Current Contract Value:	\$854,894,464

Budget/Expenditures▲	
Current Budget	\$879,676,400
Other Project Offset Credits	\$3,123,097
Expenditures to Date	\$659,133,747

1300 Summary Schedule



Chinatown Station

Contract 1300 - Work Package 1254R

Description of Work

This Work Package is to construct one subway station. Includes station finishes, AC and DC Traction Power substations elevators, escalators, lighting, emergency ventilation fans, HVAC fire alarm/ suppression/ protection, slurry wall top-down construction, settlement monitoring, building protection, PA, CCTV, signage, installation of fare collection equipment and station start-up and commissioning.

Current Work Status

- Completed construction of track slab at Crossover Cavern
- Completed contact grouting for Crossover Cavern arches
- Continue construction of guideway stem walls at South Platform Cavern
- Began construction of guideway stem walls at North Platform Cavern
- Continued placing concrete for Crosscut Cavern headwall
- Continued installing rebar for North Egress shaft
- Continued installing electrical and plumbing for Concourse slab at Headhouse
- Continued placing concrete for Concourse slab at Headhouse
- Continued installing rebar and formwork for Intermediate slab at Headhouse
- Continued removing temp level 5 struts and wales
- Began prep work for initial shotcrete layer at Slurry Walls for Underplatform Level at Headhouse
- Began construction of track slab in Platform Cavern
- Continued street work (minor), ongoing monitoring and surveying



Work Expected Next Month

- Complete waterproofing installation for South headwall
- Complete construction of guideway stem walls at South Platform Cavern
- Continue stem wall construction and placing concrete for track slab at South Platform Cavern
- Continue installing rebar and formwork for Crosscut Cavern headwall
- Begin installing rebar for Crosscut Cavern Arch final lining
- Complete construction of guideway stem walls at North Platform Cavern
- Begin stem wall construction for track slab at North Platform Cavern
- Begin shotcrete application for North Egress shaft
- Begin constructing formwork and installing rebar for North Egress upper lid
- Complete installing rebar, electrical, plumbing, and concrete placement for Concourse slab at Headhouse
- Complete removing temp level 5 struts and wales
- Begin installing rebar, electrical, and plumbing for Intermediate slab at Headhouse

Chinatown Station

Contract 1300 - Work Package 1254R

Work Expected Next Month (continued)

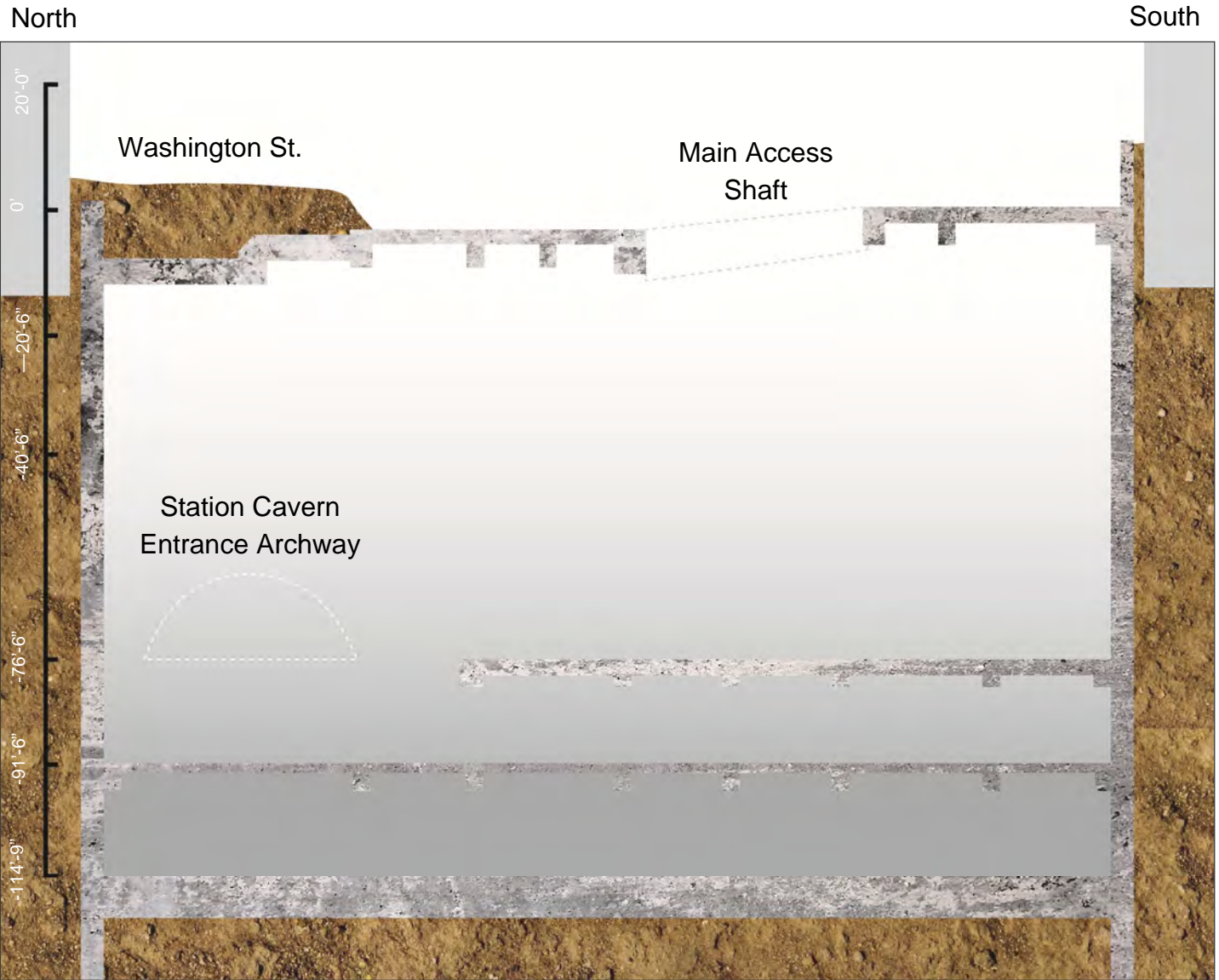
- Begin placing concrete for Intermediate slab at Headhouse
- Begin shotcrete for slurry walls, install drain mat and waterproofing for Under Platform level at Headhouse

Three Month Look Ahead

- Begin installing rail at Crossover Cavern
- Begin installing rail at South Platform Cavern
- Complete concourse slab at Crosscut Cavern
- Complete arch final lining at Crosscut Cavern
- Complete walls and staircase for North Egress Shaft and upper lid
- Underplatform and Platform levels at Headhouse: Shotcrete on Slurry Walls, Install drain mat and waterproofing on Shotcrete Walls, Build CMU partition walls
- Begin installing mechanical, electrical, plumbing, at Headhouse Underplatform and Platform Levels.
- Complete Intermediate, Lower Mezzanine, and Upper Mezzanine levels at Headhouse



Station Construction Progress Section



Chinatown Station Construction Status - Continued

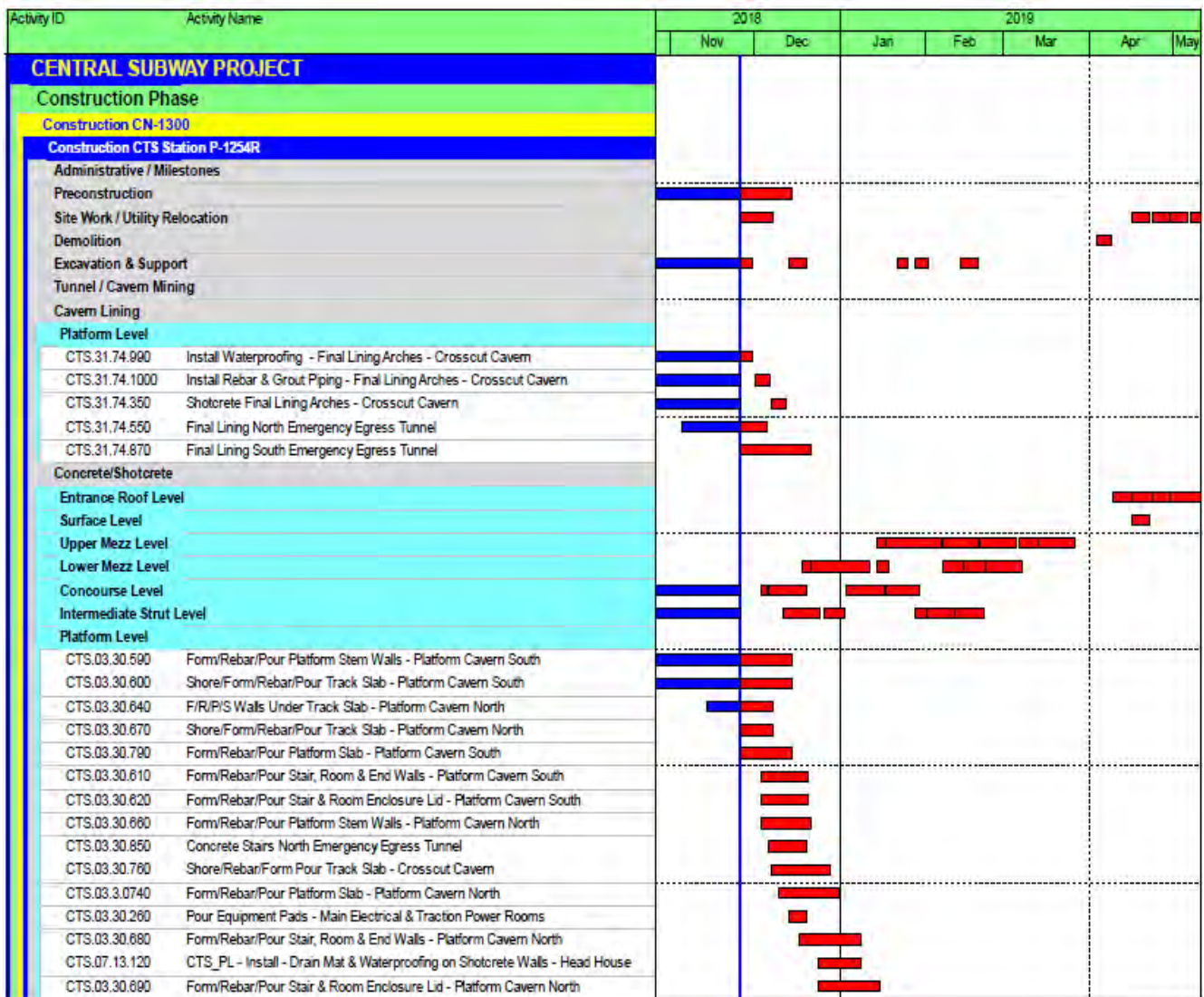
Contract Details

Contract Awarded:	May 21, 2013
Notice to Proceed:	June 17, 2013
Substantial Completion:	June 30, 2018
Contract Award Value:	\$247,567,810
Modifications to Date (\$):	\$8,983,302
Modifications to Date (Days):	140
Current Contract Value:	\$256,551,112

Budget/Expenditures

Current Budget	\$257,567,810
Other Project Offset Credits	\$75,000
Expenditures to Date	\$194,684,848

CTS Three Month Schedule



Union Square/Market Street Station

Contract 1300 Work Package 1253

Description of Work

This Work Package is to construct one subway station. Includes station finishes, AC and DC Traction Power substations elevators, escalators, lighting, emergency ventilation fans, HVAC fire alarm/ suppression/ protection, slurry wall top-down construction, settlement monitoring, building protection, PA, CCTV, signage, installation of fare collection equipment and station start-up and commissioning.

Current Status This Month

- Ellis St to Geary St on Stockton Street: Completed installation of water main/ laterals. Constructed new sidewalks and asphalt pavement roadway. Constructed new and temporary lightings. Constructed temporary traffic signals
- Platform Station: Continued construction of stairs and elevators. Continued installation of stem walls and platform deck. Installed CMU walls, placed concrete for metal decks and wale encasements at multiple levels
- North Concourse: Continued installation of CMU walls. Continued installation of fire sprinklers and MEP on Concourse and Fan levels
- South Concourse: Continued installation of fire sprinklers and MEP
- Winter Walk Stockton Street: Implemented 2018 Holiday Moratorium Winter Walk

Work Expected Next Month

- Platform Station: Continue construction of CMU Walls on Mezzanine and Intermediate Strut levels. Continue installation of stairs and elevators
- North Concourse: Complete construction of CMU walls on Fan level. Complete installation of fire sprinklers on Fan and Concourse level. Installation of Fans on Intermediate Strut level. Complete construction of equipment pads in Main Electrical Room



- South Concourse: Continue installation of MEP. Continue installation of walls
- Winter Walk Stockton Street continues

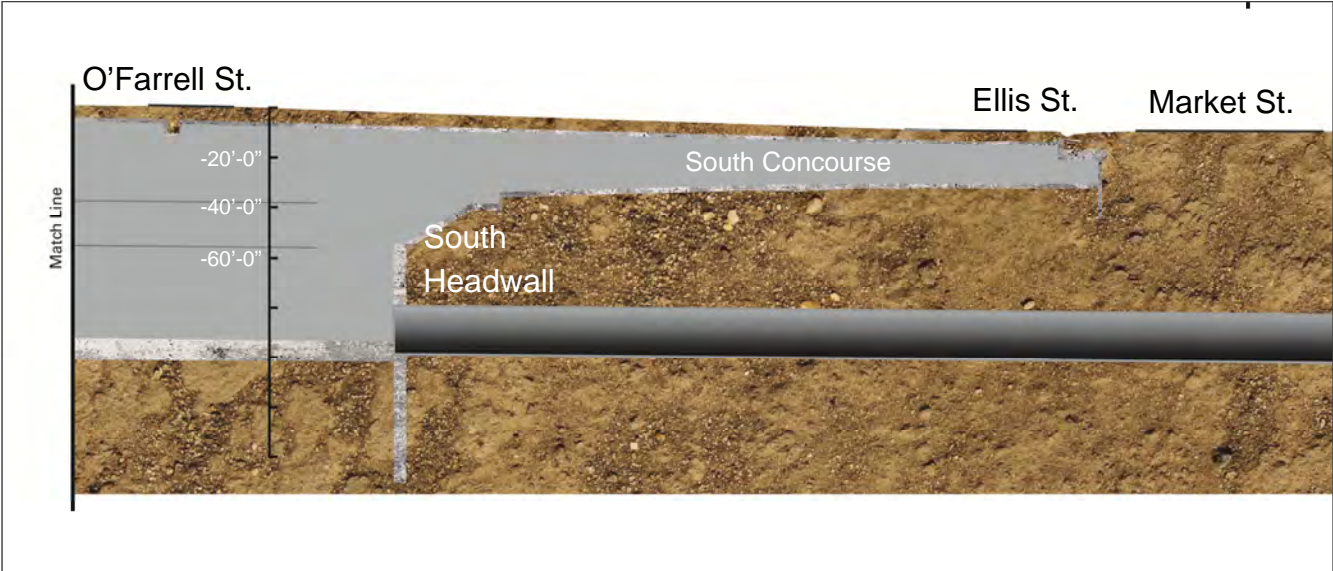
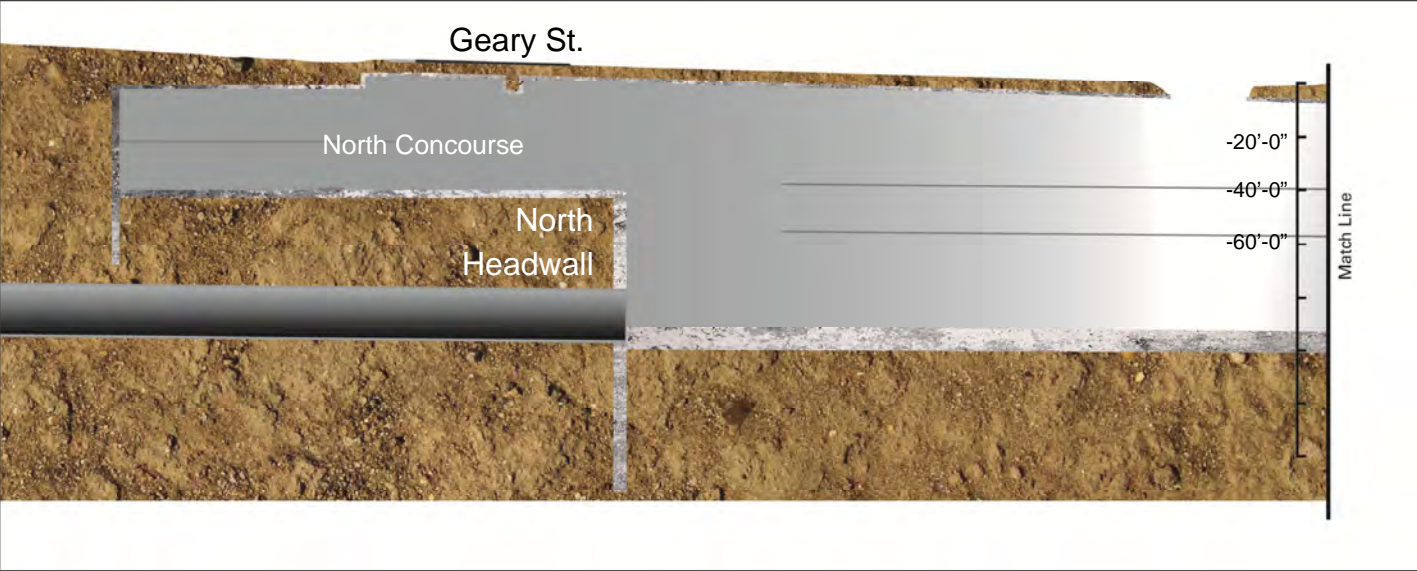
Three Month Look Ahead

- Platform Station: Complete construction of stairs and escalators. Complete CMU wall construction at Intermediate Strut, Mezzanine and Concourse levels. Complete deck installation. Complete all structural concrete work
- North Concourse: Complete Concourse level slab and equipment pads. Install ventilation fans and equipment. Complete installation of fire alarm system in USG
- South Concourse: Complete construction of the south escalators and stairs
- Street: Complete Restore traffic signal and street lights. Install OCS lines
- Open Stockton Street to traffic

Station Excavation and Construction Progress Section

North

South

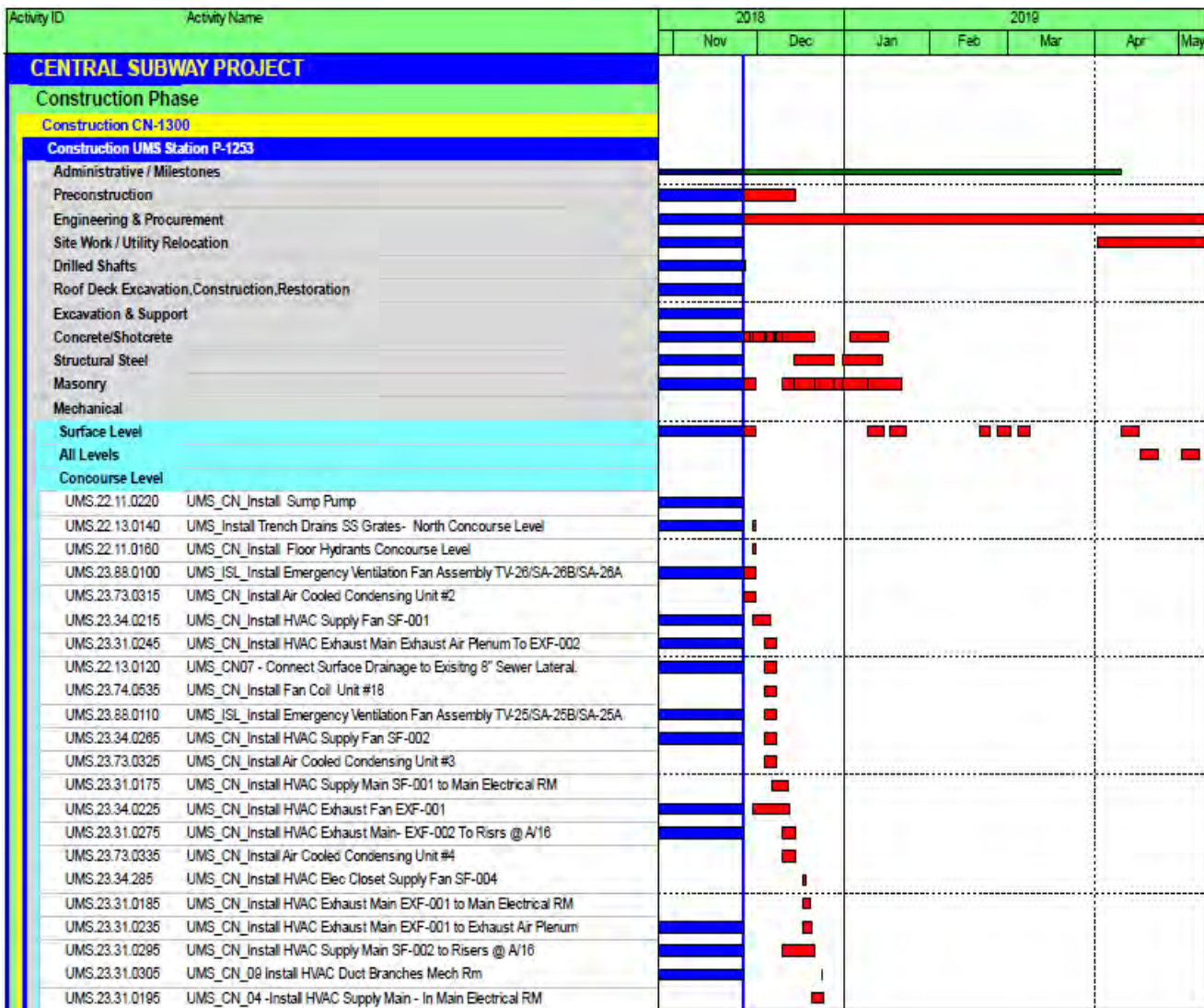


Union Square Market Street Station Construction - Continued

Contract Details	
Contract Awarded:	May 21, 2013
Notice to Proceed:	June 17, 2013
Substantial Completion:	June 30, 2018
Contract Award Value:	\$294,030,590
Modifications to Date (\$):	\$2,748,442
Modifications to Date (Days):	140
Current Contract Value:	\$296,779,032

Budget/Expenditures ▲	
Current Budget	\$314,030,590
Expenditures to Date	\$249,950,103

UMS Three Month Schedule



Yerba Buena/Moscone Station

Contract 1300 - Work Package 1255

Description of Work

This Work Package is to construct one subway station. Includes station finishes, AC and DC Traction Power substations elevators, escalators, lighting, emergency ventilation fans, HVAC fire alarm/ suppression/ protection, slurry wall top-down construction, settlement monitoring, building protection, PA, CCTV, signage, installation of fare collection equipment and station start-up and commissioning.

Current Status

- Continued re-establishing utilities at 4th & Howard.
- Installed catch basin and culvert at 4th and Clementina
- Backfilled PG&E #7 boxes at Folsom
- Completed installing Stair 4
- Continued installing Stair 6
- Continued installing Stair 7
- Continued F/R/P of Headhouse Undersurface stairs
- Continued installing Headhouse Vent Shaft
- Continued F/R/P of Headhouse Mezzanine walls
- Continued electrical work in Headhouse Concourse
- Continued installing granite art panels in Station Concourse
- Continued installing crystallized glass panels in Station Concourse
- Continued F/R/P of seismic joints (Station Platform)



- Complete F/R/P of Headhouse Surface Slab 9.4 to 8.6 line
- Begin F/R/P of Elevator 3 and 4 walls at Headhouse Surface Level
- Complete F/R/P of Headhouse Undersurface stairs
- Continue F/R/P of walls of Headhouse Vent Shaft
- Continue F/R/P of Headhouse Mezzanine walls
- Continue waterproofing of Headhouse Mezzanine
- Continue installing Granite Art Work panels (Station Concourse)
- Continue installing crystallized glass panels (Station Concourse)
- Continue installing luminous glass ceiling panels (Station Concourse)

Work Expected Next Month

- Continue re-establishing utilities at 4th & Howard.
- Begin pavement renovations at 4th and Howard
- Complete installing Stairs 1, 5, and 6

Work Expected Next Month (continued)

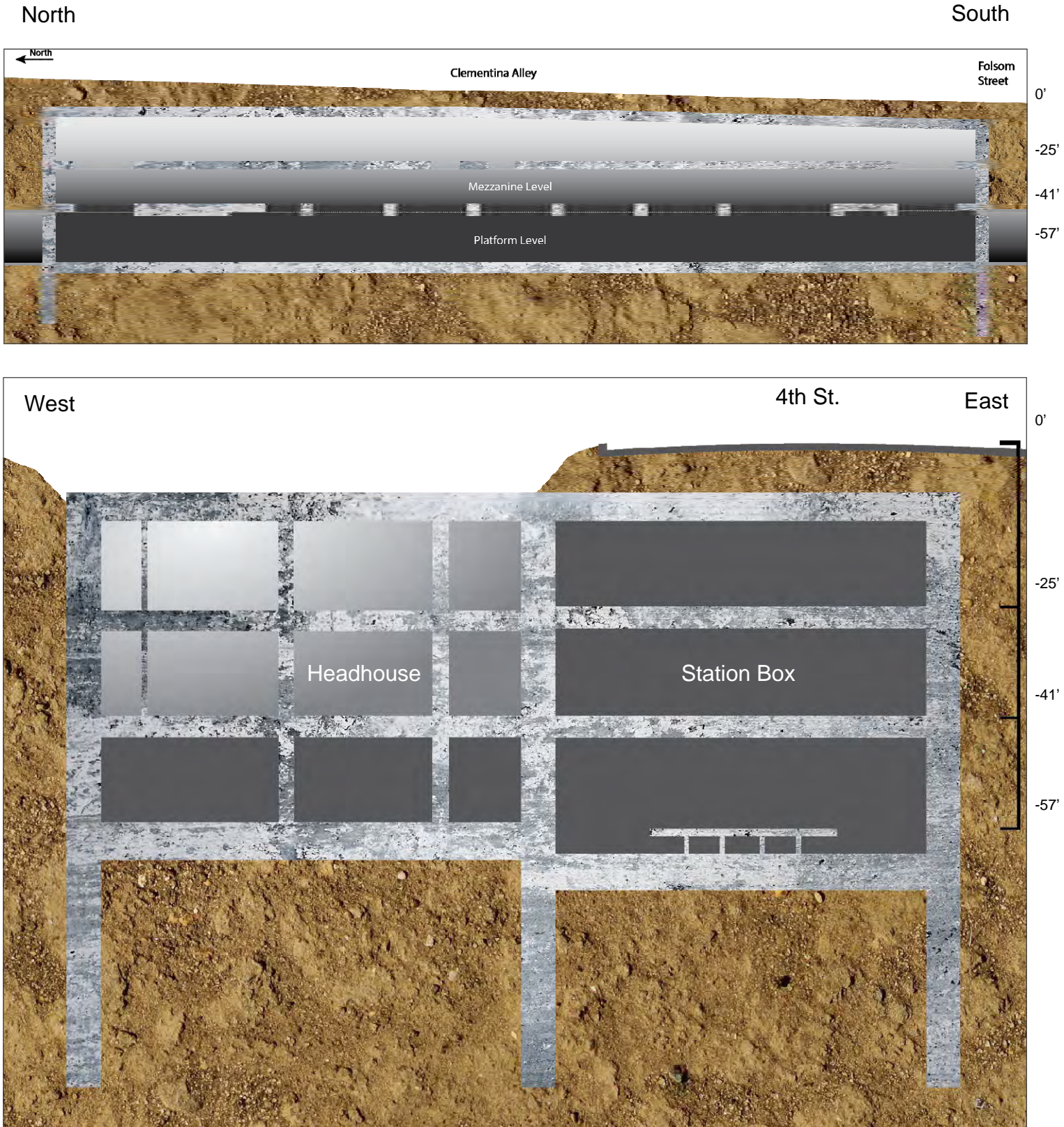
- Apply dielectric coating to Headhouse Invert slab
- Continue F/R/P of seismic joints (Station Platform)

Three Month Look Ahead

- Begin pavement renovation work on 4th and Folsom
- Pour West Side topping slab, Mezzanine – Station box
- Continue interior finishes on Mezzanine & Concourse Levels within Station Box
- Continue placement of stairs within Station and Headhouse
- Begin installation of sculpture at Surface Level
- Complete installation of artwork in Headhouse Concourse
- Install cabs for Elevators 1 and 2



Station Excavation and Construction Progress Section



Yerba Buena Moscone Station Construction - Continued

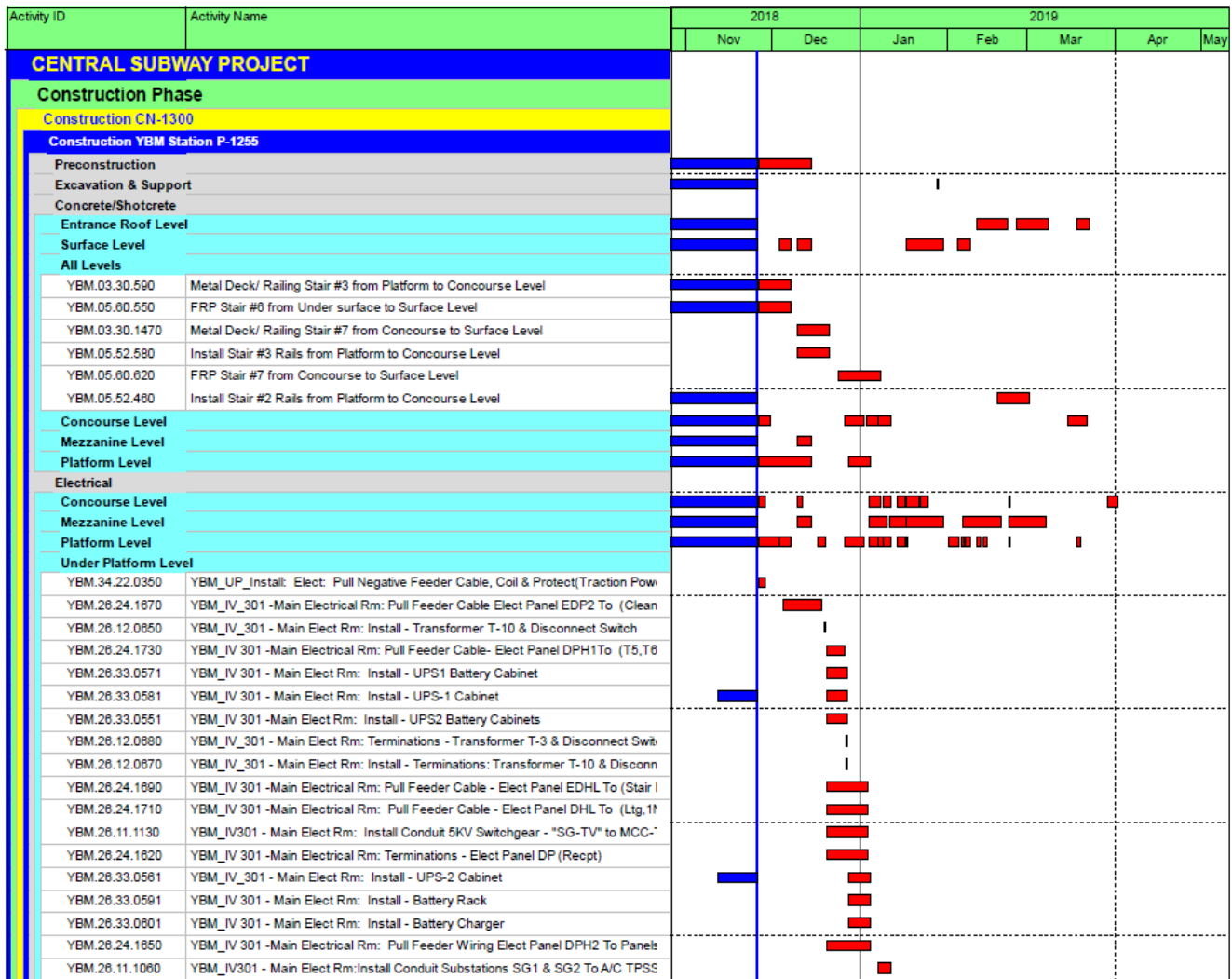
Contract Details

Contract Awarded:	May 21, 2013
Notice to Proceed:	June 17, 2013
Substantial Completion:	June 30, 2018
Contract Award Value:	\$158,089,000
Modifications to Date (\$):	\$747,056
Modifications to Date (Days):	140
Current Contract Value:	\$158,836,056

Budget/Expenditures ▲

Current Budget	\$163,089,000
Other Project Offset Credits	\$415,331
Expenditures to Date	\$131,992,580

YBM Three Month Schedule



Systems, Trackwork, & Surface Station

Contract 1300 - Work Package 1256

Description of Work

This Work Package is to construct one Surface Station. Includes light rail track and systems, track invert, track safety walkways; light rail track and systems constructed on the 2,000 foot surface for the alignment from the tunnel portal, south to the tie-in to the existing Muni T-Line at Fourth and King Streets; and the surface Fourth and Brannan Street (FBS) Station.

Current Status

- Continued traction power conduit installation inside tunnel
- Continued installing street light pole foundations on Townsend
- Continued sidewalk and curb ramp pavement renovation at 4th/Bryant
- Continued 4th/Brannan platform construction
- Ongoing strut bracket installation in both tubes working South to North
- Continued plinth construction in tunnel
- Started track slab construction at CTS Crossover Cavern

Work Expected Next Month

- Continue 4th/Brannan platform construction
- Continue traction power conduit installation inside tunnel
- Continue installing street light pole foundations on Townsend
- Continue sidewalk and curb ramp pavement renovation at 4th/Bryant
- Continue plinth construction in tunnel
- Continue track slab construction at CTS Crossover Cavern



Three Month Look Ahead

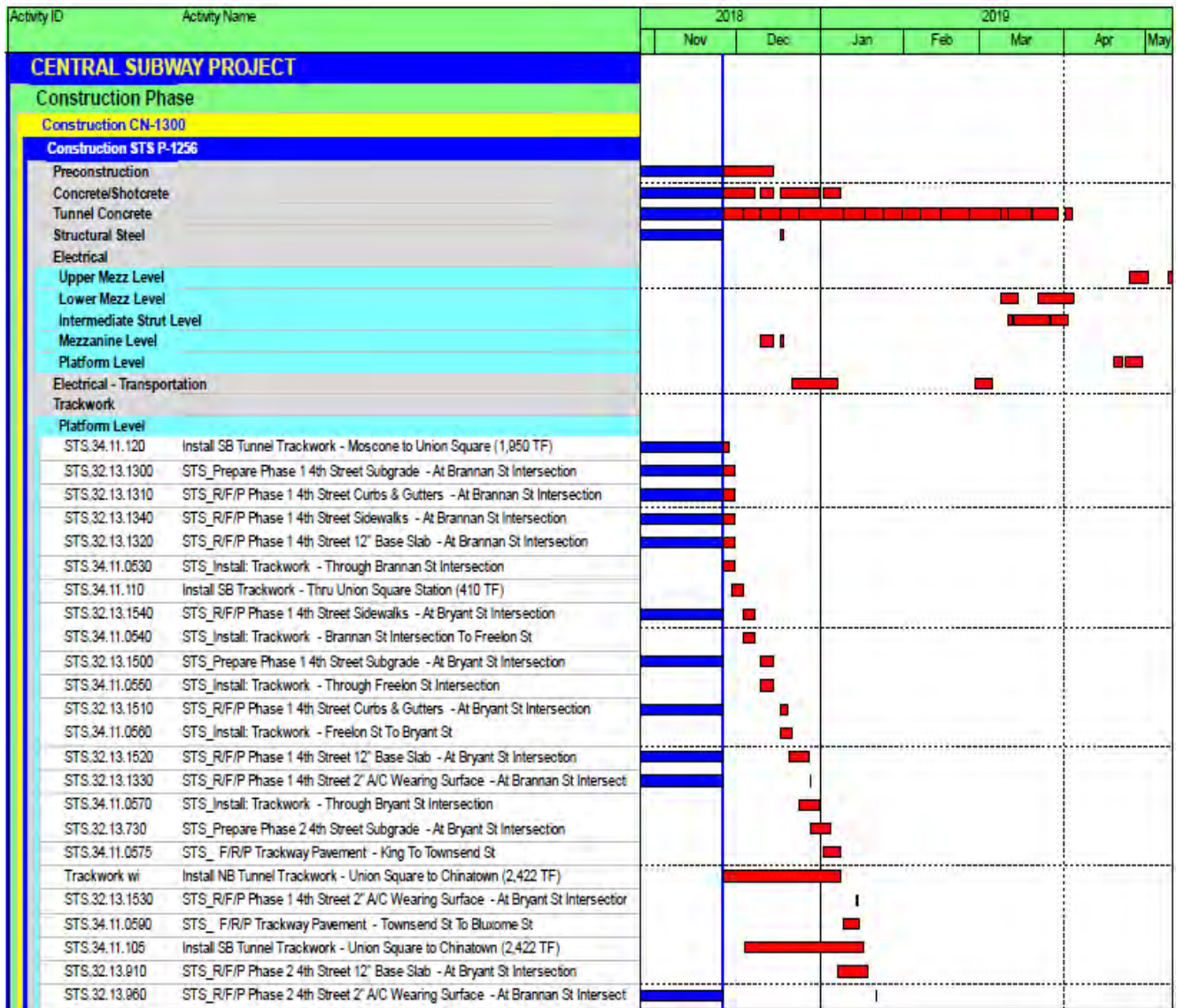
- Complete street light pole installation
- Complete pavement renovation at 4th/Bryant, 4th/Brannan, and 4th/King intersections
- Continue 4th/Brannan platform construction
- Continue surface track installation
- Continue track installation inside tunnel
- Continue walkway installation inside tunnel
- Continue electrical conduit installation inside tunnel
- Continue pulling traction power cables on 4th Street

Systems, Trackwork, & Surface Station Construction - Continued

Contract Details	
Contract Awarded:	May 21, 2013
Notice to Proceed:	June 17, 2013
Substantial Completion:	June 30, 2018
Contract Award Value:	\$139,989,000
Modifications to Date (\$):	\$2,739,264
Modifications to Date (Days):	140
Current Contract Value:	\$142,728,264

Budget/Expenditures	
Current Budget	\$144,989,000
Other Project Offset Credits	\$2,632,766
Expenditures to Date	\$79,383,119

Systems, Track and Surface Station Three Month Schedule



Program Components

Community Outreach

Outreach public information, events and presentations for November 2018 include:

- Conducted Community Advisory Group Meeting
- Conducted Chinatown Merchants Meeting
- Continued noise and dust mitigation meetings with Tutor Perini and community stakeholders
- Ongoing outreach to merchants and residents
- Conducted meetings and face-to-face visits with various merchant stakeholders along the alignment
- Preparation and dissemination of construction notices
- Produced quarterly construction update video and other multimedia content
- Responded to constituent complaints

Outreach in Support of Mitigation and Monitoring

Team members participated in weekly progress to address neighborhood concerns

Outreach and communication efforts continue in Chinatown, Union Square, and SOMA

Weekly photo documentation of project work and editing

Outreach team members met with SF Arts Commission representatives to align public art completion strategy

Weekly construction update emails sent to list of approximately 700 residents and stakeholders

Media Coverage

Central Subway Media Coverage			
Date	Title (with link to story)	Source	Reporter/Writer
11/27/2018	San Francisco holiday treat Winter Walk returns	San Francisco Chronicle	Michael Cabanatuan

Quality Assurance

Quality Assurance monthly activity of oversight, surveillance, audits, proactive feedback and QA records actively involves the Project construction management staff, the resident engineers, the prime construction contractor and their subcontractors.

Stations and Systems Contract CN1300 Quality Assurance Monitoring – On Going/As Reported Previously

- UMS structural steel installation – Continued Inspection/acceptance/documentation by Smith Emery CWI's of all welds associated with the ongoing Installation of structural and excavation support steel
- Continuation of Station construction at UMS and YBM
- Waterproofing and concrete lining of CTS Cavern
- STS invert and plinth preparation for and subsequent concrete placement for track installation continue
- STS rail preparation for and subsequent installation
- TPC QC Daily Inspection Reports posted to CM13 which includes TPC's Specialty Subcontractor's QC checklists and associated documentation and Smith Emery Inspection Reports; TPC's Subcontractor that provides laboratory and Inspection Services – including Special Inspections required for the City of San Francisco's Department of Building Inspection (DBI) for all permitted work
- Preparatory, Initial Phase, and in particular, Concrete Preplacement Meetings continue as the scheduled activities dictate
- Bi-Weekly Quality Task Force (QTF) Meetings – ongoing dialog regarding; planning for upcoming Work, identification and mitigation of in-process potentially unsatisfactory work, generation of CNCRs, welding inspection documentation, HOLD points and other items related to TPC's QC efforts in implementing TPC's approved Quality Control Program (QCP). Additionally, the Contractor's Quality Control Manager (QCM) and Assistant QCMs continue to be provided with salient information from the PQM's participation/attendance in Project and Work Package Progress Meetings
- Weekly Work Package Progress Meetings for STS, YBM, UMS and CTS
- Monthly Project Risk Mitigation, Safety and Security and weekly MEP Progress and CMB Meetings as scheduling constraints allow

Document comment and review:

- Contractor's submittals, e.g., review of welding, concrete (including shotcrete) and other Quality related submittals/comments as requested to support the RE's and CM, and RFIs related to quality and welding
- QA Staff continues random/spot checks of the 1300 Contractor's Field Testing lab results – accomplished upon review of the preliminary test reports provided as required the Contract Documents via email from the Contractor's testing laboratory which includes concrete cylinders and shotcrete cores and shotcrete C1550 flexural specimens
- Contractor Non Conformance Reports (CNCR) Status as indicated in the TPC QC CNCR Log:
 - ◇ Initial: 8 CNCRs are currently posted to the CNCR Log as INITIAL entries. (no change from October 2018) (C1300 is required to generate a CNCR within 24 hours of becoming aware of what appears to be non-conforming work).
 - ◇ Dispositioned (not acceptable):14 CNCRs are currently posted to the CNCR Log as DISPOSITIONED (NOT ACCEPTABLE) and have been returned to the Contractor because

Quality Assurance - Continued

the RE's review of the Contractor's proposed disposition determined that the proposed disposition is not appropriate and must be revised). (no change from October 2018)

- ◇ Dispositioned: 19 CNCRs are currently posted to the CNCR Log as DISPOSITIONED and are being reviewed by associated SFMTA RE to verify that the Contractor's proposed disposition is appropriate.) (+2 from October 2018)
- ◇ Approved: 32 CNCRs are currently posted to the CNCR Log as APPROVED because the suggested REPAIR dispositions have been approved and the CNCRs will remain open until the approved REPAIR procedure is performed. (+4 from October 2018)
- ◇ Closed: 288 CNCRs are currently posted to the CNCR Log as CLOSED. (+6 from October 2018)
- ◇ Voided: 43 CNCRs are currently posted to the CNCR Log as VOIDED (subsequent evaluation of the INITIAL CNCRs determined that a CNCR is not warranted). (no change from October 2018)
- ◇ 404 CNCRs are currently posted to the CNCR Log. (+2 from October 2018)

QA Issues:

- None to report for November 2018

QA Concerns:

- As described previously, typical to similar Projects, work performed prior to receipt of approval status of required submittals (including coordination and shop drawings)/RIFs with/without knowledge of QC or responsible production supervision, remains a potential item(s) of concern
- Also as previously described and typical to similar Projects, the untimely identification and mitigation (SFMTA approval) of "last minute items", such as too little clear cover for reinforcement due to unanticipated proximity of adjacent objects in a concrete lift, remains an ongoing challenge to all involved. Project quality has not suffered to date; however the aforementioned concern remains
- Continue regarding Project schedule compression demands disrupting RE and Design Staff priorities as mentioned above; quality has not suffered but the concern remains
- CNCR 354, which documents that standard strength and not high strength 115 RE rail has been furnished and is currently being installed by Tutor Perini Corporation (TPC) the C1300 Contractor. CNCR 354 was dispositioned as Use-As-Is and was then rejected by SFMTA and returned to TPC QC to address the requirements of 34 11 14 *Rail*. SFMTA has subsequently written a letter to TPC directing the removal of the non-conforming rail. Meanwhile, at a meeting with TPC and TPC's track work F & I Subcontractor, SFMTA QA was informed by TPC's Project Manager that CNCR 354 would be voided. SFMTA QA concern is that that CNCR 354 will be voided predicated by TPC perceived ambiguities in the Contract Documents; without consideration of other Contract Document requirements.. NCN CT-001 was issued, directing TPC to reinstate CNCR 354. This issue will be closely monitored by SFMTA

Other Program QA Practices Implemented:

- Close-out of Corrective Action Requests: Close outs continued as required from Quality Assurance staff's Audits, Surveillances and PMOC Quarterly Reviews. The status is tracked in the Corrective Action Log that is available to the project team and the FTA PMOC
- Project QA initiated weekly meetings with the Resident Engineers and Assistant Resident Engineers of all stations to review project quality assurance procedures and requirements and contractor quality control requirements

Risk Management

Risk Mitigation Management Meeting No. 111 was held on Tuesday, November 6, 2018. The members of the Risk Assessment Committee will review the top risks in accordance with the risk summary sheet, which have been given a rating by The Committee of six and above.

During the monthly meeting, forty-one (41) construction risks and one (1) remaining requirement risk will be tracked on the Project's Risk Register, in addition to, establishing strategies for mitigation and evaluating potential unforeseen issues or conditions.

The Committee will continue to follow risks and risks will be monitored and statuses updated with the use of the risk mitigation status sheets, providing monthly updates by the Risk owner to demonstrate the assigned mitigation strategy is being implemented.

Top Risks

Risk #	Risk Description	Risk Rating	Contract
99	Breakdown in relationships between SFMTA and Contractors during construction results in increased claims and delays to the overall construction schedule.	13	STA
240	Unresolved Assignment of Schedule Delay Responsibility (may lead to increase cost for the Program)	12	STA
255	Water leaks at YBM station	10	YBM
251	Physical activities missing (not defined) in the schedule / identify activities of undefined scope	8	STA
205	Prolong period of CMod's creates additional cost/causes bad blood between Resident Engineer and Contractor	8	STA
257	Systems Test Integration between components	8	RSD
234	Sequential Excavation Method at CTS - Contractor's propose method will induce detrimental subsidence	7	CTS
253	Do not have adequate resources defined to do the work	6	STA
52	Unacceptable settlement and impact on major utilities at CTS. (OLD SEWERS AND OTHERS WITHIN 20FT SPACE BETWEEN TOP OF CAVERN AND STREET LEVEL)	6	STA
238	Quality Program is ineffective in processing the nonconformance items causing schedule impacts	6	STA

Program Safety & Security

The San Francisco Municipal Transportation Agency is committed to the highest practical level of safety and security standards and practices in the public transit industry. The Safety and Security Management Plan (SSMP) components are reported on below as appropriate including, Safety and Security Committee, the Fire Life Safety and Security Committee the Construction Conformance Verification and Documentation and Contractor Safety and Security.

Project Management/Construction Management (PMCM) Team

Safety bulletin boards have subjects covering the complacency and daily job briefings. Weekly safety meetings are conducted twice a week so all staff have an opportunity to attend.

Safety Summary for the 1300 Stations Systems Track Construction Package

During the month of November, TPC incurred three first aid injuries. They included a foreign item in the eye, a puncture wound to the left foot, and a pain in the left shoulder. All were treated in the emergency clinic and returned to work. No lost time incidents.

Table 1300 Stations Construction Safety Record

Table 1300 below summarizes the Month to Date and Project to Date for the Stations, Systems and Track Construction contractor and subcontractors.

Next Month Look Ahead

1300 Contract

1. At the CTS station, TPC is completing waterproofing installation for the South headwall.
2. At the UMS station, 2018 Winter Walk Holiday Moratorium was implemented.
3. At the YBM station, TPC continued F/R/P of Headhouse Undersurface stairs, walls of Headhouse Vent Shaft, and Headhouse Mezzanine walls.
4. At the STS station, TPC and their sub contractors continued traction power conduit installation inside tunnel, plinth construction, and installing street light pole foundations on Townsend Street.

Program Safety & Security - continued

Project Safety Record - Contract 1300

Through Month End Nov 2018

SAFETY GOALS

OSHA Recordable Accidents, <3.4
Lost Time Cases, <1.6

JOB TO DATE	Tutor	Subs	Total Project	Rate*
OSHA Recordable Accidents	9	4	13	0.80
Job Transfer or Restricted Duty Cases	0	0	0	0.00
Lost Time Cases	2	1	3	0.18
Total Project Incidents	11		16	0.98
Man Hours Worked Through M/E Nov 2018	1,568,146	1,699,085	3,267,231	

YEAR TO DATE (Month ,Day, Year to Month, Day, Year)	Tutor	Subs	Total Project	Rate*
OSHA Recordable Accidents	4	2	6	1.82
Job Transfer or Restricted Duty Cases	0	0	0	0.00
Lost Time Cases	1	0	0	0.00
Total Project Incidents	5	0	3	0.91
Man Hours Worked Through M/E Nov 2018	431,864	226,293	658,157	

* Rate is calculated based on number of incidents divided by total number of man hours worked multiplied by 200,000 man hours.

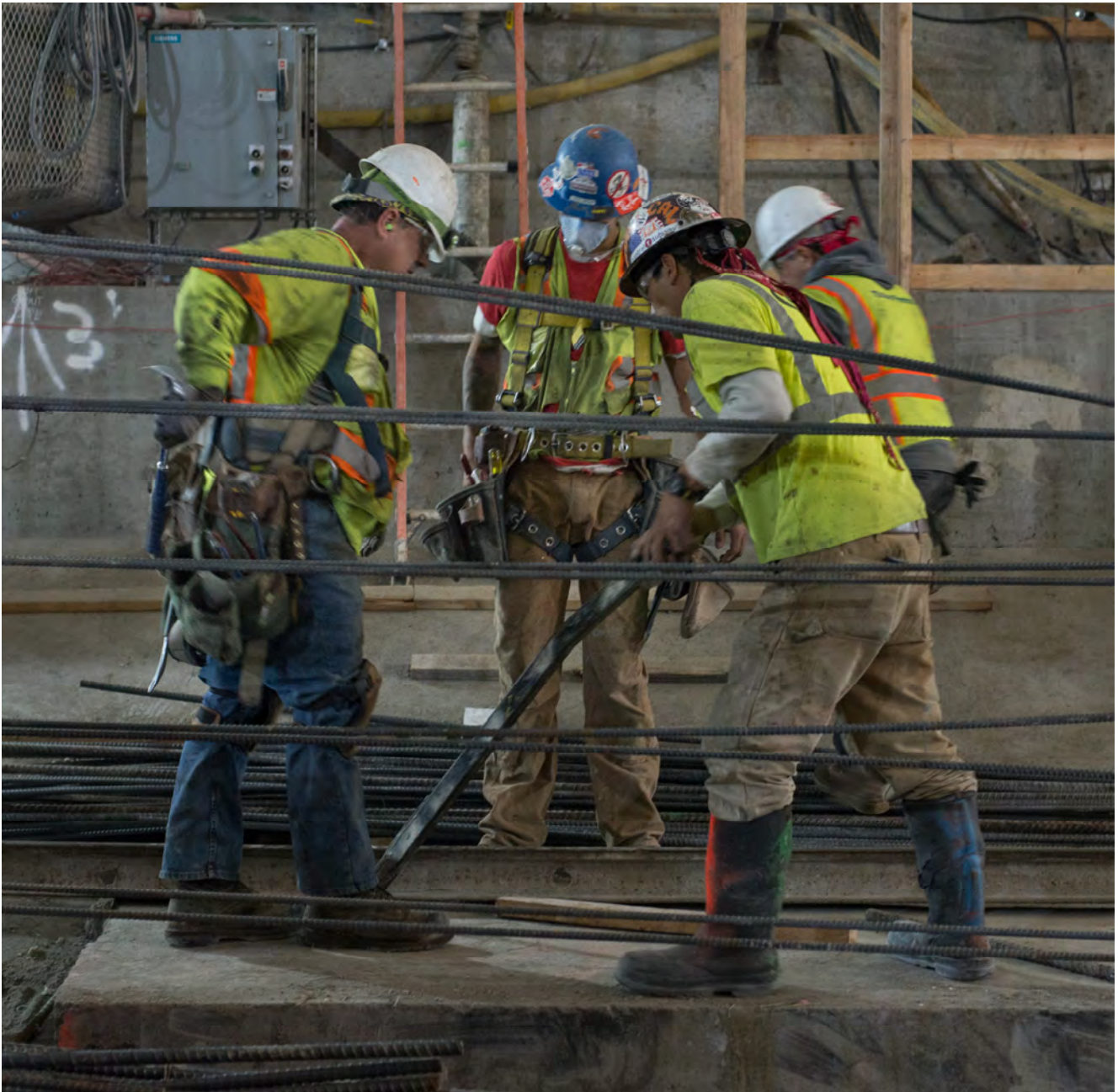
OSHA Recordable Accidents - 2008 Construction Industry Rate for Highway, Street, and Bridge Construction = 3.9

*Classifications change at a later date due to additional information becoming available, thereby, changing the numbers on the chart.

For example, what was once classified as an accident can become a first aid which leads it to no longer being recordable.

Technical Capacity

The Program is in the process of finding a permanent replacement for the Director position. Currently, an Acting Director has been named until a permanent replacement is found. In addition, the program is in the process of establishing a job description for a Startup and Testing Manager. The Program is also considering candidates for Contracts Claims Administrator or Change Order Administrator and is interested in identifying additional candidates for supplementation of the team in the areas of Construction Inspector and Office Engineer, and persons with expertise in MEP Coordination and Systems implementation.



Crewmembers meet for a moment during assembly of rebar cages for Chinatown Station platform level foundation walls.

Staffing

The Central Subway Staffing Table shows Planned and Actual full-time equivalent staff (FTEs) working on the Program by organizational function and responsibility.

	Sep-2018		Oct-2018		Nov-2018	
	Planned	Actual	Planned	Actual	Planned	Actual
Project Management						
Program Management	6.60	4.70	6.60	4.70	6.60	4.70
Quality Assurance	1.80	0.80	1.80	0.80	1.80	0.80
Contract Administration	1.40	10.40	1.40	10.40	1.40	12.40
Community Outreach	5.50	2.50	5.50	2.50	5.50	2.50
Finance	2.00	0.00	2.00	0.00	2.00	0.00
Project Controls	4.80	3.90	4.80	4.65	4.80	4.65
Subtotal	22.10	22.30	22.10	22.30	22.10	25.05
Construction Management						
CM - CN 1252	0.00	0.00	0.00	0.00	0.00	0.00
CM - CN 1300	21.55	25.20	21.55	26.50	21.55	28.75
Design Support - CN 1252	0.00	0.00	0.00	0.00	0.00	0.00
Design Support - CN 1300	9.00	9.00	9.00	9.00	9.00	9.00
Subtotal	30.55	34.20	30.55	35.50	30.55	37.75
Start Up						
Start Up / Safety & Security	5.95	0.20	5.95	0.20	5.95	0.20
Subtotal	5.95	0.20	5.95	0.20	5.95	0.20
Total	58.60	56.70	58.60	58.00	58.60	63.00

Third-Party Agreements

No activity in this reporting month.

LRV Procurement

SFMTA has initiated a new light rail vehicle procurement to acquire up to 260 vehicles over the next 15 years. The scope includes the design, manufacture, delivery and testing of up to 260 light rail vehicles together with associated services, spare parts, special tools, training and documentation. This includes an initial delivery of 24 cars, scheduled for delivery from 2017 - 2018 to supplement the fleet when the SFMTA's Third Street Phase 2 - Central Subway Project extension opens.

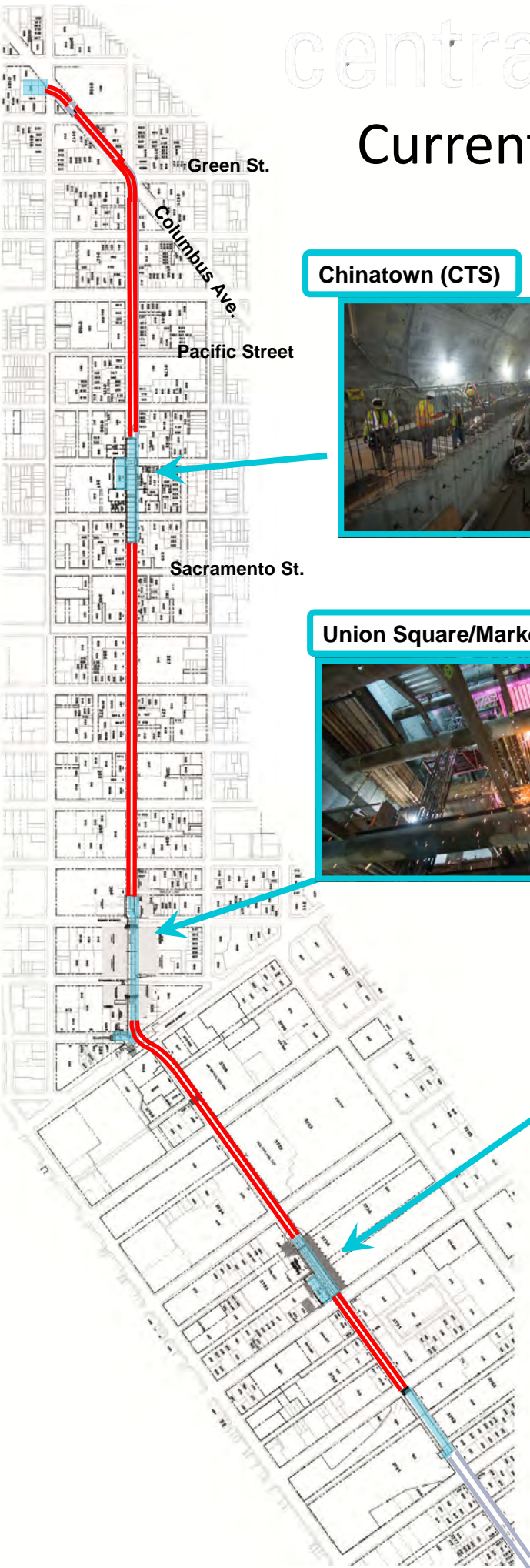
Production of the first 24 cars continues. 7 cars are now in service, with 9 more on property going through the test and commissioning process. The delivery and acceptance process is on schedule to put one vehicle in service per week.



Temporary scaffolding has been built to aid access for building concrete forms toward construction of the future station entrance structure at the surface of 4th Street.

central **T** subway

Current Construction Activity



Chinatown (CTS)



Union Square/Market Street (UMS)



Yerba Buena/Moscone (YBM)



4th St. Surface Track, Systems (STS)





Workers inside the Chinatown Station platform cavern have been building support walls for the future platform level floor slab.



Rebar and utility conduit installation work continues to build the last section of concourse-level floor slab inside the Chinatown Station headhouse, where future passengers will enter and exit through fare gates under the cross-cut cavern archway.



A Skytrak forklift rolls slowly underneath the large red truss structure used to place concrete forms inside the Chinatown Station platform cavern.



A worker on the concourse level floor slab looks across an access shaft on the east side of the Chinatown Station headhouse.



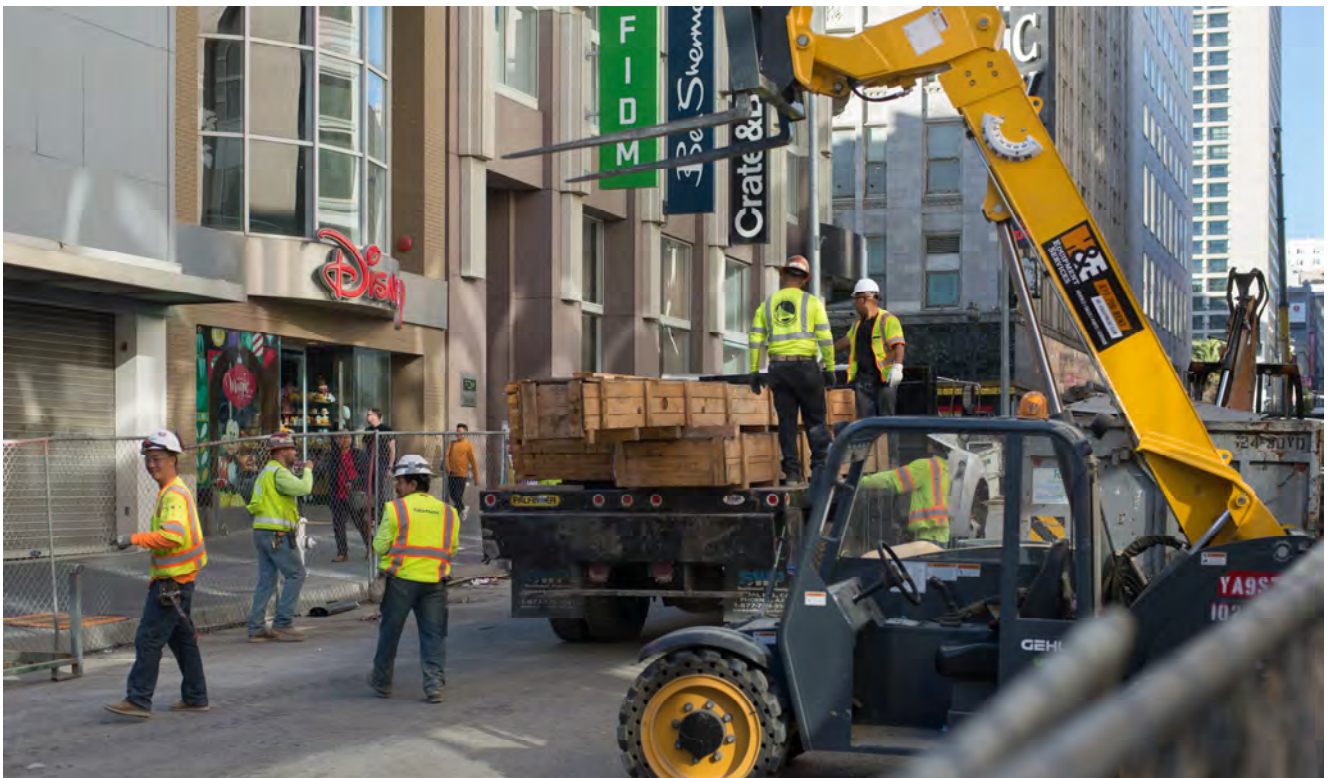
A shower of sparks falls from work to remove temporary shoring and install new sections of floor deck at the north end of the Union Square/Market Street station box.



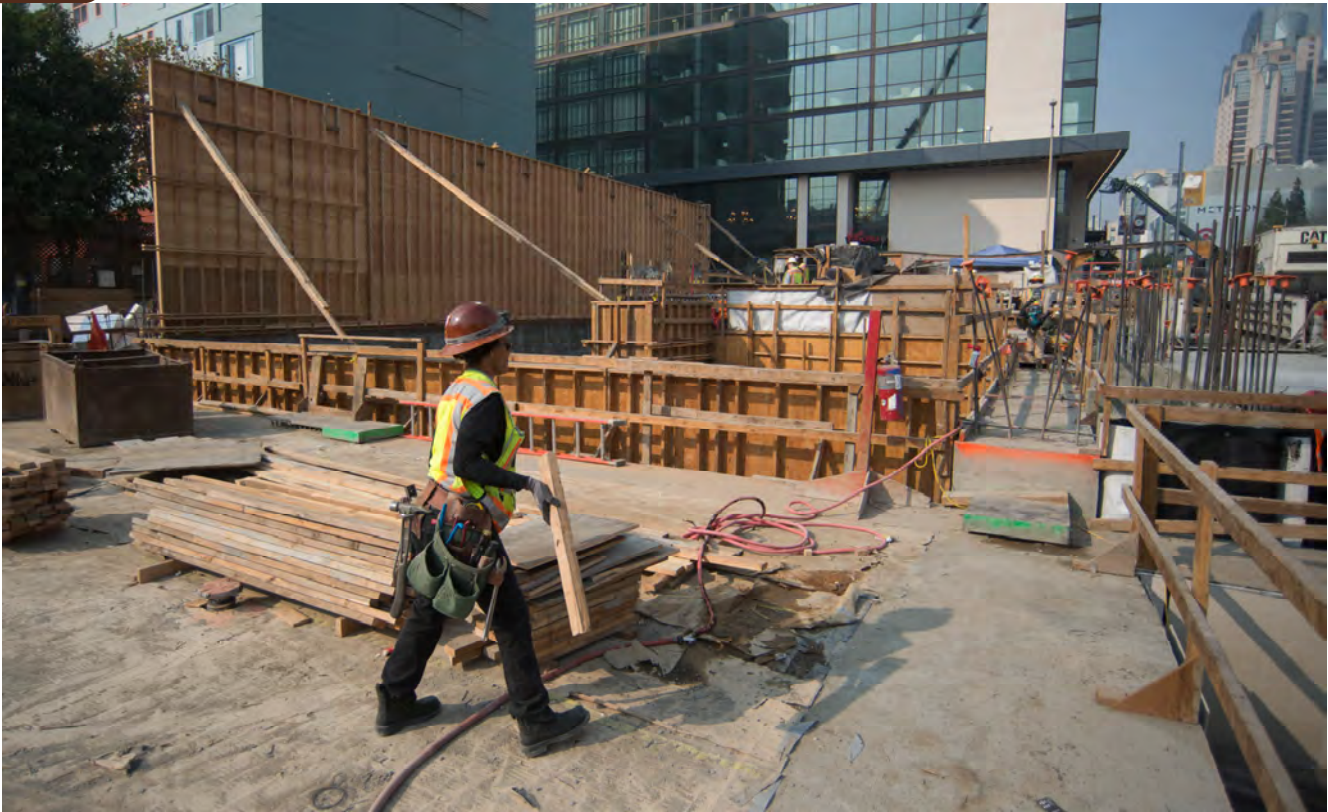
A crew positions a scissor lift at the bottom of the access shaft adjacent to the Union Square Garage, inside the north concourse of the Union Square/Market Street Station.



A new section of sidewalk begins to take shape during a concrete pour as part of sidewalk and roadway restoration work along the east side of Stockton north of O'Farrell.



A crew unloads equipment encased in wooden crates inside the surface worksite on Stockton south of O'Farrell.



A carpenter disassembles concrete forms to reuse materials atop the Yerba Buena/Moscone Station headhouse while formwork for a massive ventilation structure goes up in the background.



Crewmembers pass through an area on the mezzanine level of the Yerba Buena/Moscone Station headhouse where demolition work is underway to create an opening in the west slurry wall, allowing for a future ventilation structure.



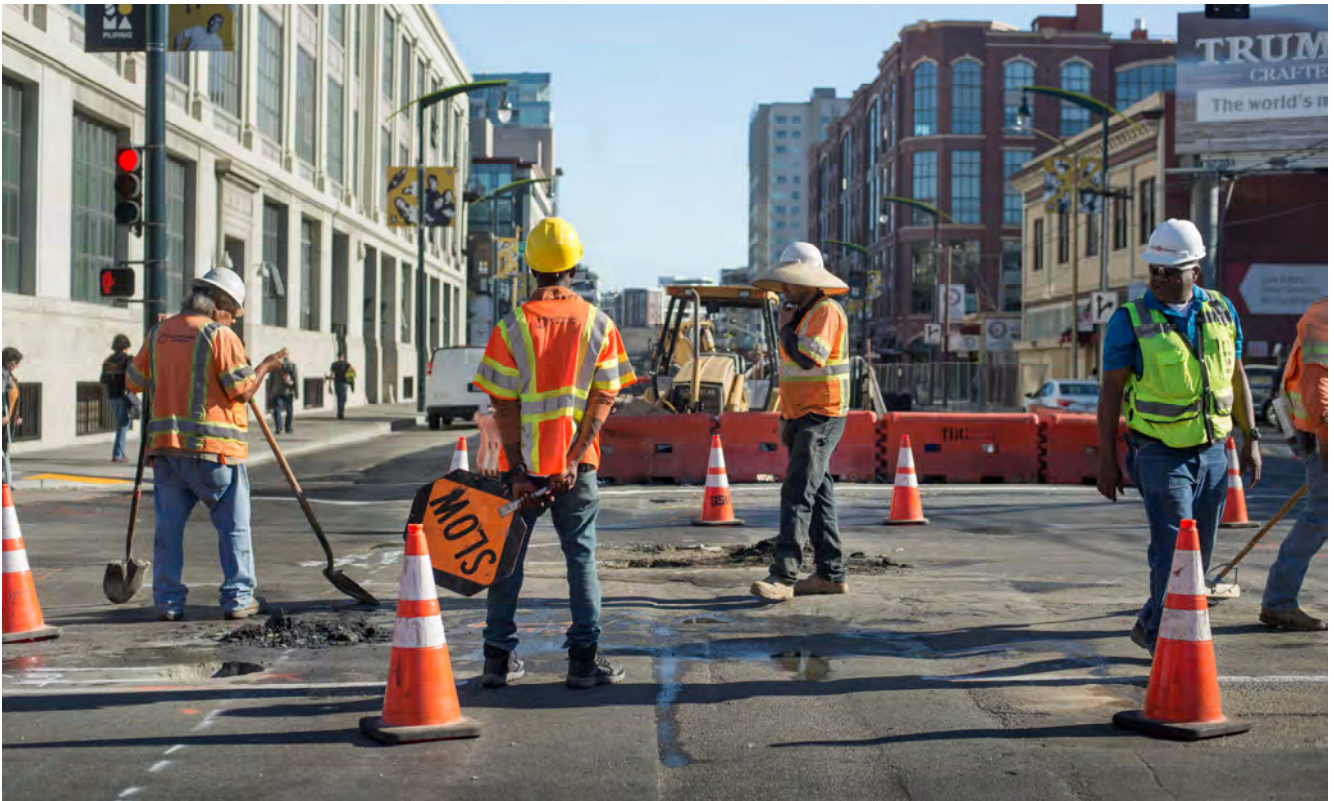
Workers patch concrete at the circular tunnel opening where the southbound track meets the Yerba Buena/Moscone Station platform.



A carpenter rips a 2x4 at the surface level of the Yerba Buena/Moscone Station head-house, to be used as part of concrete forms.



Trenching work has begun to upgrade water lines at the 4th and Brannan Intersection, ahead of roadway restoration work there.



A crew at 4th and Brannan conducts utility potholing while carefully maintaining pedestrian and vehicle flow.



In the southbound tunnel just north of the Yerba Buena/Moscone Station platform, a worker fashions sections of pipe to be installed as utility conduits in the twin tunnels.



Just south of the 4th and Brannan surface station platform, utility upgrade and installation work is underway in SoMa.

Appendix A

DETAIL COST REPORTS

*November 2018 Notice: The City is in the process of transitioning from FAMIS to Financial System Project (FSP). During the transition, we are unable to provide accurate financial updates. Once FSP is updated and validated, we will reconcile our reports accordingly. We will be projecting current expenditures and anticipate the reconciled updates will be available early next year.

1. PROJECT COST

The Current Cost Estimate (CCE) for the Central Subway Project is **\$1.578 billion** in year of expenditure dollars (\$YOE). This total project cost is shown at the top of Report 7.1, Program Project Budget. This capital cost projection incorporates allocated and unallocated contingencies to cover the risks associated with the project completion.

Total net incurred costs for the project are \$1,277.25 million, a \$7.51 million increase over last month. The cost to date figure reflects expenditures through FAMIS 786 Report (\$1,238.55 million) plus the utilities joint trench Form B Reimbursement payment (\$11.27 million), invoices currently being processed (\$23.87 million) and estimates of outstanding pay requests (\$3.59 million). This incurred amount equals 80.93% of the total project budget of \$1.578 billion.

The current funding level to date is \$1,517.03 and includes FTA \$23.02 million FY2018/2019 New Starts Grant appropriated in November 2018. This represents 96% of the total project budget.

CONTRACT	PP NO	PP PERIOD TO	PROG PYMT AMOUNT
CS155.1*	69	3/31/2016	\$ 13,280.00
CS155.1*	70	6/30/2016	\$ 24,327.00
CS155.1*	71	9/30/2016	\$ 65,000.00
CS155.1*	72	12/30/2016	\$ 50,000.00
CS155.1*	73	3/31/2017	\$ 35,282.00
CS155.2	101	7/31/2018	\$ 174,675.71
CS155.2	102	8/31/2018	\$ 163,151.96
CS155.2	103	9/30/2018	\$ 227,908.85
CS155.2*	104	10/31/2018	\$ 227,908.85
CS155.2*	105	11/30/2018	\$ 227,908.85
CS155.3	100	7/31/2018	\$ 44,624.56
CS155.3	101	8/31/2018	\$ 48,617.12
CS155.3	102	9/30/2018	\$ 57,241.04

CONTRACT	PP NO	PP PERIOD TO	PROG PYMT AMOUNT
CS155.3	103	10/31/2018	\$ 66,299.89
CS155.3*	104	11/30/2018	\$ 66,299.89
CN 1300	57	9/30/2018	\$ 6,423,452.00
CN 1300	58	10/31/2018	\$ 8,265,283.00
CN 1300	59	11/30/2018	\$ 7,726,850.00
CS149	117	8/31/2018	\$ 672,915.60
CS149*	118	9/30/2018	\$ 672,915.60
CS149*	119	10/31/2018	\$ 672,915.60
CS149*	120	11/30/2018	\$ 672,915.60
CS156*	95	10/31/2018	\$ 20,701.47
CS156*	96	11/30/2018	\$ 20,701.47
other accruals*		11/30/2018	\$ 821,718.03

* Estimated Amount \$ 27,462,894.09

2. CONTINGENCY ALLOCATIONS AND USAGE

The current Total Project Contingency is **\$64.95 million**, which is a \$39.95 million favorable balance against the current Minimum Contingency level of \$25 million. The Contingency Drawdown Curve is shown in Report 7.3. Follows by Report 7.4 Contingency Management Trend Report with the Remaining Contingency after Approved Changes Deducted contingency items in column "i".

In this reporting period, Contract 1252 Tunnel did not process any contract modifications. CN1300 Station processed two contract modifications in the amount of \$1,211,015. Refer to Report 7.5 for approved contract modifications and potential changes.

3. BUDGET TRANSFERS

Contract 1300 Station processed two contract modifications; one in the amount of \$84,537 in SCC 40 category and another for \$1,126,478 in SCC 20 category. There is a net budget transfer of \$0 in this reporting period due to the further drill down of SCC 10-50 categories; allocated contingency is in SCC 20 category. When a contract modification is processed, the funds are drawn from allocated contingency.

4. FORM B

The Utilities Joint Trench Form B Details is listed in the Table A2 below. Total utilities joint trench Form B Reimbursement payment to three construction contracts is \$11.27 million.

TABLE A2: UTILITIES JOINT TRENCH FORM B DETAILS	[A] Mar 2015 BUDGET	[B] EXPENDED TO DATE	Associated Cost Account
1.3.491.07.040.02 - FORM B - CN1250 UTILITY REIMBURSEMENT	(2,275,419)	2,463,325	1.3.081.07.040.02 - 1UTL:SITWORK: UTILITIES & RELOC
1.3.491.08.040.02 - FORM B - CN1251 UTILITY REIMBURSEMENT	(7,618,412)	3,608,217	1.3.082.08.040.02 - 2UTL:SITWORK:UTILITIES&RELOCATE
1.3.491.02.040.02 - FORM B - CN1252 UTILITY REIMBURSEMENT		3,975,656	1.3.083.02.040.02 - TUNN:Sitework:Utilities & Relocate
1.3.491.04.040.02 - FORM B - CTS: CN1300 UTILITY REIMBURSEMENT	(451,703)	441,304	1.3.085.04.040.02 - CTS.1254: SITE UTILITIES, UTILITY RELOCA
1.3.491.09.040.02 - FORM B - STS: CN1300 UTILITY REIMBURSEMENT	(1,000,000)	-	
1.3.491.03.040.02 - FORM B - UMS: CN1300 UTILITY REIMBURSEMENT	(528,370)	466,189	1.3.084.03.040.02 - UMS.1253: SITE UTILITIES, UTILITY RELOCA
1.3.491.05.040.02 - FORM B - YBM: CN1300 UTILITY REIMBURSEMENT	(100,000)	314,072	1.3.086.05.040.02 - YBM.1255: SITE UTILITIES, UTILITY RELOCA
TOTAL	(11,973,904)	11,268,764	

5. EARNED VALUE (EV) ANALYSIS

In November 2018 Report, the Preliminary Earned Value Analysis reports is based on the SFMTA November Schedule Update. The Planned Value, Earned Value, Actual Cost, Percent Complete and resulting indexes as follows:

Preliminary November Earned Value

Overall Budgeted Cost:	\$1,578,300,000
Planned Value:	\$1,537,943,252
Earned Value:	\$1,276,990,338
Actual Cost:	\$1,277,249,638
Schedule Performance Index (SPI):	0.83
Cost Performance Index (CPI):	1.00
Percent Complete:	80.8%

SFMTA, EV Chart
November 30, 2018 Update

Activity ID	Activity Name	Start	Finish	Performance % Complete	Budgeted Total Cost	Planned Value Cost (PV)	Earned Value Cost (EV)	Actual Total Cost (AC)	CPI	SPI
CENTRAL SUBWAY PROJECT										
		03-Jun-03A	29-Dec-21	80.84%	\$1,578,300,000.01	\$1,537,943,251.70	\$1,276,890,338.05	\$1,277,249,638.12	1.00	0.83
	Preliminary Engineering Phase	03-Jun-03A	07-Jan-10A	100%	\$46,542,061.34	\$46,542,061.02	\$46,542,061.02	\$46,542,060.53	1.00	1.00
	Final Design	08-Jan-10A	17-Jun-13A	100%	\$115,075,987.10	\$115,075,987.06	\$115,075,987.06	\$113,950,952.17	1.01	1.00
	Light Rail Vehicles	15-Apr-13A	11-Dec-19	8.25%	\$26,385,653.00	\$26,385,653.00	\$2,177,131.58	\$11,929,246.72	0.18	0.08
	Real Estate	01-Aug-08A	15-May-15A	100%	\$32,140,417.71	\$37,405,895.00	\$37,405,895.00	\$30,543,064.53	1.22	1.00
	Construction Phase	03-Jan-10A	20-Dec-21	80%	\$1,351,413,475.86	\$1,312,533,655.62	\$1,075,789,263.39	\$1,074,284,314.17	1.00	0.82
	Construction Support and Costs	03-Jan-10A	20-Dec-21	68.69%	\$203,186,344.33	\$167,592,813.39	\$137,294,588.21	\$152,047,110.36	0.90	0.82
	Construction Utility Contract #1- MOS & Portal CIV-1250	04-Jan-10A	23-May-11A	100%	\$11,968,150.00	\$11,968,150.00	\$11,968,150.00	\$11,968,150.00	1.00	1.00
	Construction Utility Contract #2 - UMS CIV-1251	12-Jan-11A	15-Oct-12A	100%	\$20,669,081.47	\$20,794,582.00	\$20,794,582.00	\$20,669,081.47	1.01	1.00
	Construction Tunnels CIV-1252	08-Jun-11A	26-Nov-18	99.44%	\$235,913,500.06	\$251,068,967.23	\$249,672,040.28	\$233,588,322.94	1.07	0.99
	Construction CIV-1000	03-Jun-13A	13-Oct-19	76.19%	\$879,676,400.00	\$661,109,143.00	\$656,089,902.89	\$656,010,650.00	1.00	0.76
	Unallocated Contingency	26-Nov-18	26-Dec-19	0%	\$6,742,405.00	\$0.00	\$0.00	\$0.00	0.00	0.00
	Project Management	26-Dec-19	29-Dec-21	0%	\$0.00	\$0.00	\$0.00	\$0.00	0.00	0.00

Earned Value Analysis and Definitions

SPI is a measure of schedule efficiency on a project. It is the ratio of earned value (EV) to planned value (PV). A SPI equal to or greater than one indicates more work was completed than planned and a value of less than one indicates less work was completed than planned.

A value of less than 0.9 is unfavorable.

CPI is a measure of cost efficiency on a project. It is the ratio of earned value (EV) to actual cost value (AC). A CPI equal to or greater than one indicates a cost under run and a value of less than one indicates a cost overrun. A value of less than 0.9 is unfavorable.

The following earning rules are established for each of the phase:

Cost Element Group	Planned Value (Primavera)	Earned Value (Primavera)	Actual Cost (SFMTA Cost Accounting (SAP))
Prelim. Engineering	Expenditure Plan Level of Effort (LOE)	Equals to Planned Value (LOE)	Time Keeping; Vendor Accruals and Invoices
Final Design	Expenditure Plan Level of Effort (LOE)	Equals to Planned Value (LOE)	Time Keeping; Vendor Accruals and Invoices
Procurement	Planned Delivery Date	Actual Delivery Date	Time Keeping; Vendor Accruals and Invoices
Real Estate	Expenditure Plan Level of Effort (LOE)	Equals to Planned Value (LOE)	Time Keeping; Vendor/ Material Accruals and Invoices
Construction	Schedule of Work	% Complete* x Budget at Completion (BAC)	Vendor Accruals and Invoices
Sub-Total	Performance Measurement Baseline (PMB)	Total Earned Value	Total Actual Cost
Below the Line	+ Contingency		
Total	Approved Budget		

6. FUNDING SUMMARY

The Funding Available Table below shows the total awarded funds to date vs. the total committed funds from the Project's seven funding sources.

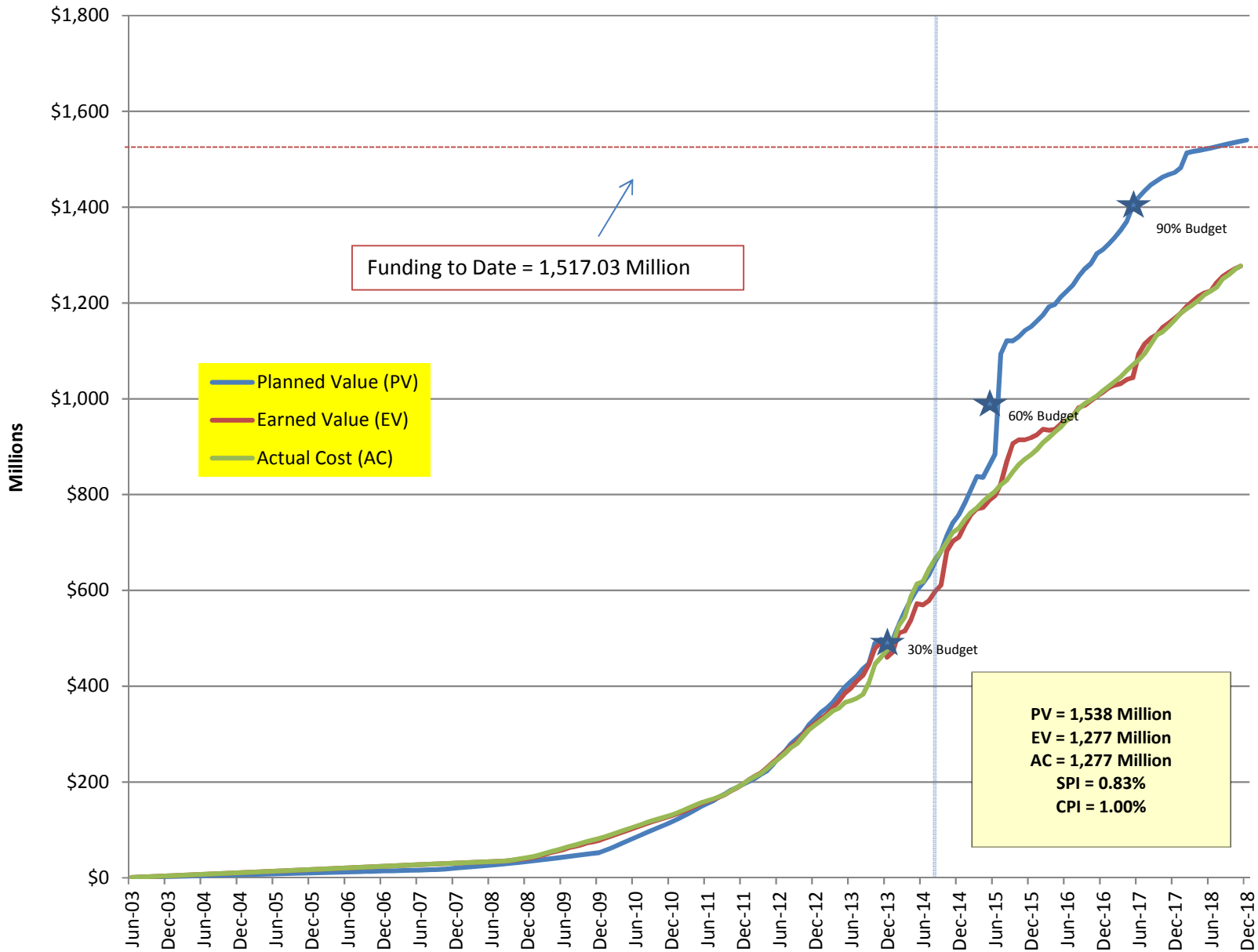
Funding Available Table		
	Funding	
	Committed Funding Sources	Total Awarded Funds to Date
Federal		
Sect. 5309-NS	\$942,200	\$942,200
CMAQ	\$41,025	\$41,025
Federal Subtotal	\$983,225	\$983,225
State		
TCRP	\$14,000	\$14,000
State RIP	\$88,000	\$12,498
Prop. 1B (I-Bond) PTIMSE	\$307,792	\$307,792
Prop. 1A (HSR-Bond)	\$61,308	\$61,308
State Subtotal	\$471,100	\$395,598
Local		
MTA	\$0	\$475
Prop. K	\$123,975	\$137,727
Local Subtotal	\$123,975	\$138,202
CPT 544 Total	\$1,578,300	\$1,517,025

7. LIST OF COST REPORTS

- 7.1 Program Project Budget
- 7.2 Earned Value Cash Flow
- 7.3 Contingency Drawdown Curve
- 7.4 Summary Contingency Management Trend Report
- 7.5 Detail Contingency Usage Report
- 7.6 Budget Revisions: Report sorted by Construction Packages & Soft Costs
- 7.7 Project Budget & Expenditure Report: Sorted by SCC Summary
- 7.8 Budget & Expenditure Report: Sorted by SCC Details
- 7.9 Detail Monthly Expenditure Report: grouped by Project Phase
- 7.10 Cost Report Notes

A. Central Subway Project						
Project	Name	Amount	PM	Funding Source	Reporting	Cost Report Notes
1	CPT544 Central Subway Project	\$1,578,300,000	J. Funghi	62% Fed, 30% State, 8% Local	yes	1
Total:		\$1,578,300,000				
B. Related SFMTA Capital Improvement Projects						
Project	Name	Amount	PM	Funding Source	Reporting	
2	CPT690 TBM Retrieval Shaft Relocation	\$9,700,000	Funghi/Magyar	MTA Operating Funds	no	2
3	CPT718 Chinatown Metro Plaza	\$6,980,000	J. Funghi	Transbay Redevelopment	no	3
4	CPT665 Central Subway Project - Goodwill	\$2,367,750	K. Magyar	I-Bond Interest	no	4
5	CPT705 MOH - Broadway/Sansome	\$8,000,000	K. Magyar	MTA Operating Funds	no	5
Total:		\$27,047,750				
C. Central Subway Project - Project Offset Credits						
	From	Amount	Index	Notes	Reporting	
1	2009-2016 Utility Co. - Form B Reimbursement	\$12,227,954	--	Construction contracts	yes	6
2	2017-2019 PG&E - Power Feed Reimbursement	\$7,624,540	--	Not yet bill PG&E	yes	7
3	6/26/2013 BART Elevator	\$90,000	68CPT544135B	Not yet rec'd BART Funds	yes	8
4	11/6/2013 Tutor Perini - CAD Files	\$2,500	68CPT5441236	Deposit to Design Index	yes	9
5	1/27/2014 SFPUC - Sewer Main	\$2,925,296	68W251	Certified in Contract 1300	yes	10
6	8/27/2014 SFMTA Traffic Effectiveness Project funded	\$694,651	68W324/686D42	Contract 1252 CMod #40	yes	11
7	9/27/2014 SFPUC - 24" Water Main	\$328,860	68CPT544135A	Contract 1252 CMod #41	yes	12
8	2/15/2015 Chinatown Plaza Construction Estimate	\$75,000	68CPT7181341	Contract 1300 CMod #6	yes	13
9	3/27/2015 SFPUC - 24" Water Main Additional Work Support for North Beach Restoration, OCS and	\$112,102	68W409	Contract 1252 CMod #48	yes	14
10	3/15/2016 Streetlighting	\$155,468	68T7373342D2/D3	Contract 1252 CMod #51	yes	15
11	6/27/2016 DPW - MOU for Water Line above YBM Station SFWD - 8' water line at the intersection of Fourth and	\$438,218	68W592	Contract 1300 CMod #20 Contract 1252 CMod #49 partial	yes	16
12	12/9/2016 Jessie Street	\$21,020	68W456	(\$2,102) and #60	yes	17
Total:		\$24,695,609				

Earned Value Cash Flow Curve

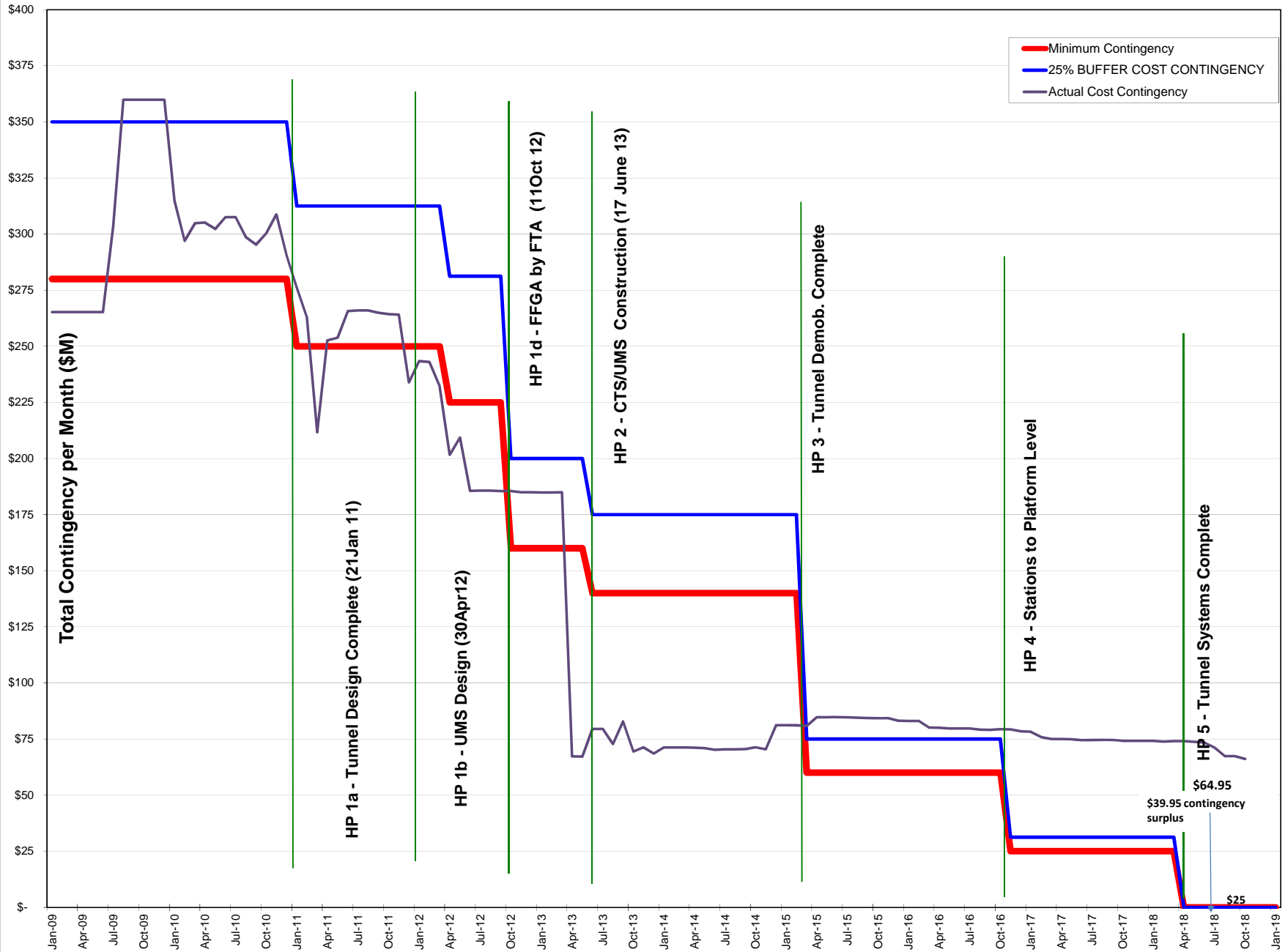


Funding to Date = 1,517.03 Million

Planned Value (PV)
Earned Value (EV)
Actual Cost (AC)

PV = 1,538 Million
EV = 1,277 Million
AC = 1,277 Million
SPI = 0.83
CPI = 1.00

Cost Contingency Drawdown



COST ELEMENT	CONTRACT COST					CONTINGENCY					BUDGET	VARIANCE	Cost Report Notes	
	ORIGINAL CONTRACT VALUE / September 2013 SUPPLEMENTAL BUDGET	APPROVED CHANGES	CURRENT CONTRACT VALUE	POTENTIAL CHANGES	ESTIMATE AT COMPLETION (EAC)	ORIGINAL CONTINGENCY / Sep 2013 SUPPLEMENTAL CONTINGENCY (Include CN 1250 & CN1251)	CONTINGENCY ADJUSTMENT TRANSFERS	REVISED AUTHORIZED CONTINGENCY (Include CN1250 & CN1251)	REMAINING CONTINGENCY AFTER APPROVED CHANGES DEDUCTED [h - b]	REMAINING CONTINGENCY AFTER POTENTIAL CHANGES DEDUCTED [i - d]	ORIGINAL CONTRACT VALUE + REVISED AUTHORIZED CONTINGENCY [a + h]	BUDGET - ESTIMATE AT COMPLETE [j - e]		
	a	b	c [a + b]	d	e [c + d]	f	g	h	i	j	j	k		
SCC 10-50 CONSTRUCTION CONTRACT PACKAGES														
1250	UTILITY RELOCATION PACKAGE #1 <i>Contract 1250 Department of Technology</i>	9,273,939	2,694,211	11,968,150		11,968,150	1,953,377	740,834	2,694,211			11,968,150		18
		166,756		166,756		166,756						166,756		
1251	UTILITY RELOCATION PACKAGE #2 <i>Contract 1251 Department of Technology</i>	16,832,550	3,836,531	20,669,081		20,669,081	5,367,297	(1,530,766)	3,836,531			20,669,081		19
		75,615		75,615		75,615						75,615		
1252	GUIDEWAY TUNNEL	233,584,015	1,363,054	234,947,069	20,000	234,967,069	23,658,464	(21,328,979)	2,329,485	966,430	946,430	235,913,500	946,430	20
1300	STATIONS	839,676,400	15,218,064	854,894,464	20,292,055	875,186,520	20,000,000	20,000,000	40,000,000	24,781,936	4,489,880	879,676,400	4,489,880	21
	1253 UNION SQUARE/MARKET ST STATION [UMS]	294,030,590	2,748,442	296,779,032	8,529,284	305,308,316	5,000,000	15,000,000	20,000,000	17,251,558	8,722,274	314,030,590	8,722,274	
	1254 CHINA TOWN STATION [CTS]	247,567,810	8,983,302	256,551,112	3,713,726	260,264,839	5,000,000	5,000,000	10,000,000	1,016,698	(2,697,029)	257,567,810	(2,697,029)	22
	1255 YERBA BUENA/ MOSCONE STATION [YBM]	158,089,000	747,056	158,836,056	5,958,324	164,794,380	5,000,000	5,000,000	5,000,000	4,252,944	(1,705,380)	163,089,000	(1,705,380)	
	1256 SURFACE TRACKWORK & SYSTEMS [STS]	139,989,000	2,739,264	142,728,264	2,090,722	144,818,986	5,000,000	5,000,000	5,000,000	2,260,736	170,014	144,989,000	170,014	
OTHER		31,233,501	1,060,000	32,293,501		32,293,501	1,160,000	1,060,000	2,220,000	1,160,000	1,160,000	33,453,501	1,160,000	23
SCC 10 - 50 Construction Sub-total		1,130,842,776	24,171,861	1,155,014,637	20,312,055	1,175,326,692	52,139,137	(1,328,979)	51,080,226	26,908,366	6,596,311	1,181,923,002	6,596,311	24
SCC 60-80 SOFT COSTS PACKAGES														
60	ROW, LAND, EXISTING IMPROVEMENTS	36,511,799	(4,265,478)	32,246,321		32,246,321	1,000,000	(1,000,000)	0	0	0	32,246,321	0	25
70	VEHICLES	24,108,712	(10,799,712)	13,309,000		13,309,000	2,276,941	10,799,712	13,076,653	13,076,653	13,076,653	26,385,653	13,076,653	26
80	PROFESSIONAL SERVICES	310,518,041	2,263,498	312,781,539		312,781,539	18,221,079		18,221,079	18,221,079	18,221,079	331,002,618	18,221,079	26a
SCC 60 - 80 Construction Sub-total		371,138,552	(12,801,692)	358,336,860	0	358,336,860	21,498,020	9,799,712	31,297,732	31,297,732	31,297,732	389,634,592	31,297,732	
SCC 90	UNALLOCATED CONTINGENCY						3,845,945	2,896,460	6,742,405	6,742,405	6,742,405	6,742,405	6,742,405	27
TOTAL		1,501,981,328	11,370,169	1,513,351,497	20,312,055	1,533,663,552	77,483,102	11,367,193	89,120,363	64,948,503	44,636,448	1,578,299,999	44,636,447	
											Total Project Budget	1,578,300,000	28	
											Estimate At Completion	1,533,663,552	29	
											Variance	44,636,447	30	

Note #17 - Adjusted Contract 1252 Guideway Tunnel contingency "column g" to reflect construction contract modifications #20, #40, #41, #48 and #51 were funded by other funding sources.

Contract Modification/Trend Log - Contract 1252 Tunnel

Awarded NTE Amount: \$233,584,015.00
Substantial Completion: 4/15/2015

Revised NTE Amount \$235,078,784.60
Revised Substantial Completion 4/15/2015

Contract Modifications				Amount	
No.	Description	SCC Code	COR/PCC No.	CMod	Contract NTE
1	Amendment of Insurance Requirements		n/a	\$0.00	\$233,584,015.00
2	Amendment of General Liability Insurance Requirements		n/a	(\$3,040,713.00)	\$230,543,302.00
3	Relocation of PG&E Ductbank & Removal/Reinstall TODCO Scaffolding		CORs 10 & 29	\$6,633.37	\$230,549,935.37
4	Investigate 48" Pipe at UMS S. Headwall		PCC 9	\$75,000.00	\$230,624,935.37
5	LB/MOS Oil Filled Pipe Abatement and MOS Asbestos Pipe Abatement		CORs 7, 22 & 27	\$23,912.54	\$230,648,847.91
6	Arch. Support at MOS Station N. Headwall		COR 12	\$16,892.96	\$230,665,740.87
7	Revisions to MOS N. Headwall Elevation		PCC 1	\$20,358.23	\$230,686,099.10
8	UMS Reduced Duration		COR 21	\$0.00	\$230,686,099.10
9	48" Pipe Investigation and Removal at the UMS Headwalls		PCC 11	\$150,000.00	\$230,836,099.10
10	PG&E Impacts to Launch Box and Flagger		CORs 6 & 41	\$8,618.96	\$230,844,718.06
11	Bart Annex Wall		PCC 7	\$15,500.00	\$230,860,218.06
12	LB - Concrete Manhole and Slurry Pipe Removal		COR 38	\$3,820.84	\$230,864,038.90
13	Retrieval Shaft - Pipe/Duct Bank Removal		CORs 31, 47, 50, 58, 66	\$9,908.04	\$230,873,946.94
14	MOS - 16" Pipe Removal @ N/S Headwalls		CORs 39, 44	\$4,551.99	\$230,878,498.93
15	MOS - S. Headwall Asbestos Pipe Abatement		COR 26	\$27,629.64	\$230,906,128.57
16	UMS Utility Removal/Construct Wall		CORs 46, 48, 68	\$21,150.28	\$230,927,278.85
17	MOS - 16" Steel Pipe Removal at N/S Headwalls/Enlarge Tie-In Hole		CORs 73, 76, 81	\$5,056.63	\$230,932,335.48
18	SFWD Support Work		PCC 13	\$20,000.00	\$230,952,335.48
19	Additional BART Instrumentation		PCC 6	\$307,860.75	\$231,260,196.23
20	Relocation of Retrieval Shaft		PCC 10	\$5,150,000.00	funded by CPT690
21	Columbus Ave. Restoration		NA	\$261,057.00	\$231,521,253.23
22	LB - Jet Grout Quantity Overrun		COR 070	\$599,900.00	\$232,121,153.23
23	Old Navy - Comp. Grout DSCs		COR 079	\$259,373.00	\$232,380,526.23
24	SFWD Excavation, Support and Backfilling		PCC 13	\$13,982.00	\$232,394,508.23
25	Cross Passage 5 - VECP		COR 087	(\$2,674.00)	\$232,391,834.23
26	Various CORs		CORs 9, 24, 37, 45, 51, 61, 71, 77, 83, 99	\$73,700.00	\$232,465,534.23
27	MOS - Reroute traffic signal lines / Equipment Standby		CORs 11, 17	\$80,719.00	\$232,546,253.23
28	PCC 12 - Comp.Grout Mtls., Accel. at R. Shaft, Haz. Mtls @ R.Shaft, Obst. at CSM panel W6 (NTE Amount CMod)		FA COs 006, 007, 008, 009	\$81,937.00	\$232,628,190.23
29	AWSS Conflict with Water Line, AWSS Restraining at Launch Box, AT&T Vault Conflict with Sewer Main		COR 1, 2, 3	\$73,045.00	\$232,701,235.23
30	MOS Headwall End-stops		COR 018	\$144,000.00	\$232,845,235.23
31	Launch Box - Slurry Wall Obstructions		COR 32, 34, 42, 43, 62, 65, 67	\$234,438.00	\$233,079,673.23

Connecting people. Connecting communities.

32	COR 8 + Markup - Associated w/ COR 001, COR 002, and COR 003		COR 1, 2, 3, 8	\$168,000.00	\$233,247,673.23
33	LB - Pre-Excavation for Slurry Walls		COR 015	\$125,000.00	\$233,372,673.23
34	Modifications to Tunnel Alignment at Market Street - Initial Design Costs		PCC 012 Part 1	\$39,930.00	\$233,412,603.23
35	Deleted AWSS Work at Union St. and Columbus Ave.		PCC 015	(\$187,181.00)	\$233,225,422.23
36	Curb Ramp Work at NE Corner of 4th and Harrison Streets (Force Account)		FA CO 011	\$5,023.00	\$233,230,445.23
37	Staging Yard Hazardous Material		COR 30, 54, 75	\$401,933.00	\$233,632,378.23
38	Modifications to AWSS Facilities at 4th and Bryant Streets		PCC 014	\$35,925.00	\$233,668,303.23
39	MOS N. Headwall Impacts, LB – Jet Grout Overrun		COR 85, 70	\$240,333.00	\$233,908,636.23
40	Culvert, Street & Sidewalk Restoration in N.Beach (includes QC testing)		PCC 20	\$694,651.00	funded by TEP
41	Install Water Main in North Beach		PCC 20	\$328,860.00	funded by SFPUC
42	UMS - Shoring Impacts due to 48" ATT Pipe at SW Headwall		COR 069	\$29,463.00	\$233,938,099.23
43	UMS Tangent Pile SRB-H DSC, UMS Jet Grout Column No. 18 DSC		COR 096, COR 102	\$60,870.00	\$233,998,969.23
44	Subcontractor Substitution		NA	\$0.00	\$233,998,969.23
45	Modifications to Tunnel Alignment - Construction Costs		PCC 12	\$883,693.00	\$234,882,662.23
46	MOS 20" Conflict with 16" AWSS @ South Headwall		COR 082	\$28,160.00	\$234,910,822.23
47	CDF Encasement on 42" RCP		COR 005	\$95,000.00	\$235,005,822.23
48	North Beach Water Main Additional Work		PCC 20	\$112,102.20	funded by SFPUC
49	Various CORS and FACOS; FACO 13-15, 17-19; COR 116, 118-121		FACO 13-15, 17-19; COR 116, 118-121	\$180,010.41	\$235,185,832.64
50	Contract Milestone Changes			\$0.00	\$235,185,832.64
51	PCC 020 - Supp 1 - North Beach Restoration, OCS and Streetlighting			\$155,468.17	funded by TEP
52	PCC 19 - Piping, Conduit and Casing at 4th Street Portal			\$11,678.00	\$235,197,510.64
53	COR 117 - Retrieval Shaft Impacts			\$30,278.08	\$235,227,788.72
54	Material Hardness at the Retrieval Shaft			\$166,182.81	\$235,393,971.53
55	PCC 020 - Supp 2 - North Beach Sewer Work			\$19,730.14	\$235,413,701.67
56	Deleted Water Utility Replacement/Water, Sewer, MRY and AWSS Design Changes		PCC 24, 28	(\$15,259.00)	\$235,398,442.67
57	YBM Concrete Overpour and Jet Grout at Headwalls		COR 072	(\$84,509.00)	\$235,313,933.67
58	Credit - Delete AWSS Material Supply			(\$201,288.32)	\$235,112,645.35
59	Credit - 1 No. AMTS Unit			(\$33,860.75)	\$235,078,784.60
60	Water Line Work at 4th and Jessie Street		PCC 60	\$21,019.80	funded by SFPUC
61	Credit - CN1300 COR's 54, 390/477, 485, 848, 1058		CORs 54, 390, 477, 485, 848, 1058	(\$68,715.45)	\$235,010,069.15
62	Credit for CP-5 Inspection			(\$63,000.00)	\$234,947,069.15
Sub Total:				\$1,363,054.15	

Pending Contract Modifications		SCC Code	COR/PCC No.	Estimated	
Description				Amount	Time
I	Other Pending Contract Modifications		Cmod # TBD	\$20,000.00	
Sub Total:				\$20,000.00	

Contract Modification/Trend Log - Contract 1300 Stations

Awarded NTE Amount	\$839,676,400	\$854,894,464
Substantial Completion	6/30/2018	6/30/2018

	UMS	CTS	YBM	STS	COST REPORT NOTES
Potential Changes	8,529,284	3,713,726	5,958,324	2,090,722	31
Change Order - Pending					
CTS COR 372 DSC Potholing @ N Acces		9,846			
CTS COR 824 Multiple Setups, Standb		1,384			
CTS COR 994 FACO#01,10,11 & Alw 13		26,319			
CTS PCC 43 Removal of Bus Bulb		15,180			
STS COR #89 Extra Sewer Work				15,561	
STS COR #91 PG&E Vault Conf 16" Wtr				68,299	
STS COR 1085 AWSS Conflit w/ 36" Swr				16,135	
STS COR 1115 Unkwn Conc Impdng AWSS				2,586	
STS COR 1194 Unkwn MH Incon MH				2,866	
STS COR 322 Existing Tunnel Cond.				399,000	
STS COR 522 Thickened Str. Stan				6,220	
STS COR 650 Asbestos conf w/ ATT DB				4,510	
STS COR 736 Addition Rein Handholes				5,905	
STS COR 844 4th & King Trcwrk Shtdn				0	
STS COR 846 Util Conf w/ 78" SW Crw				11,225	
STS COR 924 Ovpod Con inconf Catbsn				2,540	
STS COR 987 Unkn Utl conflt w/ (N)				71,823	
UMS COR 1023 CIP Wall Reinforcing	359,905				
UMS COR 1041 Culvert Removal	607				
UMS COR 1056 Embed Steel Plates	443				
UMS COR 1058 Obstruction at C01W	8,122				
UMS COR 1076 Unkwn Conduits & Conc	7,141				
UMS COR 12 Fire Hydrant @ OFarrell	45,000				
UMS COR 389 4" Steel line asbestos	951				
UMS COR 770 Fiil Void Sp with CDF	1,049				
UMS COR 781 Utility Trench Supp Slab	42,423				
UMS COR 790 Duct Bank Conflict	42,519				
UMS COR 810 Unkn Bem @ M Macys Wall	426				
UMS COR 845 Unidntfid Conc Obstrctn	6,841				
UMS COR 885 Light Pole Foundation	15,703				
UMS E Vault Conflicting Pile Line	11,895				
UMS MRY Duct Bank-East Conflict	50,485				
UMS PCC 103 PG&E S.light at Maiden	6,555				

Contract Modification/Trend Log - Contract 1300 Stations

Awarded NTE Amount	\$839,676,400	\$854,894,464
Substantial Completion	6/30/2018	6/30/2018

	UMS	CTS	YBM	STS	COST REPORT NOTES
UMS PCC 195 Reinf. of Wall to Slab	1,514				
UMS PCC 270 Receptacles & Circuits	75,361				
UMS PCC 282 Mezzanine N. Headwall	23,458				
UMS PCC 333 Water Service Offset	5,270				
UMS PCC 334 Pothole for Sewer	3,591				
UMS PCC 345 Lead Paint Abatement	221,766				
UMS PCC 356 Steel Support at Mezz.	11,324				
UMS PCC 411 Foam for Beam 95 Cavity	4,975				
USG COR 779 E WI Bm Reinf Chng	5,322				
USG PCC 102 Fire Horn-Strobe	48,149				
USG PCC 105 Adj to Row X and Row Y	28,715				
USG PCC 107 Light Pole Footings	7,205				
USG PCC 141 Wtrproof & Drn at P	261,851				
USG PCC 144 Conc. Finishing/Repairs	213,534				
USG PCC 186 Bollard on Ramps	28,124				
USG PCC 190 Plaza Level Slab Detail	9,354				
USG PCC 191 Column 14 A Changes	5,290				
USG PCC 197 Precast Planter Pots	11,029				
USG PCC 31 HVAC Trench Mod.	132,837				
USG PCC 68 Ramp Barriers Handrail	32,873				
USG PCC 73 Acceleration of Work	83,187				
USG PCC 87 Tiebacks, L2, GL 14-15	161,294				
YBM COR 1154 Elevator Glass Cab Cha			37,010		
YBM COR 1205 DSC Ductbank & 12" WD			20,127		
YBM COR 1223 Removable guardrail			13,151		
YBM COR 1238 No Weld Bead @ 30" FM			4,022		
YBM COR 1244 Conc. encase. conflict			2,802		
YBM COR 1252 Build 20"x4" Blow Off			3,995		
YBM COR 1283 Chase for Refrigerant			1,795		
YBM COR 1293 Steel Line Conflict w/			2,455		
YBM COR 1306 Hold Point Koden Check			11,635		
YBM COR 1377 (E) 20" Waterline conf			24,123		
YBM COR 1534 Square Tubing for Bike			533		
YBM COR 1620 Install Knife Valve			1,060		
YBM COR 705 Del bems & Embds/Ven sh			(22,928)		

Contract Modification/Trend Log - Contract 1300 Stations

Awarded NTE Amount	\$839,676,400	\$854,894,464
Substantial Completion	6/30/2018	6/30/2018

	UMS	CTS	YBM	STS	COST REPORT NOTES
YBM COR 841 Design Chng Stair 1 run			7,638		
YBM COR 953 Added Condensate Drain			6,059		
YBM COR 972 Ex Ut Inc W Ctch Basin			2,982		
YBM PCC 182 Primers & Vents for FDs			10,099		
YBM PCC 209 Tract Power GRS Downsiz			(23,988)		
YBM PCC 214R1 FHC and AR Phones			23,045		
YBM PCC 252 Protect In-Slab Pipes			15,173		
YBM PCC 340 Stripe Howard 3rd - 4th			316		
YBM PCC 76 AWSS, SSFM, WM Design			164,776		
Change Order Request (COR)					
CTS COR 1642 Suspension of Work Due		0			
CTS COR 1646 Reinforcing for Beams		0			
CTS COR 1655 Track Slab Pour Delay		0			
STS COR 1320 Accel Sewer Work				67,401	
STS COR 1545 ATCS Signal Recommenda				0	
Negotiation					
COR 1543 Thales ATCS Sig Recom				0	
CTS - PCC #036 (CMU Wall Bracing)		19,552			
CTS COR 1013 CTS Setlmt Mitigation		20,001			
CTS COR 1016 Locate Water Leak		4,538			
CTS COR 1031 Add Fire Hose Valves		14,896			
CTS COR 1035 FHC & Phns Dsgn Cnflct		10,001			
CTS COR 1061 S Pltfrm Unstable Grnd		150,000			
CTS COR 1070 Pltfrm Lvl Info Kiosk		10,001			
CTS COR 1107 Compensation Grout		0			
CTS COR 1159 Change Escalator 1 & 2		484			
CTS COR 1175 Compensation Grout Set		10,001			
CTS COR 1177 NDSC Unknown Utilities		5,000			
CTS COR 1186 Unknown 1" Water Serv		16,419			
CTS COR 1249 Add GFRC Panels		130,001			
CTS COR 1253 N. Emergency Egress		5,349			
CTS COR 1308 Dwg Interpretation		67,160			
CTS COR 1401 Excessive Compensation		80,001			
CTS COR 1419 Add Neutral Conductor		9,000			
CTS COR 1632 Platform GFRC Wall Pan		95,061			

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	UMS	CTS	YBM	STS	COST REPORT NOTES
CTS COR 299 Removal of Interim SW		18,253			
CTS COR 408 MSX Termination		191,291			
CTS COR 445 3x5 w/ HDPE/PVC Inside		10,001			
CTS COR 526 Connection b/t Wall & D		25,001			
CTS COR 527 Connection b/t Wall/Dec		40,939			
CTS COR 568 CMOD 019 Reservations		31,906			
CTS COR 582 Monitor at Agent Booth		36,370			
CTS COR 625 Added Cane Detc Rail		6,066			
CTS COR 626 Ad HSS & Plt @ H Beam		4,267			
CTS COR 627 Add HSS Col @ Tickt Mac		2,471			
CTS COR 628 Add Beams and Embeds		8,285			
CTS COR 659 Add Grts at Sta Ag glas		1,537			
CTS COR 679 GI Cutos & Add Sp hds		23,450			
CTS COR 681 Crss Cut Cvrn SEM Excvn		60,001			
CTS COR 686 Es & GlS Enc Slf Clning		6,716			
CTS COR 695 Mod to Grondng System		10,001			
CTS COR 866 ATCS/Thals Sngl Pwr Sor		10,001			
CTS Delete PGE Work at Vault 732		(35,036)			
CTS PCC 001 Delete DB on Stockton		(84,018)			
CTS PCC 120 Prov Cond-Lft-net Systm		0			
CTS PCC 169 JT Config Change		48,068			
CTS PCC 207 SEM Sequencing Changes		0			
CTS PCC 259 Soil/rock Offhaul to Ox		1,500,000			
CTS PCC 262 Cavern Wall Artwork		18,000			
CTS PCC 316 PSG 500 to 750 MCM		75,000			
CTS PCC 332 Add Lighting Artwork		21,000			
CTS PCC 347 PGE Permanent Elect		20,001			
CTS PCC 348 Omission of Metal Ring		40,001			
CTS PCC 349 Platform Column C1 Size		2,001			
CTS PCC 350 Escalator Raceways Mach		20,469			
CTS PCC 351 Trap primers for floor		10,000			
CTS PCC 352 Downspout Requirement		822			
CTS PCC 353 Elevators 1 & 2 Machine		0			
CTS PCC 355 FHC and Area of Refuge		5,000			
CTS PCC 364 Changes to Data Outlets		1,000			

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	UMS	CTS	YBM	STS	COST REPORT NOTES
CTS PCC 375 Removable Slab Embeds		0			
CTS PCC 378 Crosscut Tunnel Final L		50,000			
CTS PCC 379 Increase Door Size		5,000			
CTS PCC 380 Add Fire Hose Valves		11,200			
CTS PCC 404 Escalator & Stair Glass		1,000			
CTS PCC 409 Additional ATCS Conduit		4,000			
CTS PCC 412 Add a conduit for the N		1,000			
CTS PCC 415 Additional Reinforcemen		1,500			
CTS PCC 98 Slurry Wall Wr Proofing		0			
CTS PCC#25 Stairs 5, 6, 7 Mods		27,962			
CTS PCC258 Start PCN before Com PCS		0			
CTS-COR#201 Swr Line & Station Roof		37,661			
CTS-Delete Tree Planting		(3,967)			
CTS-FACO#39- Soil Testing cmplly OAB		5,478			
CTS-PCC40 Plaza Surface Slab Pntrtn		4,878			
GEN PCC 183 Electric Power Elevator			31,574		
GEN PCC 189 Anti-Graffiti Film			83,371		
GEN PCC 33 End Platform Gate Revisi			57,403		
STS - Additn manholes for 78" sewer				25,838	
STS - Deletion of ARS (Revision 1)				(5,335,785)	
STS - PCC#28 Portal Dowels				(1,753)	
STS COR #88 Modify CBs and Culverts				4,395	
STS COR #92 PG&E Vault Conf 12 AWSS				57,560	
STS COR 067 FACO #41 GW Lead Filter				22,695	
STS COR 090 Subsurface Obstrc				20,452	
STS COR 093 12" Water Conf 12" Tee				0	
STS COR 094 Unknown DB Conf 12" Wtr				0	
STS COR 1009 AWSS Lat Conflict				186,360	
STS COR 1012 ATCS Work at SFMTA				10,001	
STS COR 1022 2" Traffic Sgnl Condui				1,428	
STS COR 1034 E live DT/MRY Ductbank				39,041	
STS COR 1045 78" Conc Cap Repair				19,381	
STS COR 1072 Raised Prtl Walkway				6,275	
STS COR 1075 U Wall Section Slab				4,001	
STS COR 1091 Conc Footing & PVC				1,551	

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	UMS	CTS	YBM	STS	COST REPORT NOTES
STS COR 1097 21" Sewer Bulkhead				13,338	
STS COR 1099 Cnflct with 24" casing				34,788	
STS COR 1100 E Swr Cnflct w/ 18" HDP				48,176	
STS COR 1104 Unkwn Aluminum Pipe				1,974	
STS COR 1116 Live Ductbank in Cnflct				15,000	
STS COR 1119 48" Corrg Pipe Cnflc				16,535	
STS COR 1140 Pothole 12" AWSS				4,022	
STS COR 1147 Utility Conflict				21,783	
STS COR 1150 Unkwn 18" Stl Line				2,316	
STS COR 1165 DSC - Unknown void				4,008	
STS COR 1172 DSC Utilities Conflict				17,398	
STS COR 1180 Unkwn Conc 10" FM				5,000	
STS COR 1187 Add Pothole WD POC				16,513	
STS COR 1189 Unkwn 12" AWSS Offset				5,323	
STS COR 1201 Unkwn Stl Conduits				25,000	
STS COR 1202 Acceleration 36" FM				172,036	
STS COR 1203 GSUC Delay Backfill				11,068	
STS COR 1212 Reloc Exst 10" SSFM				10,771	
STS COR 1216 Traffic Signal Support				2,241	
STS COR 1217 Slurry Backfill Fiber				3,490	
STS COR 1219 MRY DB Confl w 36 FM				50,001	
STS COR 1233 Ext RR Ties Confl Sewe				879	
STS COR 1241 Unkn PVC Conduits				6,945	
STS COR 1251 Shal Utl Trak Slab				130,000	
STS COR 1261 Acceleration 36 FM				6,657	
STS COR 1262 12" WD Blowout				20,078	
STS COR 1266 Shallow Fiber DB				6,531	
STS COR 1268 FH Install NE 4th Brya				7,001	
STS COR 1271 Addl Parking Strip				0	
STS COR 1275 Golden State Repair				1,863	
STS COR 1276 Install Culvert 4th Br				(7,054)	
STS COR 1278 MRY Vault Cables				50,000	
STS COR 1284 Conc Obstrt AWSS				6,040	
STS COR 1285 Unkn Utl Confl TD				10,000	
STS COR 1288 Unkn DB Confl TD				10,000	

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	UMS	CTS	YBM	STS	COST REPORT NOTES
STS COR 1290 Traf Sig Box Confl				20,000	
STS COR 1295 CI Offset Conflict				166,028	
STS COR 1296 PVC DB Sewer Conflict				6,339	
STS COR 1307 Dmg MH Confl Sewer				131,481	
STS COR 1309 Brick Sewer Conflict				41,063	
STS COR 1310 Concrete Kicker				2,474	
STS COR 1314 Unkn Stl CI Conflict				14,258	
STS COR 1319 48" Sewer MH Confl				6,336	
STS COR 1323 Utl Confl AWSS FH				28,563	
STS COR 1325 Tide Flex Confl DB				5,935	
STS COR 1328 Invest Swr Lateral				3,257	
STS COR 1331 Brick Wall Conflict				2,908	
STS COR 1333 Changes to Marquee				3,496	
STS COR 1351 Tele Volt Power				0	
STS COR 1364 Culvert Confl Trk Drn				2,001	
STS COR 1372 Conc Blkt AWSS Confl				2,864	
STS COR 1381 Poly Drain Confl MRY				1,116	
STS COR 1393 AWSS Restraint Req				10,001	
STS COR 1402 Subsurface Slab				1,463	
STS COR 1434 Unknown Duct Bank Conf				796	
STS COR 1445 Unkwn Fiber Pave Reno				5,000	
STS COR 1446 PGE Vault Pave Reno				0	
STS COR 1492 Pave Reno 4th Brannan				50,001	
STS COR 1497 Pave Reno SW 4th Brann				50,001	
STS COR 1510 Sta Canopy Column Slee				5,001	
STS COR 1541 VCP Steel Casing				4,181	
STS COR 1544 ATCS Sgnl Recmmndtion				5,001	
STS COR 1562 Curved Unistruts				0	
STS COR 1593 ATT MH 4th Brannan				8,001	
STS COR 1615 Public Safety Comms				10,001	
STS COR 1622 Exist Pull Box Ramp				0	
STS COR 1634 Sewer Confl Light Pole				2,000	
STS COR 164 DSC 8" AWSS Lat Conf 78				4,077	
STS COR 211 SW conf AWSS 4th/Freelo				4,561	
STS COR 220 DSC Relocate MRY DB&Vlt				0	

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	UMS	CTS	YBM	STS	COST REPORT NOTES
STS COR 297 TC for Track Work at 4t				150,000	
STS COR 367 DSC Conf w/ CP and FM				40,783	
STS COR 371 Conflicts w/ 12" AWSS				25,364	
STS COR 392 Util at 4th-Town SW MH				13,246	
STS COR 401 AWSS Layout 4th/King				214,125	
STS COR 402 3x5 SW Confl w/ VCP SW				10,000	
STS COR 404 Contam Soil in MRY DB				9,361	
STS COR 406 Addtl TC at 4th/King				675,000	
STS COR 416 Conc DB/wall/lines conf				81,783	
STS COR 447 Added Exc for SFWD POC				29,423	
STS COR 454 Addtl Conflicts w/ 8" W				203,549	
STS COR 455 Conflicts w/ 8" WD Line				10,000	
STS COR 456 Conflicts w/ MRY Poles				20,000	
STS COR 475 Removal of Fiber Optic				1,101	
STS COR 484 New WD & AWSS Alignment				52,174	
STS COR 488 Tunnel Track Alignment				50,001	
STS COR 500 Tunnel Monuments				0	
STS COR 530 Tunnel Track Machine				20,064	
STS COR 533 Conc wall confl util				38,279	
STS COR 536 Util in Conf w 36" FM				66,287	
STS COR 559 Temporary Trolley Pole				5,890	
STS COR 567 Loct of Plinth Breaks				10,000	
STS COR 584 Debris confl w/ culvert				6,275	
STS COR 604 Duct Bank in conf. w se				(13,879)	
STS COR 609 Damaged MRY Conduit				21,792	
STS COR 615 Sump Pump Pit Cover				1,064	
STS COR 621 Additional WD Exc. per				26,293	
STS COR 623 Unkn Con Structure				2,966	
STS COR 632 Omit Unist & add Anr Bt				2,180	
STS COR 633 Ligting Arrrests Signal				10,001	
STS COR 641 SW delay due to conf ut				10,025	
STS COR 644 Wod Pier Cnf w AWSS				13,258	
STS COR 657 Tunnel Ca Bsn Dsn Chang				20,000	
STS COR 664 Threaded Dowels				0	
STS COR 666 Ukn Con Vlt/con N36"				10,118	

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	UMS	CTS	YBM	STS	COST REPORT NOTES
STS COR 675 Dct bnk & 3" Gas Line				17,569	
STS COR 682 Shtdown #1 Rail Mods				9,432	
STS COR 683 Gas Ln & unkn Duct Bank				20,391	
STS COR 694 Tr Rts in con/car pipes				821	
STS COR 699 Dct Bnk in Cnf w 36"FM				23,817	
STS COR 703 Tunl Wakway Expn jnts				386	
STS COR 737 Dct Bnk infc w AT&T Rem				2,455	
STS COR 751 Db in coflct 36" FM& MH				75,150	
STS COR 755 AWSS Material Delay				15,204	
STS COR 787 AT&T Dct Bnk InCw N 36"				35,000	
STS COR 788 Add Basin & Culvert				28,843	
STS COR 796 Woodn Piers Woodn Beams				10,001	
STS COR 807 Tunnel Invret Slab				13,198	
STS COR 813 Permissive Signal				10,001	
STS COR 826 DSC Swr Cap & I Beams				15,000	
STS COR 834 Thales ATCS TC Pwr Dist				10,001	
STS COR 849 brk Pen incof w 78" Swr				485	
STS COR 852 Caltrans Encrocmt Prmt				10,001	
STS COR 865 Sd Swr Mnhl & 15" Sw Ln				15,001	
STS COR 873 Ex L3 DcBnk Incn Pv Ren				12,096	
STS COR 874 Inter Loc of MRY Dc Bk				20,000	
STS COR 880 Ex Dbk in con Ins SC				10,001	
STS COR 890 Ex Pll Box Incon Crb Rm				3,001	
STS COR 909 PGE Gas Interruption				0	
STS COR 911 E SW Conf w 48" SW				9,632	
STS COR 920 Hoist Beam Supports CP5				(1,932)	
STS COR 926 E TS conduits conf w pa				10,000	
STS COR 927 E Gas Conf w N CB				3,000	
STS COR 929 MH Conf w SW Grout				14,297	
STS COR 930 SW Cracks Conf w Grout				5,000	
STS COR 931 Tree Removal				2,998	
STS COR 934 Revised Curb/Gutter				10,001	
STS COR 942 Radio Sys Prelim Design				50,001	
STS COR 954 Culvert Field Investiga				20,106	
STS COR 958 Trk Pave Change				(193,611)	

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	UMS	CTS	YBM	STS	COST REPORT NOTES
STS COR 962 4th/King Incomp Hrdwr				34,448	
STS COR 965 DB conf w 8" WL and Sle				102,893	
STS COR 970 16" WL Installation				76,221	
STS COR 974 Fldng frm Brkn Wtr Mn				18,610	
STS COR 976 Ex Unkwn Mnhl & Dct Bnk				25,761	
STS COR 979 (N) CB in Conf w/ (E) V				3,639	
STS COR 98 DSC MRY MH Conf w N CB				189,966	
STS COR 980 Repair Install GV				15,456	
STS COR 983 Ex Rbr in Cnflct w/ WL				1,845	
STS COR 995 E 3" Asbs Conduit Cnflct				996	
STS COR 996 Prtl WI Cnflct w/ slab				4,106	
STS COR 997 E Unkn Cncrt Encsmnt				11,292	
STS COR 999 E 18" Steel Pipe Confli				0	
STS Existing Fuel & Transite in MRY				14,101	
STS PCC 052 Deletion of ARS Pt II				(177,246)	
STS PCC 063 Del ATT/TSIC/PGE on 4th				(36,495)	
STS PCC 084 Removal of ATT DB & Vau				168,173	
STS PCC 096 4th Street SW Slip Lini				828,956	
STS PCC 114 Stdpipe & Cond @ Portal				3,895	
STS PCC 117 PDS Signs Moun Brack				50,034	
STS PCC 121 PG&E Pnts Streetlightig				282,638	
STS PCC 151 LED Street Lamps				114,108	
STS PCC 161 Delete Plat ESPBs				0	
STS PCC 206 Replace CCTV equipment				(315,264)	
STS PCC 223 4th and King Advnc Wrk				32,732	
STS PCC 224 Addn'l Water Line Work				213,215	
STS PCC 227 Paving for Temp Roadway				78,020	
STS PCC 232 WL at Welsh St				36,090	
STS PCC 239 Excavate Duct Bank				56,480	
STS PCC 249 Add Water Line on 4th				53,752	
STS PCC 265 Phone Encl Blue Light				63,385	
STS PCC 278 Rev Track Crub Drain				293,927	
STS PCC 279 WD Work S 4th Brannan				84,040	
STS PCC 283 Power Dist Single Line				24,169	
STS PCC 311 Vetag Infrastructure				52,629	

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	UMS	CTS	YBM	STS	COST REPORT NOTES
STS PCC 313 Pave Reno Weekend				217,211	
STS PCC 319 4th Bran Pave Reno Wknd				96,041	
STS PCC 331 Marquee Pole Changes				9,008	
STS PCC 359 4th Bryant Pave Wkend				0	
STS PCC 376 FiberPanels and Cabling				0	
STS PCC 377 Traffic Signal Change				0	
STS PCC 410 ATCS External Cable				0	
STS Track Switch Machine Change				88,715	
STS-FACO #48 Work Related St Lght				2,051	
UMS - DCW and Hose Bibbs	29,159				
UMS - N. Entrance Art Glass and Gla	0				
UMS COR 102 Cap on 12 inch Water	8,001				
UMS COR 1074 Traffic Signal Footing	10,001				
UMS COR 1102 New 8" WD Tie-In	93,921				
UMS COR 1190 Mtr Pedestal at Mrkt S	10,001				
UMS COR 1229 Granite Base Footing	2,001				
UMS COR 1299 E PG&E Vault at Ellis	50,000				
UMS COR 1301 Wtrprfing under 71 Ell	50,000				
UMS COR 1335 Scaffold @ NW Cmr Ell	5,000				
UMS COR 1344 Missing Fixture Info	11,246				
UMS COR 1353 Unknwn WD at Ellis	20,000				
UMS COR 1356 Unknwn Void at Ellis	25,000				
UMS COR 1366 Broken WD at Macy's	3,001				
UMS COR 1416 Brkrm Domestic Water	5,001				
UMS COR 1530 E Brick at Stairs 3&4	5,001				
UMS COR 1597 Overcurrent Studies	10,001				
UMS COR 1604 Added Electrical Scope	10,001				
UMS COR 1629 Wind Load Requirements	45,001				
UMS COR 1638 OH Structrl BM Cnflct	10,001				
UMS COR 1645 Unkwn Steel Beam	5,001				
UMS COR 1649 CN04 & CN07 Elevation	2,001				
UMS COR 1658 Powell DC Gear Punchli	50,001				
UMS COR 1660 Terrazzo Sbmttl Rspnse	2,001				
UMS COR 307 Elevator Site Hazmat	16,028				
UMS COR 391 AT&T Duct Bank Conflict	25,001				

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	UMS	CTS	YBM	STS	COST REPORT NOTES
UMS COR 403 Waterproofing at BART	63,285				
UMS COR 493 Steel Shape Inside Pile	25,001				
UMS COR 636 Bi-Fold Door Tube Steel	24,911				
UMS COR 660 Added Grommets at Booth	3,253				
UMS COR 726 Beam 213 Stair Opening	48,524				
UMS COR 747 NDSC Unidnfied Con Pile	50,000				
UMS COR 817 Odor at N. Concourse	100,000				
UMS COR 831 E BM @ GL10 Connection	4,670				
UMS FACO #31 NDSC Incomplete PGE DB	165,944				
UMS FACO #38 Pile H3 & H4 Piles	7,210				
UMS PCC 007 Geoprobe Credit	(15,600)				
UMS PCC 027 Escalator Barricade	(9,227)				
UMS PCC 029 Concrete Wale Support	41,424				
UMS PCC 038 BART Instrmnt Transfer	45,280				
UMS PCC 078 Jet Grout at O'Farrell	11,611				
UMS PCC 118 Elevator Overhead Hoist	117,170				
UMS PCC 122 Drain Piping Grout Dtls	499,825				
UMS PCC 136 Transformer in Vault 31	25,000				
UMS PCC 150 S. HW Wale Connection	37,174				
UMS PCC 153 Geary Catch Basin	(5,370)				
UMS PCC 166 Revise Feeder Schedules	97,976				
UMS PCC 184 Glass Floor Support	8,089				
UMS PCC 198 Ellis Deck Vertical Joi	22,196				
UMS PCC 201 Door Alarm Points	0				
UMS PCC 212 Trouble Lights	(201)				
UMS PCC 213 Electrical at Ellis St.	(5,994)				
UMS PCC 219 Add Data & Power Outlet	433,880				
UMS PCC 222 Delete PG&E Conduits	(12,359)				
UMS PCC 228 Added 2"x4" conduits	16,695				
UMS PCC 230 Ellis AT&T Work	20,131				
UMS PCC 246 Fiber Optic Cable	8,613				
UMS PCC 247 Changes to RP-304	49,279				
UMS PCC 257 Mezz Lvl Hdwl Wale Rein	79,599				
UMS PCC 263 LED artwork	116,391				
UMS PCC 273 N Entrnce Cncrs Lvl HVAC	49,692				

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	UMS	CTS	YBM	STS	COST REPORT NOTES
UMS PCC 291 Concourse Level Floor D	24,679				
UMS PCC 293 Gas Line on O'Farrell	124,291				
UMS PCC 298 Sewer at Stair 3 and 4	34,120				
UMS PCC 309 Bus Shelter on Geary	0				
UMS PCC 314 Potholing S.Laterals	10,458				
UMS PCC 327 Traffic Signals Ellis	2,726				
UMS PCC 330 Water Services	82,121				
UMS PCC 339 - Escalator 7 & 8 Steel	19,275				
UMS PCC 71 Rerouting of Slab Drains	1,236				
UMS PCC 86 Headwall Pile Conflict	8,982				
UMS PCC 93 Ellis Deck Seismic Joint	82,603				
UMS-FACO #30 NDSC Inadequate CDF	361,570				
UMS-Relocation of Traffic Signal Co	32,275				
USG COR 1109 Plaza Conc Strs Rein	903				
USG COR 1117 Steel Bracing	2,501				
USG COR 259 Lead Paint on Columns	87,455				
USG COR 261 8" Wall at Grid 11B	5,632				
USG COR 274 Shear Wall at Grid 10	68,945				
USG COR 293 Water on N.Concourse	1,832				
USG COR 777 Rvisd Bm Angle to Ceili	30,001				
USG COR 805 Fotngs 16A As Built Dim	10,001				
USG COR 808 Dimensions at Escalator	1,501				
USG COR 882 HSS Beam Termination	5,001				
USG COR 975 N Cncrs Invert Slab Slp	(1,806)				
USG COR 998 Class 1 Hazardous Soil	200,000				
USG Hydrant Relocation on Geary St.	58,143				
USG PCC 072 Rmval of Exstg Column	4,116				
USG PCC 080 Door Opngs in Shr Walls	41,774				
USG PCC 101 Elev. Machine Rooms	7,122				
USG PCC 106 Edge of Slab for 8" Con	39,891				
USG PCC 108 Con Wok chges du to DSC	288,022				
USG PCC 112 Glass Walk Roof System	15,744				
USG PCC 113 Elv/Esc Pit Floor Slope	20,879				
USG PCC 116 Demo Column, Const Beam	42,188				
USG PCC 128 Dowls of Rbar Conn Dtls	170,189				

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	UMS	CTS	YBM	STS	COST REPORT NOTES
USG PCC 129 Escalator Work Point	360,528				
USG PCC 133 Sheet Metal HVAC Duct	47,102				
USG PCC 134 Temp South Wall Support	90,268				
USG PCC 142 Storage Light & Elect.	26,085				
USG PCC 143 Wall Conn, at GL 10/B	5,906				
USG PCC 157 Plaza Level Vent Shaft	10,873				
USG PCC 164 GL 14 Waterproofing	68,074				
USG PCC 165 Arch. Precast Support	90,705				
USG PCC 174 Fan Trench Strut Clar.	8,810				
USG PCC 31 HVAC Trench Mod.	235,133				
YBM COR #1606 Add curb @ Elev 3 & 4			5,000		
YBM COR 1062 Elevator #3 Pit Change			52,312		
YBM COR 1146 Traction Power Neg.			48,935		
YBM COR 1151 Steel Pipe & Steel Bea			141,064		
YBM COR 1155 Live AT&T Cable			20,000		
YBM COR 1195 Stair #4 Sheet Pile In			51,883		
YBM COR 1281 T-7 Protection			4,428		
YBM COR 1294 Concrete Overpour P-1			98,767		
YBM COR 1322 Replace Missing SL Con			15,000		
YBM COR 1334 Added Reinforcing Type			5,001		
YBM COR 1337 Added Shear Key at Hea			7,501		
YBM COR 1348 Added Rebar Dowels int			6,501		
YBM COR 1349 DSC Confl w/(E) TS Box			4,001		
YBM COR 1373 (E) Unknown Red Concre			220,416		
YBM COR 1384 Added Concrete Cap Bea			10,000		
YBM COR 1388 Roof Hatch Dimensions			10,001		
YBM COR 1392 SF-1/EF-1 Missing 120V			10,489		
YBM COR 1425 Escalators 1 & 2 Stop			1,010,599		
YBM COR 1438 Provide Reinforcing in			3,898		
YBM COR 1457 Fire Hydrant Removal			12,001		
YBM COR 1458 (E) 24" Conflict w/SSFM			60,000		
YBM COR 1473 Stair #1 Added Drain			3,501		
YBM COR 1511 Change Coffe Beam			21,274		
YBM COR 1529 Changes to PG&E Power			15,000		
YBM COR 1540 Comm Conduit Conflict			15,001		

Contract Modification/Trend Log - Contract 1300 Stations

**Awarded NTE Amount
Substantial Completion**

**\$839,676,400
6/30/2018**

**\$854,894,464
6/30/2018**

	UMS	CTS	YBM	STS	COST REPORT NOTES
YBM COR 1578 Added Drain Rock around			101		
YBM COR 1584 Fastening Detail for G			0		
YBM COR 1595 Details for Crystalliz			0		
YBM COR 1596 Surface slab pour #1 C			3,000		
YBM COR 1603 Door Openings at stati			0		
YBM COR 1610 CDF backfill at surfac			30,000		
YBM COR 1628 Louvers & Vents SD			0		
YBM COR 1633 Clementina Subgrade Co			0		
YBM COR 1641 (GEN) Alt CT-03 Tile D			0		
YBM COR 1644 Changed concrete wall			0		
YBM COR 1647 Add Demo & Constr of C			8,000		
YBM COR 1651 Add Beam @ Shaft Roof			0		
YBM COR 1656 Chgs Sub. 09 53 23.006			0		
YBM COR 390 Chip Mezzanine Headwall			60,005		
YBM COR 564 Concrete Encased PG&E			140,346		
YBM COR 691 Add Gurdril to Str 5&6			45,001		
YBM COR 825 Tunnel Seg Steel Fibers			31,401		
YBM COR 939 Broken Water Dept. Line			54,572		
YBM COR 949 Escalator Raceways			(1)		
YBM COR 955 Form saver-Coupler Mods			305,906		
YBM COR 960 N & S Head WI Rbr Chng			64,027		
YBM PCC 056 OCS Pole Foundations			27,688		
YBM PCC 061 Escalator Pit Provision			94,576		
YBM PCC 126 Changes to Kiosks			174,948		
YBM PCC 132R Raise Pit Floor Elev 4			39,349		
YBM PCC 159 PG&E Ductbank Changes			(10,001)		
YBM PCC 162 Deluge Valve Door			7,889		
YBM PCC 168 Swing Gates Attachment			178,172		
YBM PCC 171R1 Additional PTZ CCTV			14,093		
YBM PCC 187R3 Escala. 1-4 HVAC chang			30,000		
YBM PCC 202 YBM Mezz Light Fixtures			74,858		
YBM PCC 21 Delete Instrument & Monit			(50,195)		
YBM PCC 235R1 Granite Art Panel			0		
YBM PCC 260 Folsom St. AWSS Valve			12,556		
YBM PCC 287 Elevator Cab Handrail			50,000		

Contract Modification/Trend Log - Contract 1300 Stations

Awarded NTE Amount
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\$839,676,400
6/30/2018

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6/30/2018

	UMS	CTS	YBM	STS	COST REPORT NOTES
YBM PCC 290 Elimination of Intercar			(25,955)		
YBM PCC 310 Revisions to Platform E			17,653		
YBM PCC 312R3 Galvanized Bent Plate			305,861		
YBM PCC 320 Upsized Wires & Breaker			95,134		
YBM PCC 328 Platform lvl metal wall			247,682		
YBM PCC 341 Add Trench Drain MER			0		
YBM PCC 361 Station Agent Booth Det			175,447		
YBM PCC 362 Rev. to Platform Displa			5,000		
YBM PCC 366 Survey of Elevations of			7,000		
YBM PCC 368 Rev. to 36" FM connect.			229,122		
YBM PCC 369 Rev. to Wind Load Resi			50,000		
YBM PCC 37 SFAC Node Sculpture			79,000		
YBM PCC 393 Doors-Beam Conflict			15,000		
YBM PCC 79 Install 12in WM to Howar			254,106		
YBM PCC 82 Delete Scope Due to Hote			(39,025)		
YBM PCC 85R Ticketing Hall Changes			57,586		
YBM PCC 91 Concourse Deck Conflict			53,133		
YBM PCC 97 Change Concourse Opening			48,774		
YBM PCC 99 Art Glass Changes			6,880		
CTS PCC 119 Pltform Lvl Wall Art In		(10,001)			
CTS PCC 135 Esc 5/6 Addl Supp Locs		10,001			
CTS PCC 167 Drn for GlS Canopy on E		5,001			
CTS PCC 177 Add Esc 5 & 6 Supp		1,001			
CTS PCC 178 Add Beams and Embeds		1			
CTS PCC 179 Added Gromets		1			
CTS PCC 180 Extra WD Work for 12" L		1			
CTS PCC 204 Shrnk Ft Prnt Emer Shft		1,001			
CTS PCC 231 Continuous Metal Ring		8,000			
CTS PCC 365 Headhouse Beam 240 Rein		(1,000)			
CTS PCC 381 Relocation of TP Boxes		0			
CTS PCC 383 Heating Refrigerating		150,000			
CTS PCC 388 Stair #5 Clear Width Di		2,000			
CTS PCC 389 Dust Monitor for Gordon		17,000			
CTS PCC 416 Fiber Optic Conduit		3,000			
STS PCC 095 Frame/Grate Change				(50,001)	

Contract Modification/Trend Log - Contract 1300 Stations

Awarded NTE Amount	\$839,676,400	\$854,894,464
Substantial Completion	6/30/2018	6/30/2018

	UMS	CTS	YBM	STS	COST REPORT NOTES
STS PCC 160 ATCS Change Reverse Run				75,000	
STS PCC 226 Axle Counter Boxes				41,602	
STS PCC 236 Pavement Survey				6,169	
STS PCC 240 Conduit SFDT Reroute				4,000	
STS PCC 248 Restab Trac Pwr Ductban				20,000	
STS PCC 250 Add 2" TP Riser Conduits				20,000	
STS PCC 255 Sidewalk Restoration				33,000	
STS PCC 264 Track Drainage Mod				(15,000)	
STS PCC 266 Track Curb Type Mod				(1,001)	
STS PCC 276 Traffic Signal Changes				3,000	
STS PCC 280 Pavement Reno Delete				(240,764)	
STS PCC 288 Delete Video Display				(2,500)	
STS PCC 306 Adv Track Slab Excav				5,000	
STS PCC 308 Crossover Rail Bonding				6,000	
STS PCC 403 Curb Ramp Sidewalk Chng				10,000	
STS PCC 424 ATCS Conduits at CTS				5,000	
UMS Locate PG&E Conduits	20,001				
UMS MRY Duct Bank-West	54,981				
UMS PCC 181 Plaza ADA Enhancements	16,383				
UMS PCC 193 Lightbox & Glazed Door	7,536				
UMS PCC 196 Exhust Fan & Escltr Cnt	20,000				
UMS PCC 215 Rmv Conduits in Casing	15,000				
UMS PCC 216 Waterproofing Drainage	50,000				
UMS PCC 217 POC for Drain Line	(20,825)				
UMS PCC 221 Slab Interaction	19,299				
UMS PCC 225 Add Two S.S. Enclosures	5,866				
UMS PCC 245 Ventilation Fan Buttons	(201)				
UMS PCC 253 Pltfrm Lvl Hdwl Wale Re	32,810				
UMS PCC 256 Rev.1 - Platform Art	(29,000)				
UMS PCC 261 Internal Drainage System	20,000				
UMS PCC 271 Metal Deck Support	30,000				
UMS PCC 272 PG&E Vaults on Ellis St	2,000				
UMS PCC 277 N Entrnc WD Tie-ins	20,000				
UMS PCC 281 Power Infrastructure	49,780				
UMS PCC 292 USG Mechanical Room	30,000				

Contract Modification/Trend Log - Contract 1300 Stations

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Substantial Completion	6/30/2018	6/30/2018

	UMS	CTS	YBM	STS	COST REPORT NOTES
UMS PCC 295 Geary Bulb Out	25,000				
UMS PCC 296 Water Meter on Ellis St.	(2,001)				
UMS PCC 299 South Concourse Opening	10,000				
UMS PCC 303 USG Ramp Island	10,001				
UMS PCC 318 Station Door Hardware	50,000				
UMS PCC 325 Escalator Slab Rebar	10,000				
UMS PCC 329 AWSS and Slurry Wall OFA	5,000				
UMS PCC 335 Light Fixtures	10,000				
UMS PCC 336 8-inch Water Main on OFA	10,000				
UMS PCC 342 3" Sleeve at Comm. Room	750				
UMS PCC 344 PG&E Streetlights	5,001				
UMS PCC 354 Signals at Geary	5,000				
UMS PCC 358 Obst. Impacting 12 AWSS	50,000				
UMS PCC 363 Ellis Entrance Finishes	100,001				
UMS PCC 367 Access at Platform Inv.	35,043				
UMS PCC 371 CN04 Electrical Room	20,000				
UMS PCC 374 Deck at Platform Strut	40,001				
UMS PCC 385 WD Kill Holes at OFA	22,455				
UMS PCC 386 Sewer Conflict at OFA	10,000				
UMS PCC 391 Gutter Connections	5,000				
UMS PCC 392 Water Services on Stktn	5,000				
UMS PCC 395 HVAC Electrical	10,001				
UMS PCC 398 Escalator Equip. Rooms	10,001				
UMS PCC 413 Traffic Signals	10,000				
UMS PCC 418 Paving Mobe and Demobe	2,884				
UMS PCC 419 Power and Data Outlets	10,000				
UMS PCC 423 WD Gate Valve	10,001				
UMS PCC 94 Clean out MRY Ductbank	18,060				
USG PCC 109 Rein Dtlis for Struc Con	10,001				
USG PCC 111 Conc Cemo & Rev Con Dtl	10,001				
USG PCC 123 South Wall Ground Beams	4,001				
USG PCC 125 Foot,SOG & Str St Chang	50,001				
USG PCC 147 Geary Streetlight	25,000				
USG PCC 154 Trench Drain at 17 Line	1,001				
USG PCC 155 16-D Footing Demolition	18,001				

Contract Modification/Trend Log - Contract 1300 Stations

Awarded NTE Amount	\$839,676,400	\$854,894,464
Substantial Completion	6/30/2018	6/30/2018

	UMS	CTS	YBM	STS	COST REPORT NOTES
USG PCC 156 CMU Footings	7,005				
USG PCC 158 Elev. Pit CDF Backfill	1,500				
USG PCC 175 Drainage Under Slab	0				
USG PCC 185 Wall at Line 10 and Hyd	15,000				
USG PCC 199R-1 Delete Bm and Plntrs	(36,102)				
USG PCC 89 E. Light Pole Foundation	2,501				
YBM PCC 140 Stair 3, Escalators 1&2			(19,505)		
YBM PCC 145 Stair 7/Escalators 3, 4			20,001		
YBM PCC 148 Elev.3, 4 Hoist Beam MP			40,001		
YBM PCC 152R1 Elevator 1 & 2 changes			100,001		
YBM PCC 210 Elev 1&2 MRL to Hydraul			(5,001)		
YBM PCC 268 Rstroom Fclty Wall Revs		0			
YBM PCC 301 Remove directional door			(20,001)		
YBM PCC 302 Delete 4th Wall of CMU			15,000		
YBM PCC 305R1 Signage Revisions			1,000		
YBM PCC 322 Fill Gap at Panel P-60			10,000		
YBM PCC 402 Replace Surface Slab Te			8,000		
YBM PCC 425 Aquafin Vapor Control Sy			0		
YBM PCC 427 Changed Concrete Wall			35,000		
YBM PCC 430 Replace 9N) Topping Sla			100,000		
YBM PCC 59 Pavers Basis of Design			7,516		
Approved	2,748,442	8,983,302	747,056	2,739,264	
Contract Modification					
CMod # 14 YBM COR 036, 078			58,526		
CMod #017 CTS CORs 001 053 & 069		54,322			
CMod #018 CTS PCC 012		60,248			
CMod #021 STS CORs 48/52/114/233/252				18,221	
CMod #025 - Various CORs			59,113		
CMod #026 YBM COR 072			84,509		
CMod #027 UMS PCC 092	0				
CMod #028 CTS PCC 017.1		97,743			
CMod #029 STS PCC 009.1				(143,668)	
CMod #033 CTS Various CORs		56,422			
CMod #034 CTS Various CORs		19,334			
CMod #035 STS PCC 077				11,147	

Contract Modification/Trend Log - Contract 1300 Stations

Awarded NTE Amount	\$839,676,400	\$854,894,464
Substantial Completion	6/30/2018	6/30/2018

	UMS	CTS	YBM	STS	COST REPORT NOTES
CMod #037 CTS Various CORs		8,886			
CMod #038 STS Various CORs				52,553	
CMod #039 UMS Various CORs	23,271				
CMod #040 YBM Analytical Soil Test			3,655		
CMod #049 STS DSC CORs				136,728	
CMod #050 STS DSC CORs				67,036	
CMod #053 STS DSC CORs				17,035	
CMod #081 Various DSC CORs & PCCs			57,886		
CMod #082 YBM COR 385			21,170		
CMod #083 YBM Various Changes			27,270		
CMod #084 YBM Various Changes			12,156		
CMod #085 YBM COR 086 Existing AT&T			156,831		
CMod #086 YBM COR 1106			1,897		
CMod #1 BART Elevator Option 1 @ Pow	90,000				
CMod #10 YBM PCC 042			64,287		
CMod #11 UMS PCC 002	12,997				
CMod #12 STS Traffic Control				1,032,302	
CMod #13 CTS COR 006		57,707			
CMod #15 YBM COR 196			3,178		
CMod #16 UMS COR 184	8,261				
CMod #19 CTS COR 007, 026		2,274,225			
CMod #20 YBM PCC 047 and 45			364,562		
CMod #22 UMS PCC 045, 046	16,198				
CMod #23 UMS PCC 058	63,838				
CMod #3 CTS Work Safely Ard Power Po		25,956			
CMod #30 YBM Various CORs			334,165		
CMod #31 UMS COR 595	53,701				
CMod #32 YBM Various PCCs			92,934		
CMod #36 YBM Conflict with Waterline			14,484		
CMod #4 CTS-Force Account Change Or		130,000			
CMod #41 YBM Class 2 Conta. Material			40,250		
CMod #42 UMS Addl. Service Conduits	36,873				
CMod #43 UMS D85 Structural Pile	65,188				
CMod #44 UMS Grade 50 Steel	572,884				
CMod #46 YBM/CTS/UMS S.walk Hatches			35,489		

Contract Modification/Trend Log - Contract 1300 Stations

Awarded NTE Amount	\$839,676,400	\$854,894,464
Substantial Completion	6/30/2018	6/30/2018

	UMS	CTS	YBM	STS	COST REPORT NOTES
CMod #47 UMS Roof Deck Schedule	76,124				
CMod #48 UMS Undgrnd. Storage Tanks	97,817				
CMod #5 YBM Deletion of Comp Groutin			(1,833,869)		
CMod #51 YBM Various CORs and PCCs			24,875		
CMod #52 YBM Undgrnd. Storage Tanks			167,393		
CMod #54 UMS USG Underpinning	732,157				
CMod #55 YBM Archeological Discovery			102,734		
CMod #56 YBM Contaminated Material			106,923		
CMod #57 STS Crossover Materials				21,245	
CMod #58 STS DSC CORs				90,081	
CMod #59 CTS DSC CORs		66,592			
CMod #6 CTS Plaza Constr Supt Servi		75,000			
CMod #60 UMS USG Two Fuel Tanks	61,312				
CMod #61 YBM Various CORs			207,181		
CMod #62 UMS Wales and Waterproofing	277,714				
CMod #63 CTS DSC CORs		38,025			
CMod #64 STS DSC CORs and SFWD				52,570	
CMod #65 UMS Various CORs and PCCs	10,320				
CMod #66 STS Sewer Notching				66,949	
CMod #67 UMS Solar/Low-e Coating	23,290				
CMod #68 STS Various CORs				59,555	
CMod #69 UMS Various CORs	49,682				
CMod #70 YBM Various CORs			178,079		
CMod #71 UMS Haz and Asbestos Abate	81,907				
CMod #72 YBM COR 249. 566			74,694		
CMod #74 UMS PCC 39 12" WtrIn Reloc	336,236				
CMod #75 UMS COR 060 New 8" Wtr Line	58,672				
CMod #76 YBM COR 806 Gardril credits			(9,611)		
CMod #77 STS Various Changes				56,629	
CMod #78 STS Various DSC CORs				191,175	
CMod #79 STS PCC 014 Traffic Signal				242,427	
CMod #80 STS Add'l Work to DSCs CORs				111,701	
CMod #87 CTS Var Slurry Wall Changes		3,596,000			
CMod #88 STS Various COR Misc Work				38,346	
CMod #89 YBM CORs 390,485 & 848			85,095		

Contract Modification/Trend Log - Contract 1300 Stations

Awarded NTE Amount	\$839,676,400	\$854,894,464
Substantial Completion	6/30/2018	6/30/2018

	UMS	CTS	YBM	STS	COST REPORT NOTES
CMod #9 YBM COR 10,15,16,18,20,25			126,663		
CMod #90 CTS DRB Reimbursement		1,296,364			
CMod #91 YBM PCC 069			84,537		
CMod #92 CTS PCC 233 & 26		1,126,478			
CMod 073 - PCC 066 PB				96,516	
CMOD 24 STS PCC 23				108,053	
CMod#7 STS FACOs 016, 017 &COR 009				80,170	
CMod#8 STS PCC 006 ATT MH, PB&Trench				225,208	
STS CMod 045 PCC 008 Tunnel Lowering				107,285	
Grand Total	11,277,726	12,697,029	6,705,380	4,829,986	

7.6 BUDGET REVISIONS: REPORT SORTED BY CONSTRUCTION PACKAGES & SOFT COSTS

Report Period: November 2018

		October 2018			November 2018				
Group by Contract & SCC	CATEGORY ITEM	October 2018 Base	October 2018 Allocated Contingency	October 2018 Base + Allocated Contingency (YOE)	November 2018 Base	November 2018 Allocated Contingency	November 2018 Base + Allocated Contingency (YOE)	BUDGET TRANSFERS [November 2018] vs. [October 2018]	Cost Report Notes
10-50	CONSTRUCTION CONTRACT PACKAGES	1,153,803,622	28,119,381	1,181,923,003	1,155,014,637	26,908,366	1,181,923,003	0	
1250	UTILITY RELOCATION PACKAGE #1	12,134,906		12,134,906	12,134,906		12,134,906	0	
	Contract 1250 Form B Credit	(2,275,419)		(2,275,419)	(2,275,419)		(2,275,419)	0	
1251	UTILITY RELOCATION PACKAGE #2	20,744,696		20,744,696	20,744,696		20,744,696	0	
	Contract 1251 Form B Credit	(7,618,412)		(7,618,412)	(7,618,412)		(7,618,412)	0	
1252	GUIDEWAY TUNNEL	234,947,069	966,430	235,913,500	234,947,069	966,430	235,913,500	0	32
	Contract 1252 Form B Credit	(254,050)		(254,050)	(254,050)		(254,050)	0	
1300	CN1300 STATIONS TOTAL	853,683,449	25,992,951	879,676,400	854,894,464	24,781,936	879,676,400	0	33
1253: UMS	UNION SQUARE/MARKET STREET STATION [UMS]	296,779,032	17,251,558	314,030,590	296,779,032	17,251,558	314,030,590	0	
	UMS 1253 Form B Credit	(528,370)		(528,370)	(528,370)		(528,370)	0	
1254: CTS	CHINA TOWN STATION [CTS]	255,424,634	2,143,176	257,567,810	256,551,112	1,016,698	257,567,810	0	
	CTS 1254 Form B Credit	(451,703)		(451,703)	(451,703)		(451,703)	0	
1255: YBM	YERBA BUENA/ MOSCONE STATION [YBM]	158,751,519	4,337,482	163,089,001	158,836,056	4,252,945	163,089,001	0	
	YBM 1255 Form B Credit	(100,000)		(100,000)	(100,000)		(100,000)	0	
1256: STS	SURFACE TRACKWORK & SYSTEMS [STS]	142,728,264	2,260,735	144,988,999	142,728,264	2,260,735	144,988,999	0	
	STS 1256 SFPUC SEWER MAIN CREDIT	(2,925,296)		(2,925,296)	(2,925,296)		(2,925,296)	0	
	STS 1256 Form B Credit	(1,000,000)		(1,000,000)	(1,000,000)		(1,000,000)	0	
OTHER	OTHER CONSTRUCTION TOTAL	47,446,751	1,160,000	48,606,751	47,446,751	1,160,000	48,606,751	0	
40.06	PUBLIC ART PROGRAM	8,175,555	1,160,000	9,335,555	8,175,555	1,160,000	9,335,555	0	
40.08	CN1300 JOB READINESS PROGRAM - OUTREACH	1,060,000		1,060,000	1,060,000		1,060,000	0	33
40.02	MISC. CONSTR CONTRCT WK (TRACTION POWER FOR 1251)	258,202		258,202	258,202		258,202	0	
40.01	CONTRACT 1300 SOIL PROCESS	500,000		500,000	500,000		500,000	0	34
50.01	TEMPORARY LICENSE AGREEMENT (ATCS CENTRAL CONTROL)	487,972		487,972	487,972		487,972	0	
50.06	MTA FARE COLLECTION EQUIPMENT	5,400,000		5,400,000	5,400,000		5,400,000	0	
50.06	BART FARE COLLECTION EQUIPMENT	700,000		700,000	700,000		700,000	0	

7.6 BUDGET REVISIONS: REPORT SORTED BY CONSTRUCTION PACKAGES & SOFT COSTS

Report Period: November 2018

		October 2018			November 2018				
Group by Contract & SCC	CATEGORY ITEM	October 2018 Base	October 2018 Allocated Contingency	October 2018 Base + Allocated Contingency (YOE)	November 2018 Base	November 2018 Allocated Contingency	November 2018 Base + Allocated Contingency (YOE)	BUDGET TRANSFERS [November 2018] vs. [October 2018]	Cost Report Notes
40.02	JOB ORDER CONTRACTS (JOCS) - CONSTRUCTION	117,255		117,255	117,255		117,255	0	
40.08	AON RISK INSURANCE	18,088,750		18,088,750	18,088,750		18,088,750	0	
40.02	PUBLIC AGENCIES UTILITY COORDINATION	3,713,215		3,713,215	3,713,215		3,713,215	0	
40.02	DEPARTMENT OF PARKING AND TRAFFIC (DPT)	1,200,000		1,200,000	1,200,000		1,200,000	0	
50.03	UNION SQUARE/ MARKET STREET STATION POWER FEED	2,959,826		2,959,826	2,959,826		2,959,826	0	
50.03	UNION SQUARE/ MARKET STREET STATIONS PERMANENT POWER	(2,350,000)		(2,350,000)	(2,350,000)		(2,350,000)	0	
50.03	CHINATOWN STATION POWER FEED	2,959,826		2,959,826	2,959,826		2,959,826	0	
50.03	CHINATOWN STATION PERMANENT POWER	(2,350,000)		(2,350,000)	(2,350,000)		(2,350,000)	0	
50.03	YERBA BUENA/ MOSCONE STATION [YBM] POWER FEED	3,125,222		3,125,222	3,125,222		3,125,222	0	
50.03	YERBA BUENA/ MOSCONE STATION [YBM] PERMANENT POWER	(2,368,540)		(2,368,540)	(2,368,540)		(2,368,540)	0	
50.03	SURFACE STATION POWER FEED	11,839		11,839	11,839		11,839	0	
50.04	COMMUNICATION CONNECTION COSTS	5,757,629		5,757,629	5,757,629		5,757,629	0	
60	ROW, LAND, EXISTING IMPROVEMENTS	32,246,321	0	32,246,321	32,246,321	0	32,246,321	0	
60.01	PURCHASE OR LEASE OF REAL ESTATE	30,065,810	0	30,065,810	30,065,810	0	30,065,810	0	35
60.02	RELOCATION OF EXISTING HOUSEHOLDS	2,180,511		2,180,511	2,180,511		2,180,511	0	
70	VEHICLES	13,309,000	13,076,653	26,385,653	13,309,000	13,076,653	26,385,653	0	
70.01	LIGHT RAIL	13,309,000	13,076,653	26,385,653	13,309,000	13,076,653	26,385,653	0	36
70.07	SPARE PARTS								
80	PROFESSIONAL SERVICES	312,781,539	18,221,079	331,002,618	312,781,539	18,221,079	331,002,618	0	
80.01	PRELIMINARY ENGINEERING	46,202,674		46,202,674	46,202,674		46,202,674	0	
80.02	FINAL DESIGN	61,318,331		61,318,331	61,318,331		61,318,331	0	
80.03	PROJECT MANAGEMENT FOR DESIGN & CONSTRUCTION	89,012,545	13,905,845	102,918,390	89,012,545	13,905,845	102,918,390	0	

7.6 BUDGET REVISIONS: REPORT SORTED BY CONSTRUCTION PACKAGES & SOFT COSTS

Report Period: November 2018

		October 2018			November 2018				
Group by Contract & SCC	CATEGORY ITEM	October 2018 Base	October 2018 Allocated Contingency	October 2018 Base + Allocated Contingency (YOE)	November 2018 Base	November 2018 Allocated Contingency	November 2018 Base + Allocated Contingency (YOE)	BUDGET TRANSFERS [November 2018] vs. [October 2018]	Cost Report Notes
80.04	CONSTRUCTION ADMINISTRATION & MANAGEMENT	93,360,379	2,956,812	96,317,191	93,360,379	2,956,812	96,317,191	0	
80.05	INSURANCES	6,800,000		6,800,000	6,800,000		6,800,000	0	
80.06	LEGAL: PERMITS. REVIEW FEES BY OTHER AGENCIES	8,212,604		8,212,604	8,212,604		8,212,604	0	
80.07	SURVEYS, TESTING, INVESTIGATION. INSPECTION	933,100		933,100	933,100		933,100	0	
80.08	START-UP	6,941,907	1,358,422	8,300,329	6,941,907	1,358,422	8,300,329	0	
	ALL SCC CATEGORIES 10 TO 80	1,512,140,482	59,417,113	1,571,557,595	1,513,351,497	58,206,098	1,571,557,595		37
90	UNALLOCATED CONTINGENCIES			6,742,408			6,742,408		38
	TOTAL PROJECT COST 10 TO 100			1,578,300,003			1,578,300,003		
	TOTAL CONTINGENCY			66,159,521			64,948,506		
	CONTINGENCY MINIMUM			25,000,000			25,000,000		
	BELOW OR ABOVE MINIMUM			41,159,521			39,948,506		

COST STATUS BY CATEGORY	SCC CODES	Supplemental 2013 Budget/Original Budget	BUDGET October 2018	BUDGET TRANSFERS	BUDGET November 2018	November 2018 CTD	Remaining Budget (Column H - Column I)	November 2018 EAC	November 2018 Contingency	Cost Report Notes
		A	B	C	D	E	F	G	H	
GUIDEWAY & TRACK ELEMENTS	SCC 010	282,227,872	285,227,879	-	285,227,879	269,981,603	15,246,276	284,696,904	966,430	39
STATIONS, STOPS, TERMINALS, INTERMODAL SITEWORK & SPECIAL CONDITIONS	SCC 020	573,099,645	580,852,508	(84,537.00)	580,767,971	433,236,814	147,531,157	561,777,997	24,781,936	39
SYSTEMS	SCC 040	235,514,097	220,258,363	84,537.00	220,342,900	213,578,980	6,763,920	231,393,200	1,160,000	39
ROW, LAND, EXISTING IMPROVEMENTS	SCC 050	90,774,397	95,584,254	-	95,584,254	37,495,018	58,089,236	97,458,591		39
VEHICLES	SCC 060	37,511,799	32,246,321	-	32,246,321	30,648,969	1,597,352	32,246,321		
PRELIM ENGINEERING	SCC 070	26,385,653	26,385,653	-	26,385,653	11,929,247	14,456,406	13,309,000	13,076,653	
FINAL DESIGN	SCC 080.01	46,202,673	46,202,674	-	46,202,674	46,202,675	(1)	46,202,674		
PM FOR DESIGN & CONSTRUCTION	SCC 080.02	61,137,604	61,318,331	-	61,318,331	61,199,308	119,023	61,318,331		
OTHER PROF SRVCS	SCC 080.03 - 080.04	197,146,664	199,235,580	-	199,235,580	160,414,767	38,820,813	182,372,923	16,862,657	39
UNALLOC CONTINGENCY	SCC 080.05 - 080.08	24,416,118	24,246,033	-	24,246,033	12,562,258	11,683,775	22,887,611	1,358,422	
	SCC 090	3,883,480	6,742,405	-	6,742,405		6,742,405		6,742,408	39
Grand Total		1,578,300,000	1,578,300,001	0	1,578,300,001	1,277,249,638	301,050,363	1,533,663,553	64,948,506	

SCC DESCRIPTION	November 2018 BUDGET	November 2018 CTD
010 - GUIDEWAY & TRACK ELEMENTS	285,227,879	269,981,603
020 - STATIONS, STOPS, TERMINALS, INTERMODAL	580,767,971	433,236,814
040 - SITEWORK & SPECIAL CONDITIONS	220,342,900	213,578,980
050 - SYSTEMS	95,584,254	37,495,018
060 - ROW, LAND, EXISTING IMPROVEMENTS	32,246,321	30,648,969
070 - VEHICLES (number)	26,385,653	11,929,247
080 - PROFESSIONAL SERVICES (applies to Cats. 10-50)	331,002,618	280,379,008
090 - UNALLOCATED CONTINGENCY	6,742,405	
Grand Total	1,578,300,001	1,277,249,638

SCC DESCRIPTION	November 2018 BUDGET	November 2018 CTD
010.02-Guideway: At grade semi-exclusive (allows cross-traffic)	2,860,000	2,156,500
010.06-Guideway: Underground cut & cover	69,816,407	64,243,880
010.07-Guideway: Underground tunnel	201,340,746	194,033,178
010.09-Track: Direct fixation	6,761,089	5,949,046
010.12-Track: Special (switches, turnouts)	4,449,637	3,599,000
020.01-At-grade station, stop, shelter, mall, terminal, platform	7,602,857	3,257,530
020.02-Aerial station, stop, shelter, mall, terminal, platform	2,260,735	0
020.03-Underground station, stop, shelter, mall, terminal, platform	549,202,541	419,047,431
020.07-Elevators, escalators	21,701,837	10,931,853
040.01-Demolition, Clearing, Earthwork	12,382,885	12,078,515
040.02-Site Utilities, Utility Relocation	65,062,128	72,241,380
040.03-Haz. mat'l, contam'd soil removal/mitigation, ground water treatments	7,534,128	6,769,024
040.04-Environmental mitigation, e.g. wetlands, historic/archeologic, parks	1,122,899	756,590
040.05-Site structures including retaining walls, sound walls	2,708,328	2,706,431
040.06-Pedestrian / bike access and accommodation, landscaping	9,790,995	3,566,925
040.07-Automobile, bus, van accessways including roads, parking lots	6,579,099	5,432,260
040.08-Temporary Facilities and other indirect costs during construction	115,162,438	110,027,855
050.01-Train control and signals	28,127,939	7,619,133
050.02-Traffic signals and crossing protection	12,804,956	11,628,882
050.03-Traction power supply: substations	21,465,073	13,601,787
050.04-Traction power distribution: catenary and third rail	12,441,113	2,048,553
050.05-Communications	12,030,586	2,104,390
050.06-Fare collection system and equipment	6,100,000	152,852
050.07-Central Control	2,614,586	339,421
060.01-Purchase or lease of real estate	30,065,810	28,239,539
060.02-Relocation of existing households and businesses	2,180,511	2,409,430
070.01-Light Rail	26,385,653	11,929,247
080.01-Preliminary Engineering	46,202,674	46,202,675
080.02-Final Design	61,318,331	61,199,308
080.03-Project Management for Design and Construction	102,918,390	73,587,214
080.04-Construction Administration & Management	96,317,191	86,827,554
080.05-Professional Liability and other Non-Construction Insurance	6,800,000	6,340,196
080.06-Legal; Permits; Review Fees by other agencies, cities, etc.	8,212,604	5,344,391
080.07-Surveys, Testing, Investigation, Inspection	933,100	877,671
080.08-Start up	8,300,329	0
090.00-Unallocated Contingency	6,742,405	
Grand Total	1,578,300,001	1,277,249,638

[A] Cost Account Description	BUDGET	ACTUAL COSTS				[G] VARIANCE (B - F)	COST REPORT NOTES
	[B] November 2018 Budget (YOE)	[C] PRIOR MONTH Total	[D] PRIOR MONTH Monthly	[E] CURRENT Monthly	[F] CURRENT Total		
TOTAL PRELIMINARY ENGINEERING	46,542,061	46,542,061	0	0	46,542,061	0	40
11 - SFMTA PROJECT MANAGEMENT	8,800,164	8,253,957	0	0	8,253,957	546,208	41
12 - SFMTA ENGINEERING SERVICES	11,425,594	11,425,594	0	0	11,425,594	0	42
16 - DEPARTMENT OF PARKING AND TRAFFIC (DPT)	921,906	802,883	0	0	802,883	119,023	
21 - ARTS COMMISSION	1,500,570	1,500,570	0	0	1,500,570	0	43
22 - FIRE DEPARTMENT	33,825	33,825	0	0	33,825	0	
23 - CITY ATTORNEY'S OFFICE	1,234,754	1,234,754	0	0	1,234,754	0	
24 - RISK MANAGEMENT	0	0	0	0	0	0	
26 - PLANNING	99,604	99,604	0	0	99,604	0	
27 - DEPARTMENT OF PUBLIC HEALTH (DPH)	4,420	4,420	0	0	4,420	0	
29 - CITY AUDITOR	336,735	336,735	0	0	336,735	0	44
32 - DPW - IDC ENGINEERING (HYDRAULIC)	3,336,432	3,336,432	0	0	3,336,432	0	
34 - DPW - IDC CONSTRUCTION (CAPTITAL)	17,462	17,462	0	0	17,462	0	
36 - DPW - BSM INFRASTRUCTURE (MAPPING)	76,549	76,549	0	0	76,549	0	
39 - DPW - PCS SITE ASSESSMENT & REMEDIATION (SAR)	13,993	13,993	0	0	13,993	0	
51 - 821 HOWARD STREET	1,005,653	1,005,653	0	0	1,005,653	0	
55 - 651 BRANNAN	2,294,910	2,294,910	0	0	2,294,910	0	45
63 - CENTRAL SUBWAY PARTNERSHIP - AECOM-EPC JV CONTRACT 149	26,793,234	26,793,234	0	0	26,793,234	0	46
66 - ANIL VERMA	395,204	395,204	0	0	395,204	0	47
67 - HILL INTERNATIONAL CONTRACT 156	6,716,294	6,716,294	0	0	6,716,294	0	48
68 - ARTHUR GALLAGER & CO. CS 164	6,800,000	6,340,196	0	0	6,340,196	459,804	
71 - TUNNEL/UTILITIES - CONTRACT # CONTRACT 155-1	5,469,336	5,469,336	0	0	5,469,336	0	49
72 - STATIONS - CONTRACT # CONTRACT 155-2	26,220,609	26,220,609	0	0	26,220,609	0	50
73 - SYSTEMS/INTEGRATION - CONTRACT 155-3	11,432,312	11,432,312	0	0	11,432,312	0	51
331 - BAY AREA RAPID TRANSIT (BART)	146,427	146,427	0	0	146,427	0	
332 - SAN FRANCISCO COUNTY TRANSPORTATION AUTHORITY (SFCTA)	0	0	0	0	0	0	
TOTAL FINAL DESIGN	115,075,988	113,950,952	0	0	113,950,952	1,125,035	
11 - SFMTA PROJECT MANAGEMENT	16,500,000	13,086,662	382,023	70,508	13,157,170	3,342,830	
1.3.011.01.080.03 - CM:SFMTA LABOR-PROJECT MANAGEMENT	16,500,000	13,086,662	382,023	70,508	13,157,170	3,342,830	
12 - SFMTA ENGINEERING SERVICES	2,923,582	2,632,812	22,529	4,971	2,637,783	285,799	
1.3.012.02.080.04 - CM: SFMTA LABOR-ENGINEERING CONTRACT 1252	123,582	57,648	0	0	57,648	65,934	
1.3.012.06.080.04 - CM: SFMTA LABOR-ENGINEERING CONTRACT 1300	2,800,000	2,575,164	22,529	4,971	2,580,135	219,865	
13 - SFMTA CONSTRUCTION MANAGEMENT	37,432,035	18,243,714	656,065	120,708	18,364,422	19,067,613	
1.3.013.01.080.04 - CM:SFMTA LABOR-CONSTR. MANAGEM	37,432,035	18,243,714	656,065	120,708	18,364,422	19,067,613	
16 - DEPARTMENT OF PARKING AND TRAFFIC (DPT)	3,659,313	2,183,212	10,533	2,727	2,185,938	1,884,228	
1.3.016.01.080.04 - DPT CONTRACT 1300 SUPPORT UMS	299,600	129,297	5,187	852	130,149	169,451	
1.3.016.01.080.04 - DPT CONTRACT 1300 SUPPORT CTS	274,900	118,776	833	342	119,117	155,783	
1.3.016.01.080.04 - DPT CONTRACT 1300 SUPPORT YBM	238,400	177,077	4,225	1,324	178,402	59,998	
1.3.016.01.080.04 - DPT CONTRACT 1300 SUPPORT STS	876,876	263,926	288	209	264,135	612,741	
1.3.016.02.040.08 - DPT: FIELD OPS TUNNEL [B84]	0	1,464	0	0	1,464	(1,464)	

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	[B] November 2018 Budget (YOE)	[C] PRIOR MONTH Total	[D] PRIOR MONTH Monthly	[E] CURRENT Monthly	[F] CURRENT Total		
1.3.016.02.040.08 - DPT: FIELD OPS TUNNEL [B86]	0	204,261	0	0	204,261	(204,261)	
1.3.016.06.040.02 - DPT:DPT TRAFFIC SHOP CONTRACT 1300	1,200,000	0	0	0	0	1,200,000	
1.3.016.08.040.08 - DPT:PCOS:2UTL [68A]	400,728	400,728	0	0	400,728	0	
1.3.016.08.040.08 - DPT:SSD CN:2UTL	0	108,020	0	0	108,020	(108,020)	
1.3.016.08.080.04 - DPT:SSD [1326]	252,536	252,536	0	0	252,536	0	
1.3.016.08.080.04 - DPT:SSD [13BN]	23,302	23,302	0	0	23,302	0	
1.3.016.08.080.04 - DPT:SSD [13CN]	963	963	0	0	963	0	
1.3.016.08.080.04 - DPT:SSD [B85]	92,008	92,008	0	0	92,008	0	
1.3.016.03.040.08 - PCOS:1300/UMS [68CPT544132W.CPT544132W]	0	161,753	0	0	161,753	(161,753)	
1.3.016.04.040.08 - PCOS:1300/CTS [68CPT544132X.CPT544132X]	0	0	0	0	0	0	
1.3.016.05.040.08 - PCOS:1300/YBM [68CPT544132Y.CPT544132Y]	0	4,052	0	0	4,052	(4,052)	
1.3.016.09.040.08 - PCOS:1300/STS [68CPT544132Z.CPT544132Z]	0	245,049	0	0	245,049	(245,049)	
17 - MOTIVE POWER	2,195	0	0	0	0	2,195	
1.3.017.07.040.02 - PWR:SFMTA-MOTIVE POWER-UTL.REL	2,195	0	0	0	0	2,195	
18 - SFMTA OPERATIONS	400,000	76,354	0	9,106	85,460	235,063	
1.3.018.04.040.02 - OPS:SUPPORT TO CONTRACT 1300/CTS	100,000	26,469	0	0	26,469	73,531	
1.3.018.06.080.07 - OPS:SUPPORT TO CONTRACT 1300 - UMS O/L	50,255	35,203	0	9,106	44,310	5,945	
1.3.018.06.080.07 - OPS:SUPPORT TO CONTRACT 1300/UMS	249,745	14,681	0	0	14,681	235,063	
19 - OTHER SFMTA	1,000,000	944,829	0	0	944,829	55,171	
1.3.019.07.080.07 - OTH.MTA SFMTA-SURVEY; TSTG [6840]	1,800	714	0	0	714	1,086	
1.3.019.08.040.08 - OTH.MTA 1251 MATERIALS	150,000	126,149	0	0	126,149	23,851	
1.3.019.08.080.07 - OTH.MTA OPERATION SUPPORT DURI	848,200	817,966	0	0	817,966	30,234	
21 - ARTS COMMISSION	12,010,886	4,394,441	12,729	9,149	4,403,590	7,607,296	
1.3.021.01.040.06 - ARTS:CTYCO-ARTS COMMISSION CONSTRUCTION COSTS	4,772,555	0	0	0	0	4,772,555	
1.3.021.01.080.03 - ARTS:CTYCO-ARTS COMMISSION [1227]	1,902,044	388,167	0	0	388,167	1,513,877	52
1.3.021.01.080.04 - ARTS:CTYCO-ARTS COMMISSION [PWE335MPFUNA.CPT5441227]	21,000	12,465	0	0	12,465	8,535	
1.3.021.06.080.03 - ARTS:CTYCO-ARTS COMMISSION PM [285MC.132J]	653,244	757,076	7,032	4,720	761,796	(108,552)	
1.3.021.01.080.03 - ARTS:CTYCO-ARTS COMMISSION [PWA335MPFUNA.CPT5441327]	8,512	9,205	0	944	10,149	(1,637)	
1.3.021.01.080.03 - ARTS:CTYCO-ARTS COMMISSION [PWE335MPFUNA.CPT5441327]	4,439	4,439	0	0	4,439	0	
1.3.021.06.040.06 - ARTS:CTYCO-ARTS COMMISSION [68CPT5441327.CPT5441327]	1,500,000	1,393,660	0	0	1,393,660	106,340	
1.3.021.06.040.06 - ARTS:CTYCO-ARTS COMMISSION [285MCPFUNA.CPT5441327]	1,903,000	1,829,429	5,697	3,485	1,832,914	70,086	
1.3.021.01.080.03 - ARTS:CTYCO-ARTS COMMISSION [132J]	86,091	0	0	0	0	86,091	
1.3.021.97.040.06 - ARTS:ARTS COMMISSION ALLOC CO	1,160,000	0	0	0	0	1,160,000	
23 - CITY ATTORNEY'S OFFICE	2,171,781	1,864,039	0	0	1,864,039	307,742	
1.3.023.01.080.06 - ATTY:CN LEGAL-CITY ATTORNEY OF	2,171,781	1,864,039	0	0	1,864,039	307,742	
25 - PUBLIC UTILITIES COMMISSION SEWER	(2,925,296)	0	0	0	0	(2,925,296)	
1.3.025.09.040.02 - STS.1256: SITE UTILITIES SFPUC SEWER MAIN	(2,925,296)	0	0	0	0	(2,925,296)	
26 - PLANNING	137,062	26,697	0	0	26,697	110,365	
1.3.026.01.080.06 - CM:CTYCO-PLANNING DEPARTMENT	137,062	26,697	0	0	26,697	110,365	
28 - PUBLIC UTILITIES COMMISSION WATER	4,242,012	3,926,971	60,217	1,052	3,928,023	313,989	
1.3.028.02.040.02 - CM:CTYCO-PUBLIC UTIL COMM. (PUC)	0	4,745	0	0	4,745	(4,745)	
1.3.028.02.040.08 - PUC: FIELD OPERATIONS TUNNEL	398,400	509,867	0	0	509,867	(111,467)	
1.3.028.02.080.04 - PUC:MTA CSP CN1252 [470465]	105,000	91,587	0	0	91,587	13,413	

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	[B] November 2018 Budget (YOE)	[C] PRIOR MONTH Total	[D] PRIOR MONTH Monthly	[E] CURRENT Monthly	[F] CURRENT Total			
1.3.028.03.040.02 - PUC:CDD CONTRACT 1300/UMS SUPPORT	606,354	484,759	51,996	681	485,441	120,913		
1.3.028.03.080.04 - PUC:CMB CONTRACT 1300/UMS INSPECTION	230,000	34,508	0	0	34,508	195,492		
1.3.028.04.040.02 - PUC:CDD CONTRACT 1300/CTS SUPPORT	271,755	201,959	0	0	201,959	69,796		
1.3.028.04.080.04 - PUC:CMB CONTRACT 1300/CTS INSPECTION	115,000	55,773	(643)	0	55,773	59,227		
1.3.028.05.040.02 - PUC:CDD CONTRACT 1300/YBM SUPPORT	450,282	441,714	8,184	0	441,714	8,568		
1.3.028.05.080.04 - PUC:CMB CONTRACT 1300/YBM INSPECTION	184,000	98,272	680	370	98,643	85,357		
1.3.028.06.040.02 - PUC:CMB CONTRACT 1300/SFWD AWSS MATERIAL	225,079	249,247	0	0	249,247	(24,168)		
1.3.028.07.040.02 - PUC:PUC CDD WATER CONNECTION CONTRACT 1250	248,035	291,501	0	0	291,501	(43,466)		
1.3.028.07.080.04 - PUC:PUC CMB INSPECTION CONTRACT 1250	74,468	113,844	0	0	113,844	(39,376)		
1.3.028.08.040.02 - PUC:PUC CDD WATER CONNECTION CONTRACT 1251 [445]	340,310	318,130	0	0	318,130	22,180		
1.3.028.08.080.04 - PUC:PUC CMB INSPECTION CONTRACT 1251	266,252	289,424	0	0	289,424	(23,172)		
1.3.028.09.040.02 - PUC:CMB CONTRACT 1300/STS SUPPORT	520,077	484,348	0	0	484,348	35,729		
1.3.028.09.080.04 - PUC:CMB CONTRACT 1300/STS INSPECTION	207,000	257,294	0	0	257,294	(50,294)		
32 - DPW - IDC ENGINEERING (HYDRAULIC)	1,150,459	526,605	1,619	1,825	528,430	687,301		
1.3.032.01.080.04 - CM:DPW:1424J-BUREAU OF ENGINEERING (BOE) [AB12]	(285,405)	(285,405)	0	0	(285,405)	0.00		
1.3.032.03.080.04 - DPW IDC HYDRAULIC CN1300 UMS SUPPORT	297,938	116,648	855	220	116,869	181,069		
1.3.032.04.080.04 - DPW IDC HYDRAULIC CN1300 CTS SUPPORT	295,639	22,125	0	0	22,125	273,514		
1.3.032.05.080.04 - DPW IDC HYDRAULIC CN1300 YBM SUPPORT	301,882	51,854	425	84	51,938	249,944		
1.3.032.06.080.04 -1424J-BOE LABOR [PWE1X5MPFUNA.CPT544112B112]	85,275	85,275	0	0	85,275	0.00	53	
1.3.032.06.080.04-1424J-BOE LABOR [PWE1X5MPFUNA.CPT544112C112]	109,658	109,658	0	0	109,658	0.00	54	
1.3.032.06.080.04 -1424J-BOE LABOR [PWE1X5MPFUNA.CPT544112D112]	15,791	15,791	0	0	15,791	0.00	55	
1.3.032.06.080.04 -1424J-BOE LABOR [PWE1X5MPFUNA.CPT544112E112]	11,193	11,193	0	0	11,193	0	56	
1.3.032.06.080.04 -1424J-BOE LABOR [PWE1X5MPFUNA.CPT544112F112]	107,798	107,798	0	0	107,798	0	57	
1.3.032.06.080.04 -1424J-BOE LABOR [PWE1X5MPFUNA.CPT544112G112]	21,690	47,917	0	0	47,917	(26,227)	58	
1.3.032.08.080.04 - DPW.HYRDDPW-BOE IDC ENG SVC DC	9,000	0	0	0	0	9,000		
1.3.032.09.080.04 - DPW IDC HYDRAULIC CN1300 STS SUPPOR	180,000	243,750	339	1,521	245,272	(65,272)		
34 - DPW - IDC CONSTRUCTION (CAPITAL)	6,703,969	6,345,071	0	0	6,345,071	358,898		
1.3.034.01.080.04 - DPW:BCM LABOR [2113]	2,140,142	2,140,142	0	0	2,140,142	0		
1.3.034.02.080.04 - DPW:CONSTR:1252 CM [CD12]	1,207,603	1,207,603	0	0	1,207,603	0		
1.3.034.02.080.04 - DPW:CONSTR:1252 CM [13AC12]	138,397	138,397	0	0	138,397	0		
1.3.034.06.080.04 - DPW:SITE ASSESSMENT & REMEDIATION (SAR) [132112]	506,858	506,858	0	0	506,858	0		
1.3.034.06.080.04 - DPW:CONSTR:1300 CM [13CP12]	2,710,969	2,352,071	0	0	2,352,071	358,898		
36 - DPW - BSM INFRASTRUCTURE (MAPPING)	465,562	158,741	0	0	158,741	306,821		
1.3.036.01.080.04 - DPW:MPPG:DPW-BUREAU OF ST USE	367,129	32,680	0	0	32,680	334,449		
1.3.036.02.080.04 - DPW:MPPG:1300-DPW-BUREAU OF ST USE [13CG12]	50,000	33,084	0	0	33,084	16,916		
1.3.036.02.080.06 - DPW:MPPG:DPW-BUREAU OF ST USE [13CF]	48,433	92,977	0	0	92,977	(44,544)		
37 - DPW - PCS MATERIAL TESTING LABORATORY	83,100	0	0	0	0	83,100		
1.3.037.01.080.07 - DPW.MTL.LABDPW-MATERIAL TESTIN	83,100	0	0	0	0	83,100		
39 - DPW - PCS SITE ASSESSMENT & REMEDIATION (SAR)	613,853	438,455	0	0	438,455	175,398		
1.3.039.01.080.04 - DPW:SITE ASSESSMENT & REMEDIATION (SAR) [2213]	92,459	92,459	0	0	92,459	0		
1.3.039.01.080.04 - DPW:SITE ASSESSMENT & REMEDIATION (SAR) [2250]	78,400	78,400	0	0	78,400	0		
1.3.039.01.080.04 -DPW:SITE ASSESSMENT & REMEDIATION (SAR) [2257]	151,515	151,515	0	0	151,515	0		
1.3.039.01.080.04 - DPW:SITE ASSESSMENT & REMEDIATION (SAR) [2313]	24,343	24,343	0	0	24,343	0		

[A] Cost Account Description	BUDGET		ACTUAL COSTS				[G] VARIANCE (B - F)	COST REPORT NOTES
	[B] November 2018 Budget (YOE)	[C] PRIOR MONTH Total	[D] PRIOR MONTH Monthly	[E] CURRENT Monthly	[F] CURRENT Total			
1.3.039.01.080.04 - DPW:SITE ASSESSMENT & REMEDIATION	58,757	10,109	0	0	10,109	48,648		
1.3.039.01.080.04 - DPW:SITE ASSESSMENT & REMEDIATION (SAR) [CE13]	31,367	31,367	0	0	31,367	0		
1.3.039.01.080.04 - DPW:SITE ASSESSMENT & REMEDIATION (SAR) [CH13]	100,000	8,621	0	0	8,621	91,379		
1.3.039.01.080.04 - DPW:SITE ASSESSMENT & REMEDIATION (SAR)	17,000	0	0	0	0	17,000		
1.3.039.02.080.04 - DPW: SITE ASSESSMENT & REMEDIATION (SAR) – CN1252 [13CE11]	18,632	16,880	0	0	16,880	1,753		
1.3.039.02.080.04 - DPW: SITE ASSESSMENT & REMEDIATION (SAR) – CN1300 [13CH11]	41,379	24,761	0	0	24,761	16,618		
46 - MACY'S WEST - SFPUC SEWER WORK	258,202	258,202	0	0	258,202	0		
1.3.046.08.040.02 - MCY.SWRC. CONTRACT: MACY'S-SEW	258,202	258,202	0	0	258,202	0		
51 - 821 HOWARD STREET	770,843	632,416	115	0	632,416	138,427		
1.3.051.01.080.03 - ODC.HWRD:ODCs - 821 HOWARD STR	696,753	601,290	115	0	601,290	95,463		
1.3.051.02.080.04 - ODC.HWRD:ODCs - TUNNEL CONTRACT 1252	10,000	1,056	0	0	1,056	8,944		
1.3.051.06.080.04 - ODC.HWRD:ODCs - STATION CONTRACT 1300	55,000	19,148	0	0	19,148	35,852		
1.3.051.06.080.04 - ODC.HWRD:W/MTA INST WTR SVC @ STS&YBM TRAILER	9,090	10,923	0	0	10,923	(1,833)		
55 - 651 BRANNAN	10,348	10,348	0	0	10,348	0		
1.3.055.01.080.03 - CM:ODCs - 651 BRANNAN STREET	10,348	10,348	0	0	10,348	0	59	
63 - CENTRAL SUBWAY PARTNERSHIP - AECOM-EPC JV CONTRACT 149	47,966,331	45,919,667	1,030,846	(73,162)	45,846,505	2,119,827		
1.3.063.01.080.03 - CM:PM:AECOM.CS149 OM-EPC JV CS149-PM	5,017,804	5,017,804	0	0	5,017,804	0	60	
1.3.063.01.080.04 - CM:AECOM.CS149OM-EPC JV CS-149 [3B]	1,969,213	1,969,213	0	0	1,969,213	(0)		
1.3.063.01.080.04 - CM:AECOM.CS149OM-EPC JV CS-149 [3E]	6,386,250	6,386,250	0	0	6,386,250	(0)		
1.3.063.01.080.03 - CM:AECOM.CS149OM-EPC JV CS-149 [3E][PM]	1,596,563	1,596,563	0	0	1,596,563	0		
1.3.063.01.080.04 - CM:AECOM.CS149OM-EPC JV CS-149 [3F]	4,101,465	4,101,466	0	0	4,101,466	(0)		
1.3.063.01.080.03 - CM:AECOM.CS149OM-EPC JV CS-149 [3F][PM]	1,025,366	1,025,366	0	0	1,025,366	0		
1.3.063.01.080.04 - CM:AECOM.CS149OM-EPC JV CS-149 [3G]	5,167,381	5,167,381	0	0	5,167,381	(0)		
1.3.063.01.080.03 - CM:AECOM.CS149OM-EPC JV CS-149 [3G][PM]	1,291,845	1,291,845	0	0	1,291,845	(0)		
1.3.063.01.080.04 - CM:AECOM.CS149OM-EPC JV CS-149 [3H]	2,746,876	4,380,849	0	0	4,380,849	(1,633,973)		
1.3.063.01.080.03 - CM:AECOM.CS149OM-EPC JV CS-149 [3H][PM]	1,095,212	1,095,212	0	0	1,095,212	(0)		
1.3.063.01.080.04 - CM:AECOM.CS149OM-EPC JV CS-149 [3i]	0	3,836,262	601,025	(310,310)	3,525,951	(3,525,951)		
1.3.063.01.080.03 - CM:AECOM.CS149OM-EPC JV CS-149 [3i][PM]	1,400,000	959,065	150,256	(77,578)	881,488	518,512		
1.3.063.01.080.04 - CM:AECOM.CS149OM-EPC JV CS-149 [9B]	11,042	11,042	0	0	11,042	0		
1.3.063.01.080.04 - CM:AECOM.CS149OM-EPC JV CS-149 [9D]	515,694	515,694	0	0	515,694	(0)		
1.3.063.01.080.04 - CM:AECOM.CS149OM-EPC JV CS-149 [9E]	523,943	523,943	0	0	523,943	0		
1.3.063.01.080.04 - CM:AECOM.CS149OM-EPC JV CS-149 [9F]	461,196	461,196	0	0	461,196	0		
1.3.063.01.080.04 - CM:AECOM.CS149OM-EPC JV CS-149 [9G]	501,912	501,912	0	0	501,912	0		
1.3.063.01.080.04 - CM:AECOM.CS149OM-EPC JV CS-149 [9H]	542,110	1,219,093	0	0	1,219,093	(676,983)		
1.3.063.01.080.04 - CM:AECOM.CS149OM-EPC JV CS-149 [9i]	1,000,000	279,565	279,565	314,726	594,291	405,709		
1.3.063.01.080.04 - FD:CM:EPC JV CS49-PM [123A]	489,930	5,579,945	0	0	5,579,945	(5,090,015)		
1.3.063.97.080.03 - AECOM.CS149 ALLOCAT CONTING	12,122,528					12,122,528		
64 - CN1300 JOB READINESS PROGRAM	1,060,000	866,895	160,125	0	866,895	193,105	61	
1.3.064.06.040.08 - CN1300 JOB READINESS PROGRAM	1,060,000	866,895	160,125	0	866,895	193,105		
67 - HILL INTERNATIONAL CONTRACT 156	11,000,000	2,897,833	20,701	(165,612)	2,732,221	8,267,779		
1.3.067.01.080.03 - HILL.CS156:HILL INTL. CS-156 [1336]	920,426	920,426	0	0	920,426	0		
1.3.067.01.080.03 - HILL.CS156:HILL INTL. CS-156 [1337]	533,148	533,148	0	0	533,148	0		
1.3.067.01.080.03 - HILL.CS156:HILL INTL. [1330]	127,261	127,261	0	0	127,261	0		

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	[B] November 2018 Budget (YOE)	[C] PRIOR MONTH Total	[D] PRIOR MONTH Monthly	[E] CURRENT Monthly	[F] CURRENT Total			
1.3.067.01.080.03 - HILL INTERNATIONAL CS156 AWP 2016 [68CPT5441340.CPT5441340]	8,852,240	883,631	0	0	883,631	7,968,609		
1.3.067.01.080.03 - HILL INTERNATIONAL CS156 AWP 2017 [68CPT5441346.CPT5441346]	566,925	433,368	20,701	(165,612)	267,756	299,169		
69 - BAYLAND SOIL PROCESS CONTRACT 175	500,000	255,144	0	0	255,144	244,856	62	
1.3.069.06.040.01 - BAYLAND.CS175:BAYLAND SOIL PROCESS [133K]	500,000	255,144	0	0	255,144	244,856		
71 - TUNNEL/UTILITIES - CONTRACT # CONTRACT 155-1	2,158,846	2,098,874	0	0	2,098,874	59,972		
1.3.071.01.080.04 - CM: CS155.1 DESIGN SUPPORT DURING CM [1232]	0	(87,201)	0	0	(87,201)	87,201	63	
1.3.071.02.080.04 - CM: CS155.1 DESIGN SUPPORT DURING CM [1332]	2,158,846	2,186,076	0	0	2,186,076	(27,230)		
72 - STATIONS - CONTRACT # CONTRACT 155-2	9,612,416	15,623,757	233,786	(214,142)	15,409,615	(5,797,199)		
1.3.072.01.080.04 - CM: CS155.2 DESIGN SUPPORT DURING CM [1233]	51,351	53,588	642	0	53,588	(2,237)	64	
1.3.072.01.080.04 - CM: CS155.2 DESIGN SUPPORT DURING CM [1333]	9,561,065	15,570,169	233,144	(214,142)	15,356,027	(5,794,962)		
73 - SYSTEMS/INTEGRATION - CONTRACT 155-3	4,828,269	3,890,622	65,865	14,264	3,904,887	923,382		
1.3.073.01.080.04 - CM: CS155.3 DESIGN SUPPORT DURING CM [1236]	90,000	89,791	0	0	89,791	209		
1.3.073.01.080.04 - CM: CS155.3 DESIGN SUPPORT DURING CM [1334]	4,738,269	3,800,832	65,865	14,264	3,815,096	923,173		
81 - UTILITIES RELOCATION #1 (PORTAL & MOS) - CONTRACT 1250	11,968,150	11,968,150	0	0	11,968,150	0		
1.3.081.07.040.01 - UR1.CONTRACT 1250:SITWORK: DEMOLIT	167,458	167,458	0	0	167,458	0		
1.3.081.07.040.02 - UR1.CONTRACT 1250:SITWORK: UTILITI	10,099,341	10,099,341	0	0	10,099,341	0		
1.3.081.07.040.03 - UR1.CONTRACT 1250:SITWORK:HAZMAT	453,321	453,321	0	0	453,321	0		
1.3.081.07.040.08 - UR1.CONTRACT 1250:SITWORK:TEMPORAR	1,248,030	1,248,030	0	0	1,248,030	0		
82 - UTILITIES RELOCATION #2 (UMS) - CONTRACT 1251	20,669,081	20,669,081	0	0	20,669,081	(0)	65	
1.3.082.08.040.01 - UR2.CONTRACT 1251:SITWORK: DEMOLIT	752,240	752,240	0	0	752,240	0		
1.3.082.08.040.02 - UR2.CONTRACT 1251:SITWORK:UTILITI	10,202,543	10,202,543	0	0	10,202,543	(0)		
1.3.082.08.040.03 - UR2.CONTRACT 1251:SITWORK:HAZMAT	172,712	172,712	0	0	172,712	0		
1.3.082.08.040.05 - UR2.CONTRACT 1251:SITWORK: STRUCTU	2,706,431	2,706,431	0	0	2,706,431	0		
1.3.082.08.040.06 - UR2.CONTRACT 1251:SITWORK:PEDESTRA	319,317	319,317	0	0	319,317	0		
1.3.082.08.040.07 - UR2.CONTRACT 1251:SITWORK:AUTO/BUS	190,362	190,362	0	0	190,362	0		
1.3.082.08.040.08 - UR2.CONTRACT 1251:SITWORK:TEMP FAC	6,325,476	6,325,476	0	0	6,325,476	0		
GUIDEWAY TUNNELS TOTAL	235,913,500	233,589,322	0	0	233,589,322	2,324,178		
83 - GUIDEWAY TUNNELS - CONTRACT # 1252 BASE	233,584,015	231,914,862	0	0	231,914,862	1,669,153	66	
1.3.083.02.010.06 - CONTRACT 1252:GUIDEWAY:UNDERGRN'D CUT	60,446,425	60,446,425	0	0	60,446,425	0		
1.3.083.02.010.07 - CONTRACT 1252:GUIDEWAY:UNDERGROUND	105,423,090	105,181,925	0	0	105,181,925	241,165		
1.3.083.02.020.03 - CONTRACT 1252: STATIONS: UNDERGROUND	21,685,000	21,685,000	0	0	21,685,000	0		
1.3.083.02.040.01 - CONTRACT 1252:SITWORK:DEMO CLEARING	2,440,000	2,440,000	0	0	2,440,000	0		
1.3.083.02.040.02 - CONTRACT 1252:SITWORK:UTILITIES & RE	10,895,000	10,487,676	0	0	10,487,676	407,324		
1.3.083.02.040.03 - CONTRACT 1252:SITWORK:HAZMAT&MITIGAT	200,000	0	0	0	0	200,000		
1.3.083.02.040.04 - CONTRACT 1252:SITWORK:ENVIRON. MITIG	300,000	54,292	0	0	54,292	245,708		
1.3.083.02.040.06 - CONTRACT 1252:SITWORK:PED/BIKE ACCES	50,000	4,532	0	0	4,532	45,468		
1.3.083.02.040.07 - CONTRACT 1252:SITWORK:AUTO/BUS ACCES	1,345,000	1,345,000	0	0	1,345,000	0		
1.3.083.02.040.08 - CONTRACT 1252:SITWORK:TEMP FACILITIE	30,799,500	30,270,012	0	0	30,270,012	529,488		
83 - GUIDEWAY TUNNELS - CONTRACT # 1252 CMODs	1,363,054	1,674,460	0	0	1,674,460	(311,406)	67	
1.3.083.83.010.06 - CONTRACT 1252: CONTRACT MOD	112,251	112,251	0	0	112,251	0		
1.3.083.83.010.07 - CONTRACT 1252: CONTRACT MOD	1,810,094	1,670,233	0	0	1,670,233	139,861		
1.3.083.83.020.03 - CONTRACT 1252: CONTRACT MOD	1,004,156	1,004,156	0	0	1,004,156	0		
1.3.083.83.040.02 - CONTRACT 1252: CONTRACT MOD	1,035,588	935,588	0	0	935,588	100,000		

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1.3.083.83.040.03 - CONTRACT 1252: CONTRACT MOD	453,475	453,475	0	0	453,475	0		
1.3.083.83.040.08 - CONTRACT 1252: CONTRACT MOD	(3,052,510)	(2,501,244)	0	0	(2,501,244)	(551,266)		
1.3.083.93.010.07 - CONTRACT 1252: TUNNEL ALLOC CONTING	966,431	0	0	0	0	966,431	68	
CONTRACT 1300 - STATIONS, TRACKWORK AND SYSTEMS TOTAL	879,676,400	648,283,800	8,265,283	7,726,850	656,010,650	223,665,750	69	
84 - UNION SQUARE/MARKET STREET STATION (UMS) - WORK PACKAGE 1253	294,030,590	243,640,271	2,516,259	3,669,102	247,309,373	46,721,217	21	
1.3.084.03.020.03 - UMS.1253: UNDERGROUD STATION	253,081,452	212,736,426	2,447,535	2,656,241	215,392,667	37,688,785		
1.3.084.03.020.07 - UMS.1253: ELEVATORS ESCALATOR	9,465,694	3,767,776	(152,544)	989,802	4,757,578	4,708,116		
1.3.084.03.040.01 - UMS.1253: DEMOLITION CLEARING	6,071,588	6,054,088	0	0	6,054,088	17,500		
1.3.084.03.040.02 - UMS.1253: SITE UTILITIES UTIL	4,360,395	3,806,653	63,891	54,890	3,861,543	498,852		
1.3.084.03.040.03 - UMS.1253: HAZARDOUS MATERIALS	550,000	550,000	0	0	550,000	0		
1.3.084.03.040.04 - UMS.1253: ENVIRONMENTAL MITIGA	244,500	244,500	0	0	244,500	0		
1.3.084.03.040.06 - UMS.1253: PEDESTRIAN/BIKE	18,969	16,501	0	0	16,501	2,468		
1.3.084.03.040.07 - UMS.1253: AUTOMOBILE BUS ACCE	1,158,410	724,951	100,000	0	724,951	433,459		
1.3.084.03.040.08 - UMS.1253: TEMPORARY FACILITIES	11,139,701	10,369,329	40,029	(31,831)	10,337,498	802,203		
1.3.084.03.050.02 - UMS.1253: TRAFFIC SIGNALS AND	4,773,076	4,731,076	0	0	4,731,076	42,000		
1.3.084.03.050.03 - UMS.1253: TRACTION POWER SUPPL	1,815,534	1,815,907	0	0	1,815,907	1,399,627		
1.3.084.03.050.04 - UMS.1253: TRACTION POWER DISTR	216,957	67,178	0	0	67,178	149,779		
1.3.084.03.050.05 - UMS.1253: COMMUNICATIONS	1,134,314	155,886	17,348	0	155,886	978,428		
84 - UNION SQUARE/MARKET STREET STATION (UMS) CMODs	2,748,442	2,640,730	0	0	2,640,730	107,712		
1.3.084.84.020.03 - CMOD:UMS.1253: UNDERGROUD STATION	60,746	35,897	0	0	35,897	24,849		
1.3.084.84.020.07 - CMOD:UMS.1253: ELEVATORS, ESCALATORS	90,000	90,000	0	0	90,000	0		
1.3.084.84.040.01 - CMOD:UMS.1253: DEMOLITION CLEARING	944,987	944,987	0	0	944,987	0		
1.3.084.84.040.02 - CMOD:UMS.1253: SITE UTILITIES UTIL	1,494,678	1,411,815	0	0	1,411,815	82,863		
1.3.084.84.040.03 - CMOD:UMS.1253: HAZARDOUS MATERIALS	81,907	81,907	0	0	81,907	0		
1.3.084.84.040.08 - CMOD:UMS.1253: TEMPORARY FACILITIES	76,124	76,124	0	0	76,124	0		
1.3.084.94.020.03 - UMS.1253: AC: ALLOC CONTING	17,251,558	0	0	0	0	17,251,558	41	
85 - CHINATOWN STATION (CTS) - WORK PACKAGE 1254	247,567,810	184,882,098	3,119,076	1,330,196	186,212,294	61,355,516		
1.3.085.04.010.07 - CTS.1254: GUIDEWAY: UNDERGROUND TUNNEL	76,417,579	76,417,579	0	0	76,417,579	0		
1.3.085.04.020.03 - CTS.1254: UNDERGROUND STATION	133,001,053	79,607,294	3,107,076	1,224,556	80,831,850	52,169,203		
1.3.085.04.020.07 - CTS.1254: ELEVATORS ESCALATOR	6,812,856	2,118,673	0	43,140	2,161,813	4,651,043		
1.3.085.04.040.01 - CTS.1254: DEMOLITION CLEARING	400,000	400,000	0	0	400,000	0		
1.3.085.04.040.02 - CTS.1254: SITE UTILITIES UTIL	6,001,718	4,933,586	0	0	4,933,586	1,068,132		
1.3.085.04.040.03 - CTS.1254: HAZARDOUS MATERIALS	350,000	347,500	0	0	347,500	2,500		
1.3.085.04.040.04 - CTS.1254: ENVIRONMENTAL MITIGA	325,665	206,064	8,000	0	206,064	119,601		
1.3.085.04.040.06 - CTS.1254: PEDESTRIAN/BIKE	15,000	0	0	0	0	15,000		
1.3.085.04.040.07 - CTS.1254: AUTOMOBILE BUS ACCE	225,677	29,500	0	0	29,500	196,177		
1.3.085.04.040.08 - CTS.1254: TEMPORARY FACILITIES	16,571,322	16,315,272	0	0	16,315,272	256,050		
1.3.085.04.050.02 - CTS.1254: TRAFFIC SIGNALS AND	1,599,593	1,328,670	0	10,000	1,338,670	260,923		
1.3.085.04.050.03 - CTS.1254: TRACTION POWER SUPPL	4,063,927	2,842,243	0	52,500	2,894,743	1,169,184		
1.3.085.04.050.04 - CTS.1254: TRACTION POWER DISTRIBUTION	124,481	81,940	0	0	81,940	42,541		
1.3.085.04.050.05 - CTS.1254: COMMUNICATIONS	1,658,938	253,776	4,000	0	253,776	1,405,162		
85 - CHINATOWN STATION (CTS) CMODs	8,983,302	7,346,076	894,709	1,126,478	8,472,554	510,749	71	
1.3.085.85.020.03 - CMOD:CTS.1254: UNDERGROUND STATION	1,201,478	0	0	1,126,478	1,126,478	75,000		

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1.3.085.85.040.01 - CMOD:CTS.1254: POWER POLE	155,956	148,212	0	0	148,212	7,744	
1.3.085.85.040.02 - CMOD:CTS.1254: SITE UTILITIES UTIL	3,997,572	3,971,225	0	0	3,971,225	26,347	
1.3.085.85.040.03 - CMOD:CTS.1254: HAZARDOUS MATERIALS	2,274,225	2,274,223	0	0	2,274,223	3	
1.3.085.85.040.08 - CMOD:CTS.1254: TEMPORARY FACILITIES	1,354,071	952,416	894,709	0	952,416	401,655	
1.3.085.95.020.03 - CTS.1254: AC: ALLOC CONTING	1,016,698	0	0	0	0	1,016,698	72
86 - YERBA BUENA MOSCONE STATION (YBM) - WORK PACKAGE 1255	158,089,000	130,912,623	701,293	824,785	131,737,408	26,351,592	
1.3.086.05.020.03 - YBM.1255: UNDERGROUND STATION	118,405,840	99,671,965	738,168	646,808	100,318,773	18,087,067	
1.3.086.05.020.07 - YBM.1255: ELEVATORS ESCALATOR	5,333,287	3,812,212	0	110,250	3,922,462	1,410,825	
1.3.086.05.040.01 - YBM.1255: DEMOLITION CLEARING	657,000	657,000	0	0	657,000	0	
1.3.086.05.040.02 - YBM.1255: SITE UTILITIES UTIL	7,163,278	7,104,151	0	40,000	7,144,151	19,127	
1.3.086.05.040.03 - YBM.1255: HAZARDOUS MATERIALS	2,629,439	2,109,839	0	0	2,109,839	519,600	73
1.3.086.05.040.04 - YBM.1255: ENVIRONMENTAL MITIGA	100,000	100,000	0	0	100,000	0	
1.3.086.05.040.06 - YBM.1255: PEDESTRIAN/BIKE	16,665	1	0	0	1	16,664	
1.3.086.05.040.07 - YBM.1255: AUTOMOBILE BUS ACCE	1,542,725	1,235,523	0	0	1,235,523	307,202	
1.3.086.05.040.08 - YBM.1255: TEMPORARY FACILITIES	15,564,753	13,000,391	(50,000)	(21,831)	12,978,560	2,586,193	
1.3.086.05.050.02 - YBM.1255: TRAFFIC SIGNALS AND	1,726,492	1,723,992	0	0	1,723,992	2,500	
1.3.086.05.050.03 - YBM.1255: TRACTION POWER SUPPL	3,708,425	917,550	13,125	6,500	924,050	2,784,375	
1.3.086.05.050.05 - YBM.1255: COMMUNICATIONS	1,241,096	579,999	0	43,058	623,057	618,039	
86 - YERBA BUENA MOSCONE STATION (YBM) CMODs	747,056	170,635	0	84,537	255,172	491,884	
1.3.086.86.020.03 - CMOD:YBM.1255: UNDERGROUND STATION	(1,758,385)	(1,347,391)	0	0	(1,347,391)	(410,994)	
1.3.086.86.040.01 - CMOD:YBM.1255: DEMOLITION CLEARING	293,656	259,386	0	0	259,386	34,270	73
1.3.086.86.040.02 - CMOD:YBM.1255: SITE UTILITIES UTIL	1,833,122	1,001,900	0	0	1,001,900	831,222	
1.3.086.86.040.03 - CMOD:YBM.1255: HAZARDOUS MATERIALS	150,828	150,828	0	0	150,828	0	
1.3.086.86.040.04 - CMOD:YBM.1255: ENVIRONMENTAL MITIGA	102,734	102,734	0	0	102,734	0	
1.3.086.86.040.05 - CMOD:YBM.1255: SITE STRUCTURES INCLUDING WALLS	1,897	0	0	0	0	1,897	
1.3.086.86.040.06 - CMOD:YBM.1255: PEDESTRIAN/BIKE	35,489	0	0	0	0	35,489	
1.3.086.86.040.08 - CMOD:YBM.1255: TEMPORARY FACILITIES	87,715	3,178	0	84,537	87,715	0	
1.3.086.96.020.03 - YBM.1255: AC: ALLOC CONTING	4,252,945	0	0	0	0	4,252,945	74
87 - SURFACE TRACKWORK AND SYSTEMS -WORK PACKAGE 1256	139,989,000	76,413,271	1,033,946	691,752	77,105,023	62,883,977	
1.3.087.09.010.02 - STS.1256: GUIDEWAY: AT-GRADE SEMI-EXCLUSIVE (ALLOWS CROSS	2,860,000	2,156,500	219,000	0	2,156,500	703,500	
1.3.087.09.010.06 - STS.1256: GUIDEWAY: UNDERGROUND CUT & CVR	9,257,731	3,485,204	266,250	200,000	3,685,204	5,572,527	
1.3.087.09.010.07 - STS.1256: GUIDEWAY: UNDERGROUN	16,723,552	10,854,884	145,000	(91,444)	10,763,440	5,960,112	
1.3.087.09.010.09 - STS.1256: TRACK DIRECT FIXATION	6,761,089	5,949,046	151,130	0	5,949,046	812,044	
1.3.087.09.010.12 - STS.1256: TRACK: SPECIAL	4,449,637	3,599,000	0	0	3,599,000	850,637	
1.3.087.09.020.01 - STS.1256: AT-GRADE STATION	7,602,857	3,264,561	(6,199)	(7,031)	3,257,530	4,345,327	
1.3.087.09.040.02 - STS.1256: SITE UTILITIES, UTILITY RELOCA	17,464,046	14,264,379	0	0	14,264,379	3,199,667	
1.3.087.09.040.03 - STS.1256: HAZARDOUS MATERIALS	200,000	157,000	0	0	157,000	43,000	73
1.3.087.09.040.04 - STS.1256: ENVIRONMENTAL MITIGATION	50,000	49,000	0	0	49,000	1,000	73
1.3.087.09.040.07 - STS.1256: AUTOMOBILE BUS ACCE	2,116,925	1,903,424	0	3,500	1,906,924	210,001	
1.3.087.09.040.08 - STS.1256: TEMPORARY FACILITIES	13,896,832	11,658,642	(196,045)	3,986	11,662,628	2,234,203	
1.3.087.09.050.01 - STS.1256: TRAIN CONTROL AND SIGNALS	27,543,451	7,569,133	0	0	7,569,133	19,974,318	
1.3.087.09.050.02 - STS.1256: TRAFFIC SIGNALS AND	4,463,368	3,615,144	0	0	3,615,144	848,224	
1.3.087.09.050.03 - STS.1256: TRACTION POWER SUPPL	9,889,014	4,876,539	413,461	533,565	5,410,104	4,478,910	

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	[B] November 2018 Budget (YOE)	[C] PRIOR MONTH Total	[D] PRIOR MONTH Monthly	[E] CURRENT Monthly	[F] CURRENT Total		
1.3.087.09.050.04 - STS.1256: TRACTION POWER DISTRIBUTION	6,099,675	1,619,725	28,480	29,176	1,648,901	4,450,774	
1.3.087.09.050.05 - STS.1256: COMMUNICATIONS	7,996,237	1,051,669	12,869	20,000	1,071,669	6,924,568	
1.3.087.09.050.07 - STS.1256: CENTRAL CONTROL	2,614,586	339,421	0	0	339,421	2,275,165	
87 - SURFACE TRACKWORK AND SYSTEMS (STS) CMODs	2,739,264	2,278,096	0	0	2,278,096	461,168	
1.3.087.89.040.02 - CMOD:STS.1256: SITE UTILITIES, UTILITY RELOCA	1,328,553	1,165,880	0	0	1,165,880	162,673	
1.3.087.89.040.03 - CMOD:STS.1256: HAZARDOUS MATERIALS	18,221	18,219	0	0	18,219	2	
1.3.087.89.040.08 - CMOD:STS.1256: TEMPORARY FACILITIES	1,053,547	873,997	0	0	873,997	179,550	
1.3.087.89.050.01 - CMOD:STS.1256: TRAIN CONTROL	96,516	0	0	0	0	96,516	
1.3.087.89.050.02 - CMOD:STS.1256: TRAFFIC SIGNALS AND	242,427	220,000	0	0	220,000	22,427	
1.3.087.99.020.01 - STS.1256: AC: ALLOC CONTING	2,260,735	0	0	0	0	2,260,735	75
88 - STATIONS CONTRACT 1300	2,263,498	0	0	0	0	2,263,498	
1.3.088.06.080.04 - CN1300 CONSTRUCTION TRAILER [68CPT5441316.CPT5441316]	80,000	0	0	0	0	80,000	
1.3.088.06.080.04 - DT-CN1300 COMMUNICATIONS INSTALL [68CPT5441317.CPT5441317]	1,430,594	0	0	0	0	1,430,594	
1.3.088.06.080.04 - MTA Communications - Business Liaison to support CN1300 CON[68CPT5441319]	420,000	0	0	0	0	420,000	
1.3.088.06.080.04 - IT-CN1300 Installation [68CPT5441319.CPT5441319]	332,904	0	0	0	0	332,904	
141 - CONSTRUCTION ADMINISTRATION	2,956,812	0	0	0	0	2,956,812	
1.3.141.97.080.04 - CONSTR.ADMIN:ALLOC CONTING	2,956,812	0	0	0	0	2,956,812	
142 - LEGAL/PERMITS	2,014,204	0	0	0	0	2,014,204	
1.3.142.01.080.06 - LGL.PRMTSF:LEGAL; PERMITS	2,014,204	0	0	0	0	2,014,204	
144 - STARTUP	8,300,329	0	0	0	0	8,300,329	
1.3.144.01.080.08 - STRT: STARTUP	6,941,907	0	0	0	0	6,941,907	
1.3.144.97.080.08 - STRTA: AC STARTUP ALLOC CONTIN	1,358,422	0	0	0	0	1,358,422	
151 - TEMPORARY LICENSE AGREEMENT	17,000	0	0	0	0	17,000	
1.3.151.01.080.06 - TEMP.LICPORARY LICENSE AGREEME	17,000	0	0	0	0	17,000	
170 - COMMUNICATIONS CONNECTIONS	5,757,629	0	0	0	0	5,757,629	
1.3.170.01.050.04 - COMM.CONNN:COMMUNICATION CONN	5,757,629	0	0	0	0	5,757,629	
181 - AON RISK INSURANCE CS 163	18,113,750	18,798,132	0	0	18,798,132	(684,382)	
1.3.181.01.040.08 - AON.CS163 AON RISK INS.	18,088,750	18,773,132	0	0	18,773,132	(684,382)	
1.3.181.01.080.03 - AON.CS171 AON RISK INS. STUDY	25,000	25,000	0	0	25,000	0	
191 - FARE COLLECTION CONTRACTOR	5,400,000	152,852	0	0	152,852	5,247,148	
1.3.191.01.050.06 - FARE.CONSUM:FARE COLLECTION	5,400,000	152,852	0	0	152,852	5,247,148	
192 - THALES T&S CENTRAL CONTROL	487,972	50,000	0	0	50,000	437,972	
1.3.192.01.050.01 - THALES T&S ATCS	487,972	50,000	0	0	50,000	437,972	
202 - JOC2-022.0	63,938	0	0	0	0	63,938	
1.3.202.01.040.02 - JOC2-022:15&22 POTHOLING UTILI LGHT FNDS	63,938	0	0	0	0	63,938	
203 - JOC2-029.0	53,317	0	0	0	0	53,317	
1.3.203.07.040.02 - JOC0292-029: RELOCATE VAULTS-S	53,317	0	0	0	0	53,317	
302 - PG&E	1,988,173	3,956,983	0	0	3,956,983	(1,968,810)	
1.3.302.03.050.03 - PGE PERMANENT POWER UMS	(2,350,000)	0	0	0	0	(2,350,000)	
1.3.302.03.050.03 - PGE POWER FEED UMS	2,959,826	1,387,761	0	0	1,387,761	1,572,065	
1.3.302.04.050.03 - PGE PERMANENT POWER CTS	(2,350,000)	0	0	0	0	(2,350,000)	
1.3.302.04.050.03 - PGE POWER FEED CTS	2,959,826	0	0	0	0	2,959,826	

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	[B] November 2018 Budget (YOE)	[C] PRIOR MONTH Total	[D] PRIOR MONTH Monthly	[E] CURRENT Monthly	[F] CURRENT Total		
1.3.302.05.050.03 - PGE PERMANENT POWER YBM	(2,368,540)	0	0	0	0	(2,368,540)	
1.3.302.05.050.03 - PGE POWER FEED YBM	3,125,222	2,569,222	0	0	2,569,222	556,000	
1.3.302.09.050.03 - PGE POWER FEED STS	11,839	0	0	0	0	11,839	
331 - BAY AREA RAPID TRANSIT (BART)	951,356	471,063	0	0	471,063	480,293	
1.3.331.01.080.04 - CM:SFMTA LABOR-ENG SVCS-IRP/BART/SF	50,000	33,152	0	0	33,152	16,848	
1.3.331.01.080.06 - CM: BAY AREA RAPID TRANSIT (BART) [122A]	901,356	437,911	0	0	437,911	463,445	
333 - AMERICAN PUBLIC TRANSP. ASSOCIATION (APTA) CS-APTA	146,500	62,112	0	0	62,112	84,388	
1.3.333.01.080.03 - APTA:APTA - IRP [2G]	46,500	31,054	0	0	31,054	15,446	
1.3.333.01.080.03 - APTA:APTA - IRP [2C]	100,000	31,058	0	0	31,058	68,942	
334 - BART FARE COLLECTION SYSTEM	700,000	0	0	0	0	700,000	
1.3.334.01.050.06 - BART:BART FARE COLLECTION EQP	700,000	0	0	0	0	700,000	
401 - ECONOMIC AND WORKFORCE DEVELOPMENT (EWD)	17,600	17,600	0	0	17,600	0	
1.3.401.01.080.04 - EWD: MAYORS OFFICE ECON DEV	17,600	17,600	0	0	17,600	0	
402 - DEPARTMENT OF TECHNOLOGY	242,371	250,534	0	0	250,534	(8,163)	
1.3.402.07.050.04 - DT:1UTL:COMM. CONNECTIONS	166,756	179,179	0	0	179,179	(12,423)	
1.3.402.08.050.04 - DT:2UTL:COMM.CONNECTIONS	75,615	71,354	0	0	71,354	4,261	
404 - DEPARTMENT OF BUILDING INSPECTION (DBI)	1,204,081	1,204,081	0	0	1,204,081	0	
1.3.404.01.080.06 - DPT OF BUILDING INSPECTION	1,204,081	1,204,081	0	0	1,204,081	0	
491 - FORM B - REIMBURSEMENT	(12,227,954)	0	0	0	0	(12,227,954)	
1.3.491.02.040.02 - FORMB - CONTRACT 1252 UTILITY REIMBUR	(254,050)	0			0	(254,050)	76
1.3.491.03.040.02 - FORMB - UMS:CONTRACT 1300 UTILITY REIMBURSEMENT	(528,370)	0			0	(528,370)	77
1.3.491.04.040.02 - FORMB - CTS:CONTRACT 1300 UTILITY REIMBURSEMENT	(451,703)	0			0	(451,703)	78
1.3.491.05.040.02 - FORMB - YBM:CONTRACT 1300 UTILITY REIMBURSEMENT	(100,000)	0			0	(100,000)	79
1.3.491.06.040.02 - FORMB - CONTRACT 1300 UTILITY REIMBUR	0	0			0	0	80
1.3.491.07.040.02 - FORMB - CONTRACT 1250 UTILITY REIMBUR	(2,275,419)	0			0	(2,275,419)	81
1.3.491.08.040.02 - FORMB - CONTRACT 1251 UTILITY REIMBUR	(7,618,412)	0			0	(7,618,412)	82
1.3.491.09.040.02 - FORMB - STS:CONTRACT 1300 UTILITY REIMBURSEMENT	(1,000,000)	0			0	(1,000,000)	83
TOTAL CONSTRUCTION PHASE	1,351,413,475	1,066,776,070	10,922,436	7,508,244	1,074,284,314	275,262,312	
1.4.091.01.070.01 - LRVS: LIGHT RAIL VEHICLES RFP [34B]	1,324,123	1,319,773	0	0	1,319,773	4,350	
1.4.091.01.070.01 - LRVS: LIGHT RAIL VEHICLES PROJECT MGT [68E]	828,009	828,009	0	0	828,009	0	
1.4.091.01.070.01 - LRVS: LRV PROCUREMENT ODC	25,000	0	0	0	0	25,000	
1.4.091.01.070.01 - LRVS: LRV PROCUREMENT	11,131,868	9,781,465	0	0	9,781,465	1,350,404	
1.4.091.97.070.01 - LRVA:AC: VEHICLES ALLOC CONTI	13,076,653					13,076,653	26
TOTAL VEHICLES	26,385,654	11,929,247	0	0	11,929,247	14,456,407	
1.5.015.01.060.01 - RE: EASEMENT ACQUISIT	400,000	322,939	0	0	322,939	77,061	
1.5.015.01.060.01 - RE: REAL EST SITE ACQ	15,955,138	14,224,616	0	0	14,224,616	1,730,522	
1.5.015.01.060.01 - RE: REAL ESTATE	766,272	766,272	0	0	766,272	0	
1.5.015.01.060.01 - RE: REC & PARK MOU	6,987,624	6,987,624	0	0	6,987,624	0	
1.5.015.01.060.01 - RE:-DEPT OF TRANSPOR	2,686,000	2,686,000	0	0	2,686,000	0	
1.5.015.01.060.01 - RE:-LICENSES FEES	400,000	381,311	0	0	381,311	18,689	
1.5.023.01.060.01 - ATTY:REAL ES	2,764,872	2,764,872	0	0	2,764,872	0	
1.5.101.01.060.02 - RES.RELO: RELOCATION COST	1,275,200	1,289,701	0	0	1,289,701	(14,501)	

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1.5.102.01.060.02 - COMM.RELO-RELOC COMMERCIAL	905,311	1,119,729	0	0	1,119,729	(214,418)	
TOTAL ROW, LAND, EXISTING IMPROVEMENTS	32,140,418	30,543,065	0	0	30,543,065	1,597,353	
90 - CONTINGENCY	64,948,506	0	0	0	0	64,948,506	
1.7.500.91.090.00 - UNALLOCATED CONTINGENCY	6,742,405					6,742,405	84
TOTAL ALLOCATED CONTINGENCY	58,206,101					58,206,101	
TOTAL PROJECT COST	1,578,300,000	1,269,741,394	10,922,436	7,508,244	1,277,249,638	299,183,512	

7.1 Program Project Budget	
1	The Central Subway Project (CSP) (SFMTA Capital Program CPT 544) is defined in the FTA-SFMTA October 2012 Full Funding Grant Agreement with a budget of \$1.578 billion.
2	The TBM Retrieval Shaft Relocation (SFMTA Capital Program CPT 690) is one of four capital projects that is related to CSP. These projects are reported for background information as needed outside of the main body of the Project Monthly Progress Report.
3	The Chinatown Plaza (CPT 718) is for Chinatown Station enhancement capital project. The project has funding outside of the Central Subway Project. The construction is carried out in Contract 1300 Contract Modifications.
4	CPT 665 is a Real Estate project to relocation in compliance with California regulations for business relocations but outside of the Central Subway Project as defined by the FTA FFGA.
5	CPT 705 is an SFMTA capital improvement between the Agency and community stakeholders outside of the Central Subway Project.
6	Utility company reimbursements (Form B) result in funds received for work carried out on behalf of utilities concurrent to CSP work to achieve efficiencies.
7	PG&E Power Feed reimbursement funds are the refunds from PG&E when completion of Stations construction and switch to permanent power.
8	BART Elevator funds are reimbursements for work carried out on behalf of BART to install BART Powell Street Station elevator.
9	The Tutor Perini - CAD Files funds are the result of payments by the contractor for project documentation not included in the contract.
10	SFPUC Sewer Main funds are reimbursements for work carried out on behalf of San Francisco PUC (includes 10% construction contingency).
11	Traffic Effectiveness Project funded Contract Modification #40 for Culvert, Street & Sidewalk Restoration in North Beach are reimbursements for work carried out in Contract 1252 on behalf of SFMTA SSD.
12	SFPUC 24" Water Main funds are reimbursements for work carried out in Contract 1252 Contract Modification #41 on behalf of San Francisco PUC (includes construction management cost).
13	SFPUC North Beach 24" Water Main Additional Work funds are reimbursements for work carried out in Contract 1252 Contract Modification #48 on behalf of San Francisco PUC (includes construction management cost).
14	CN1300 Contract Modification #6 is funded by Chinatown Plaza (CPT 718) project.
15	Traffic Effectiveness Project funded Contract Modification #51 for support for North Beach Restoration, OCS and Streetlighting which are reimbursements for work carried out in Contract 1252.
16	Public Works' funds are for reimbursements for work carried out on behalf of Public Works MOU for Water Line above YBM Station.
17	SFWD funded Contract Modification #60 for 8' water line at the intersection of Fourth and Jessie Street which are reimbursements for work carried out in Contract 1252.

7.4 Contingency Management Trend Report	
18	In Oct 2014 Report, updated Contract 1250 contract cost to segregate contract amount and contract modification amount. Note that September 2013 Supplemental Authorized Contingency "column f" did not include completed contract.
19	In Oct 2014 Report, updated Contract 1251 contract cost to segregate contract amount and contract modification amount. Note that September 2013 Supplemental Authorized Contingency "column f" did not include completed contract.
20	Contract 1252 Original Contract Value "column a" and Original Contingency "column f" did not match September 2013 Supplemental due to Supplemental were used the revised value to reflect Contract Modifications #3-#18. Reduced Contract 1252 contingency to reflect CMod #20 for retrieval shaft relocation cost \$5.15M funded by CPT690, CMod #40 for Culvert, Street & Sidewalk Restoration cost \$694,651 funded by Traffic Effectiveness Project (TEP), and CMod #41 for install 24" Water Main in North Beach cost \$328,860 funded by SFPUC. In August 2015 report, release \$15M CN1252 Tunnel assigned contingency to program unallocated contingency. In March 2106 report, reduced Contract 1252 contingency by \$377,435 cost to reflect certification of five CMODS. CMod#49, #52 and #53 total \$221,967 are funded by CPS. CMod#51 Support for North Beach Restoration, OCS and Streetlighting cost of \$155,468 is being funded by TEP. Released \$155,468 CN1252 allocated contingency to program's unallocated contingency. In May 2016 report, reduced Contract 1252 contingency by \$185,913 cost to reflect certification of two CMODS. In July 2016 report, increased Contract 1252 contingency by \$15,259 cost to reflect certification of one CMOD. In October 2016 report, increased Contract 1252 contingency by \$319,658 to reflect certification of three credit CMODs. In March 2018 report, increased Contract 1252 contingency by \$131,715 cost to reflect certification of two CMODS.
21	BART Elevator scope and SFPUC Sewer Main scope is in Contract 1300; effort will be funded by BART. In January 2015 Report, corrected Station Contract value to match awarded amount.
22	In March 2016 Report, lowered Contract 1300 Stations CTS contingency by \$75,000 because Contract Modification #6 was funded by Project CPT718. In Nov 2016 report, reversed moving contingency.
23	In December 2017 Report, \$1,060,000 Job Readiness Budget is shown as an approved change in Column "b".
24	In December 2017 Report, there is a change in Column "f" and Column "h" to reflect reporting to include CN1250 and CN1251. Prior to this, Column "f" and Column "h" reporting excluded CN1250 and CN1251.
25	In April 2015 report, real estate budget stated in RAMP Rev5 is \$36.7M, including \$1M contingency. The cost workbook ROW & contingency budget reflects this with \$36,511,799 and \$1,000,000 respectively. Revised cost book ROW budget & contingency to be \$37,511,799. The \$4,265,478 Caltrans lease savings is allocated to ROW allocated contingency. In February 2017, released \$5,265,478 from completed phase Real Estate assigned contingency to program unallocated contingency.
26	In Dec 2014 Report, redistributed LRV budget to reflect recent firm bid cost per vehicle (\$3,327,250/unit) from vehicle procurement contract award. (SFMTA Board meeting 15JUL14, calendar item #11). Vehicle line item total budget remains unchanged, redistributed fund by reducing base amount to \$13,309,000, column "c" and increased allocated contingency column "h", by same amount.

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26a	In July 2018 Report, increased SCC 80 Professional Services category budget by \$2,263,498 due to additional costs related to CN1300 stations; cost was transferred from program unallocated contingency.
27	In Oct 2014 Report, made two corrections: i) revised Professional Services, Original Contract Value "column a" from \$310,518,041 to \$310,618,041, ii) revised Original Cogency. "column f" unallocated contingency from \$3,883,481 to \$3,845,945. In April 2015 report, used \$500K program contingency for CS-175 Bayland Soil Process contract. In August 2015 Report, added \$15M from Contract 1252. In March 2016 Report, the \$155,468 costs funded by other project offset credits added to program's unallocated contingency. In August 2016 Report, used \$15M to UMS contingency and \$5M to CTS contingency. In February 2017, increased \$5,265,478 from real estate contingency to program unallocated contingency and used \$1M for CN1300 Job Readiness Program contract. In July 2018 report, used \$2,263,498 to increase SCC 80 Professional Services category regarding matters related to stations from program unallocated contingency.
28	The total Central Subway Project budget of \$1.578 billion, based on the October 2012 FFGA with the FTA, is the primary MPR report reference.
29	Estimate at Completion is shown at Column "e".
30	Estimate at Completion vs. Budget variance is shown at Column "k".
7.5 Contract Modification/Trend Log - Contract 1300 Stations	
31	Reported all trend cost for Contractor Change Order Requests and Proposed Contract Change and applied probability to forecasted trends. In April 2016 Report, reviewed probability formula and adjusted cost. In May 2017, updated probability formula and adjusted cost.
7.6 Budget Revisions: Report Sorted by Construction Packages	
32	In Dec 2014 Report, reduced CN1252 allocated contingency by \$28K to execute Contract Modification #46, see Report 7.5 Detail Contingency Usage Report. In August 2015 report, release \$15M CN1252 Tunnel assigned contingency to program unallocated contingency. In March 2016 report, five contract modifications certified totaling \$377,435 of which \$155,468 is using another source of funding. Released \$155,648 from CN1252 Tunnel assigned contingency to program unallocated contingency. In May 2016 Report, reduced CN1252 allocated contingency by \$186K to execute Contract Modification #54 and #55, see Report 7.5 Detail Contingency Usage Report. In October 2016 Report, increased CN1252 allocated contingency by \$319,658 to execute three contract modifications (#57, #58 and #59), see Report 7.5 Detail Contingency Usage Report. In March 2018 Report, increased CN1252 allocated contingency by \$131,715 to execute two contract modifications (#61 and #62), see Report 7.5 Detail Contingency Usage Report.
33	In February 2017 report, initiated budget from program unallocated contingencies for CN1300 Job Readiness Program. CN1300 Job Readiness Program budget was part of CN1300 base value, a deduction contract modification will lower CN1300 contract value.
34	In April 2015, initiated budget from program unallocated contingencies for CS-175 Bayland Soil Process contract, refer to Note 20.
35	In February 2017, released completed phase real estate assigned contingency \$5,265,478 to program unallocated contingency.

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36	In Dec 2014 Report, redistributed LRV budget to reflect recent firm bid cost per vehicle (\$3,327,250/unit) from vehicle procurement contract award. (SFMTA Board meeting 15JUL14, calendar item #11). Vehicle line item total budget remains unchanged, redistributed fund by reducing base amount to \$13,309,000 and increased allocated contingency by same amount.
37	In October 2016 report, 1252 program contingency increased by \$319,658 due to execution of three contract modifications as credit offsets. In November 2016 report, took away \$75,000 funding from program's unallocated contingency and moved to CTS allocated contingency. In February 2017 report, initiated budget from program unallocated contingencies for CN1300 Job Readiness Program. CN1300 Job Readiness Program budget was part of CN1300 base value, a deduction contract modification will lower CN1300 contract value. Also released \$5,265,478 assigned real estate contingency to program unallocated contingency. In June 2017, initiated budget from Contract 1251's contract value (true final administrative close out cost) to program unallocated contingency, a deduction contract modification that lowered CN1251's contract value by \$125,501. In March 2018 report, 1252 program contingency increased by \$131,715 due to execution of two contract modifications as credit offsets. In July 2018, increased SCC category Professional Services in 80.04 Construction Management by \$2,263,498 by reducing program unallocated contingency.
38	In April 2015 report, program contingency decreased by \$500,000. In August 2015 report, release \$15M CN1252 Tunnel assigned contingency to program unallocated contingency. In March 2016 report, released \$155,468 from Contract 1252 Tunnel assigned contingency and \$75,000 from Contract 1300 Stations assigned contingency totaling \$230,956. In August 2016, released a total of \$20M unassigned contingency to assigned contingency; \$15M to CN1300 UMS station and \$5M to CTS station. In February 2017 report, initiated budget from program unallocated contingencies for CN1300 Job Readiness Program. CN1300 Job Readiness Program budget was part of CN1300 base value, a deduction contract modification will lower CN1300 contract value. Also released \$5,265,478 assigned real estate contingency to program unallocated contingency. In July 2017, increased program unallocated contingency by \$125,501 due to CN1251's revised contract value. In July 2018, reduced program unallocated contingency by \$2,263,498 to fund additional costs for SCC category Professional Services in 80.04 Construction Management.
7.7 Budget Expenditures by SCC Codes	
39	In March 2017, added new columns for "Supplemental 2013 Budget" and "Remaining Budget". In April, added new column for "Contingency". In May 2017, added new column for "Report Note". In May 2017, breakdown the combined SCC codes 10 to 50 into individual row for 10, 20, 40, 50 categories. Assigned SCC code to all CN1300 potential changes. Contract 1300 Station assigned contingency SCC are 20.01 and 20.03. The budget transfer is using assigned contingency to process contract modifications. In June 2017, adjusted and realigned SCC codes. In July 2018, the budget transfer is using SCC 90 program unallocated contingency to process an increase in budget for category SCC 80.03-90.04 PM For Design & Construction.
7.9 Detail Monthly Expenditure Report	
Phase 1 Preliminary Engineering	
40	In February 2017, line item budget was adjusted to line-up expenditures. Famis cost for Preliminary Engineering (PE) is \$48,210,903.71. Cost Report for Preliminary Engineering (PE) is \$46,542,060. Some Design cost reported in Famis were moved to Design Phase.

Phase 2 Design Phase	
41	Famis cost adjustment to transfer Project Management cost from July 2013 to Phase 3 Construction Phase.
42	Famis Phase 1 PE Index Code: 357906.CPT5441112 cost is \$10,222,939 \$8,949,300 is reported in Cost Report Phase 1 PE and the balance of \$1,273,639 is reported in Phase 2 Design.
43	1.2.021.01.080.03 - FD:CTYCO-ARTS COMMISSION [357909ART001.CPT5441227]: FAMIS: \$1,425,167 Cost Report: \$1,425,167 cost is reported in Phase 2 Design, 1.2.021.01.080.03 Cost Transfer: Remaining cost is reported in Phase 3 Construction, 1.3.021.01.080.03 - ARTS:CTYCO-ARTS COMMISSION [357909ART001.CPT5441227]
44	In December 2016 Report, Central Subway Project has re-activated CSA Audit Work Order to perform overhead audit for three consultant forms.
45	1.2.055.01.080.02 - FD:ODCs - 651 BRANNAN STREET [35CPT5441241.CPT5441241]: FAMIS: \$2,294,910 Cost Report: \$2,294,910 1.2.055.01.080.02 Cost Transfer: Future costs to be allocated to 1.3.055.01.080.02 - FD:ODCs - 651 BRANNAN STREET [35CPT5441241.CPT5441241]
46	1.2.063.01.080.03 - AECOM.CS149 OM-EPC JV CS149-PM [68CPT544133D.CPT544133D]: FAMIS: \$4,698,167 Cost Report: \$4,698,167 on 1.2.063.01.080.03 Cost Transfer: Future costs to 1.3.063.01.080.03 - AECOM.CS149 OM-EPC JV CS149-PM [68CPT544133D.CPT544133D]
47	AVA Cost \$395,204 is reported in Phase 2 Final Design 1.2.066.01.080.03
48	In January 2017 Report, remove variance amount of (\$920,555) that was incorrectly reported in August 2016.
49	1.2.071.01.080.02 - FD:FINAL DESIGN-DP1 [35CPT5441232.CPT5441232]: FAMIS: \$5,608,147 Cost Report: \$5,469,336 Cost Transfer: \$138,811 to 1.3.071.01.080.04 - FD:FINAL DESIGN-DP1 [35CPT5441232.CPT5441232]
50	1.2.072.01.080.02 - FD:FINAL DESIGN-DP2 [35CPT5441233.CPT5441233]: FAMIS: \$26,268,511 COST REPORT: \$26,220,609 COST TRANSFER: \$47,902 to 1.3.072.01.080.04 - FD:FINAL DESIGN-DP2 [35CPT5441233.CPT5441233]
51	1.2.073.01.080.02 - FD:FINAL DESIGN-DP3 [35CPT5441236.CPT5441236]: FAMIS: \$11,502,372 COST REPORT: \$11,432,312 COST TRANSFER: \$70,060 to 1.3.073.01.080.04 - CM: DP3 [35CPT5441236.CPT5441236]

Phase 3 Construction Phase	
52	1.3.021.01.080.03 - ARTS:CTYCO-ARTS COMMISSION [357909ART001.CPT5441227]: FAMIS: \$1,525,982 Cost Report: \$1,425,167 1.2.021.01.080.03 Cost Transfer: any future costs to 1.3.021.01.080.03
53	In January 2017 Report, revised SCC Code from 1.2.032.02.080.02 -1424J-BOE LABOR [PWE1X5MPFUNA.CPT544112B112] to 1.3.032.06.080.04 to correct incorrect SCC assignment for DPW support to construction phase.
54	In January 2017 Report, revised SCC Code from 1.2.032.02.080.02 -1424J-BOE LABOR [PWE1X5MPFUNA.CPT544112C112] to 1.3.032.06.080.04 to correct incorrect SCC assignment for DPW support to construction phase.
55	In January 2017 Report, revised SCC Code from 1.2.032.02.080.02 -1424J-BOE LABOR [PWE1X5MPFUNA.CPT544112D112] to 1.3.032.06.080.04 to correct incorrect SCC assignment for DPW support to construction phase.
56	In January 2017 Report, revised SCC Code from 1.2.032.02.080.02 -1424J-BOE LABOR [PWE1X5MPFUNA.CPT544112E112] to 1.3.032.06.080.04 to correct incorrect SCC assignment for DPW support to construction phase.
57	In January 2017 Report, revised SCC Code from 1.2.032.02.080.02 -1424J-BOE LABOR [PWE1X5MPFUNA.CPT544112F112] to 1.3.032.06.080.04 to correct incorrect SCC assignment for DPW support to construction phase.
58	In January 2017 Report, revised SCC Code from 1.2.032.02.080.02 -1424J-BOE LABOR [PWE1X5MPFUNA.CPT544112G112] to 1.3.032.06.080.04 to correct incorrect SCC assignment for DPW support to construction phase.
59	1.3.055.01.080.02 - FD:ODCs - 651 BRANNAN STREET [35CPT5441241.CPT5441241]: FAMIS: \$2,294,910 Cost Report: \$2,294,910 1.2.055.01.080.02 - FD:ODCs - 651 BRANNAN STREET [35CPT5441241.CPT5441241] Cost Transfer: Future costs to be allocated to 1.3.055.01.080.02
60	1.3.063.01.080.03 - AECOM.CS149 OM-EPC JV CS149-PM [68CPT544133D.CPT544133D]: FAMIS: \$4,698,167 Cost Report: \$4,698,167 on 1.2.063.01.080.03 Cost Transfer: Future costs to 1.3.063.01.080.03 - AECOM.CS149 OM-EPC JV CS149-PM [68CPT544133D.CPT544133D]
61	In February 2017, transferred \$1,060,000 from programs unallocated contingency to initiate CN1300 JOB READINESS contracts, (cost account code 1.3.064.06.040.08). A deductive Construction Modification to CN1300 will process.
62	Used \$500K program contingency for CS-175 Bayland Soil Process contract. Refer to Report Notes #20.
63	1.3.071.01.080.04 - FD:FINAL DESIGN-DP1 [35CPT5441232.CPT5441232]: FAMIS: \$5,608,147 Cost Report: \$5,469,336 Cost Transfer: \$138,811 to 1.3.071.01.080.04 - FD:FINAL DESIGN-DP1 [35CPT5441232.CPT5441232]

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64	1.3.072.01.080.04 - FD:FINAL DESIGN-DP2 [35CPT5441233.CPT5441233]: FAMIS: \$26,268,511 COST REPORT: \$26,220,609 COST TRANSFER: \$47,902 to 1.3.072.01.080.04 - FD:FINAL DESIGN-DP2 [35CPT5441233.CPT5441233]
65	Contract 1251 Final cost is \$20,794,582.
66	In March 2016, July 2016 and October 2016, contract 1252 modifications budget and actuals have been realigned and adjusted to reflect actuals costs.
67	In March 2016, July 2016 and October 2016, contract 1252 modifications budget and actuals have been realigned and adjusted to reflect actuals costs.
68	Revised Contract 1252 allocated contingency SCC code from 040.08 to 010.07.
69	In July 2015 Report, used Contract 1300 Contractor schedule to report budget and actual cost. The Standard Cost Categories (SCC) allocation changed from previous reports. In August 2015 Report, adjusted some of Contract 1300 Contractor SCC assignment to match most of previous SCC assignment. In March 2016, \$75,000 Cmod#6 subtracted from CN1300 Stations contingency (using CPT718 funding) and transferred to Program contingency; this lead to the total CN1300 Station budget being lowered.
70	Revised Contract 1300/UMS allocated contingency SCC code from 040.08 to 020.03.
71	In March 2016 Report, reduced Contract 1252 contingency by \$377,435 cost to reflect certification of five CMODS.
72	Revised Contract 1300/CTS allocated contingency SCC code from 040.08 to 020.03.
73	Negative Current or Prior Monthly expenditure is due to replenish allowance expenses by approved Contract Modifications.
74	Revised Contract 1300/YBM allocated contingency SCC code from 040.08 to 020.03.
75	Revised Contract 1300/STS allocated contingency SCC code from 040.08 to 020.01.
76	Revised Form B Reimbursements SCC code from 900.01 to 040.02
77	Revised Form B Reimbursements SCC code from 900.01 to 040.02
78	Revised Form B Reimbursements SCC code from 900.01 to 040.02
79	Revised Form B Reimbursements SCC code from 900.01 to 040.02
80	Revised Form B Reimbursements SCC code from 900.01 to 040.02
81	Revised Form B Reimbursements SCC code from 900.01 to 040.02
82	Revised Form B Reimbursements SCC code from 900.01 to 040.02
83	Revised Form B Reimbursements SCC code from 900.01 to 040.02
84	Increase Program contingency \$1,023,508. Refer to Report Notes #11 and #12. In April 2015 report, program contingency decreased by \$500,000. Refer to Report Notes #20. In August 2015 report, release \$15M CN1252 Tunnel assigned contingency to program unallocated contingency. In March 2016 report, program unallocated contingency increased by \$230,468. In August 2016, released \$20M to CN1300 Construction assigned contingency from program unallocated contingency. In February 2017, used \$1,060,000 for CN1300 Job Readiness Program from unallocated contingency, refer to Note 30. Also, released \$5,265,478 assigned real estate contingency to program unallocated contingency, refer to Note 27. In July 2018 report, used \$2,263,498 to fund SCC 80 Professional Services category regarding matters related to stations from program unallocated contingency.

Appendix B

DETAIL SCHEDULE REPORTS

SCHEDULE HIGHLIGHTS

The Master Project Schedule (MPS) below includes progress through November 2018. The November 2018 Schedule Update submittal from Contract 1300 Contractor was not submitted as the CN1300 Contractor has not provided the updated corrections to their June, through May 2018 Schedule Updates. The Contract 1300 schedule represented in this report is based on the SFMTA November 2018 Schedule Update.

The MPS shows a forecast Revenue Service Date of December 2019.

The controlling critical (longest) path of the MPS runs through the tunnel Concrete work, followed by Electrical activities, STS Startup & Testing, Commissioning and Pre-Revenue Activities to the Baseline Finish and Revenue Service Date. See Appendix B – Longest Path. The latest schedule shows the longest path running through the Chinatown Station (CTS). Contractor is required to implement a Recovery Schedule to put the Project back on schedule.

Schedule Contingency is fully utilized on the critical path of the MPS, which is below the Minimum Schedule Contingency level of 6 months. A schedule re-evaluation will be performed, utilizing the updated Contract 1300 Schedule. Recovery options are being implemented in key areas as work proceeds. SFMTA continues to meet with Contractor to discuss all schedule concerns and comments. Contract 1300 Schedule shows a delay in November due to Low productivity on electrical work within the tunnels caused by reduced number of resources on site has delayed the project with a new forecasted Revenue Service Date of 26 December 2019.

Contract 1300 Contractor submitted forty five (45) Schedule Updates from December 2014 to October 2018. SFMTA rejected twenty seven (27) Schedule Updates from January 2016 to April 2016 and June 2016 to May 2018 due to multiple and repetitive issues that vary from incorrect working sequences to unrealistic forecasted completion dates to artificially steering the schedule longest path through certain portions of the project. SFMTA approved as noted December 2014 through December 2015, and May 2016 Schedule Updates. Contractor has been directed to develop a Recovery Schedule as required by Contract to mitigate the current forecasted project delay. The 18 month “gap” of missing Schedule Updates at the beginning of the job has interfered with efficient resolution of Contractor’s assertions of Unavoidable Delay to the project-wide Substantial Completion date, which is additionally impacting the Contractor’s review of options for schedule recovery.

Contract 1300 - WP1253 UMS / WP1254R CTS / WP1255 YBM / WP1256 STS:

The Contractor, Tutor Perini Corporation’s (TPC) baseline schedule is incorporated into the master program schedule. The preliminary SFMTA Contract 1300 November 2018 schedule is used within the November Report. The SFMTA Contract 1300 November 2018 schedule is based on the approved baseline schedule logic with adjustments made for fixing retained logic and lags. The SFMTA will continue to use the SFMTA Contract 1300 schedule update as a forecasting tool going forward until the Contract 1300 Contractor submits an acceptable schedule that addresses all of SFMTA scheduling concerns.

Work Package P-1254R (CTS) has performed the following work this month:

- Completed construction of track slab at Crossover Cavern
- Completed contact grouting for Crossover Cavern arches
- Continue construction of guideway stem walls at South Platform Cavern
- Began construction of guideway stem walls at North Platform Cavern
- Continued placing concrete for Crosscut Cavern headwall
- Continued installing rebar for North Egress shaft
- Continued installing electrical and plumbing for Concourse slab at Headhouse
- Continued placing concrete for Concourse slab at Headhouse
- Continued installing rebar and formwork for Intermediate slab at Headhouse
- Continued removing temp level 5 struts and wales
- Began prep work for initial shotcrete layer at Slurry Walls for Underplatform Level at Headhouse
- Began construction of track slab in Platform Cavern
- Continued street work (minor), ongoing monitoring and surveying

Work Package P-1254R (CTS) will perform the following work next month:

- Complete waterproofing installation for South headwall
- Complete construction of guideway stem walls at South Platform Cavern
- Continue stem wall construction and placing concrete for track slab at South Platform Cavern
- Continue installing rebar and formwork for Crosscut Cavern headwall
- Begin installing rebar for Crosscut Cavern Arch final lining
- Complete construction of guideway stem walls at North Platform Cavern
- Begin stem wall construction for track slab at North Platform Cavern
- Begin shotcrete application for North Egress shaft
- Begin constructing formwork and installing rebar for North Egress upper lid
- Complete installing rebar, electrical, plumbing, and concrete placement for Concourse slab at Headhouse
- Complete removing temp level 5 struts and wales
- Begin installing rebar, electrical, and plumbing for Intermediate slab at Headhouse
- Begin placing concrete for Intermediate slab at Headhouse
- Begin shotcrete for slurry walls, install drain mat and waterproofing for Under Platform level at Headhouse

Activity ID	Activity Name	2018			2019			
		Oct	Nov	Dec	Jan	Feb	Mar	Apr
CENTRAL SUBWAY PROJECT								
Construction Phase								
Construction CN-1300								
Construction CTS Station P-1254R								
Administrative / Milestones								
Preconstruction								
Site Work / Utility Relocation								
Demolition								
Excavation & Support								
Tunnel / Cavern Mining								
Cavern Lining								
Concrete/Shotcrete								
Entrance Roof Level								
Surface Level								
Upper Mezz Level								
Lower Mezz Level								
Concourse Level								
Intermediate Strut Level								
CTS.03.30.130	Form/Rebar/Pour - Intermediate Level Slab							
CTS.03.30.085	Form/Rebar/Pour/Strip - Columns Intermediate Slab to Lower Mezz Level							
CTS.03.30.140	Concrete Cure - Intermediate Level Slab							
CTS.03.37.110	CTS_IM - Place - Shotcrete on Slurry Walls - Head House							
CTS.07.13.150	CTS_IM - Install - Drain Mat & Waterproofing on Shotcrete Walls - Head House							
CTS.03.30.097	Form/Rebar/Pour/Strip - Perimeter Walls Intermediate Slab to Lower Mezz Level							
Platform Level								
CTS.03.30.785	Rebar/Form/ Pour Corbels & Walls - Crosscut Cavern							
CTS.03.30.565	F/R/P/S Walls Under Track Slab - Platform Cavern South							
CTS.03.30.590	Form/Rebar/Pour Platform Stem Walls - Platform Cavern South							
CTS.03.30.600	Shore/Form/Rebar/Pour Track Slab - Platform Cavern South							
CTS.03.30.640	F/R/P/S Walls Under Track Slab - Platform Cavern North							
CTS.03.30.670	Shore/Form/Rebar/Pour Track Slab - Platform Cavern North							
CTS.03.30.660	Form/Rebar/Pour Platform Stem Walls - Platform Cavern North							
CTS.03.30.760	Shore/Rebar/Form Pour Track Slab - Crosscut Cavern							
CTS.03.30.790	Form/Rebar/Pour Platform Slab - Platform Cavern South							
CTS.03.3.0740	Form/Rebar/Pour Platform Slab - Platform Cavern North							
CTS.03.30.850	Concrete Stairs North Emergency Egress Tunnel							
CTS.03.30.260	Pour Equipment Pads - Main Electrical & Traction Power Rooms							
CTS.03.30.610	Form/Rebar/Pour Stair, Room & End Walls - Platform Cavern South							
CTS.03.30.620	Form/Rebar/Pour Stair & Room Enclosure Lid - Platform Cavern South							
CTS.03.30.680	Form/Rebar/Pour Stair, Room & End Walls - Platform Cavern North							

Work Package P-1253 (UMS) has performed the following work this month:

- Ellis St to Geary St on Stockton Street: Completed installation of water main/laterals. Constructed new sidewalks and asphalt pavement roadway. Constructed new and temporary lightings. Constructed temporary traffic signals.
- Platform Station: Continued construction of stairs and elevators. Continued installation of stem walls and platform deck. Installed CMU walls, placed concrete for metal decks and wale encasements at multiple levels.
- North Concourse: Continued installation of CMU walls. Continued installation of fire sprinklers and MEP on Concourse and Fan levels.
- South Concourse: Continued installation of fire sprinklers and MEP
- Winter Walk Stockton Street: Implemented 2018 Holiday Moratorium Winter Walk

Work Package P-1253 (UMS) will perform the following work next month:

- Platform Station: Continue construction of CMU Walls on Mezzanine and Intermediate Strut levels. Continue installation of stairs and elevators.
- North Concourse: Complete construction of CMU walls on Fan level. Complete installation of fire sprinklers on Fan and Concourse level. Installation of Fans on Intermediate Strut level. Complete construction of equipment pads in Main Electrical Room.
- South Concourse: Continue installation of MEP. Continue installation of walls
- Winter Walk Stockton Street continues

Activity ID	Activity Name	2018			2019			
		Oct	Nov	Dec	Jan	Feb	Mar	Apr
CENTRAL SUBWAY PROJECT								
Construction Phase								
Construction CN-1300								
Construction UMS Station P-1253								
Administrative / Milestones								
Preconstruction								
Engineering & Procurement								
Drilled Shafts								
Roof Deck Excavation, Construction, Restoration								
Excavation & Support								
Concrete/Shotcrete								
Structural Steel								
Masonry								
Mechanical								
Surface Level								
All Levels								
Concourse Level								
UMS.22.13.0120	UMS_CN07 - Connect Surface Drainage to Existing 8" Sewer Lateral.							
UMS.23.31.0245	UMS_CN_Install HVAC Exhaust Main Exhaust Air Plenum To EXF-002							
UMS.23.31.0275	UMS_CN_Install HVAC Exhaust Main- EXF-002 To Risers @ A/16							
UMS.23.31.0295	UMS_CN_Install HVAC Supply Main SF-002 to Risers @ A/16							
UMS.23.31.0305	UMS_CN_09 Install HVAC Duct Branches Mech Rm							
UMS.22.11.0150	UMS_CN09_Install DCW - Sector 2							
UMS.23.74.0945	UMS_CN_Install Refrigerant Rack Piping - ACCU's to Chase @ A/16							
UMS.21.13.0365	UMS_CN_Install Sprinkler Branches & Drops - Concourse Section 6							
UMS.22.13.0140	UMS_Install Trench Drains SS Grates- North Concourse Level							
UMS.23.74.0755	UMS_CN_Install Refrigerant Piping - ACCU #1 to Branch Selector Unit #12							
UMS.22.16.0110	UMS_CN_Install Air Replenishment Piping (AP & APT) - Sectors 1-6							
UMS.21.13.0265	UMS_CN_Install 4" & 6" Sprinkler Mains - Concourse Section 3							
UMS.21.13.0285	UMS_CN_Install Sprinkler Branches & Drops - Concourse Section 3							
UMS.22.11.0220	UMS_CN_Install Sump Pump							
UMS.23.34.0225	UMS_CN_Install HVAC Exhaust Fan EXF-001							
UMS.23.34.0265	UMS_CN_Install HVAC Supply Fan SF-002							
UMS.23.31.0235	UMS_CN_Install HVAC Exhaust Main EXF-001 to Exhaust Air Plenum							
UMS.23.88.0100	UMS_ISL_Install Emergency Ventilation Fan Assembly TV-26/SA-26B/SA-26A							
UMS.21.13.0246	UMS_CN_Install Pre-Action Fire Suppression System Piping & Detectors - Main I							
UMS.21.13.0295	UMS_CN_Install 4" & 6" Sprinkler Mains - Concourse Section 4							
UMS.21.13.0305	UMS_CN_Install Sprinkler Branches & Drops - Concourse Section 4							
UMS.23.88.0110	UMS_ISL_Install Emergency Ventilation Fan Assembly TV-25/SA-25B/SA-25A							
UMS.23.88.0120	UMS_ISL_Install Emergency Ventilation Fan Assembly TV-24/SA-23b/SA-24A							
UMS.23.34.0215	UMS_CN_Install HVAC Supply Fan SF-001							

Work Package P-1255 (YBM) has performed the following work this month:

- Continued re-establishing utilities at 4th & Howard.
- Installed catch basin and culvert at 4th and Clementina
- Backfilled PG&E #7 boxes at Folsom

- Completed installing Stair 4
- Continued installing Stair 6
- Continued installing Stair 7
- Continued F/R/P of Headhouse Undersurface stairs
- Continued installing Headhouse Vent Shaft
- Continued F/R/P of Headhouse Mezzanine walls
- Continued electrical work in Headhouse Concourse
- Continued installing granite art panels in Station Concourse
- Continued installing crystallized glass panels in Station Concourse
- Continued F/R/P of seismic joints (Station Platform)

Work Package P-1255 (YBM) will perform the following work next month:

- Continue re-establishing utilities at 4th & Howard
- Begin pavement renovations at 4th and Howard
- Complete installing Stairs 1, 5, and 6
- Complete F/R/P of Headhouse Surface Slab 9.4 to 8.6 line
- Begin F/R/P of Elevator 3 and 4 walls at Headhouse Surface Level
- Complete F/R/P of Headhouse Undersurface stairs
- Continue F/R/P of walls of Headhouse Vent Shaft
- Continue F/R/P of Headhouse Mezzanine walls
- Continue waterproofing of Headhouse Mezzanine
- Continue installing Granite Art Work panels (Station Concourse)
- Continue installing crystallized glass panels (Station Concourse)
- Continue installing luminous glass ceiling panels (Station Concourse)
- Apply dielectric coating to Headhouse Invert slab
- Continue F/R/P of seismic joints (Station Platform)

Activity ID	Activity Name	2018			2019			
		Oct	Nov	Dec	Jan	Feb	Mar	Apr
CENTRAL SUBWAY PROJECT								
Construction Phase								
Construction CN-1300								
Construction YBM Station P-1255								
Preconstruction								
Excavation & Support								
Concrete/Shotcrete								
Electrical								
Concourse Level								
Mezzanine Level								
Platform Level								
YBM.26.05.1076	YBM_PL 207 - Aux Elect Rm: Install - System Ground Bus							
YBM.26.05.1066	YBM_PL 106 - Aux Elect Rm: Install - System Ground Bus							
YBM.26.11.0940	YBM_IV301 - Main Elect Rm: Install Riser Conduit In Chase To Mezz Mech Rm							
YBM.26.24.1350	YBM_PL 207 - Aux Elect Rm: Install Raceway - Panelboard 1SHL							
YBM.26.24.1320	YBM_PL 207 - Aux Elect Rm: Install - Panelboard 1SHP (NB/SB Tunnel Power - F							
YBM.26.24.1190	YBM_PL 106 - Aux Elect Rm: Install - Panelboard E1NP (Receptacles)							
YBM.26.24.1040	YBM_PL 106 - Aux Elect Rm: Install - Panelboard E1NHL (NB/SB Tunnel Lighting							
YBM.26.24.1070	YBM_PL 207 - Aux Elect Rm: Install - Panelboard E1SP (Receptacles)							
YBM.26.24.1220	YBM_PL 106 - Aux Elect Rm: Install - Panelboard 1NHP (NB/SN Tunnel - Future)							
YBM.34.05.0200	YBM_PL_Install Conduit PB01 to SB Feeder Boxes PS03 & PS06 (Traction Power							
YBM.34.05.0210	YBM_PL_Install Conduit PB02 to NB Feeder Box To PS01& PS05 (Traction Power							
YBM.34.05.0240	YBM_PL_Install Conduit NB Positive Feeder Box PS01 to PS04 (Traction Power)							
YBM.34.05.0220	YBM_PL_Install Conduit SB Positive Feeder Box PS02 to PS06 (Traction Power)							
YBM.26.24.1310	YBM_PL 207 - Aux Elect Rm: Terminations - Panelboard 1SP1							
YBM.26.24.1060	YBM_PL 106 - Aux Elect Rm: Terminations - Panelboard 1NHP							
YBM.26.24.1180	YBM_PL 106 - Aux Elect Rm: Terminations - Panelboard LCP-1N (Lighting Contro							
YBM.26.24.1430	YBM_PL 207 - Aux Elect Rm: Terminations - Panelboard LCP-1S							
YBM.26.24.1210	YBM_PL 106 - Aux Elect Rm: Terminations - Panelboard E1NP (Receptacles)							
YBM.26.12.0660	YBM_IV 301-Main Electrical Rm: Install - Transformer T-4 & Disconnect Switch							
YBM.26.24.1400	YBM_PL 207 - Aux Elect Rm: Terminations - Panelboard E1SHL							
YBM.26.12.0630	YBM_PL 207 - Aux Elect Rm: Terminations - Transformer T-9 & Disconnect Switcl							
YBM.26.24.1100	YBM_PL 106 - Aux Elect Rm: Terminations - Panelboard 1NHL							
YBM.26.24.1130	YBM_PL 106 - Aux Elect Rm: Terminations - Panelboard E1NHL							
YBM.26.24.1150	YBM_PL 207 - Aux Elect Rm: Terminations - Panelboard E1SP							
YBM.26.24.1360	YBM_PL 207 - Aux Elect Rm: Terminations - Panelboard 1SHP							
YBM.26.24.1370	YBM_PL 207 - Aux Elect Rm: Terminations - Panelboard 1SHL							
YBM.26.24.1240	YBM_PL 106 - Aux Elect Rm: Terminations - Panelboard 1NHP							
YBM.34.22.0320	YBM_PL 001_Terminate NB Cable Positive Feeder Box PS05 (From PB-02)							
YBM.34.22.0300	YBM_PL 001_Terminate SB Cable - Positive Feeder Box PS06 (From PB-01)							
YBM.34.22.0280	YBM_PL 001_Terminate SB Cable - Positive Feeder Box PS02 to PS06							

Work Package P-1256 (STS) has performed the following work this month:

- Continued traction power conduit installation inside tunnel
- Continued installing street light pole foundations on Townsend
- Continued sidewalk and curb ramp pavement renovation at 4th/Bryant
- Continued 4th/Brannan platform construction
- Ongoing strut bracket installation in both tubes working South to North
- Continued plinth construction in tunnel
- Completed track slab construction at CTS Crossover Cavern

Work Package P-1256 (STS) will perform the following work next month:

- Continue 4th/Brannan platform construction
- Continue traction power conduit installation inside tunnel
- Continue installing street light pole foundations on Townsend
- Continue sidewalk and curb ramp pavement renovation at 4th/Bryant
- Continue plinth construction in tunnel

Activity ID	Activity Name	2018			2019			
		Oct	Nov	Dec	Jan	Feb	Mar	Apr
CENTRAL SUBWAY PROJECT								
Construction Phase								
Construction CN-1300								
Construction STS P-1256								
Preconstruction								
Concrete/Shotcrete								
Tunnel Concrete								
Structural Steel								
Electrical								
Electrical - Transportation								
Trackwork								
Platform Level								
STS.32.13.960	STS_R/F/P Phase 2 4th Street 2" A/C Wearing Surface - At Brannan St Intersecti							
STS.32.13.1500	STS_Prepare Phase 1 4th Street Subgrade - At Bryant St Intersection							
STS.32.13.1510	STS_R/F/P Phase 1 4th Street Curbs & Gutters - At Bryant St Intersection							
STS.32.13.1540	STS_R/F/P Phase 1 4th Street Sidewalks - At Bryant St Intersection							
STS.32.13.1520	STS_R/F/P Phase 1 4th Street 12" Base Slab - At Bryant St Intersection							
STS.34.11.120	Install SB Tunnel Trackwork - Moscone to Union Square (1,950 TF)							
STS.32.13.1300	STS_Prepare Phase 1 4th Street Subgrade - At Brannan St Intersection							
STS.32.13.1310	STS_R/F/P Phase 1 4th Street Curbs & Gutters - At Brannan St Intersection							
STS.32.13.1320	STS_R/F/P Phase 1 4th Street 12" Base Slab - At Brannan St Intersection							
STS.32.13.1340	STS_R/F/P Phase 1 4th Street Sidewalks - At Brannan St Intersection							
STS.32.13.1330	STS_R/F/P Phase 1 4th Street 2" A/C Wearing Surface - At Brannan St Intersecti							
STS.34.11.0530	STS_Install: Trackwork - Through Brannan St Intersection							
Trackwork wi								
STS.34.11.110	Install NB Tunnel Trackwork - Union Square to Chinatown (2,422 TF)							
STS.34.11.110	Install SB Trackwork - Thru Union Square Station (410 TF)							
STS.34.11.0540	STS_Install: Trackwork - Brannan St Intersection To Freelon St							
STS.34.11.105	Install SB Tunnel Trackwork - Union Square to Chinatown (2,422 TF)							
STS.34.11.0550	STS_Install: Trackwork - Through Freelon St Intersection							
STS.34.11.0560	STS_Install: Trackwork - Freelon St To Bryant St							
STS.34.11.0570	STS_Install: Trackwork - Through Bryant St Intersection							
STS.32.13.730	STS_Prepare Phase 2 4th Street Subgrade - At Bryant St Intersection							
STS.34.11.0575	STS_ F/R/P Trackway Pavement - King To Townsend St							
STS.32.13.910	STS_R/F/P Phase 2 4th Street 12" Base Slab - At Bryant St Intersection							
STS.34.11.0590	STS_ F/R/P Trackway Pavement - Townsend St To Bluxome St							
STS.32.13.1530	STS_R/F/P Phase 1 4th Street 2" A/C Wearing Surface - At Bryant St Intersection							
STS.34.11.0600	STS_ F/R/P Trackway Pavement - Bluxome St To Brannan							
STS.32.13.1000	STS_R/F/P Phase 2 4th Street 2" A/C Wearing Surface - At Bryant St Intersector							
STS.34.11.0610	STS_ F/R/P Trackway Pavement - Through Brannan St Intersection							
STS.32.13.9997	STS_4th Street Restoration Punchlist Work							
STS.34.11.0620	STS_ F/R/P Trackway Pavement - Brannan St Intersection To Freelon St							

SCHEDULE REVISIONS

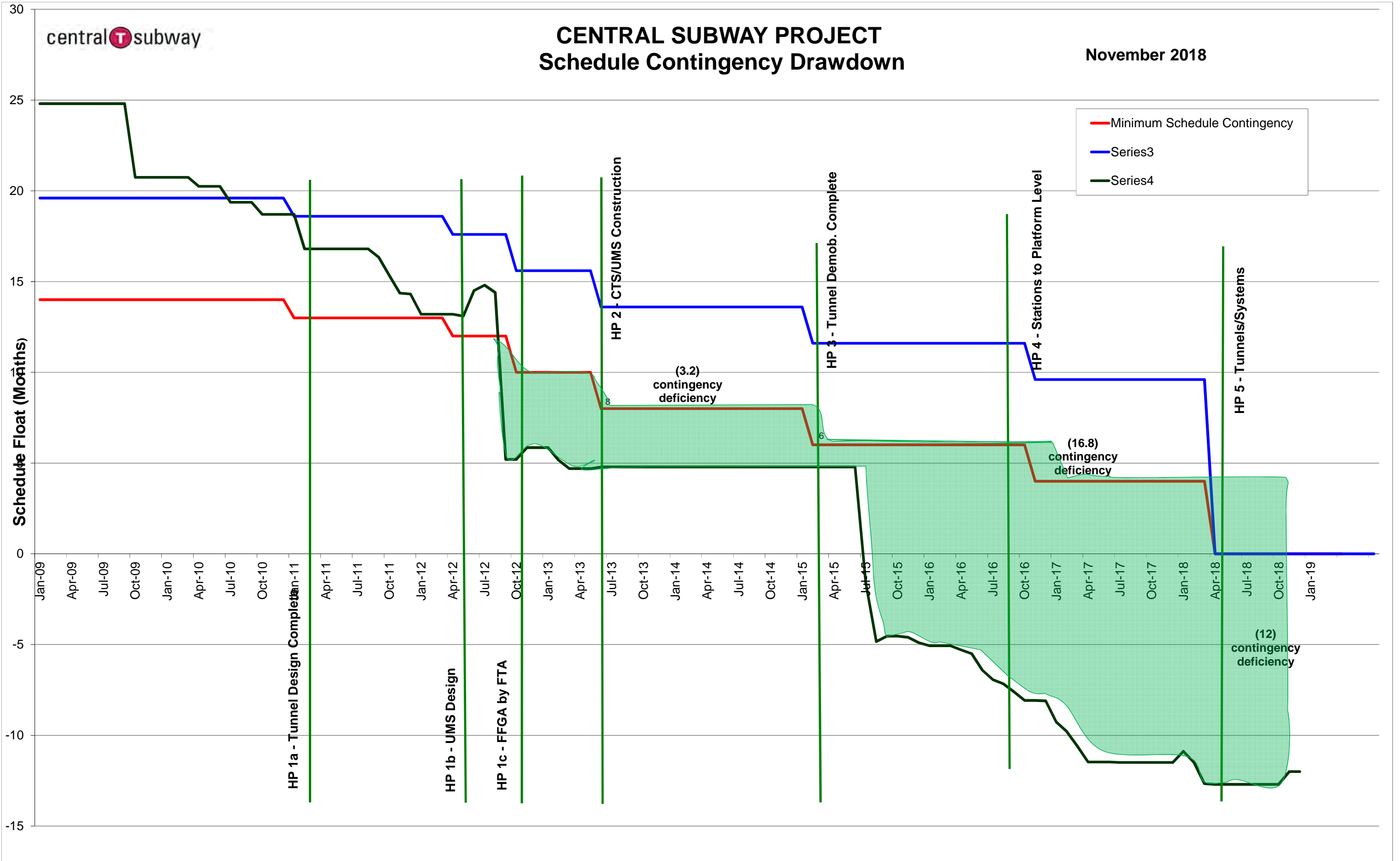
The SFMTA Contract 1300 November 2018 schedule update was added this period to the Central Subway Project Master Schedule.

LIST OF SCHEDULE REPORTS

- 1.1. Schedule Contingency Drawdown
- 1.2. Master Summary Schedule
- 1.3. Program Critical Path Schedule
- 1.4. Construction Contract Summary Schedule
- 1.5. Detail Schedule for Remaining Work

CENTRAL SUBWAY PROJECT Schedule Contingency Drawdown

November 2018



Activity ID	Activity Name	Original Duration	Start	Finish	2019				2020			
					Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3
CENTRAL SUBWAY PROJECT		4606	03-Jun-03 A	28-Jan-21								
Program Level Milestones		4377	03-Jun-03 A	26-Dec-19	Program Level Milestones							
PJD1000	Central Subway Project Start	0	03-Jun-03 A									
MS0004A	Tunnel Excavation Complete - Project Milestone #4A	0		05-Sep-14 A								
MS0019	Baseline Finish Date: 12-26-2018	0		26-Dec-19*								
MS0009	CSP Revenue Service Date	0		26-Dec-19*								
Preliminary Engineering Phase		2661	03-Jun-03 A	07-Jan-10 A								
Final Design		1811	08-Jan-10 A	17-Jun-13 A								
Light Rail Vehicles		2432	15-Apr-13 A	11-Dec-19								
Real Estate		3130	01-Aug-08 A	02-Jan-14 A								
Construction Phase		2887	04-Jan-10 A	28-Jan-21								
Construction Support and Costs		3308	04-Jan-10 A	28-Jan-21								
Construction Utility Contract #1- MOS & Portal CN-1250		505	04-Jan-10 A	23-May-11 A								
Construction Utility Contract #2 - UMS CN-1251		643	12-Jan-11 A	15-Oct-12 A								
Construction Tunnels CN-1252		1518	08-Jun-11 A	26-Nov-18								
Construction CN-1300		1704	03-Jun-13 A	13-Oct-19								
CN- 1300 Milestone		1648	17-Jun-13 A	13-Oct-19								
Construction UMS Station P-1253		1704	17-Jun-13 A	03-Aug-19								
Construction CTS Station P-1254R		1585	17-Jun-13 A	16-Jul-19								
Construction YBM Station P-1255		1651	10-Jun-13 A	16-Jul-19								
Construction STS P-1256		1642	03-Jun-13 A	03-Oct-19								
Project Start Up		165	15-Jul-19	27-Dec-19								
Unallocated Contingency		272	26-Nov-18	26-Dec-19								
CO1.700	Cost Activity Unallocated Contingency (LOE) - 1.7.500.99.090.00 - Contingency	272	26-Nov-18	26-Dec-19								

Activity ID	Activity Name	Original Duration	Start	Finish	Total Float	2019										2020				2021			
						Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2							
Program Level Milestones						0	26-Dec-19	26-Dec-19	-251														
MS0019	Baseline Finish Date: 12-26-2018	0		26-Dec-19*	-251																		◆ Baseline Finish Date: 12-26-2018
MS0009	CSP Revenue Service Date	0		26-Dec-19*	-251																		◆ CSP Revenue Service Date
CN- 1300 Milestone						0	15-Jul-19	15-Jul-19	-502														
MS-10	Substantial Completion - 1,700 Calendar Days (SP-4.B) { 10-Feb-18 }	0		15-Jul-19*	-502																		◆ Substantial Completion - 1,700 Calendar Days (SP-4.B) {
Construction CTS Station P-1254R						196	02-Apr-18 A	24-Apr-19	-358														
CTS.31.74.990	Install Waterproofing - Final Lining Arches - Crosscut Cavern	15	02-Apr-18 A	30-Nov-18	-426																		Install Waterproofing - Final Lining Arches - Crosscut Cavern
CTS.31.74.1000	Install Rebar & Grout Piping - Final Lining Arches - Crosscut Cavern	15	04-Jun-18 A	06-Dec-18	-426																		Install Rebar & Grout Piping - Final Lining Arches - Crosscut Cavern
CTS.31.74.350	Shotcrete Final Lining Arches - Crosscut Cavern	20	21-Jun-18 A	12-Dec-18	-426																		Shotcrete Final Lining Arches - Crosscut Cavern
CTS.03.30.760	Shore/Rebar/Form Pour Track Slab - Crosscut Cavern	15	10-Dec-18	31-Dec-18	-349																		Shore/Rebar/Form Pour Track Slab - Crosscut Cavern
CTS.03.30.780	Rebar/Form/ Pour Platforms - Crosscut Cavern	20	02-Jan-19	29-Jan-19	-349																		Rebar/Form/ Pour Platforms - Crosscut Cavern
CTS.03.30.770	Shore/Rebar/Form Pour Concourse Level Slab - Crosscut Cavern	15	16-Jan-19	05-Feb-19	-349																		Shore/Rebar/Form Pour Concourse Level Slab - Crosscut Cavern
CTS.09.83.664	CTS_PL 05 Spray - Set Up Scaffold- Platform Level	5	07-Feb-19	13-Feb-19	-349																		CTS_PL 05 Spray - Set Up Scaffold- Platform Level
CTS.09.83.544	CTS_PL 05 Spray - Acoustical Vermiculite Wall Plaster NB - Sector 2	5	14-Feb-19	20-Feb-19	-349																		CTS_PL 05 Spray - Acoustical Vermiculite Wall Plaster NB - Sector 2
CTS.09.83.554	CTS_PL 05 Spray - Acoustical Vermiculite Wall Plaster SB - Sector 2	5	21-Feb-19	27-Feb-19	-349																		CTS_PL 05 Spray - Acoustical Vermiculite Wall Plaster SB - Sector 2
CTS.34.21.122	CTS_PL Station Platform: Install - Traction Power Box PS01 @ SB Track - Sec	3	28-Feb-19	04-Mar-19	-349																		CTS_PL Station Platform: Install - Traction Power Box PS01 @ SB T
CTS.34.21.183	CTS_PL Station Platform: Install - Traction Power Box PS02 @ SB Track - Sec	3	05-Mar-19	07-Mar-19	-349																		CTS_PL Station Platform: Install - Traction Power Box PS02 @ SB T
CTS.34.21.123	CTS_PL Station Platform: Install - Traction Power Box PS03 @ NB Track - Sect	3	08-Mar-19	12-Mar-19	-349																		CTS_PL Station Platform: Install - Traction Power Box PS03 @ NB T
CTS.34.05.130	CTS_PL_Install Conduit NB Positive Feeder Box PS03 to PS06 (Traction Power)	5	13-Mar-19	19-Mar-19	-349																		CTS_PL_Install Conduit NB Positive Feeder Box PS03 to PS06 (Tra
CTS.34.05.140	CTS_PL_Install Conduit NB Positive Feeder Box PS03 to PS09 (Traction Power)	4	20-Mar-19	25-Mar-19	-349																		CTS_PL_Install Conduit NB Positive Feeder Box PS03 to PS09 (Tra
CTS.34.05.150	CTS_PL_Install Conduit NB Positive Feeder Box PS04 to PS10 (Traction Power)	5	26-Mar-19	01-Apr-19	-349																		CTS_PL_Install Conduit NB Positive Feeder Box PS04 to PS10 (Tr
CTS.34.22.230	CTS_PL_Pull Cable & Terminate NB Positive Feeder Box PS04 to PS10 (Tracti	2	02-Apr-19	03-Apr-19	-349																		CTS_PL_Pull Cable & Terminate NB Positive Feeder Box PS04 to
CTS.34.21.213	CTS_PL 15 - Traction Power Rm: Terminations - DC Switchgear	10	04-Apr-19	17-Apr-19	-349																		CTS_PL 15 - Traction Power Rm: Terminations - DC Switchgear
CTS.34.21.935	CTS_PL 18 - Main Elect Rm: Energize DC TPSS Equipment	5	18-Apr-19	24-Apr-19	-349																		CTS_PL 18 - Main Elect Rm: Energize DC TPSS Equipment
Construction STS P-1256						56	25-Apr-19	15-Jul-19	-349														
STS.34.42.425	Startup & Testing - Tunnel & ATSC Systems	56	25-Apr-19	15-Jul-19	-349																		Startup & Testing - Tunnel & ATSC Systems
Project Start Up						165	15-Jul-19	27-Dec-19	-365														
STU1010	S&S Certification / Pre-Revenue Activities	114	15-Jul-19	27-Dec-19	-251																		S&S Certification / Pre-Revenue Activities
BUF0018	Muni Float	0	26-Dec-19	26-Dec-19	-251																		Muni Float

Activity Name	Original Duration	Start	Finish	2012				2013				2014				2015				2016				2017				2018		2019	
				Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2		
CENTRAL SUBWAY PROJECT	2232	08-Jun-11 A	13-Oct-19																												
Construction Phase	2232	08-Jun-11 A	13-Oct-19																												
Construction Tunnels CN-1252	1518	08-Jun-11 A	15-May-15 A																												
1252 Tunnel Contract BIH	1518	08-Jun-11 A	15-May-15 A																												
Contract Milestones	1437	08-Jun-11 A	15-May-15 A																												
General Conditions	1480	01-Aug-11 A	15-May-15 A																												
4th & Bryant St TBM Launch Box Construction	686	30-Mar-12 A	02-Jun-14 A																												
Moscone Station Headwalls	430	14-May-12 A	20-Sep-13 A																												
UMS Station Headwalls	425	24-Jul-12 A	22-Nov-13 A																												
UMS - Remove Geary to Ellis OCS	5	24-Jul-12 A	26-Jul-12 A																												
UMS - Setup Traffic Control for Headwall Construction	1	30-Jul-12 A	30-Jul-12 A																												
North Headwall	237	27-Feb-13 A	22-Nov-13 A																												
South Headwall	404	31-Jul-12 A	22-Nov-13 A																												
4th St and Market Compensation Grouting	707	28-Jan-13 A	30-Apr-15 A																												
Southbound Tunneling	451	27-Apr-13 A	13-Oct-14 A																												
Ellis St Compensation Grouting	561	31-Jul-12 A	09-May-14 A																												
Green St Compensation Grouting	320	05-Aug-13 A	30-Jun-14 A																												
Retrieval Shaft	1070	31-Oct-11 A	20-Mar-15 A																												
Cross Passage 1-5	339	22-Mar-14 A	16-Apr-15 A																												
Cross Passage 1	79	14-Jun-14 A	13-Sep-14 A																												
Cross Passage 2	105	10-May-14 A	09-Sep-14 A																												
Cross Passage 3	127	31-Mar-14 A	28-Aug-14 A																												
Cross Passage 4	114	22-Mar-14 A	31-Jul-14 A																												
Cross Passage 5	277	31-May-14 A	16-Apr-15 A																												
Portal Structure	196	02-Sep-14 A	15-Apr-15 A																												
Contract Close Out	307	03-Mar-14 A	15-May-15 A																												
Construction CN-1300	1704	03-Jun-13 A	13-Oct-19																												
CN- 1300 Milestone	1648	17-Jun-13 A	13-Oct-19																												
Construction UMS Station P-1253	1704	17-Jun-13 A	03-Aug-19																												
Construction CTS Station P-1254R	1585	17-Jun-13 A	16-Jul-19																												
Construction YBM Station P-1255	1651	10-Jun-13 A	16-Jul-19																												
Construction STS P-1256	1642	03-Jun-13 A	03-Oct-19																												

Activity ID	Activity Name	Original Duration	Start	Finish	Total Float	2019				2020				2021	
						Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1
CENTRAL SUBWAY PROJECT															
Light Rail Vehicles															
Construction Phase															
Construction Support and Costs															
Construction CN-1300															
CN- 1300 Milestone															
No 13-Disp															
Construction UMS Station P-1253															
Preconstruction															
Engineering & Procurement															
Site Work / Utility Relocation															
Drilled Shafts															
Roof Deck Excavation,Construction,Restoration															
Excavation & Support															
Concrete/Shotcrete															
Surface Level															
Concourse Level															
UMS.03.30.2510	UMS_Form/Rebar - North Concourse Walls -Concourse To Roof Pour #2	3	09-Apr-18 A	28-Nov-18	-330										
UMS.03.30.1212	UMS_Place Concrete / On Metal Deck - Concourse Level - Pour #2	1	10-Jul-18 A	30-Nov-18	-332										
UMS.03.30.1211	UMS_Place Concrete / On Metal Deck - Concourse Level - Pour #1	1	06-Nov-18 A	29-Nov-18	-332										
UMS.03.30.2201	UMS_Cure Concrete / On Metal Deck - Concourse Level - Pour #1	10	07-Nov-18 A	13-Dec-18	-328										
UMS.03.30.1201	UMS_Form/Rebar/ On Metal Deck - Concourse Level - Pour #1	3	12-Nov-18 A	28-Nov-18	-332										
UMS.07.81.2580	UMS_Set up Scaffold- Concourse Level	7	26-Nov-18	04-Dec-18	-329										
Intermediate Strut Level															
UMS.03.30.1135	UMS_Shotcrete / Exterior Walls - Intermediate Strut Level To Concourse Level - Pour #5	1	26-Nov-18	26-Nov-18	-328										
UMS.05.30.1017	UMS_Install Metal Deck - Intermediate Strut Level - Pour #7	3	29-Nov-18	03-Dec-18	-331										
UMS.03.30.1187	UMS_Form/Rebar/ On Metal Deck - Intermediate Strut Level - Pour #7	3	04-Dec-18	06-Dec-18	-331										
UMS.03.30.1197	UMS_Place Concrete / On Metal Deck - Intermediate Strut Level - Pour #7	1	07-Dec-18	07-Dec-18	-331										
UMS.03.30.1137	UMS_Shotcrete / Exterior Walls - Intermediate Strut Level To Concourse Level - Pour #7	1	10-Dec-18	10-Dec-18	-328										
UMS.03.30.2197	UMS_Cure Concrete / On Metal Deck - Intermediate Strut Level - Pour #7	10	10-Dec-18	21-Dec-18	-331										
Mezzanine Level															
Structural Steel															
Masonry															
Mechanical															
Electrical															
Electrical - Transportation															
Architectural Finishes															
Conveyances															
Stairs															
Startup & Testing															
No 13-Disp															
Construction CTS Station P-1254R															
Preconstruction															
Site Work / Utility Relocation															
Excavation & Support															
Cavern Lining															
Concrete/Shotcrete															
Entrance Roof Level															
Surface Level															
Upper Mezz Level															

Activity ID	Activity Name	Original Duration	Start	Finish	Total Float	2019				2020				2021		
						Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2
Lower Mezz Level																
Concourse Level																
Intermediate Strut Level																
Platform Level																
CTS.03.30.590	Form/Rebar/Pour Platform Stem Walls - Platform Cavern South	15	17-Oct-18 A	14-Dec-18	-329											
CTS.03.30.600	Shore/Form/Rebar/Pour Track Slab - Platform Cavern South	15	25-Oct-18 A	14-Dec-18	-329											
CTS.03.30.640	F/R/P/S Walls Under Track Slab - Platform Cavern North	10	14-Nov-18 A	07-Dec-18	-323											
CTS.03.30.670	Shore/Form/Rebar/Pour Track Slab - Platform Cavern North	10	26-Nov-18	07-Dec-18	-323											
CTS.03.30.790	Form/Rebar/Pour Platform Slab - Platform Cavern South	15	26-Nov-18	14-Dec-18	-329											
CTS.03.30.610	Form/Rebar/Pour Stair, Room & End Walls - Platform Cavern South	14	03-Dec-18	20-Dec-18	-329											
CTS.03.30.620	Form/Rebar/Pour Stair & Room Enclosure Lid - Platform Cavern South	14	03-Dec-18	20-Dec-18	-329											
CTS.03.30.660	Form/Rebar/Pour Platform Stem Walls - Platform Cavern North	15	03-Dec-18	21-Dec-18	-323											
CTS.03.30.850	Concrete Stairs North Emergency Egress Tunnel	10	06-Dec-18	19-Dec-18	-326											
CTS.03.30.760	Shore/Rebar/Form Pour Track Slab - Crosscut Cavern	15	10-Dec-18	31-Dec-18	-349											
CTS.03.3.0740	Form/Rebar/Pour Platform Slab - Platform Cavern North	15	10-Dec-18	31-Dec-18	-323											
CTS.03.30.260	Pour Equipment Pads - Main Electrical & Traction Power Rooms	5	13-Dec-18	19-Dec-18	-305											
CTS.03.30.680	Form/Rebar/Pour Stair, Room & End Walls - Platform Cavern North	15	17-Dec-18	08-Jan-19	-323											
CTS.07.13.120	CTS_PL - Install - Drain Mat & Waterproofing on Shotcrete Walls - Head House	10	24-Dec-18	08-Jan-19	-339											
CTS.03.30.690	Form/Rebar/Pour Stair & Room Enclosure Lid - Platform Cavern North	15	24-Dec-18	15-Jan-19	-323											
CTS.03.30.780	Rebar/Form/ Pour Platforms - Crosscut Cavern	20	02-Jan-19	29-Jan-19	-349											
CTS.03.30.175	Form/Rebar/Pour/Strip -Perimeter Walls Platform Slab to Concourse Level	10	09-Jan-19	22-Jan-19	-344											
CTS.03.30.770	Shore/Rebar/Form Pour Concourse Level Slab - Crosscut Cavern	15	16-Jan-19	05-Feb-19	-349											
CTS.03.30.920	Form/Rebar/Pour/Strip -Perimeter Walls Concourse Slab to Intermediate Level	10	30-Jan-19	12-Feb-19	-342											
Under Platform Level																
Structural Steel																
Masonry																
Mechanical																
Misc Metal																
Electrical																
Electrical - Transportation																
Architectual Finishes																
Conveyances																
Startup & Testing																
No 13-Disp																
Construction YBM Station P-1255																
Preconstruction																
Excavation & Support																
Concrete/Shotcrete																
Entrance Roof Level																
Surface Level																
All Levels																
YBM.03.30.590	Metal Deck/ Railing Stair #3 from Platform to Concourse Level	10	13-Nov-17 A	07-Dec-18	-287											
YBM.05.60.550	FRP Stair #6 from Under surface to Surface Level	10	21-Aug-18 A	07-Dec-18	-212											
YBM.05.52.460	Install Stair #2 Rails from Platform to Concourse Level	10	15-Oct-18 A	01-Mar-19	-304											
YBM.03.30.1470	Metal Deck/ Railing Stair #7 from Concourse to Surface Level	10	10-Dec-18	21-Dec-18	-301											
YBM.05.52.580	Install Stair #3 Rails from Platform to Concourse Level	10	10-Dec-18	21-Dec-18	-255											
YBM.05.60.620	FRP Stair #7 from Concourse to Surface Level	10	24-Dec-18	08-Jan-19	-301											
Concourse Level																
YBM.22.14.220	YBM_CN Install Domestic Water- Headhouse Concourse	5	18-Dec-17 A	09-Jan-19	-284											
YBM.03.30.1570	F/R/P In-Fill Walls at Mezzanine GL D	5	26-Nov-18	30-Nov-18	-304											
YBM.22.14.160	YBM_CN Install Air Replenishment Piping-Concourse Level	5	26-Dec-18	02-Jan-19	-284											

Activity ID	Activity Name	Original Duration	Start	Finish	Total Float	2019				2020				2021		
						Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2
YBM.05.52.610	Install SS Emergency Exit Gate- Concourse Level	5	07-Jan-19	11-Jan-19	-300											
YBM.05.52.620	Install Tube Steel Structure for Station Agebt Booth, Concourse Level	5	11-Mar-19	15-Mar-19	-329											
Mezzanine Level																
YBM.22.14.170	YBM_MZ Install Vents & Drains- Headhouse Mezzanine	5	18-Dec-17 A	14-Dec-18	-258											
YBM.22.14.180	YBM_MZ Install Domestic Water- Headhouse Mezzanine	5	18-Dec-17 A	14-Dec-18	-258											
Platform Level																
YBM.03.30.1490	Seismic Joints- Platform Level, Station (North and south wall at Tunnel)	15	12-Dec-17 A	14-Dec-18	-312											
YBM.05.52.590	Install SS Emergency Exit Gate- Platform Level	5	28-Dec-18	04-Jan-19	-300											
Electrical																
Electrical - Transportation																
Conveyances																
Startup & Testing																
No 13-Disp																
Construction STS P-1256																
Preconstruction																
Concrete/Shotcrete																
Tunnel Concrete																
Structural Steel																
Electrical																
Upper Mezz Level																
Lower Mezz Level																
Intermediate Strut Level																
Mezzanine Level																
Platform Level																
Electrical - Transportation																
Trackwork																
Platform Level																
STS.32.13.960	STS_R/F/P Phase 2 4th Street 2" A/C Wearing Surface - At Brannan St Intersection	1	20-Jun-17 A	21-Jan-19	-242											
STS.32.13.1500	STS_Prepare Phase 1 4th Street Subgrade - At Bryant St Intersection	5	17-Aug-17 A	14-Dec-18	-242											
STS.32.13.1510	STS_R/F/P Phase 1 4th Street Curbs & Gutters - At Bryant St Intersection	5	18-Aug-17 A	19-Dec-18	-242											
STS.32.13.1540	STS_R/F/P Phase 1 4th Street Sidewalks - At Bryant St Intersection	5	22-Aug-17 A	07-Dec-18	-212											
STS.32.13.1520	STS_R/F/P Phase 1 4th Street 12" Base Slab - At Bryant St Intersection	5	23-Aug-17 A	27-Dec-18	-242											
STS.34.11.120	Install SB Tunnel Trackwork - Moscone to Union Square (1,950 TF)	25	18-Sep-17 A	28-Nov-18	-339											
STS.34.11.0440	STS_ F/R/P Trackway Slab - Bluxome St To Brannan	10	14-Dec-17 A	13-Feb-19	134											
STS.34.11.0430	STS_ F/R/P Trackway Slab - Townsend St To Bluxome St	10	18-Dec-17 A	07-Feb-19	134											
STS.32.13.1300	STS_Prepare Phase 1 4th Street Subgrade - At Brannan St Intersection	5	20-Jan-18 A	30-Nov-18	-237											
STS.32.13.1310	STS_R/F/P Phase 1 4th Street Curbs & Gutters - At Brannan St Intersection	5	20-Jan-18 A	30-Nov-18	-232											
STS.32.13.1320	STS_R/F/P Phase 1 4th Street 12" Base Slab - At Brannan St Intersection	5	20-Jan-18 A	30-Nov-18	-229											
STS.32.13.1340	STS_R/F/P Phase 1 4th Street Sidewalks - At Brannan St Intersection	5	21-Jan-18 A	30-Nov-18	-212											
STS.32.13.1330	STS_R/F/P Phase 1 4th Street 2" A/C Wearing Surface - At Brannan St Intersection	1	22-Jan-18 A	28-Dec-18	-229											
STS.34.11.0530	STS_Install: Trackwork - Through Brannan St Intersection	5	26-Nov-18	30-Nov-18	-279											
STS.34.11.110	Install SB Trackwork - Thru Union Square Station (410 TF)	3	29-Nov-18	03-Dec-18	-339											
STS.34.11.0540	STS_Install: Trackwork - Brannan St Intersection To Freelon St	5	03-Dec-18	07-Dec-18	-279											
STS.34.11.105	Install SB Tunnel Trackwork - Union Square to Chinatown (2,422 TF)	30	04-Dec-18	16-Jan-19	-337											
STS.34.11.0550	STS_Install: Trackwork - Through Freelon St Intersection	5	10-Dec-18	14-Dec-18	-279											
STS.34.11.0560	STS_Install: Trackwork - Freelon St To Bryant St	5	17-Dec-18	21-Dec-18	-279											
STS.34.11.0570	STS_Install: Trackwork - Through Bryant St Intersection	5	24-Dec-18	31-Dec-18	-279											
STS.32.13.730	STS_Prepare Phase 2 4th Street Subgrade - At Bryant St Intersection	5	28-Dec-18	04-Jan-19	-242											
STS.34.11.0575	STS_ F/R/P Trackway Pavement - King To Townsend St	5	02-Jan-19	08-Jan-19	-263											
STS.32.13.910	STS_R/F/P Phase 2 4th Street 12" Base Slab - At Bryant St Intersection	10	07-Jan-19	18-Jan-19	-242											
STS.34.11.0590	STS_ F/R/P Trackway Pavement - Townsend St To Bluxome St	5	09-Jan-19	15-Jan-19	-263											

Activity ID	Activity Name	Original Duration	Start	Finish	Total Float	2019				2020				2021		
						Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2
STS.32.13.1530	STS_R/F/P Phase 1 4th Street 2" A/C Wearing Surface - At Bryant St Intersection	1	14-Jan-19	14-Jan-19	-238											
STS.34.11.0600	STS_ F/R/P Trackway Pavement - Bluxome St To Brannan	5	16-Jan-19	22-Jan-19	-263											
STS.34.11.0260	STS_ Prepare Trackway Subgrade - Through Brannan St Intersection	5	21-Jan-19	25-Jan-19	134											
STS.32.13.1000	STS_R/F/P Phase 2 4th Street 2" A/C Wearing Surface - At Bryant St Intersection	1	22-Jan-19	22-Jan-19	-242											
STS.34.11.0610	STS_ F/R/P Trackway Pavement - Through Brannan St Intersection	5	23-Jan-19	29-Jan-19	-263											
STS.34.11.0295	STS_ Prepare Trackway Subgrade - Through Bryant St Intersection	5	28-Jan-19	01-Feb-19	134											
STS.34.11.0620	STS_ F/R/P Trackway Pavement - Brannan St Intersection To Freelon St	5	30-Jan-19	05-Feb-19	-263											
STS.34.11.0630	STS_ F/R/P Trackway Pavement - Through Freelon St Intersection	5	06-Feb-19	12-Feb-19	-263											
STS.34.11.170	Install SB Trackwork & Crossover - Thru Chinatown Station (539TF)	20	06-Feb-19	05-Mar-19	-338											
STS.34.11.185	Install NB Trackwork & Crossover - Thru Chinatown Station (539 TF)	20	06-Feb-19	05-Mar-19	-332											
STS.34.11.0640	STS_ F/R/P Trackway Pavement - Freelon St To Bryant St	5	13-Feb-19	19-Feb-19	-263											
STS.34.11.0450	STS_ F/R/P Trackway Slab - Through Brannan St Intersection	5	14-Feb-19	20-Feb-19	134											
STS.34.11.0650	STS_ F/R/P Trackway Pavement - Through Bryant St Intersection	5	20-Feb-19	26-Feb-19	-263											
STS.34.11.0460	STS_ F/R/P Trackway Slab - Brannan St Intersection To Freelon St	10	21-Feb-19	06-Mar-19	134											
STS.34.11.160	Install SB Trackwork - Chinatown Station to North Limit (339 TF)	5	06-Mar-19	12-Mar-19	-338											
STS.34.11.175	Install NB Trackwork - Chinatown Station to North Limit (339 TF)	5	06-Mar-19	12-Mar-19	-330											
STS.34.11.0470	STS_ F/R/P Trackway Slab - Through Freelon St Intersection	5	07-Mar-19	13-Mar-19	134											
STS.34.11.0475	STS_ F/R/P Trackway Slab - Freelon St To Bryant St	10	14-Mar-19	27-Mar-19	134											
STS.34.11.0480	STS_ F/R/P Trackway Slab - Through Bryant St Intersection	5	28-Mar-19	03-Apr-19	134											
Track System Work		1212	06-Apr-15 A	03-Oct-19	-343											
Startup & Testing		56	25-Apr-19	15-Jul-19	-349											
No 13-Disp		1434	09-Jan-14 A	15-Jul-19	64											
Unallocated Contingency		272	26-Nov-18	26-Dec-19	-251											

Appendix C

PROJECT SCOPE AND FUNDING OVERVIEW

Project Overview

The Central Subway Project will construct a modern, efficient light-rail line that will improve public transit in San Francisco. This new 1.7-mile extension of Muni's T Third Line will provide direct connections to major retail, sporting and cultural venues while efficiently transporting people to jobs, educational opportunities and other amenities throughout the city.

The Central Subway Project is Phase 2 of the San Francisco Municipal Transportation Agency's (SFMTA) Third Street Light Rail Transit Project. Phase 1 of the project constructed a 5.1-mile light-rail line along the densely populated 3rd Street corridor. It began revenue service in April 2007, restoring light-rail service to a high transit-ridership area of San Francisco for the first time in 50 years.

The Central Subway Project will extend the T Third Line from the 4th Street Caltrain Station to Chinatown, providing a direct, rapid transit link from the Bayshore and Mission Bay areas to SoMa, Union Square and downtown.

Four new stations will be built along the 1.7-mile project alignment—an above-ground station at 4th and Brannan streets and three underground stations at Moscone Center, Union Square and Chinatown.

The Central Subway will run through the burgeoning technology and digital-media hub in SoMa, where dozens of companies have taken up residence along the 4th Street corridor. Increased



Project Overview - continued

transit options will attract new employers – the Central Subway makes travel more convenient throughout the corridor and improves connections to downtown, local and regional rail and the Muni bus system.

The Central Subway Project will contribute to San Francisco’s economic competitiveness and help secure the city’s status of a regional, national and global hub. It will provide a pollution-free transit option that will reduce the environmental impact of transportation in the city, save natural resources, reduce traffic congestion and improve public transit for thousands of San Franciscans.

Funding Overview

The Central Subway Project is funded by the federal government, the State of California, the Metropolitan Transportation Commission, the San Francisco County Transportation Authority (SFCTA) and the City and County of San Francisco.

The majority of funding for the Central Subway Project is expected to be provided by the Federal Transit Administration’s (FTA) New Starts program, with a total commitment over the life of the project of \$942.2 million. To date, \$41 million in Department of Transportation Congestion Mitigation and Air Quality Improvement Program funds have been committed and expended.

With the addition in the December 2013 MPR of work to relocate the retrieval site for two tunnel boring machines (TBMs), the SFMTA’s baseline budget for the Central Subway Project is \$1.588 billion. In total, about half of the Third Street Light Rail Transit Project’s funding is from federal sources, with the remaining half from state and local sources. This is in line with the expectations of the FTA for New Starts-financed programs.

The table below summarizes the local, state and federal fund sources for both phases of the T Third Line including with the addition of the retrieval shaft to the Phase 2 totals.

	T Third (Phase 1)	Central Subway (Phase 2 + Retrieval Shaft Relocation)	Total (Phase 1 + Phase 2 + Retrieval Shaft Relocation)	Percentage of Total
Federal	\$123.380	\$983.225	\$1,106.605	49.5%
State	\$160.700	\$471.100	\$631.800	28.2%
Local	\$364.380	\$133.675	\$498.055	22.3%
Total	\$648.460	\$1,588.000	\$2,236.460	100.0%

All amounts in millions of dollars

The six charts that follow summarize use of fund sources by phase and with the addition of the retrieval shaft relocation additional budget and funding:

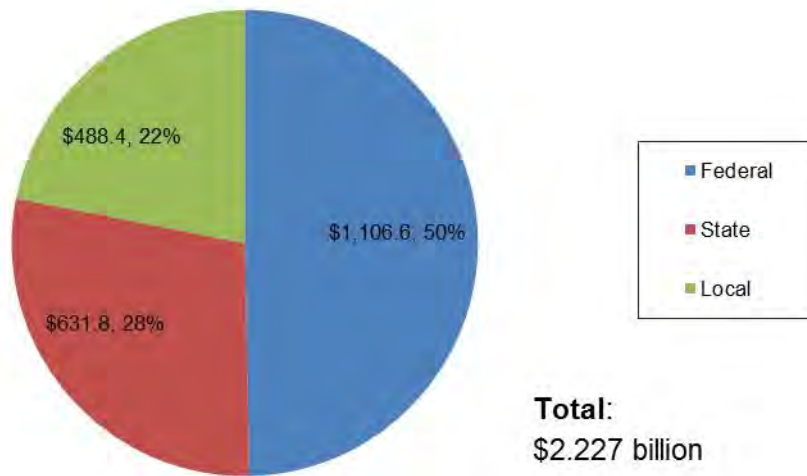
- Phase 1 + Phase 2 of the T Third Line federal, state and local funding percentages previous to the addition of the retrieval shaft relocation budget and funding in December 2013.

Funding Overview - continued

- Phase 2 Central Subway Project only total funding source percentages previous to the addition of the retrieval shaft relocation budget and funding.
- Phase 2 Central Subway Project only detail of the six State and Local funding sources previous to the addition of the retrieval shaft relocation.
- The next three charts that follow are the above three data sets above with the retrieval shaft relocation budget and funding added to the overall presentation.

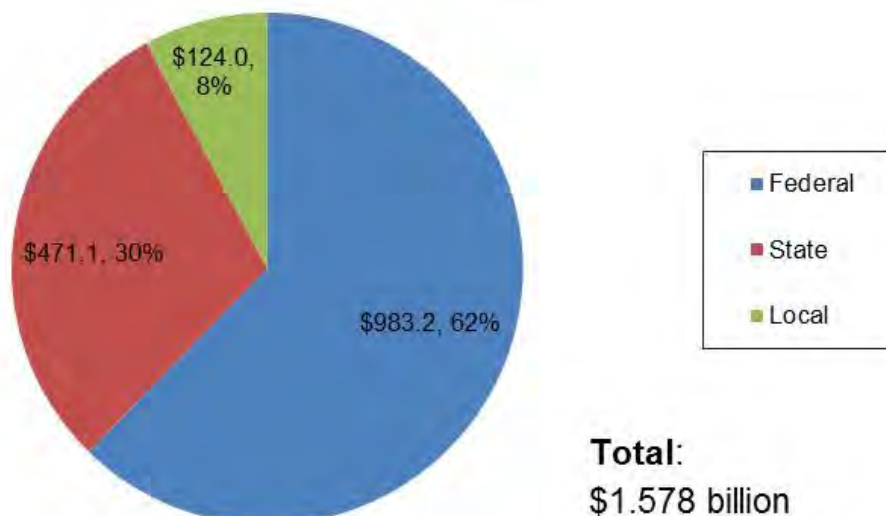
Third Street Light Rail Transit Project Funding

Phase 1 + Phase 2
(\$ in millions)



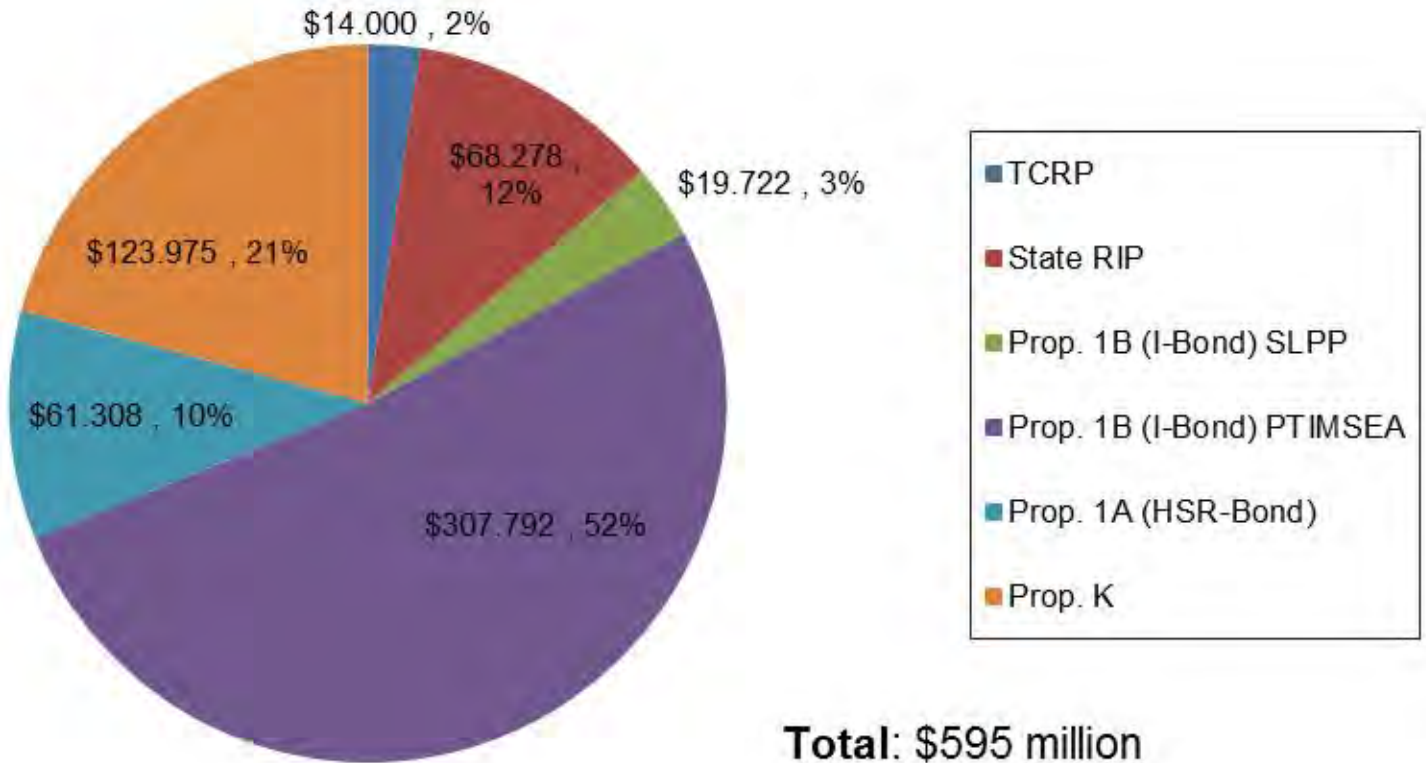
Central Subway Project Funding

Phase 2
(\$ in millions)



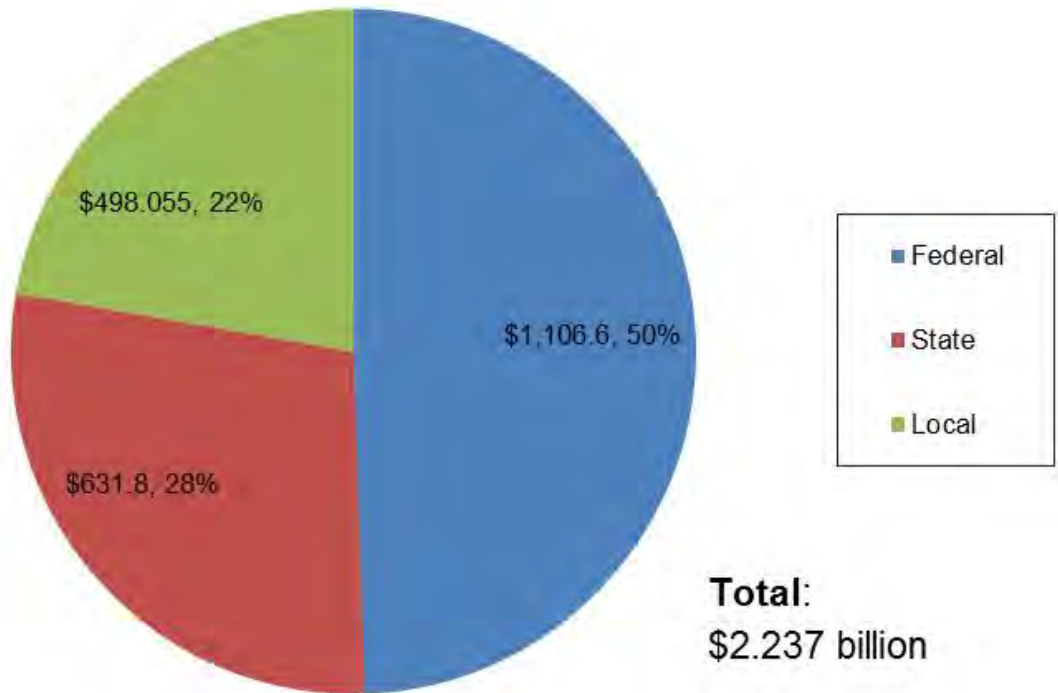
State and Local Funding

Phase 2
(\$ in millions)



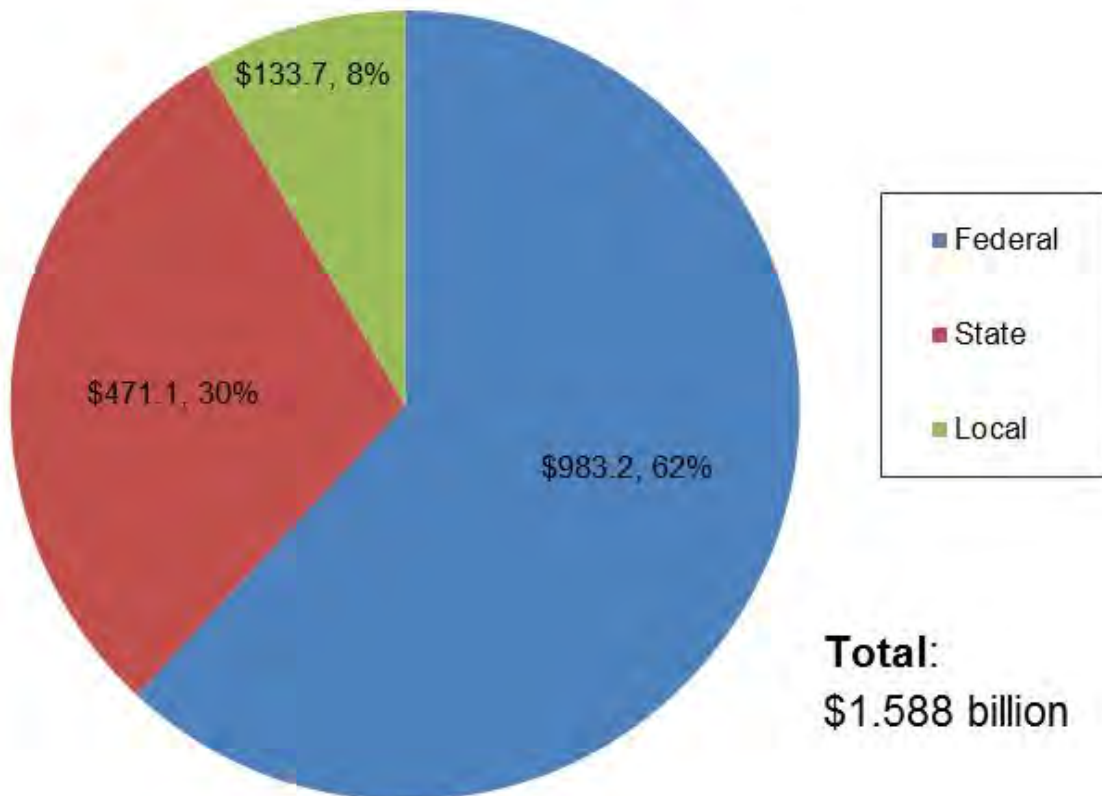
Third Street Light Rail Transit Project Funding

Phase 1 + Phase 2 + Retrieval Shaft Relocation
(\$ in millions)

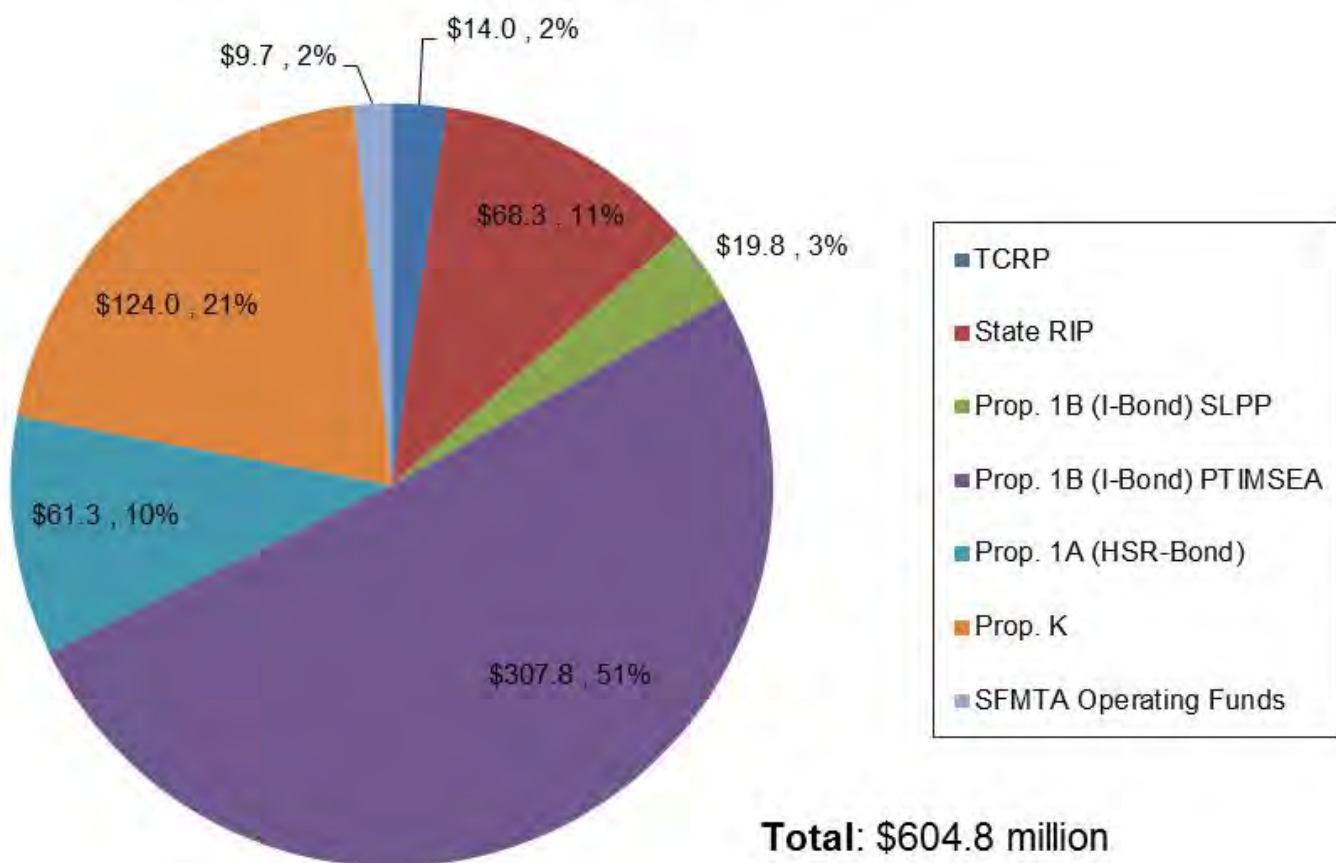


Central Subway Project Funding

Phase 2 + Retrieval Shaft Relocation
(\$ in millions)



State and Local Funding Phase 2 + Retrieval Shaft Relocation (\$ in millions)



Appendix D

COMPLETED CONTRACTS

Moscone Station and Portal Utility Relocation

Contract 1250

Contractor: Synergy Project Management, Inc.

Budget/Expenditures

Category	Amount
Original Budget	\$11,227,316
Expenditures Final	\$11,968,150
Utility Reimbursements	(\$2,275,419)
Final Program Cost	\$9,692,731
Budget Impact (Underrun)	(\$1,534,585)

Contract Details

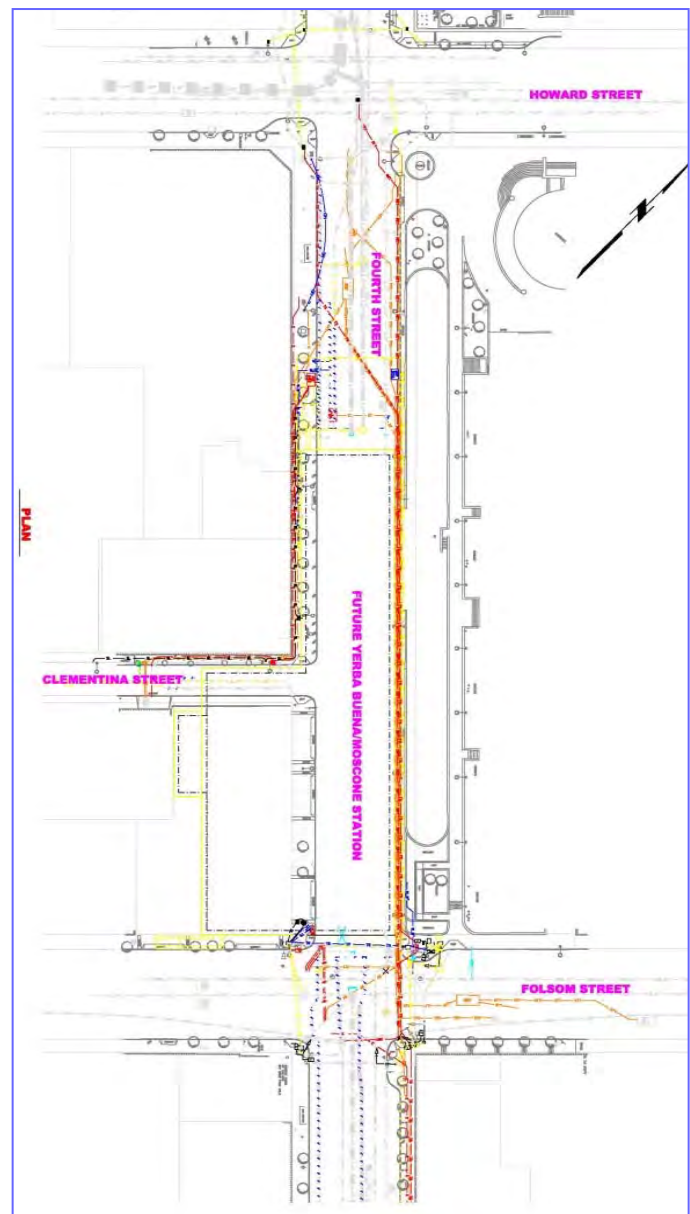
Contract Awarded:	November 17, 2009
Notice to Proceed:	January 4, 2010
Substantial Completion:	June 23, 2011
Contract Award Value:	\$ 9,273,939
Modifications Final :	\$ 2,694,211
Final Contract Value:	\$11,968,150

Status

- Work complete
- Project closeout administration and documentation
- Final Completion Date: June 23, 2011

Description

This project relocates utilities within the footprint of the proposed Yerba Buena/Moscone Station and the 4th Street Portal where the tunnel boring machines will descend underground. Also included is installation of building protections and monitoring of buildings adjacent to utility trenches.



Union Square/Market Street Station Utility Relocation

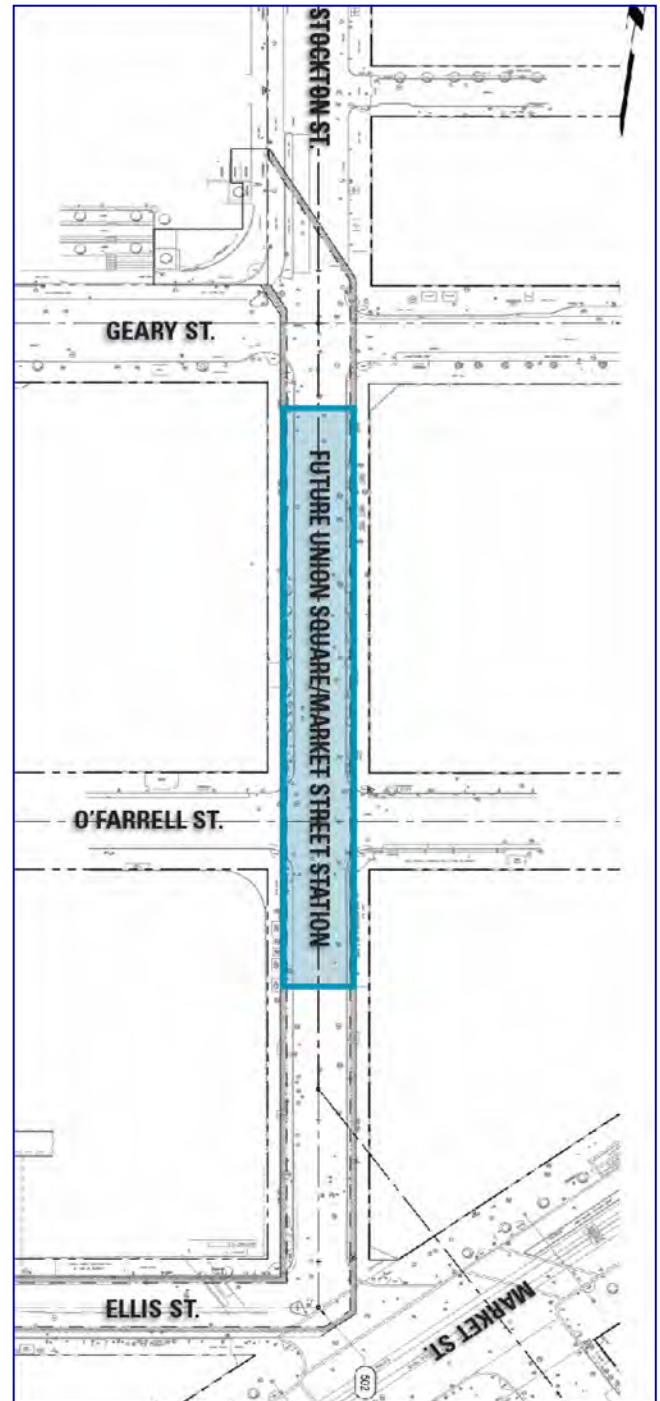
Contract 1251

Contractor: Synergy Project Management, Inc.

Budget/Expenditures	
Category	Amount
Original Budget	\$22,199,847
Expenditures Final	\$20,699,081
Utility Reimbursements	(7,413,510)
Final Program Costs	\$13,176,169
Budget Impact (Underrun)	(\$9,023,678)

Description

This project relocates utilities for the Union Square/Market Street Station and temporarily reroutes existing trolley coach lines around the construction.



Contract Details	
Contract Awarded:	December 7, 2010
Notice to Proceed:	January 12, 2011
Substantial Completion:	August 16, 2012
Contract Award Value:	\$16,832,550
Modifications Final:	\$3,836,531
Final Contract Value:	\$20,669,081

Status

- Final completion on October 15, 2012
- Completed punch list work
- Project Final Acceptance on November 15, 2013
- Completed final construction contract administrative closeout in June 2017

Central Subway Pagoda Palace Demolition

Contract 1277 Contractor: MH Construction

Work Description

Demolish and clear the former Pagoda Theater for use the site to recover the tunnel boring machines when tunnels are completed in 2015. Locate and supply contractor facilities and installations. Obtain permits and approvals and coordinate work with City agencies and utility companies. Furnish and install signs and distribute notices to the local community prior to commencing with construction, cleanup and remove of debris from the site.

- Contract funded by SFMTA Operating funds
- Work was substantially completed September 24, 2013
- Completed administrative closeout in June 2016

Budget/Expenditures	
Category	Amount
Current Budget	\$648,976
Expenditures to Date	\$648,976

Contract Details	
Contract Awarded:	June 12, 2013
Notice to Proceed:	July 15, 2013
Substantial Completion:	Sept. 24, 2013
Contract Award Value:	\$498,995
Modifications to Date:	\$149,981
Current Contract Value:	\$648,976



Central Subway Tunneling

Contract 1252 Contractor: Barnard Impregilo Healy Joint Venture

Description of Work

1.5-mile twin bore tunnels from Hwy I-80 to North Beach using two tunnel boring machines (TBMs). Contractor procurement and installation of the TBMs; construction of the TBM launch box and retrieval shaft excavation support; Yerba Buena/Moscone Station and Union Square/Market Street Station end walls; tunnel excavation and installation of precast segmental lining, the 4th Street portal transition to the surface and cross passages. Throughout, settlement monitoring and protection of existing utilities, buildings and BART tunnels.

Status

- Final Completion Date: May 15, 2015
- Administrative closeout in progress

Budget/Expenditures	
Category	Amount
Current Budget	\$235,913,500
Other Project Budget	\$5,150,000
Other Offset Credits	\$1,312,101
Expenditures to Date	\$233,589,322

Contract Details	
Contract Awarded:	June 28, 2011
Notice to Proceed 1:	January 27, 2012
Notice to Proceed 2:	March 14, 2012
Partial NTP 3:	April 12, 2012
Notice to Proceed 3:	October 15, 2012
Substantial Completion:	April 13, 2015
Contract Award Value:	\$233,584,015
Modifications to Date:	\$7,825,155
Current Contract Value:	\$241,409,170



Appendix E

SBE PARTICIPATION

Quarterly Report

Current Report: July 2018 – September 2018

PROGRAM SUPPORT CONTRACTS – SBE PARTICIPATION

Appendix E presents the Central Subway Program Small Business Enterprise or SBE goals and the actual SBE participation achieved to date – as of September 30, 2018.¹

CS Program SBE Summary Table for Professional Services and Construction Contracts

The summary compares the dollar value of the Base Contracts, the SBE Contract Goals, the percent and dollar value expended to date and the SBE actual participation to date.

Appendix E - Monthly Progress Report - Reported Quarterly in 2018

CS Program SBE Summary Table for Professional Services and Construction Contracts

Contract No.	Contractor	Services/Segment	A	B	C	D	E	F	G	
			Contract Amount	SFMTA SBE Contract Goal	Contract Expenditure to Date (Est.)	SBE Actual to Date	SBE Contract \$s = A * B	SBE Amount to Date = C * D	Contractor's SBE Goal (in Bid)	
A Project Professional Services Contracts			<i>millions</i>		<i>millions</i>		<i>millions</i>		<i>millions</i>	
1	149	CS Partnership	Project Management	\$85.14	30%	\$72.67	32.4%	\$25.54	\$23.53	31.4%
2	156	Hill International	Project Controls Task 1	\$17.11	26%	\$10.08	30.0%	\$4.45	\$3.02	26.0%
3	155-1	PB Telemon	Tunnels Design	\$7.94	30%	\$7.90	30.2%	\$2.38	\$2.39	31.6%
4	155-2	CS Design Group	Stations Design	\$37.05	30%	\$42.20	31.6%	\$11.12	\$13.33	36.4%
5	155-3	HNTB, Inc. - B&C	Systems, Track & Surface Station Design	\$17.23	30%	\$15.28	25.9%	\$5.17	\$3.96	30.0%
Subtotal Professional Services				\$164.48		\$148.13		\$48.66	\$46.23	
B Project Construction Contracts			<i>millions</i>		<i>millions</i>		<i>millions</i>		<i>millions</i>	
1	1250	Synergy Inc	Utility Relocation 1	\$11.97	20%	\$11.97	97.2%	\$2.39	\$11.63	96.4%
2	1251	Synergy Inc	Utility Relocation 2	\$20.79	20%	\$20.79	87.4%	\$4.16	\$18.18	94.9%
3	1252	BIH	Tunnels and Portal - in Construction	\$241.29	6%	233.59	5.8%	\$14.48	\$13.51	6.1%
4	1277	MH Construction	Pagoda Demolition	\$0.65	100%	\$0.65	100.0%	\$0.65	\$0.65	100.0%
5	1300	Tutor-Perini	Stations/Track/Systems - in Construction	\$47.40	20%	\$643.14	22.9%	\$169.48	\$147.24	25.5%
Subtotal Construction Contracts				\$1,122.11		\$910.14		\$191.16	\$191.21	
Contract	Contractor	Services/Segment	Base Contract	SFMTA Goal	Expenditures	SBE Actual	= A * B	= C * D	Bid Goal	
			A	B	C	D	E	F	G	

SBE Summary Table Notes and Sources:

- a) Column A is the base contract amount awarded. Column B is the Agency SBE goal percent for each contract awarded.
The SFMTA SBE Contract Goals are also on the Central Subway web site under the listing of on-going contracts – see **“Closed and Awarded Contracts”** at this link: <http://centralsubwaysf.com/content/closed-and-awarded-contracts>
- b) Column C shows each contract’s current amount expended to date (estimated) including accruals. Column D is the actual SBE percent level of each contract based on payments to date.
Column E is the expected SBE dollar amount when the contract amount is completed and the SFMTA SBE goal achieved using this calculation: Columns A * B = Column E, the SBE Expected \$ Amount.

¹ An SBE is a for-profit, small business concern with a three (3) year average gross revenue not exceeding \$14 million or \$12 million, depending on the scope of work to be performed, that is certified under any of the following programs: the State of California's Small Business Program with the Department of General Services ("State Program"), the City and County of San Francisco's LBE Program ("City Program"), or the California Unified Certification Program ("Federal DBE program").

Column F is the actual SBE dollar amount out of the total contract expenditure to date:

Columns C * D = Column F, the SBE Expended \$ Amount.

The source of the SBE Actual percent to date and dollar amounts are Progress Payment Applications and Contractor's monthly submittals that may include the current estimated accruals.

The BIH SBE percent is from the contractor's progress payment #40, Form 6.

- c) Column G, the Contractor's SBE Goal in the submitted bid, is background information that is not calculated in the table. The table source of the Contractor's SBE Goals is from the SFMTA Contract Compliance Office. A Contractor's SBE goal in the bid is one source used by SFMTA Contract Compliance to assess and propose the Agency's SBE goal for a contract.
- d) The three constructions contracts shown in **bold type, 1250, 1251 and 1277**, with gray background, are completed contracts. Little to no changes will be shown in future reports.
- e) The SBE Hill International Actual to Date SBE participation is 30.0% for the overall SFMTA contract. The Hill International data is for the Central Subway Task 1 portion of the Hill International contract to provide SFMTA Project Controls services and systems.
- f) The SBE SFMTA goal for Contract 1300 Tutor-Perini is 20% SBE with a provision of 50% for trucking.

The 1300 Tutor-Perini SBE percent Actual is based on the SBE data provided in Progress Payment #57, September 2018, SFMTA SBE FORM No. 6.
- g) The SBE SFMTA goal for Contract 1277 MH Construction was based on an SBE set-aside.

SBE Participation Details

The two tables that follow present the Central Subway's professional services and construction contract amounts, expenditures and SBE levels with additional details.

Active Professional Services Contracts - SBE Participation Details

As of: 9/30/2018

Contract:	Project Management and Construction management	
Contract No.	CS-149 Central Subway Partnership*	
Status:	On-going	
	Base Contract Value	\$85,139,092
	Approved Change Orders	-0-
	Current Contract Value	\$85,139,092
	Expended to Date (est.)	72,666,838
	% Expended	85.4%
	SBE SFMTA Goal	30.0%
	SBE Participation	32.4%

Contract:	Project Controls Cost and Schedule Support	
Contract No.	CS 156 Hill International Task 1*	
Status:	On-going	
	Base Contract Value	\$17,112,873
	Approved Change Orders	-0-
	Current Contract Value	\$17,112,873
	Expended to Date (est.)	\$10,081,808
	% Expended	58.9%
	SBE SFMTA Goal	26.0%
	SBE Participation	30.0%

Contract:	Design Package 1 for CNs 1250, 1251 and 1252 Tunnels	
Contract No.	CS-155-1 PB / Telemon*	
Status:	Design is completed. Construction support ongoing	
	Base Contract Value	\$5,795,000
	Approved Change Orders (7)	\$2,145,159
	Current Contract Value	\$7,940,159
	Expended to Date (est.)	\$7,904,713
	% Expended	99.6%
	SBE SFMTA Goal	30.0%
	SBE Participation	30.2%

Contract:	Design Package 2 for 1253 UMS, 1254 CTS, 1255 YBM Stations.	
Contract No.	CS-155-2 Central Subway Design Group*	
Status:	Design is completed. Construction support ongoing	
	Base Contract Value	\$39,949,948
	Approved Change Orders (4)	\$7,950,658
	Current Contract Value	\$47,900,606
	Expended to Date (est.)	\$42,196,304
	% Expended	88.1%
	SBE SFMTA Goal	30.0%
	SBE Participation	31.6%

Contract:	DP 3 Systems, Track work, Surface station.	
Contract No.	CS-155-3 HNTB-B&C*	
Status:	Design is completed. Construction support ongoing	
	Base Contract Value	\$16,864,250
	Approved Change Orders (6)	\$1,637,474
	Current Contract Value	\$18,501,724
	Expended to Date (est.)	15,275,838
	% Expended	82.6%
	SBE SFMTA Goal	30.0%
	SBE Participation	25.9%

* denote accrual

Active and Completed Construction Contracts - SBE Participation Details

		Data as of:	9/30/2018
Contract:	Synergy Inc Utility Relocation 1 YBM & Launch Box		
Contract No.	1250		
Status:	Contract is completed and closed out		
	Base Contract Value		\$9,273,939
	Approved Change Orders		\$2,694,211
	Final Contract Value		\$11,968,150
	% Expended		100%
	SBE SFMTA Goal		20%
	SBE Participation To Date		97.2%
Contract:	Synergy Inc Utility Relocation 2 UMS		
Contract No.	1251		
Status:	Contract is completed and closed out		
	Base Contract Value		\$16,832,550
	Approved Change Orders		3,836,531
	Final Contract Value		\$20,699,081
	% Expended		100%
	SBE SFMTA Goal		20.0%
	SBE Participation To Date		87.4%
Contract:	Pagoda Palace Demolition / MH Construction		
Contract No.	1277		
Status:	Contract is completed and closed out		
	Base Contract Value		\$498,995
	Approved Change Orders		\$149,981
	Current Contract Value		\$648,976
	Expended to Date (est.)		\$648,976
	% Expended		100.00%
	SBE SFMTA Goal		100.0%
	SBE Participation To Date		100.0%
Contract:	Tunnels Barnard/Impregilo/Haley		
Contract No.	1252		
Status:	Construction is underway and ongoing		
	Base Contract Value		\$233,584,015
	Approved Change Orders		\$7,825,155
	Current Contract Value		\$241,409,170
	Expended to Date (est.)		\$233,589,322
	% Expended		96.8%
	SBE SFMTA Goal		6.0%
	SBE Participation To Date		5.8%
Contract:	Stations and Systems / Tutor Perini		
Contract No.	1300		
Status:	Construction is underway and ongoing		
	Base Contract Value		\$839,676,400
	Approved Change Orders		\$12,710,685
	Current Contract Value		\$852,387,085
	Expended to Date (est.)		\$643,141,614
	% Expended		75.5%
	SBE SFMTA Goal		20.0%
	SBE Participation To Date		22.9%

Photos on the next page:

(top to bottom) September 2018: At Chinatown Station, waterproofing, rebar, and concrete forms are installed inside the track crossover and platform caverns. Steel framing for escalators and stairs was installed at the south headwall, in addition to many interior walls and structures at Union Square/Market Street Station. At Yerba Buena/Moscone station, the final headhouse roof slab elements are constructed following the completion of the major structural elements. Concrete forms are built to construct the Surface station platform at 4th and Brannan Streets.

central subway

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