

central subway

Chinatown's North Access Shaft

Nearing the bottom of a future emergency exit stairwell, workers have begun connecting it with the future station cavern.



Progress Report

August 2016



SFMTA
Municipal
Transportation
Agency

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Table of Contents

Executive Summary	5
Key Milestones	6
Costs and Schedule	7
Costs	7
Earned Value Analysis	7
Schedule Highlights.....	8
Master Project Schedule	10
Contracts & Construction	11
Contract 1300 Stations, Surface, Track and Systems.....	12
Work Package1254 Chinatown Station	13
Work Package1253 Union Square / Market Street Station	16
Work Package1255 Yerba Buena / Moscone Station	19
Work Package1256 Systems, Trackwork & Stations	22
Program Components	24
Community Outreach	24
Quality Assurance	26
Risk Management	28
Program Safety & Security	29
Technical Capacity	31
Staffing	32
Third-Party Agreements and Licenses	33
LRV Procurement.....	33
Current Construction Activity and Progress Photos	34

See Table of Content page that follows for Cover Photo captions.

Table of Contents - continued

Appendices

Appendix A: Cost Report.....	A1
Appendix B: Schedule Report.....	B1
Appendix C: Program Overview of Scope and Funding.....	C1
Appendix D: Completed Contracts	D1
Appendix E: SBE Participation by Contract (Reported Quarterly).....	E1
Central Subway Project Contact Information.....	E5



Cover photo: *At the bottom of the north access shaft for Chinatown Station, workers inspect the beginning of work to excavate a tunnel which will connect the shaft with the station cavern, yet to be excavated itself. This shaft will eventually become an emergency exit stairwell at the north end of the station. **More photos can be found starting on page 33.***

Above photos: *A concrete truck driver cleans the chute of his truck while workers smooth concrete poured in a utility trench at 4th and Bluxome. Utilities are being upgraded and relocated on 4th Street between Bryant and King to prepare for construction of the concrete rail bed next year.*

See the Appendix E final page for CS websites hyperlinks and public outreach on line resources. The Project main web site is at: <http://www.centraSubwaysf.com/>



View looking up from the bottom of the north access shaft for Chinatown Station. The shaft is located on the east side of Stockton between Jackson and Washington.

Executive Summary

Continue excavation at YBM, UMS and CTS Stations for the future stations. STS continue works on Southbound and Northbound tunnel invert drain pipe and catch basins.

Chinatown Station - Headhouse excavated to 6' below Temp Level 5.0 and install walers and struts. Crosscut Cavern excavation is 75% complete. Completed North Emergency Egress Shaft excavation. Incidental street work (minor), ongoing monitoring and surveying.

Union Square/Market Street Station - North Concourse: Installed traffic signal conduits, sidewalk and ADA ramps and restored street. Installed vertical drain pipe, lookouts, star packs and shotcrete benches. Install concourse level walers and struts. Platform Station: Completed drilling and installing compensation grout pipes and performed preconditioning of compensation grout pipes. UMS Garage: Completed pour garage roof level shear walls. Commence excavation, formwork and rebar installation for the pile caps. Completed installation of new column footings along south wall of the garage. Continue FRP new shear walls along GL10, and B-line on Level 1

Yerba Buena/Moscone Station - Shotcrete for interior walls in Concourse is in progress. Placing cast-in-place concrete walls in Mezzanine is complete. Excavation and preparation for Vent Shaft floor placement in progress. Continue utility installation on east side of the 4th Street north of the north headwall.

Surface, Track and Systems— Continue 78" sewer rehabilitation. Continue 36" sewer force main. Continue Auxiliary Water Supply System (AWSS) installation. Continue tunnel drainage system installation. Continue tunnel invert slab drain pipe and catch basins work.

Tunnel - Contract administrative closeout is ongoing.

Total project costs to date are \$977.81 million, an increase of \$14.29 million over last month. The total cost to date equals 61.95% of the total project budget of \$1.578 billion. The Master Project Schedule forecast the Revenue Service Date of August 2019.

The Stations Contractors' Safety Reports show no recordable accidents took place this month and the rates of work site accident incidents by the man hours worked continue to be below industry standards - see tables on page 30.

Key Milestones

Installation of temporary steel supports inside UMS



1 Excess concrete is removed and walls are prepared for steel brackets



2 Compensation grouting work is performed ahead of bracket installation

MILESTONE	DATE EXPECTED
General	
Revenue Service	August 2019
Contract 1300 Stations, Surface, Track, Systems	
Notice to Proceed (NTP 1)	June 17, 2013 (A)
Notice to Proceed (NTP 2)	January 12, 2014 (A)
Substantial Completion	February 10, 2018



3 Brackets, walers, and struts are installed before the next phase of excavation begins.

Costs and Schedule

Costs (See Appendix A for Details)

The Current Cost Estimate (CCE) for the Central Subway Project is **\$1.578 billion** in year of expenditure dollars (\$YOE). This total project cost is shown at the top of Report 7.1, Program Project Budget. This capital cost projection incorporates allocated and unallocated contingencies to cover the risks associated with the project completion.

Total net incurred costs for the project are \$977.81 million, a \$14.29 million increase over last month. The cost to date figure reflects expenditures through FAMIS 786 Report (\$936.49 million) plus the utilities joint trench Form B Reimbursement payment (\$10.98 million), invoices currently being processed (\$9.09 million) and estimates of outstanding pay requests (\$21.24 million). This incurred amount equals 61.95% of the total project budget of \$1.578 billion.

The current funding level to date is \$1,179.79 million. This represents 75% of the total project budget.

Earned Value Analysis

In August 2016 Report, the Preliminary Earned Value Analysis reports is based on the SFMTA August Schedule Update. The Planned Value, Earned Value, Actual Cost, Percent Complete and resulting indexes as follows:

Preliminary August Earned Value

Overall Budgeted Cost:	\$1,578,300,000
Planned Value:	\$1,255,855,938
Earned Value:	\$980,430,272
Actual Cost:	\$977,805,430
Schedule Performance Index (SPI):	0.78
Cost Performance Index (CPI):	1.00
Percent Complete:	62.0%

Schedule Highlights

The Master Project Schedule (MPS) below includes progress through August 2016. The August 2016 Schedule Update submittal from Contract 1300 Contractor is rejected due to incorrect schedule logic to evaluate contract longest critical path. The Contract 1300 schedule represented in this report is based on the SFMTA August 2016 Schedule Update.

The MPS shows a forecast Revenue Service Date of August 2019.

The controlling critical (longest) path of the MPS runs through CTS Excavation succeeded by STS Startup & Testing, Commissioning and Pre-Revenue Activities to the Baseline Finish and Revenue Service Date. See Appendix B – Longest Path. The latest schedule shows the longest path running through the Chinatown Station (CTS). Contractor is required to implement a Recovery Schedule to put the Project back on schedule.

Schedule Contingency is fully utilized on the critical path of the MPS, which is below the Minimum Schedule Contingency level of 6 months. A schedule re-evaluation will be performed, utilizing the updated Contract 1300 Schedule. Recovery options are being implemented in key areas as work proceeds. SFMTA continues to meet with Contractor to discuss all schedule concerns and comments. Excavation and support of the Lower Left & Right Sidedrifts began in early August after additional excavation to reach the appropriate working elevation was achieved. TPC continues to experience lower production rates during their mining activity that resulted in a loss of 7 calendar days in month of August. Mitigation measures were continued by TPC in an effort to recover some lost time, extending workday shifts to two-twelve hours shifts, and progressing breakout of the remaining bottom center slurry wall. Contract 1300 Schedule delay in August results in the forecast Revenue Service Date slipping to August 1, 2019.

Contract 1300 Contractor submitted twenty-one (21) Schedule Updates from December 2014 to August 2016. SFMTA rejected eleven (11) Schedule Updates from September 2015 to April 2016 and June to August 2016. SFMTA approved as noted the May 2016 Schedule Update. Contractor has been directed to develop a Recovery Schedule as required by Contract and correct out-of-sequence and Retained Logic driving many of the forecast dates. Review of schedule updates as well as identifying recovery options is ongoing.

Contract 1300 Stations, Surface, Track and Systems

The Contractor, Tutor Perini Corporation's (TPC) baseline schedule is incorporated into the master program schedule. The preliminary SFMTA Contract 1300 August 2016 schedule is used within the August Report. The SFMTA Contract 1300 August 2016 schedule is based on the approved baseline schedule logic with adjustments made for fixing retained logic and lags. The SFMTA will continue to use the SFMTA Contract 1300 schedule update as a forecasting tool going forward until the Contract 1300 Contractor submits an acceptable schedule that addresses all of SFMTA scheduling concerns.



Workers use angle grinders to smooth edges of brackets ahead of water installation inside the station box.

Schedule Highlights - Continued

Work Package P-1254R (CTS) has performed the following work this month:

- Head house- excavated to 6' below Temp Level 5.0 and install walers and struts
- Cross Cut Cavern: Approximately 75% done with CCC under Stockton Street
- Incidental street work (minor), ongoing monitoring and surveying
- North Emergency Egress (NEE) Shaft – completed; NEE Tunnel approximately 50% completed

Work Package P-1253 (UMS) has performed the following work this month:

- Restored one lane on Stockton Street between Post and Geary Streets
- North Concourse: Installed traffic signal conduits, sidewalk, and ADA ramps; completed backfill and installed paving. Below deck: installed vertical drain pipe as well as lookouts, starpacks, and shotcrete benches. All North Concourse level wales and struts were previously installed with the exception of wales 9 and 10, which jog around the north headwall
- Platform Station: Completed drilling and installing compensation grout pipes and performed preconditioning of compensation grout pipes. Installed all remaining Concourse level wales with the exception of wales 9 and 10 and began installing struts. Installed most vertical drain pipes. Performed coring for jet grout verification. At roof deck pour 13-1A, performed pile repairs and installed steel decking to partially construct a portion of roof deck 1A, the remainder of which is being left open for access. Installed AWSS pipe at night
- South Concourse: Performed preconditioning of compensation grout pipes, primarily on the west side of the station. Excavated to expose escalator piles, chipped concrete to expose beams, and began installation of the temporary escalator wale/strut supports.
- Ellis: Installed rebar for the west side structural invert slab and repaired escalator shaft walls. Installed sewer culvert
- UMS Garage: Completed PCC030 Underpinning along GL-16. Commence excavation, formwork and rebar installation for the pile caps. Completed installation of new column footings along south wall of the garage. Continue FRP new shear walls along GL10, and B-line on Level 1

Work Package P-1255 (YBM) has performed the following work this month:

- Continue utility installation on east side of 4th Street north of the north headwall
- Shotcrete for interior walls in Concourse is in progress
- Placing cast-in-place concrete walls in Mezzanine is complete
- Excavation and preparation for Vent Shaft floor placement in progress

Work Package P-1256 (STS) has performed the following work this month:

- Started work on the 78" sewer offset manhole at 4th/Bluxome
- 78" Sewer slip-line work on 4th between Townsend and King is ongoing. Sewer is being prepped to receive the 66" HOBAS fiberglass pipes
- Auxiliary Water Supply System (AWSS) work on 4th between Townsend and King is on hold pending completion of 78" sewer slipline work
- Continue to install 48" sewer and 36" force main on 4th Freelon and Welsh
- Started MRY ductbank at 4th/Brannan intersection
- Continuing to install tunnel drain and pour tunnel invert between UMS and YBM

Master Project Schedule

Activity ID	Activity Name	Original Duration	2016			2017			2018			2019			2020		
			Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3		
CENTRAL SUBWAY PROJECT																	
Program Level Milestones																	
PJ01000	Central Subway Project Start	0															
M800044	Tunnel Excavation Complete - Project Milestone #44	0															
M800019	Baseline Finish Date: 12-26-2018	0															
M800009	CSP Revenue Service Date	0															
Preliminary Engineering Phase																	
Final Design																	
Light Rail Vehicles																	
Real Estate																	
Construction Phase																	
Construction Support and Costs																	
Construction Utility Contract #1- MOS & Portal CN-1250																	
Construction Utility Contract #2 - UMS CN-1251																	
Construction Tunnels CN-1252																	
Construction CN-1300																	
CN-1300 Milestone																	
Construction UMS Station P-1253																	
Construction CTS Station P-1254R																	
Construction YBM Station P-1255																	
Construction STS P-1256																	
Project Start Up																	
Unallocated Contingency																	
CO1.700	Cost Activity Unallocated Contingency (LOE) - 17,500,990,000 - Contingency	115															



A bulldozer loads a large steel container one bucket at a time. This container is used to remove all excavated material excavated from the headhouse and station cavern.

Contracts & Construction

Construction Contracts In Progress

Contract 1300: Combined Work Packages 1253, 1254, 1255, 1256

- Contractor: Tutor - Perini Corporation
- Amount: \$843.45 million
- Contract Status: 49.42% complete construction

Contracts Completed

[See Appendix D](#)

Contract 1250: Moscone Station and Portal Utilities Relocation

Contract 1251: Union Square/Market Street Station Utility Relocation

Contract 1277: Pagoda Theater Site Demolition (Funded separately from the CS Project budget)

Contract 1252: Central Subway Tunneling

Contract SBE Participation (Updated Quarterly) [See Appendix E](#)

Stations, Surface, Track and Systems

Contract 1300 Contractor: Tutor-Perini Corporation

Description of Work

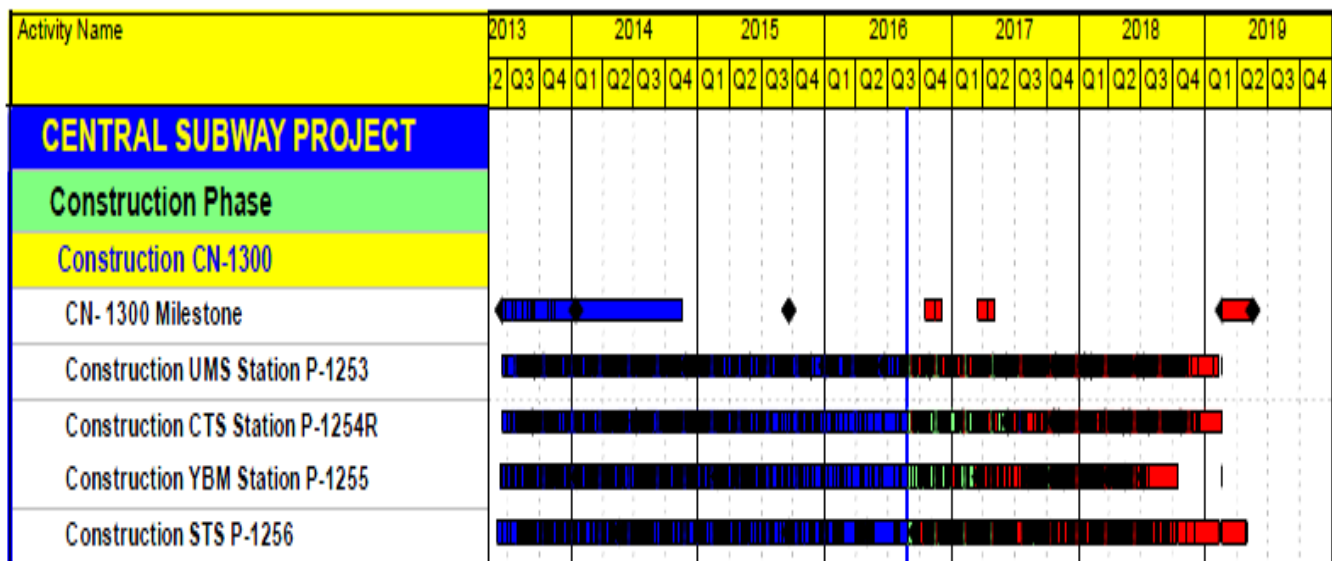
The Contract 1300 scope is to construct the Central Subway's three subway stations, one surface station, construct the 2,000 feet of surface track, and install track and operating systems throughout the new alignment. The separate station and systems work packages are presented in the following pages.

Work includes station finishes, AC and DC substations, elevators, escalators, lighting, emergency ventilation fans, HVAC fire alarm/ suppression/ protection, Cutter Soil Mixing, secant pile bottom up and Sequential Excavation Method construction, settlement monitoring, building protection, connecting to and modifying the BART Powell Street Station, PA, CCTV, signage, installation of fare collection equipment and station start-up and commissioning.

Contract Details	
Contract Awarded:	May 21, 2013
Notice to Proceed:	June 17, 2013
Substantial Completion:	February 10, 2018
Contract Award Value:	\$839,676,400
Modifications to Date:	\$3,770,307
Current Contract Value :	\$843,446,707

Budget/Expenditures▲	
Current Budget	\$879,601,400
Expenditures to Date	\$412,374,120

1300 Summary Schedule



Chinatown Station

Contract 1300 - Work Package 1254R



Description of Work

This Work Package is to construct one subway station. Includes station finishes, AC and DC Traction Power substations elevators, escalators, lighting, emergency ventilation fans, HVAC fire alarm/ suppression/ protection, slurry wall top-down construction, settlement monitoring, building protection, PA, CCTV, signage, installation of fare collection equipment and station start-up and commissioning.

Current Work Status

- Head house- excavated to 6' below Temp Level 5.0 and install walers and struts
- Cross Cut Cavern: Approximately 75% done with CCC under Stockton Street
- Incidental street work (minor), ongoing monitoring and surveying
- North Emergency Egress (NEE) Shaft completed; NEE Tunnel approximately 50% completed

Work Expected Next Month

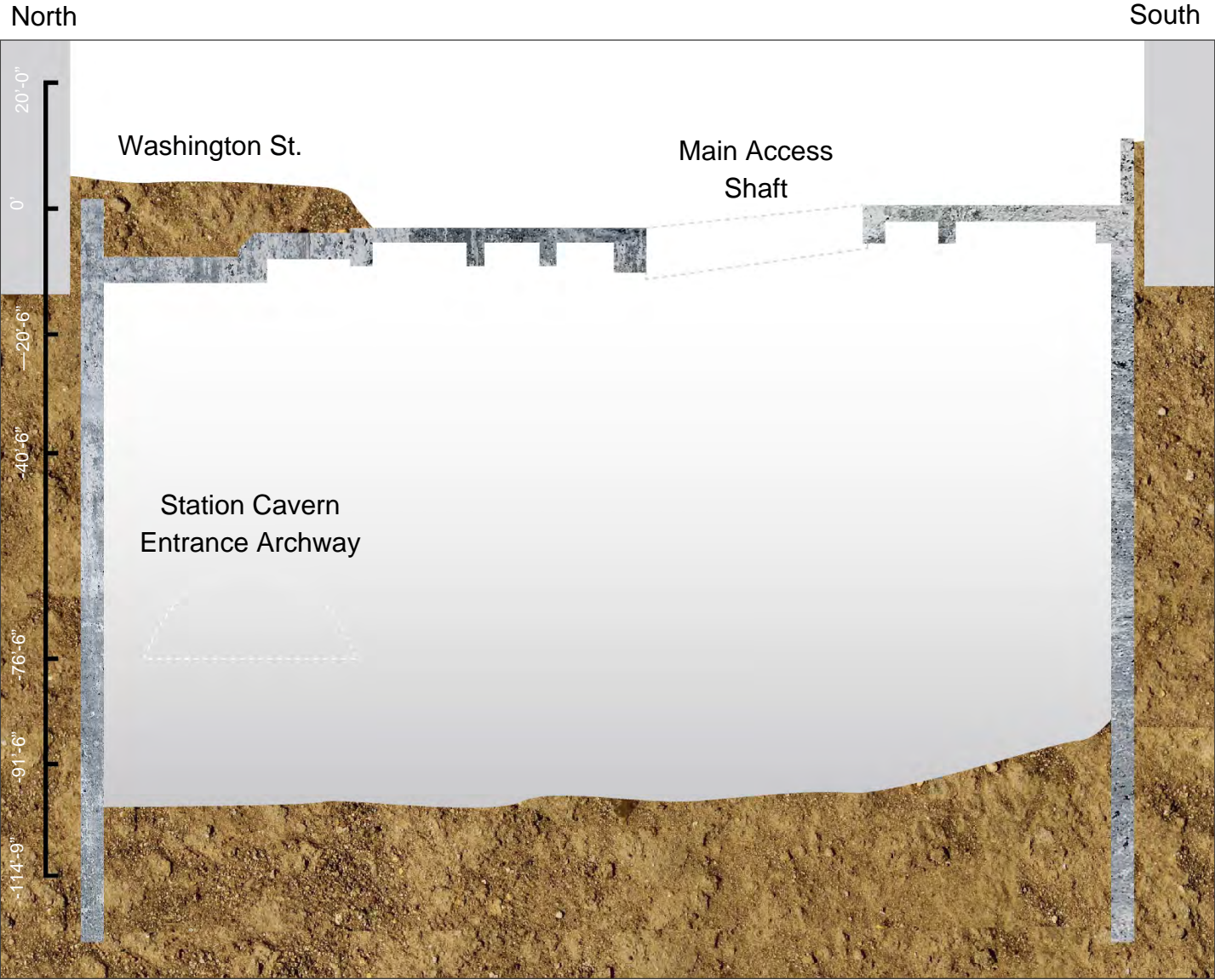
- Crosscut Cavern: complete Crosscut Cavern under Stockton Street
- Begin and complete barrel vault pipe drilling for North and South Platform Tunnels



Three Month Look Ahead

- Head house: Provide logistic support area for tunnel excavation
- North to South crosscut Cavern complete; Ongoing Platform Cavern excavation commences

Station Excavation and Construction Progress Section

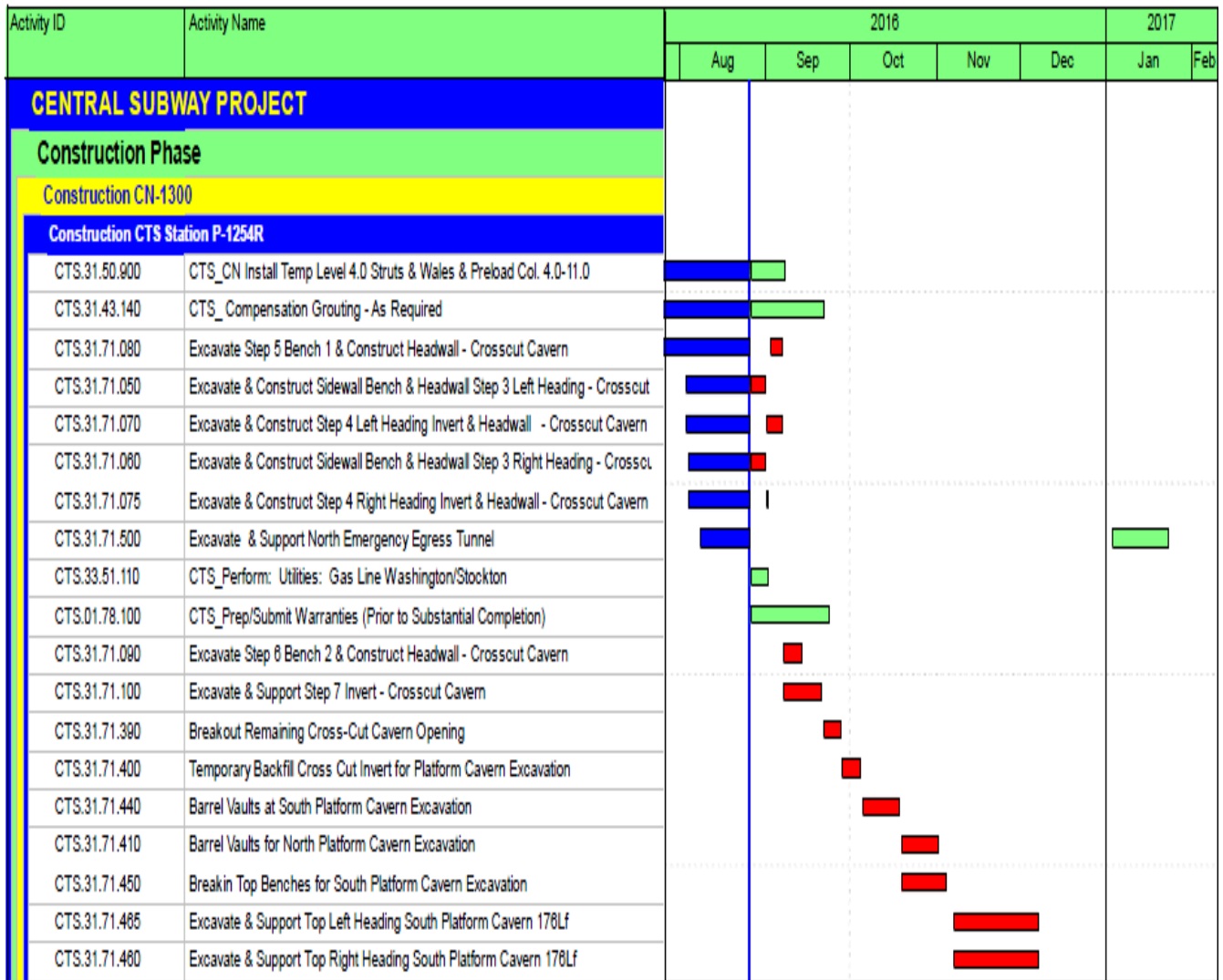


Chinatown Station Construction Status - Continued

Contract Details	
Contract Awarded:	May 21, 2013
Notice to Proceed:	June 17, 2013
Substantial Completion:	February 10, 2018
Contract Award Value:	\$247,567,810
Modifications to Date:	\$2,850,957
Current Contract Value:	\$250,418,767

Budget/Expenditures	
Current Budget	\$257,492,810
Expenditures to Date	\$113,226,610

CTS Three Month Schedule



Schedule: Contract 1300 August 2016 Update

Union Square/Market Street Station

Contract 1300 Work Package 1253

Description of Work

This Work Package is to construct one subway station. Includes station finishes, AC and DC Traction Power substations elevators, escalators, lighting, emergency ventilation fans, HVAC fire alarm/ suppression/ protection, slurry wall top-down construction, settlement monitoring, building protection, PA, CCTV, signage, installation of fare collection equipment and station start-up and commissioning.

Current Status This Month

- Restored one lane on Stockton Street between Post and Geary Streets
- North Concourse: Installed traffic signal conduits, sidewalk, and ADA ramps; completed backfill and installed paving. Below deck: installed vertical drain pipe as well as lookouts, starpacks, and shotcrete benches. All North Concourse Concourse level wales and struts were previously installed with the exception of wales 9 and 10, which jog around the north headwall
- Platform Station: Completed drilling and installing compensation grout pipes and performed preconditioning of compensation grout pipes. Installed all remaining Concourse level wales with the exception of wales 9 and 10 and began installing struts. Installed most vertical drain pipes. Performed coring for jet grout verification. At roof deck pour 13-1A, performed pile repairs and installed steel decking to partially construct a portion of roof deck 1A, the remainder of which is being left open for access. Installed AWSS pipe at night
- South Concourse: Performed preconditioning of compensation grout pipes, primarily on the west side of the station. Excavated to expose escalator piles, chipped concrete to expose beams, and began installation of the temporary escalator wale/strut supports.
- Ellis: Installed rebar for the west side structural invert slab and repaired escalator shaft walls. Installed sewer culvert
- UMS Garage: Completed PCC030 Underpinning along GL-16. Commence excavation, formwork and rebar installation for the pile caps. Completed installation of new column footings along south wall of the garage. Continue FRP new shear walls along GL10, and B-line on Level 1

Work Expected Next Month

- Nighttime lane closures expected for jet grout coring and utility work
- North Concourse: Below deck: install wales 9 and 10; installed shotcrete and steel for pile repair. Excavate North Concourse Fan Level
- Platform Station: Install wales 9 and 10. Complete preconditioning of compensation grout pipes. Installed lookouts, benches and wales. Install rebar and concrete for Roof Deck 13-1A. Complete installation of AWSS pipe at night.



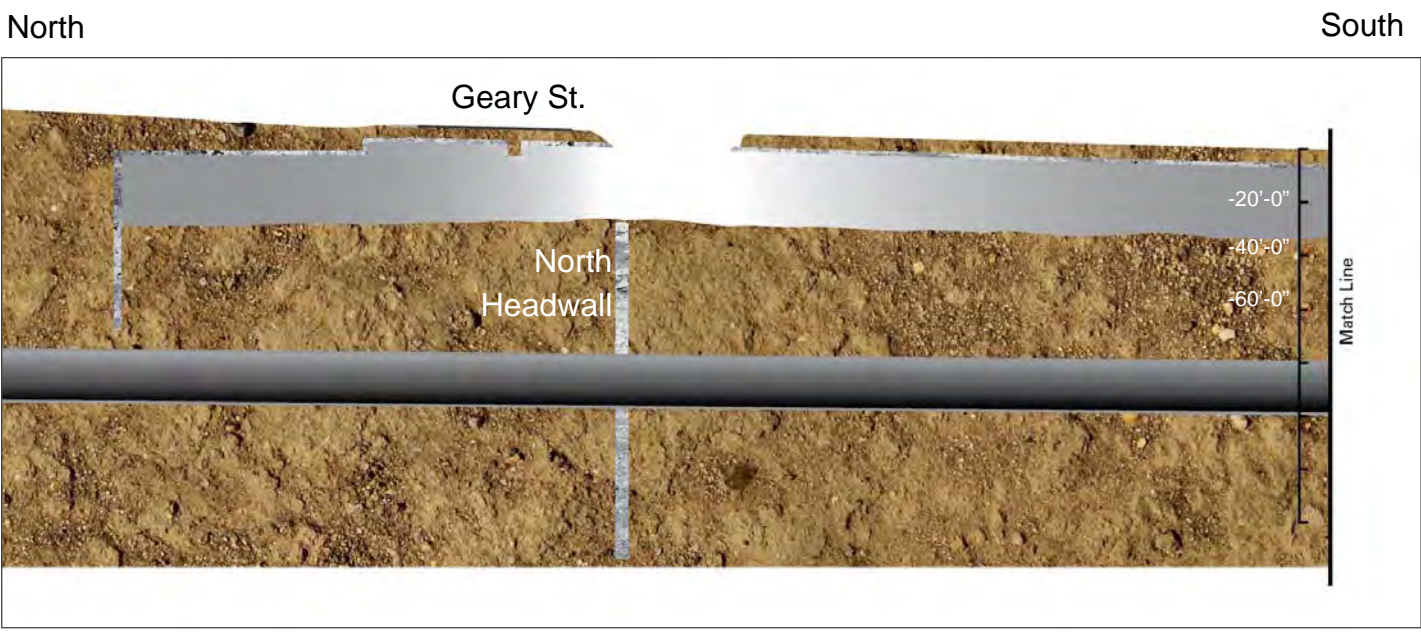
Continue excavation and support of Stairs 3&4 shaft on O'Farrell Street

- South Concourse: Complete preconditioning of compensation grout pipes. Continue to excavate and install support wales/struts at the escalator piles to reach the invert elevation of the pit
- Ellis Annex: Install and test seismic roof deck joint. Install waterproofing and lightweight concrete over roof deck joint. Backfill and restore
- UMS Garage: Begin excavation and construction of escalator pits. Install steel framework along the Geary South Wall. Tie-in remaining micropiles to existing footings. Install underpinning below the 16.3 to 17 line and install soil nail wall. Continue installing formwork, waterproofing, and install rebar and concrete for the plaza level deck

Three Month Look Ahead

- Platform Station: Continue jet grout activities; backfill, install utilities and restore street; dewatering; install permanent wales; excavate bench; install studs, mesh, drain pipe & shotcrete pile walls
- Access Shaft: break through battered piles & frame construction
- North Concourse: MRY conductor installation, continue compensation grout pipe installation and preconditioning, station power installation and gas line installation
- UMS Garage: Complete structural demolition; Complete underpinning and foundation work, commence fan level excavation within garage. Commence new foundations and erection of structural steel

Station Excavation and Construction Progress Section



Union Square Market Street Station Construction - Continued

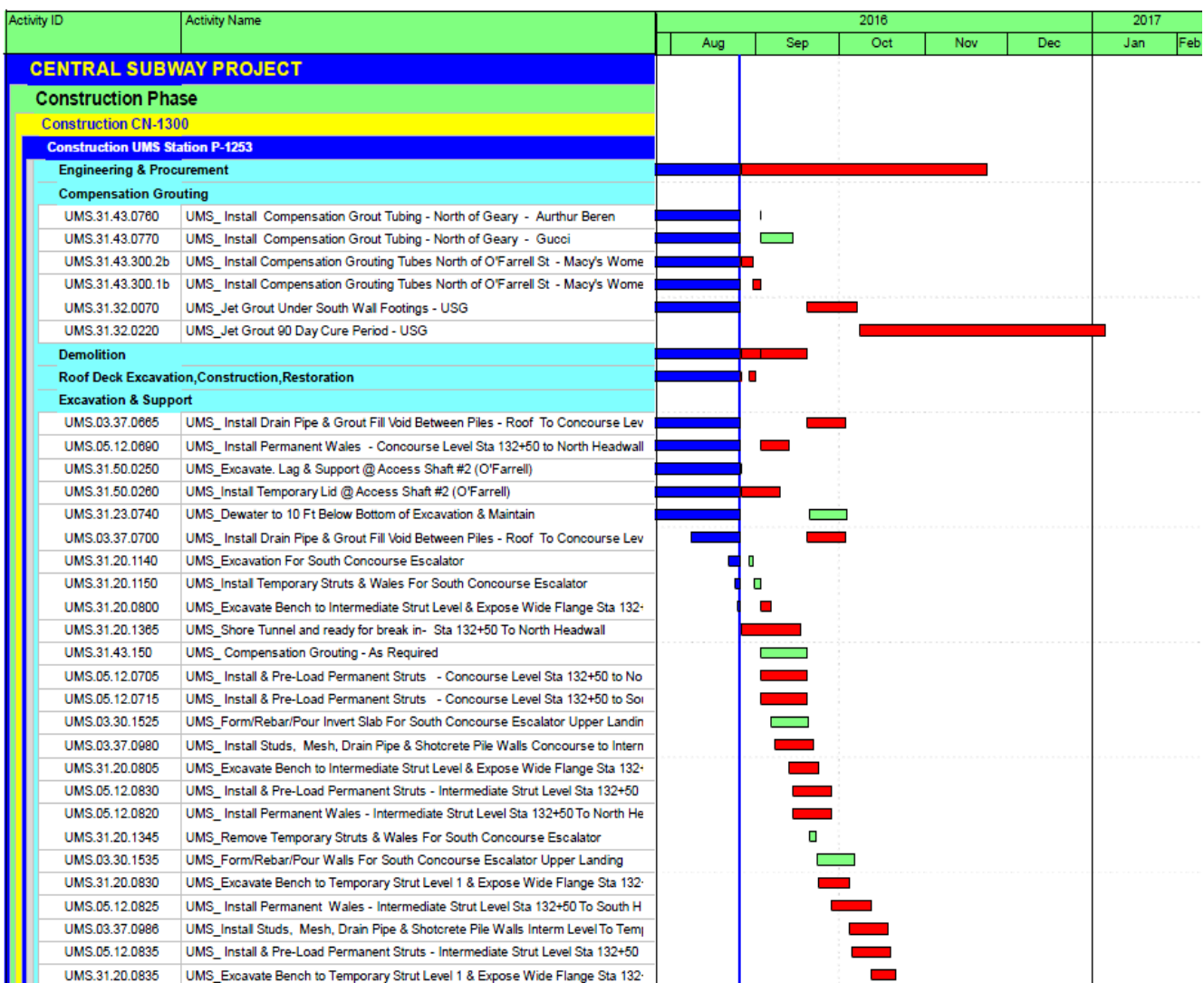
Contract Details

Contract Awarded:	May 21, 2013
Notice to Proceed:	June 17, 2013
Substantial Completion:	February 10, 2018
Contract Award Value:	\$294,030,590
Modifications to Date:	\$244,995
Current Contract Value:	\$294,275,585

Budget/Expenditures ▲

Current Budget	\$314,030,590
Expenditures to Date	\$164,967,079

UMS Three Month Schedule



Schedule: Contract 1300 August 2016 Update

Yerba Buena/Moscone Station

Contract 1300 - Work Package 1255



Description of Work

This Work Package is to construct one subway station. Includes station finishes, AC and DC Traction Power substations elevators, escalators, lighting, emergency ventilation fans, HVAC fire alarm/ suppression/ protection, slurry wall top-down construction, settlement monitoring, building protection, PA, CCTV, signage, installation of fare collection equipment and station start-up and commissioning.

Current Status

- Continue utility installation on east side of 4th Street north of the north headwall
- Shotcrete for interior walls in Concourse is in progress
- Placing cast-in-place concrete walls in Mezzanine is complete
- Excavation and preparation for Vent Shaft floor placement in progress

Work Expected Next Month

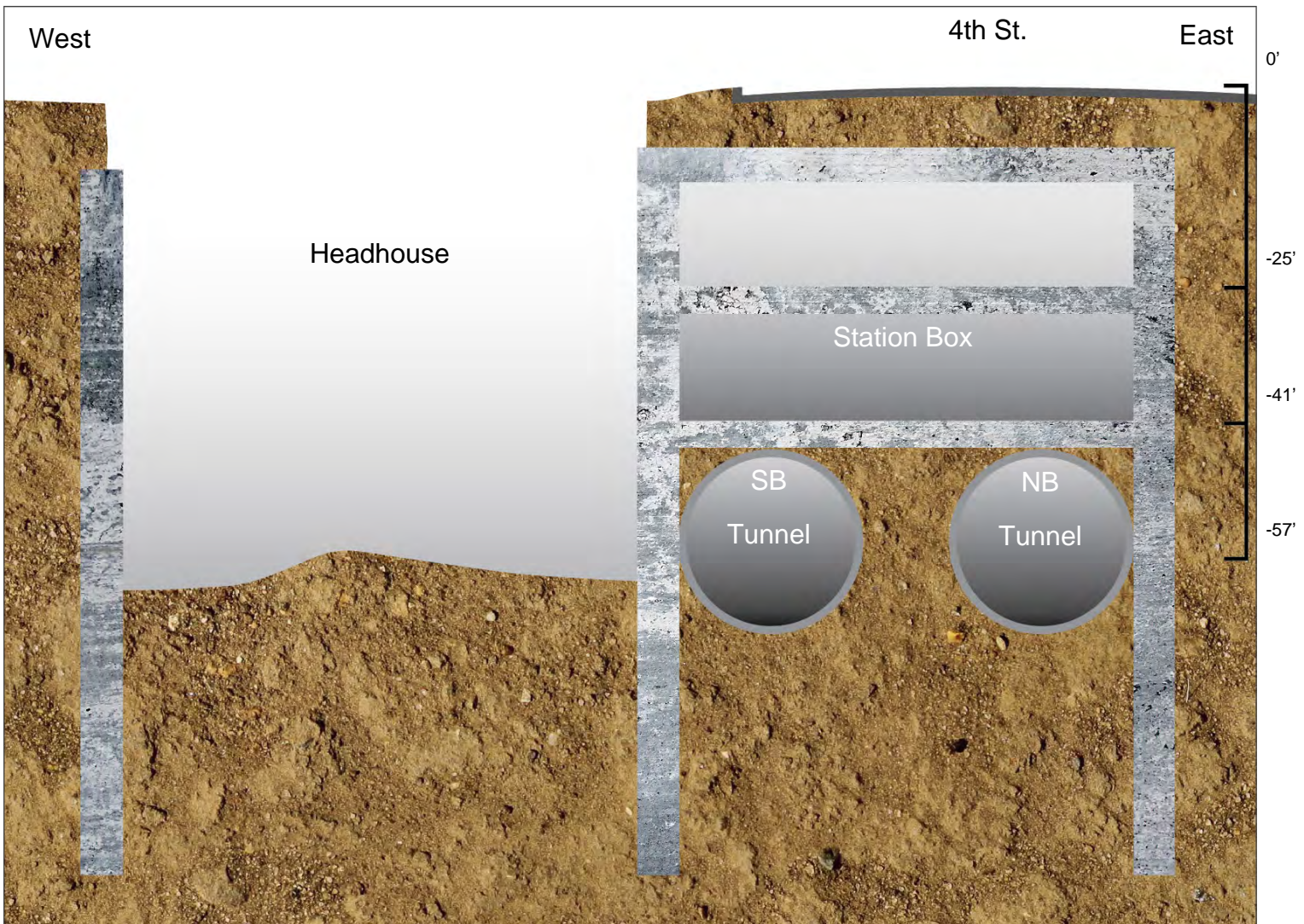
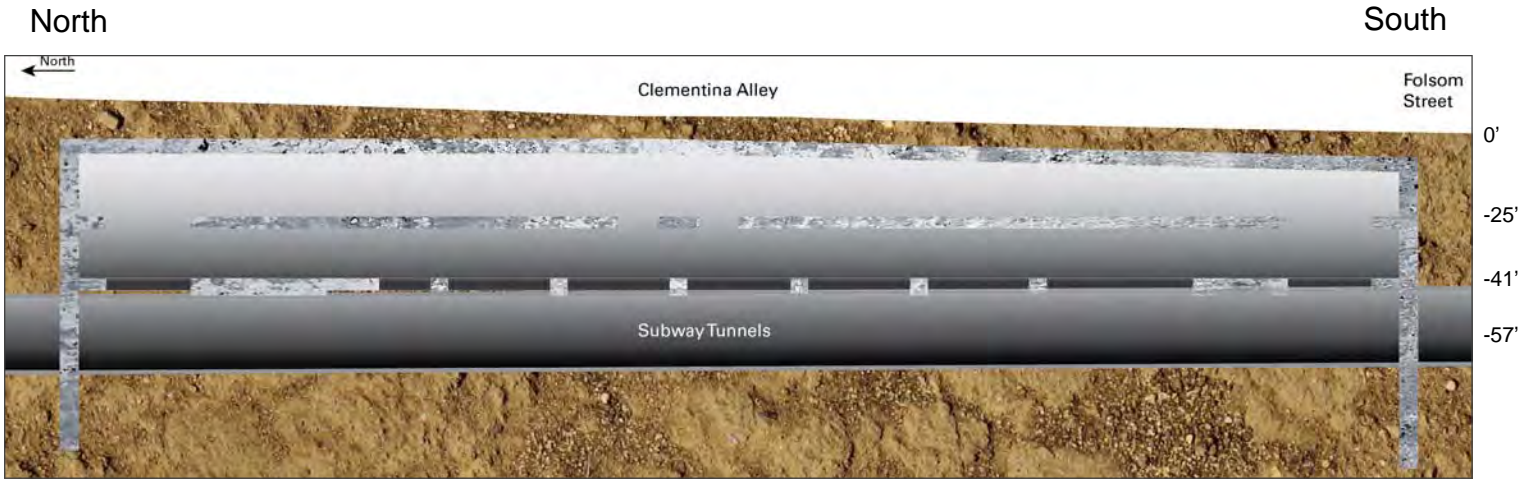
- Continue utility installation on east side of 4th Street north of the north headwall
- Continue placing shotcrete interior perimeter walls in Concourse
- Complete Vent Shaft floor waterproofing and concrete slab
- Begin breakthrough of tunnel liner and excavation beneath Concourse slab in Station box
- Preparation for placement of stairs within station box
- Preparation for CMU interior wall placement in Mezzanine Level



Three Month Look Ahead

- Complete tunnel liner removal and excavation in station box beneath Concourse level
- Complete installation of Level 6 Temporary Struts
- Prepare to place Invert within Station limits
- Continue shotcrete and CMU interior walls

Station Excavation and Construction Progress Section

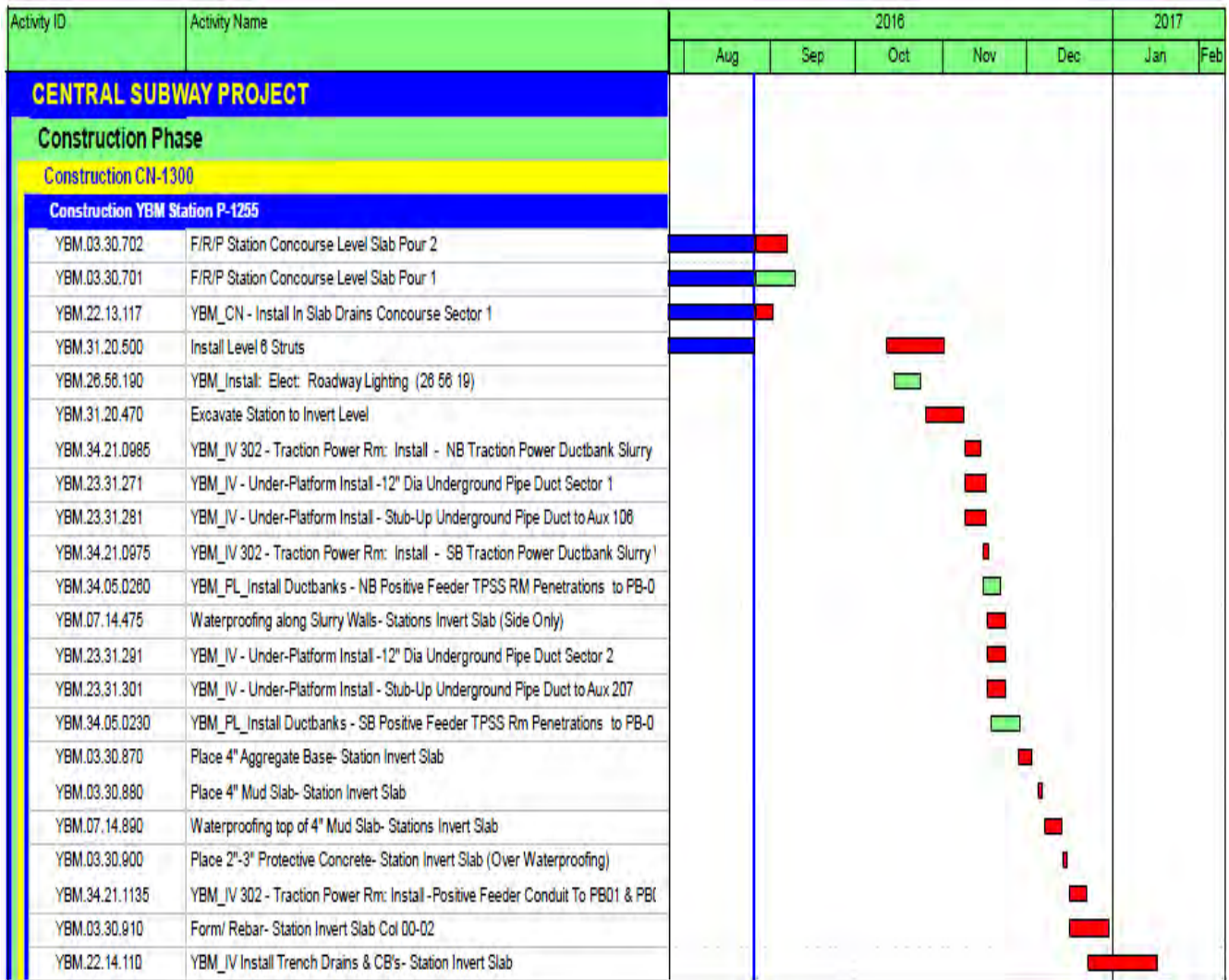


Yerba Buena Moscone Station Construction - Continued

Contract Details	
Contract Awarded:	May 21, 2013
Notice to Proceed:	June 17, 2013
Substantial Completion:	February 10, 2018
Contract Award Value:	\$158,089,000
Modifications to Date:	(\$645,932)
Current Contract Value:	\$157,443,068

Budget/Expenditures ▲	
Current Budget	\$163,089,000
Expenditures to Date	\$89,151,549

YBM Three Month Schedule



Schedule: Contract 1300 August 2016 Update

Systems, Trackwork, & Surface Station

Contract 1300 - Work Package 1256

Description of Work

This Work Package is to construct one Surface Station. Includes light rail track and systems, track invert, track safety walkways; light rail track and systems constructed on the 2,000 foot surface for the alignment from the tunnel portal, south to the tie-in to the existing Muni T-Line at Fourth and King Streets; and the surface Fourth and Brannan Street (FBS) Station.

Current Status

- Started work on the 78" sewer offset man-hole at 4th/Bluxome
- 78" Sewer slip-line work on 4th between Townsend and King is ongoing. Sewer is being prepped to receive the 66" HOBAS fiberglass pipes
- Auxiliary Water Supply System (AWSS) work on 4th between Townsend and King is on hold pending completion of 78" sewer slipline work
- Continue to install 48" sewer and 36" force main on 4th Freelon and Welsh
- Started MRY ductbank at 4th/Brannan intersection
- Continuing to install tunnel drain and pour tunnel invert between UMS and YBM



Work Expected Next Month

- Continue 78" sewer rehabilitation
- Continue 36" sewer force main installation
- Continue 48" sewer force main installation
- Continue MRY ductbank installation
- Resume AWSS installation
- Continue track drain and tunnel invert pour

Three Month Look Ahead

- Waterline installation
- AWSS installation
- Muni ductbank installation
- 36" sewer force main installation
- 48" sewer force main installation
- 78" sewer rehabilitation
- Track drain and tunnel invert construction
- Permanent pavement renovation

Systems, Trackwork, & Surface Station Construction - Continued

Contract Details	
Contract Awarded:	May 21, 2013
Notice to Proceed:	June 17, 2013
Substantial Completion:	February 10, 2018
Contract Award Value:	\$139,989,000
Modifications to Date:	\$1,320,286
Current Contract Value:	\$141,309,286

Budget/Expenditures	
Current Budget	\$144,989,000
Expenditures to Date	\$45,028,882

Systems, Track and Surface Station Three Month Schedule

Activity ID	Activity Name	2016					2017	
		Aug	Sep	Oct	Nov	Dec	Jan	Feb
CENTRAL SUBWAY PROJECT								
Construction Phase								
Construction CN-1300								
Construction STS P-1256								
STS.33.11.210	STS_Install: Utilities: Auxiliary Water Supply 12" Main 4th St (Through Townsend							
STS.33.31.480	Install New 48" Gravity Sewer Main - Welsh St. To Freelon St							
STS.33.31.510	Install New 36" Force Main Sewer 4th St - Welsh To Freelon St							
STS.33.31.480	Install New 48" Gravity Sewer Main Manhole @ 4th/Brannan St							
STS.34.42.1080	STS_Install: Security - SB Portal Intrusion Devices							
STS.28.05.120	STS_Install: Utilities: 230Kv Electrical Transmission Casing - Assist PG&E - 4th							
STS.33.31.380	STS_Install New Offset Sewer Manhole @ 4th St/Townsend							
STS.33.11.170	STS_Install: Utilities: Auxiliary Water Supply 12" Main 4th St (Through Brannan St							
STS.33.51.150	STS_Pothole: Utilities: Pothole for Gas Distribution							
STS.33.31.630	Sewer Manhole relocation at King St/ 4th Street							
STS.33.51.115	STS_PG&E Design Detail Period: Utilities: Gas Distribution 6" Main/Casing - 4th							
STS.33.31.420	STS_Install New CB's Manhole, 10" & 15" Sewer Piping @ 4th St/Townsend - We							
STS.33.11.340	STS_Install: Utilities: Track Drainage- 4th St (Brannan St To Bluxome St)							
STS.33.11.350	STS_Install: Utilities: Track Drainage- 4th St (Bluxome St To Townsend St)							
STS.33.31.400	STS_Install New 27" Sewer Pipe/Casing Connecting Offset Manhole To East MH @							
STS.33.31.340	STS_Install New Sewer Laterals & Sleeves East Side 4th Street - Brannan to Blux							
STS.33.31.445	Install New 48" Gravity Sewer Main Manhole @ 4th/Welsh St							
STS.33.31.410	STS_Install New CB's, 10" & 24" Sewer Piping @ 4th St/Townsend - East Side							
STS.33.31.440	STS_Install New Sewer Laterals & Sleeves East Side 4th Street - Townsend To Ki							
STS.33.31.450	Install New 48" Gravity Sewer Main - Bryant St To Welsh St.							
STS.33.31.470	Install New 48" Gravity Sewer Main Manhole @ 4th/Freelon St							
STS.33.31.250	STS_Install New Manhole, Sewer Piping & Catch Basins - East Side Welsh St/4th							
STS.33.11.270	STS_Install: Utilities: Sewer: Casing for 10" Force Main - 4th St Sta @ Brannan S							
STS.28.05.0290	STS_Install: Tunnel Electrical - Unistrut For Conduit & Signal Supports - NB Porta							
STS.28.05.0530	STS_Install: Tunnel Electrical - Unistrut For Conduit & Signal Supports - SB Porta							
STS.33.31.390	STS_Install New Sewer Manhole @ 4th St/Townsend - East Side							
STS.33.31.280	STS_Install New 18" Sewer Lateral In Welsh St To Future 48" Manhole							
STS.33.31.430	STS_Install New Sewer Laterals & Sleeves West Side 4th Street - Townsend To K							

Schedule: Contract 1300 August 2016 Update

Program Components

Community Outreach

Outreach public information, events and presentations for August 2016 include:

- Conducted Community Advisory Group Meeting and tour of Chinatown Station site
- Prepared and distributed construction notices
- Conducted meetings with various merchant stakeholders along the alignment
- Responded to constituents' questions and concerns
- Conducted Central Subway tour for U.S. Senate Appropriations Committee
- Participated in planning meeting for Winter Walk 2016

Outreach in Support of Mitigation and Monitoring

- Team members conducted weekly progress meetings to address stakeholders concerns
- Outreach and communication efforts continue in Chinatown, Union Square, and SOMA; Addressed Chinatown night noise and merchants' complaints
- Continue noise mitigation meetings with Tutor Perini and community stakeholders
- Address parking concern and white zone parking from Gum Moon Women's Residence and the Asian Women's Resource Center
- Weekly photo documentation of project work and editing
- Weekly construction update emails sent to list of approximately 700 residents and stakeholders

Media Coverage

Central Subway Media Coverage			
Date	Title (with link to story)	Source	Reporter/Writer
8/3/16	Pedestrian Group Tries to Save Car-Free Stockton St. Proposal	SF Examiner	Joe Fitzgerald Rodriguez
8/7/16	Rose Pak Flashes Red on Stockton St. Pedestrian Mall	San Francisco Chronicle	Matier & Ross
8/8/16	Closing Stockton Street Won't Be a Walk in the Park	San Francisco Chronicle	C.W. Nevius

Community Outreach - Continued

Central Subway Media Coverage			
Date	Title (with link to story)	Source	Reporter/Writer
8/8/16	Rose Pak Threatens City Hall Blockade Over Stockton St. Proposal	CBS KPIX 5	Phil Matier
8/9/16	Rose Pak Vows City Hall Blockade to Stop Stockton St. Pedestrian Mall	SFist	Caleb Pershan
8/10/16	Waiting and Wondering About All These Big Bay Area Transportation Projects	SF Gate	Sasha Lekach
8/10/16	Union Square wants Stockton closure, Chinatown power player says no	CURBD	Adam Brinklow
8/18/16	SF Public Invited to Help Shape Future of City Subways	SF Gate	Michael Cabanatuan
8/18/16	City Planning for Flood of Development Along SF's Southern Waterfront	SF Examiner	Joe Fitzgerald Rodriguez
8/18/16	San Francisco Public Invited to Help Shape Future of Subways	Mass Transit Magazine	Michael Cabanatuan
8/19/16	SF Wants You to Design Its Future Transit System	CITYLAB	John Metcalfe
8/19/16	SFMTA Wants Your Help Planning San Francisco's Subway Future	STREETSBLOG SF	Roger Rudick
8/22/16	Here's Why San Francisco's Light-Rail Layout is Such a Mess	Bold Italic	Chelsea Iversen

Quality Assurance

Quality Assurance monthly activity of oversight, surveillance, audits, proactive feedback and QA records actively involves the Project construction management staff, the resident engineers, the prime construction contractor and their subcontractors.

Stations and Systems Contract CN1300 Quality Assurance Monitoring – On Going/As Reported Previously

- UMS roof beam to pile WF installation – Continued Inspection/acceptance/documentation by Smith Emery CWI's of all welds associated with the ongoing Installation of some structural and mostly excavation support steel
- UMS and YBM Water Proofing Subcontractor – substrate preparation and application inspection/acceptance of work performed by Waterproofing Experts by RDH continues for the few remaining roof placements
- TPC QC Daily Inspection Reports posted to CM13 which includes TPC's Specialty Subcontractor's QC checklists and associated documentation and Smith Emery Inspection Reports; TPC's Subcontractor that provides laboratory and Inspection Services – including Special Inspections required for the City of San Francisco's Department of Building Inspection (DBI) for all permitted work
- Installation of tunnel invert concrete continues
- Preparatory and Initial Phase Meetings continue as scheduled. Additional Initial Phase Meetings are conducted at the request of the SFMTA or TPC QC as circumstances demand or as unanticipated issues occur. As noted previously, additional Initial Phase Meetings are usually very effective in preventing recurring unsatisfactory work, primarily because responsible Field Supervision is required to attend
- Sequential Excavation Method (SEM) Meetings (daily at 8:00 am at the CTS site) and Instrumentation/Monitoring and Construction Management Task Force (CMTF) Meetings (daily at 9:00 am).
- Bi-Weekly Quality Task Force (QTF) Meetings – ongoing dialog regarding; planning for upcoming Work, identification and mitigation of in-process potentially unsatisfactory work, generation of CNCR and other items related to TPC's QC efforts in implementing TPC's approved Quality Control Program (QCP)
- Weekly Work Package Progress Meetings for STS, YBM, UMS and CTS
- Monthly Project Risk Mitigation and Safety and Security Meetings

Document comment and review:

- Contractor's submittals, e.g., review of welding, concrete (including shotcrete) and other Quality related submittals/comments as requested to support the RE's and CM, and RFIs related to quality and welding
- QA Staff continues random/spot checks of the 1300 Contractor's Field Testing lab results; the now few items requiring further action/investigation (missing or inconsistent data, compressive strength results that appear to have a very broad range of values, failure to adequately identify the location where tests/specimens are taken and such) are brought to the attention of the Contractor

Quality Assurance - Continued

- Contractor Non Conformance Reports (CNCR) Status as indicated in the TPC QC CNCR Log:
 - ◇ 16 CNCRs are currently posted to the CNCR Log as INITIAL entries (C1300 is required to generate a CNCR within 24 hours of becoming aware of what appears to be non conforming work).
 - ◇ 12 CNCRs are currently posted to the CNCR Log as DISPOSITIONED and are being reviewed by associated SFMTA RE to verify that the Contractor's proposed disposition is appropriate.)
 - ◇ 4 CNCRs are currently posted to the CNCR Log as DISPOSITIONED (NOT ACCEPTABLE) and have been returned to the Contractor because the RE's review of the Contractor's proposed disposition determined that the proposed disposition is not appropriate and must be revised).
 - ◇ 16 CNCRs are currently posted to the CNCR Log as APPROVED because the suggested REPAIR dispositions have been approved and the CNCRs will remain open until the approved REPAIR procedure is performed.
 - ◇ 153 CNCRs are currently posted to the CNCR Log as CLOSED
 - ◇ 27 CNCRs are currently posted to the CNCR Log as VOIDED (subsequent evaluation of the INITIAL CNCRs determined that a CNCR is not warranted.
 - ◇ 229 CNCRs are currently posted to the CNCR Log

QA Issues:

- None to report for this period

QA Concerns:

- TPC performing work prior to receipt of approval status of required submittals/RIFs with/without knowledge of TPC QC remains a potential item(s) of concern. Note that TPC QC's preparation for both Preparation and Initial Phase Meetings, which includes comprehensive listing and status of all associated submittals, is greatly facilitating the performance of acceptable Work. Additionally, through SFMTA's refusal to release SFMTA Hold Points primarily for concrete placement, results in TPC QC ensuring that all required submittals/RIFs/CNCRs, that often manifest themselves "at the last minute" prior to placement of concrete, have been approved
- Appropriate design review/sign-off of composite drawings for tunnel inverts, prior to concrete placement (assuring that all required systems/facilities conduits are embedded as required)
- TPC's Record Document (as-built) effort refinement, to include CNCRs and a timely recording of Work performed that is different than what is required by the latest approved Conformed Design Drawings; note that a meeting was held with the UMS RE and ARE and TPC's PE and Office/ Filed Engineer in which it was agreed that TPC's current As-Build effort is closer to meeting the intent of the Record Document requirements of the Contract; sans Utility As-Builts (which are to be provided for our review) and for clarification of some existing redline mark-ups

Other Program QA Practices Implemented

- Close-out of Corrective Action Requests: Close outs continued from Quality Assurance staff's Audits, Surveillances and PMOC Quarterly Reviews. The status is tracked in the Corrective Action Log that is provided to the project team and the FTA PMOC
- TPC's response to Quality Assurance Audit Report QAA 026, Implementation of TPC's Quality Control Program (QCP) was received and is currently under review

Risk Management

Risk Mitigation Management Meeting No. 85 was held on August 4, 2016. The Risk Assessment Committee reviewed and discussed Risks that included Construction Risks with ratings above 6; and Active Construction Risks rated below 6.

To date, the Project is tracking forty (40) remaining construction risk items and one (1) remaining requirement risk on the Project Risk Register. During CSP's monthly Risk Management – Risk Mitigation Meeting, the Committee focused on the top 10 risk items, identified and assessed as currently representing the greatest risk exposure to the Project. The status of these risk items are closely monitored and updated monthly by the risk owner. Generally tracking the progress made with the use of individual risk mitigation status sheet, to determine if the assigned mitigation strategies are successfully being implemented.

The other thirty-one risk on the Project Risk Register are viewed in rotation, as they relate to current construction activities taking place in the field.

During the meeting the Committee also conducted a risk assessment and applied mitigation strategies to one new risk on the register and retired and older risk being tracked, which no longer poses a risk to the Project. Two other risks on the register received an update to the risk description to accurately describe the actual risk.

Top Ten Risk

Risk #	Risk Description	Risk Owner	Risk Category	Risk Rating
232	Behind Schedule - Unable to Recover from Delay to 1300 Contract	ES	C	12
233	Shotcrete Substitution - Final Finish Concrete Lining is Inferior	DJ	C	9
240	Unresolved Assignment of Schedule Delay Responsibility (may lead to increase cost)	ES	C	8
243	Contractor becomes complacent in third party insurance claims - could increase cost to the project	AH	C	8
234	Sequential Excavation Method at CTS - Contractor's propose method will induce subsidence	DJ	C	7
52	Unacceptable settlement and impact on major utilities at CTS. (OLD SEWERS AND OTHERS WITHIN 20FT SPACE BETWEEN TOP OF CAVERN AND STREET LEVEL)	DJ	C	6
46	Public complaints result in unanticipated restrictions on construction at CTS. (schedule and estimate for underground work assumes 6 day work week and 2 shifts per day)	DJ	C	6
238	Quality Program is ineffective in processing the nonconformance items causing schedule impacts	ML	C	6
205	Prolong period of CMod's creates additional cost/causes bad blood between Resident Engineer and Contractor	ES	C	6
229	CN1300 System Acceptance Testing	AH	C	6

Program Safety & Security

The San Francisco Municipal Transportation Agency is committed to the highest practical level of safety and security standards and practices in the public transit industry. The Safety and Security Management Plan (SSMP) components are reported on below as appropriate including, Safety and Security Committee, the Fire Life Safety and Security Committee the Construction Conformance Verification and Documentation and Contractor Safety and Security.

Project Management/Construction Management (PMCM) Team

Safety bulletin boards have subjects covering the complacency and daily job briefings.

Safety Summary for the 1300 Stations Systems Track Construction Package

During the month of August, safety work on the 1300 contract progressed as follows:

STS had no recordable or first aid incidents. Work on the invert continues and has reached the YBM station.

YBM had no recordable injuries but one first aid injury this month. Work taking place is in the upper levels of the station.

UMS had no injuries in August. Layne is getting close to completing their tasks for the time.

CTS had one first aid injury this month. A flagger tripped and fell while walking across an intersection. Excavation of the two drifts is almost completed. Frontier Kemper excavated the upper section of the barrel section.

Table 1300 Stations Construction Safety Record

Table 1300 below summarizes the Month to Date and Project to Date for the Stations, Systems and Track Construction contractor and subcontractors.

Next Month Look Ahead

1300 Contract

1. AT CTS, Frontier-Kemper (FK) continues with the excavation of the barrel vault. Ventilation clarifications are still being worked on to ensure good ventilation exists. FK should get getting close to finishing barrel vault and start excavating the main cavern.

Program Safety & Security - continued

2. At UMS, Layne has completed their tasks for the present time. TPC is starting to lower struts and walers into the hole. After they are placed, they will start excavating to the next lower level.
3. AT YBM, TPC will start breaking into the tunnel.
4. At STS, work continues in the tunnels. They have gone just past the YBM station. Valverde is building a large jacking pit and traffic concerns are of major concern.

Project Safety Record - Contract 1300

SAFETY GOALS

Through Month End - Aug 2016

OSHA Recordable Accidents, <3.4 Lost Time Cases, <1.6
--

JOB TO DATE	Tutor	Subs	Total Project	Rate*
OSHA Recordable Accidents	4	1	5	0.77
Job Transfer or Restricted Duty Cases	0	0	0	0.00
Lost Time Cases	0	0	0	0.00
Total Project Incidents	4	1	5	0.77
Man Hours Worked Through M/E August 2016	624,371	676,331	1,300,701	

YEAR TO DATE (Month ,Day, Year to Month, Day, Year)	Tutor	Subs	Total Project	Rate*
OSHA Recordable Accidents	0	1	1	0.47
Job Transfer or Restricted Duty Cases	0	0	0	0.00
Lost Time Cases	0	0	0	0.00
Total Project Incidents	0	0	1	0.47
Man Hours Worked Through M/E August 2016	222,656	199,485	422,141	

* Rate is calculated based on number of incidents divided by total number of man hours worked multiplied by 200,000 man hours.
 OSHA Recordable Accidents - 2008 Construction Industry Rate for Highway, Street, and Bridge Construction = 3.9

*Classifications change at a later date due to additional information becoming available, thereby, changing the numbers on the chart.
 For example, what was once classified as an accident can become a first aid which leads it to no longer being recordable.

Technical Capacity

No Project positions are currently open or unfilled.



The site foreman says hello while workers sift through rebar lowered into the headhouse below. On the bottom level, the southbound tunnel's casing is exposed prior to demolition.

Staffing

The Central Subway Staffing Table shows Planned and Actual full-time equivalent staff (FTEs) working on the Program by organizational function and responsibility.

	Jun-2016		Jul-2016		Aug-2016	
	Planned	Actual	Planned	Actual	Planned	Actual
Project Management						
Program Management	6.60	6.00	6.60	6.00	6.60	6.00
Quality Assurance	1.80	2.30	1.80	2.30	1.80	2.30
Contract Administration	1.40	1.40	1.40	1.40	1.40	1.40
Community Outreach	5.50	4.00	5.50	4.00	5.50	4.00
Finance	2.00	2.00	2.00	2.00	2.00	2.00
Project Controls	4.80	6.30	4.80	6.30	4.80	6.30
Subtotal	22.10	22.00	22.10	22.00	22.10	22.00
Construction Management						
CM - CN 1252	1.00	1.00	1.00	1.00	1.00	1.00
CM - CN 1300	30.00	31.00	29.98	30.98	29.98	30.98
Design Support - CN 1252	0.00	0.00	0.00	0.00	0.00	0.00
Design Support - CN 1300	12.40	12.40	12.40	12.40	12.40	12.40
Subtotal	43.40	44.40	43.38	44.38	43.38	44.38
Start Up						
Start Up / Safety & Security	3.00	2.10	3.00	2.10	3.00	2.10
Subtotal	3.00	2.10	3.00	2.10	3.00	2.10
Total	68.50	68.50	68.48	68.48	68.48	68.48

Third-Party Agreements

No activity in this reporting month.

LRV Procurement

SFMTA has initiated a new light rail vehicle procurement to acquire up to 260 vehicles over the next 15 years. The scope includes the design, manufacture, delivery and testing of up to 260 light rail vehicles together with associated services, spare parts, special tools, training and documentation. This includes an initial delivery of 24 cars, scheduled for delivery in 2018 to supplement the fleet when the SFMTA's Third Street Phase 2 - Central Subway Project extension opens.

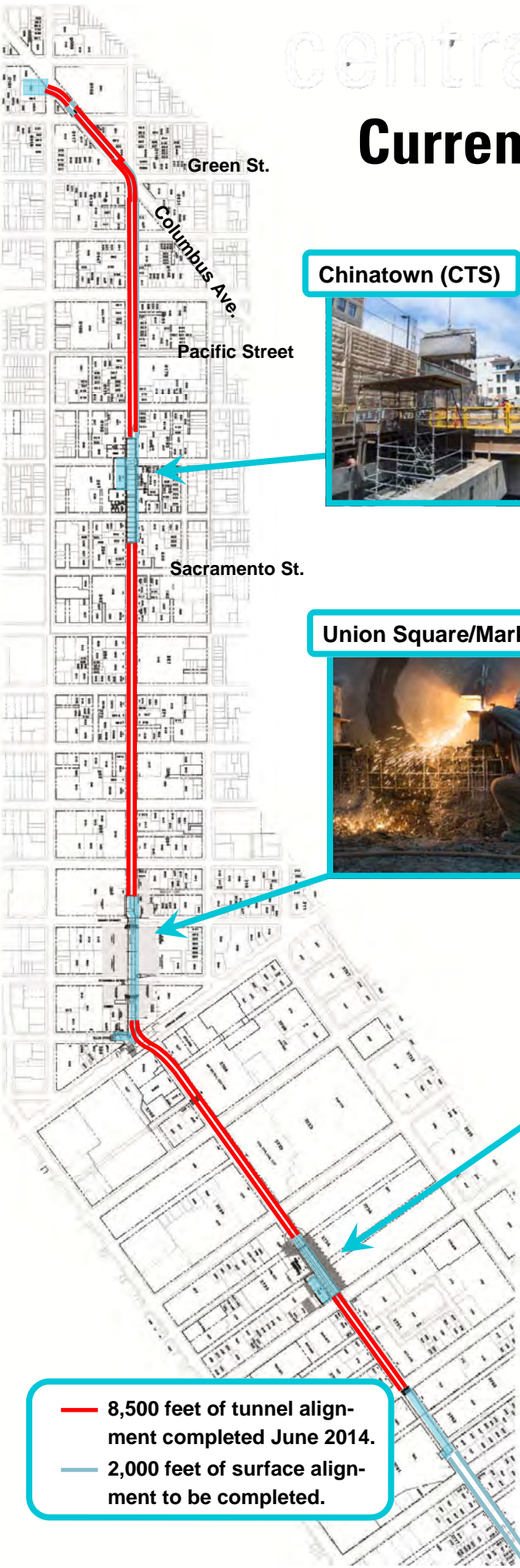
Production of the first four cars is underway; the first vehicle has started testing at the Siemens' facility in Sacramento, the second car structure is complete and is in finishing, and the third and fourth car structure is being completed. The fourth car shell structure will be used for compression testing of the car shell. The first car set of trucks is approaching completion.



A jackhammer pulverizes bedrock uncovered during excavation work inside the Chinatown Station headhouse.

central **T** subway

Current Construction Activity



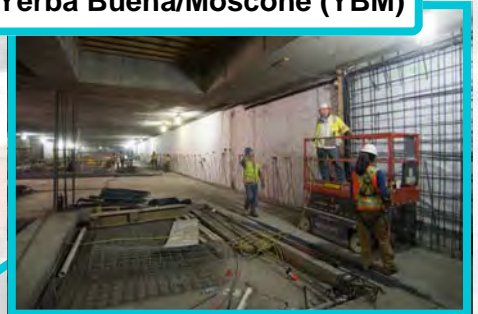
Chinatown (CTS)



Union Square/Market Street (UMS)



Yerba Buena/Moscone (YBM)



4th St. Surface Track, Systems (STS)





Atop the roof deck, a crane operator slowly swings the large steel bucket used to remove excavated material from inside the station headhouse.



Workers spray a fibrous quick-hardening slurry solution called shotcrete on the outer walls of the cross-cut cavern following excavation to prevent against ground movement.



Large struts span the width of the headhouse to provide stability against the exterior slurry walls, while the steel bucket used to lift excavated material is carried to the surface.



A worker peers toward the surface dozens of feet above, from the bottom of the north access shaft, where a connecting tunnel is being excavated.

UMS



Sparks fly as a worker precisely cuts steel brackets to align with each other ahead of installing even larger reinforcing steel inside the station box.



A long view looking north under Stockton Street, down the full length of the station interior. Water is sprayed on jackhammering to keep dust down inside the station box, at the south headwall.



Workers step around equipment and machinery at the south headwall, where a drill rig is being used for compensation grouting work.



Old utility vaults and abandoned basement walls are removed on O'Farrell prior to site excavation and the construction of an emergency exit staircase.

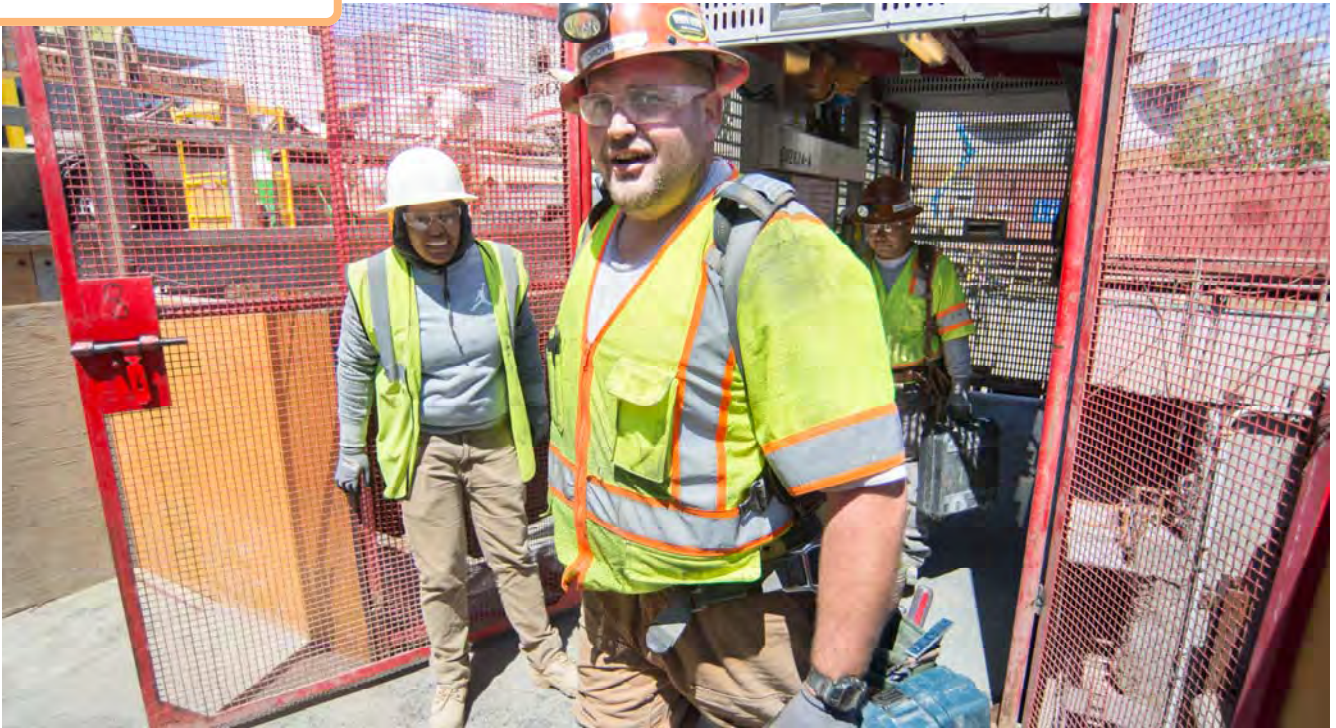


Workers prepare the walls of the station box for installation of rebar, concrete, and waterproofing materials.



Workers drill holes to install rebar against the outer slurry walls of the station box. Sheets of waterproofing and finally the finished outer station walls will be installed against them.

YBM - continued



A crew exits the elevator installed at the headhouse to make the climb in and out of the work area a bit easier.

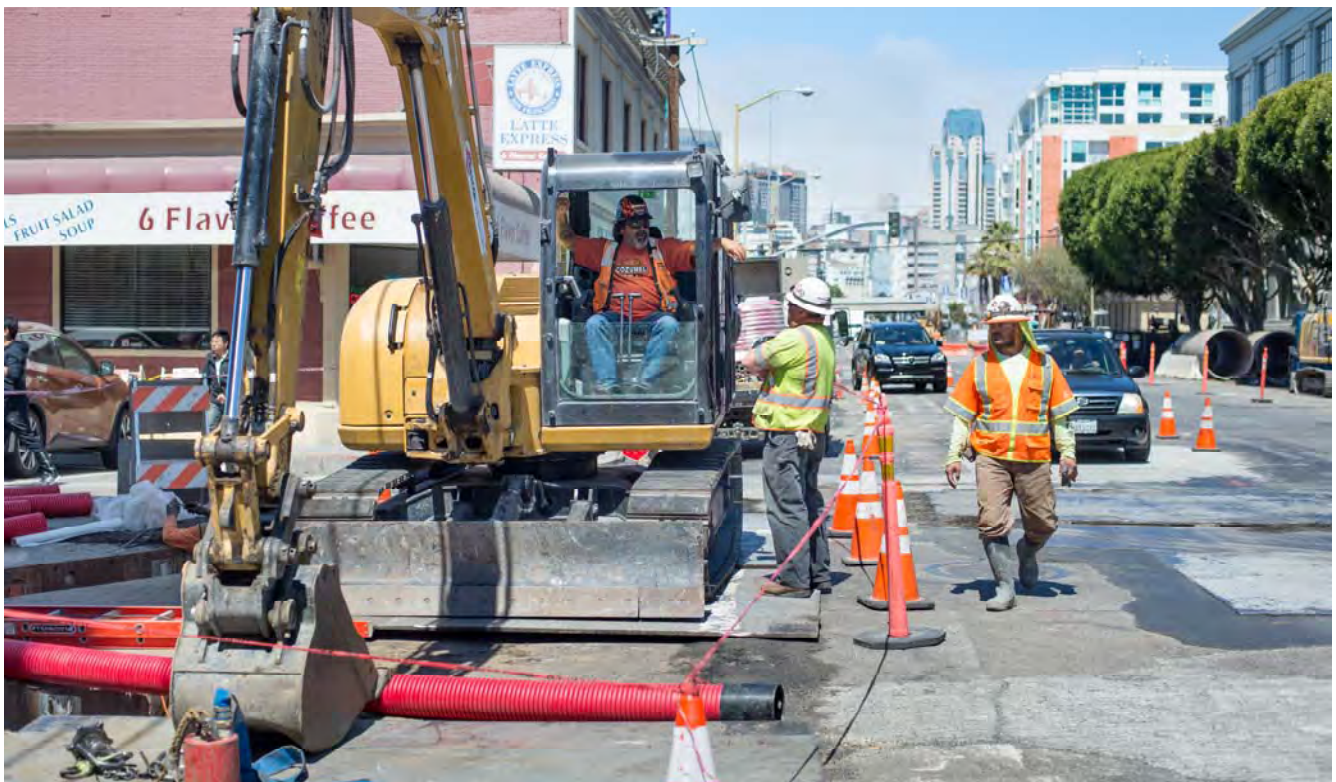


The shell of the southbound tunnel is seen exposed, adjacent to the slurry wall of the headhouse. Sections of tunnel inside the station box will be demolished, and a permanent floor slab will be built in their place.

STS

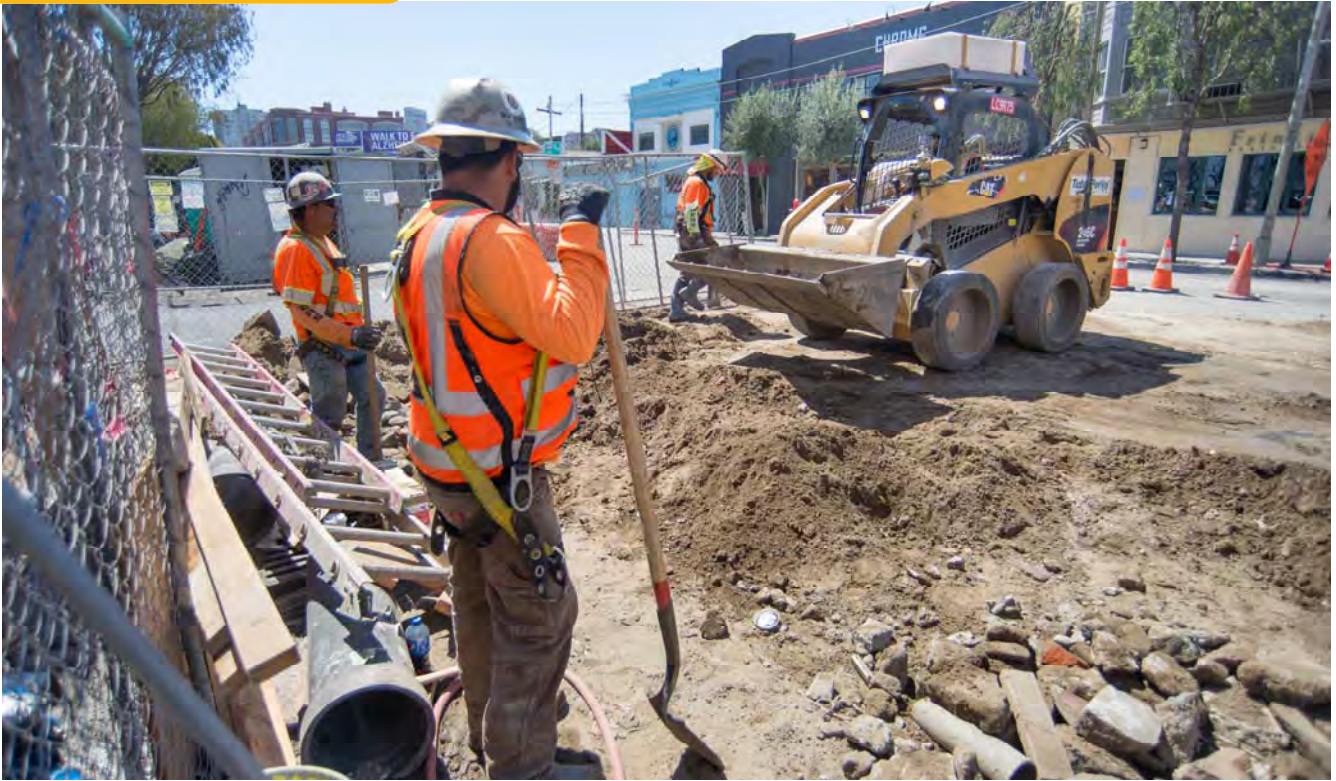


Utility trenches split off at Bluxome, where new conduits and other utilities are being installed and relocated.



An excavator operator discusses operations with another worker at Bluxome, where utility relocation and upgrade work is ongoing.

STS—Continued



Workers sift materials and grade the soil following the backfilling of a utility trench at Freelon Alley and 4th.



New dual sewer lines are being installed on the east side of 4th between Brannan and Bryant.

Appendix A

DETAIL COST REPORTS

1. PROJECT COST

The Current Cost Estimate (CCE) for the Central Subway Project is **\$1.578 billion** in year of expenditure dollars (\$YOE). This total project cost is shown at the top of Report 7.1, Program Project Budget. This capital cost projection incorporates allocated and unallocated contingencies to cover the risks associated with the project completion.

Total net incurred costs for the project are \$977.81 million, a \$14.29 million increase over last month. The cost to date figure reflects expenditures through FAMIS 786 Report (\$936.49 million) plus the utilities joint trench Form B Reimbursement payment (\$10.98 million), invoices currently being processed (\$9.09 million) and estimates of outstanding pay requests (\$21.24 million). This incurred amount equals 61.95% of the total project budget of \$1.578 billion.

The current funding level to date is \$1,179.79 million. This represents 75% of the total project budget.

CONTRACT	PP NO	PP PERIOD TO	PROG PYMT AMOUNT
CS155.1	52	3/31/2014	\$ 26,671.53
CS155.1	53	4/30/2014	\$ 19,500.26
CS155.1	54	5/31/2014	\$ 29,166.76
CS155.1	55	6/30/2014	\$ 22,666.39
CS155.1	56	7/31/2014	\$ 36,602.38
CS155.1	57	8/31/2014	\$ 40,273.52
CS155.1	58	9/30/2014	\$ 37,269.95
CS155.1	59	10/31/2014	\$ 30,018.32
CS155.1	60	11/30/2014	\$ 20,183.22
CS155.1	61	12/31/2014	\$ 18,828.97
CS155.1	62	1/31/2015	\$ 19,921.99
CS155.1	63	2/28/2015	\$ 18,116.08
CS155.1	64	3/31/2015	\$ 15,554.73
CS155.1	65	4/30/2015	\$ 15,550.60
CS155.1*	66	5/31/2015	\$ 2,121.00
CS155.1*	67	6/30/2015	\$ 1,180.00
CS155.1*	68	7/31/2015	\$ 7,137.00
CS155.1*	69	8/31/2015	\$ 1,805.00
CS155.1*	70	9/30/2015	\$ 1,583.00
CS155.1*	71	10/31/2015	\$ 3,186.00
CS155.1*	72	11/30/2015	\$ 203.00
CS155.1*	73	12/31/2015	\$ 7,695.00
CS155.1*	74	1/31/2016	\$ 12,080.00
CS155.1*	75	2/29/2016	\$ 874.00
CS155.1*	76	3/31/2016	\$ 326.00
CS155.1*	77	4/30/2016	\$ 246.00
CS155.1*	78	5/31/2016	\$ 1,843.00
CS155.1*	79	6/30/2016	\$ 22,238.00
CS155.1*	80	7/31/2016	\$ 21,969.00
CS155.1*	81	8/31/2016	\$ 21,969.00
CS155.2	71	1/31/2016	\$ 172,443.00
CS155.2	72	2/29/2016	\$ 192,842.72

CONTRACT	PP NO	PP PERIOD TO	PROG PYMT AMOUNT
CS155.2	73	3/31/2016	\$ 208,315.48
CS155.2*	74	4/30/2016	\$ 360,535.00
CS155.2*	75	5/31/2016	\$ 414,983.06
CS155.2*	76	6/30/2016	\$ 538,750.00
CS155.2*	77	7/31/2016	\$ 375,543.00
CS155.2*	78	8/31/2016	\$ 375,543.00
CS155.3	68	11/30/2015	\$ 216,568.55
CS155.3	69	12/31/2015	\$ 122,025.20
CS155.3	70	1/31/2016	\$ 110,249.51
CS155.3	71	2/29/2016	\$ 99,146.17
CS155.3	72	3/31/2016	\$ 163,760.53
CS155.3	73	4/30/2016	\$ 119,901.00
CS155.3	74	5/31/2016	\$ 120,270.46
CS155.3*	75	6/30/2016	\$ 161,864.82
CS155.3*	76	7/31/2016	\$ 106,037.51
CS155.3*	77	8/31/2016	\$ 116,898.00
Contract 1252	40	7/31/2016	\$ 510,008.61
Contract 1300	31	7/31/2016	\$ 6,844,330.00
Contract 1300*	32	8/31/2016	\$ 13,356,123.00
CS149	85	2/29/2016	\$ 436,870.78
CS149*	86	3/31/2016	\$ 580,057.01
CS149*	87	4/30/2016	\$ 500,904.15
CS149*	88	5/31/2016	\$ 550,000.00
CS149*	89	6/30/2016	\$ 550,000.00
CS149*	90	7/31/2016	\$ 550,000.00
CS149*	91	8/31/2016	\$ 550,000.00
CS156.1*	66	5/31/2016	\$ 73,400.47
CS156.1*	67	6/30/2016	\$ 61,772.44
CS156.1*	68	7/31/2016	\$ 65,718.00
CS156.1*	69	8/31/2016	\$ 60,522.65
other accruals*		8/31/2016	\$ 1,214,254.72

* Estimated Amount

\$ 30,336,418.54

2. CONTINGENCY ALLOCATIONS AND USAGE

The current Total Project Contingency is **\$79.22 million**, which is a \$19.22 million favorable balance against the current Minimum Contingency level of \$60 million. The Contingency Drawdown Curve is shown in Report 7.3. Follows by Report 7.4 Contingency Management Trend Report with the Remaining Contingency after Approved Changes Deducted contingency items in column "i".

In this reporting period, Contract 1252 Tunnel did not process any contract modifications. Contract 1300 Station processed six contract modifications in the amount of \$412,888. Refer to Report 7.5 for approved contract modifications and potential changes.

3. BUDGET TRANSFERS

In this reporting period, there was a budget transfer for an increase of \$20,000,000 to two Construction Contracts Project from unallocated contingency, refer to Report 7.6 for details.

4. FORM B

The Utilities Joint Trench Form B Details is listed in the Table A2 below. Total utilities joint trench Form B Reimbursement payment to three construction contracts is \$10.98 million.

TABLE A2: UTILITIES JOINT TRENCH FORM B DETAILS	[A] Mar 2015 BUDGET	[B] EXPENDED TO DATE	Associated Cost Account
1.3.491.07.040.02 - FORM B - CN1250 UTILITY REIMBURSEMENT	(2,275,419)	2,463,325	1.3.081.07.040.02 - 1UTL:SITWORK: UTILITIES & RELOC
1.3.491.08.040.02 - FORM B - CN1251 UTILITY REIMBURSEMENT	(7,618,412)	3,608,217	1.3.082.08.040.02 - 2UTL:SITWORK:UTILITIES&RELOCATE
1.3.491.02.040.02 - FORM B - CN1252 UTILITY REIMBURSEMENT	(254,050)	4,256,660	1.3.083.02.040.02 - TUNN:Sitework:Utilities & Relocate
1.3.491.04.040.02 - FORM B - CTS: CN1300 UTILITY REIMBURSEMENT	(451,703)		
1.3.491.09.040.02 - FORM B - STS: CN1300 UTILITY REIMBURSEMENT	(1,000,000)		
1.3.491.03.040.02 - FORM B - UMS: CN1300 UTILITY REIMBURSEMENT	(528,370)	428,712	1.3.084.03.040.02 - UMS.1253: SITE UTILITIES, UTILITY RELOCA
1.3.491.05.040.02 - FORM B - YBM: CN1300 UTILITY REIMBURSEMENT	(100,000)	221,203	1.3.086.05.040.02 - YBM.1255: SITE UTILITIES, UTILITY RELOCA
TOTAL	(12,227,954)	10,978,116	

5. EARNED VALUE (EV) ANALYSIS

In August 2016 Report, the Preliminary Earned Value Analysis reports is based on the SFMTA August Schedule Update. The Planned Value, Earned Value, Actual Cost, Percent Complete and resulting indexes as follows:

Preliminary August Earned Value

Overall Budgeted Cost:	\$1,578,300,000
Planned Value:	\$1,255,855,938
Earned Value:	\$980,430,272
Actual Cost:	\$977,805,430
Schedule Performance Index (SPI):	0.78
Cost Performance Index (CPI):	1.00
Percent Complete:	62.0%

SFMTA, EV Chart

August 31, 2016 Update

Activity ID	Activity Name	Start	Finish	Performance % Complete	Budgeted Total Cost	Planned Value Cost (PV)	Earned Value Cost (EV)	Actual Total Cost (AC)	CPI	SPI
CENTRAL SUBWAY PROJECT										
		06/03/03A	08/04/21	62.06%	1,578,300,000	1,255,855,638	980,430,272	977,805,430	1.00	0.78
	Preliminary Engineering Phase	06/03/03A	01/07/10A	100%	46,542,061	46,542,061	46,542,061	46,542,061	1.00	1.00
	Final Design	01/08/10A	06/17/13A	100%	115,075,987	115,075,987	115,075,987	114,309,159	1.01	1.00
	Light Rail Vehicles	04/15/13A	12/18/18	8.25%	26,385,653	2,177,132	2,177,132	2,147,782	1.01	1.00
	Real Estate	08/01/08A	08/26/16	80.31%	37,405,895	34,184,259	30,041,759	30,625,553	0.98	0.88
	Construction Phase	01/03/10A	09/20/19	58.49%	1,348,140,480	1,057,876,500	786,583,333	784,180,876	1.00	0.74
	Construction Support and Costs	01/03/10A	09/20/19	47.35%	199,862,849	99,372,005	94,626,394	105,250,124	0.90	0.95
	Construction Utility Contract #1 - MOS & Portal CN-12	01/04/10A	05/23/11A	100%	11,988,150	11,988,150	11,988,150	11,988,150	1.00	1.00
	Construction Utility Contract #2 - UMS CN-1251	01/12/11A	10/15/12A	100%	20,794,582	20,794,582	20,794,582	20,794,582	1.00	1.00
	Construction Tunnels CN-1252	06/08/11A	08/27/16	93.05%	235,913,500	251,089,328	233,608,894	233,793,900	1.00	0.93
	Construction CN-1300	06/03/13A	05/19/19	48.42%	879,601,399	674,672,435	425,595,312	412,374,120	1.03	0.63
	Unallocated Contingency	02/19/19	07/31/19	0%	4,749,924	0	0	0	0.00	0.00
	Project Management	08/01/19	08/04/21	0%	0	0	0	0	0.00	0.00

The preliminary Contract 1300 cost shows Earned Value Cost \$14M higher than Actual Cost.

Earned Value Analysis and Definitions

SPI is a measure of schedule efficiency on a project. It is the ratio of earned value (EV) to planned value (PV). A SPI equal to or greater than one indicates more work was completed than planned and a value of less than one indicates less work was completed than planned. A value of less than 0.9 is unfavorable.

CPI is a measure of cost efficiency on a project. It is the ratio of earned value (EV) to actual cost value (AC). A CPI equal to or greater than one indicates a cost under run and a value of less than one indicates a cost overrun. A value of less than 0.9 is unfavorable.

The following earning rules are established for each of the phase:

Cost Element Group	Planned Value (Primavera)	Earned Value (Primavera)	Actual Cost (SFMTA Cost Accounting (SAP))
Prelim. Engineering	Expenditure Plan Level of Effort (LOE)	Equals to Planned Value (LOE)	Time Keeping; Vendor Accruals and Invoices
Final Design	Expenditure Plan Level of Effort (LOE)	Equals to Planned Value (LOE)	Time Keeping; Vendor Accruals and Invoices
Procurement	Planned Delivery Date	Actual Delivery Date	Time Keeping; Vendor Accruals and Invoices
Real Estate	Expenditure Plan Level of Effort (LOE)	Equals to Planned Value (LOE)	Time Keeping; Vendor/ Material Accruals and Invoices
Construction	Schedule of Work	% Complete* x Budget at Completion (BAC)	Vendor Accruals and Invoices
Sub-Total	Performance Measurement Baseline (PMB)	Total Earned Value	Total Actual Cost
Below the Line	+ Contingency		
Total	Approved Budget		

6. FUNDING SUMMARY

The Funding Available Table below shows the total awarded funds to date vs. the total committed funds from the Project's seven funding sources.

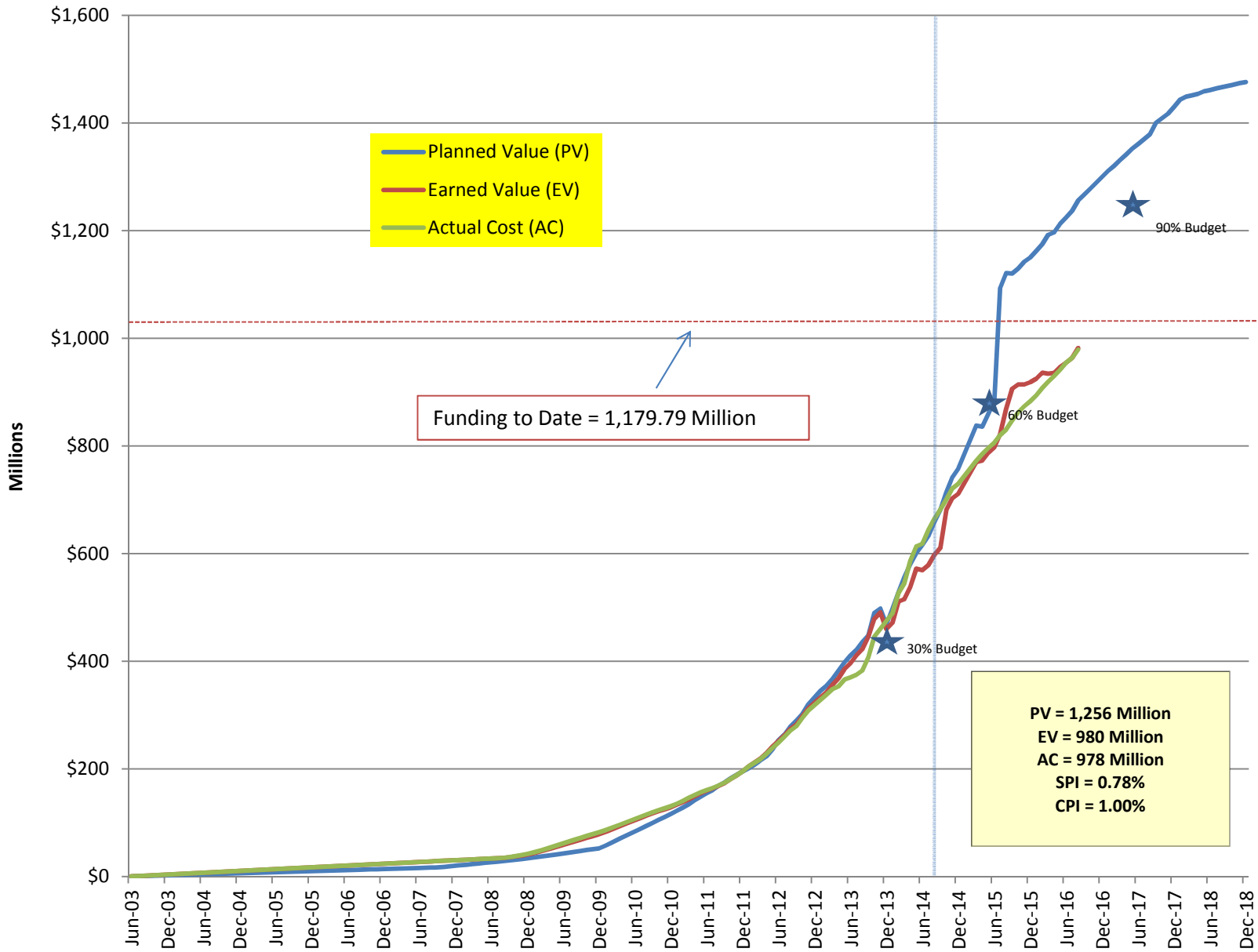
Funding Available Table		
	Funding	
	Committed Funding Sources	Total Awarded Funds to Date
Federal		
Sect. 5309-NS	\$942,200	\$619,196
CMAQ	\$41,025	\$41,025
Federal Subtotal	\$983,225	\$660,221
State		
TCRP	\$14,000	\$14,000
State RIP	\$88,000	\$12,498
Prop. 1B (I-Bond) PTIMSE	\$307,792	\$307,792
Prop. 1A (HSR-Bond)	\$61,308	\$61,308
State Subtotal	\$471,100	\$395,598
Local		
Prop. K	\$123,975	\$123,975
Local Subtotal	\$123,975	\$123,975
CPT 544 Total	\$1,578,300	\$1,179,794

7. LIST OF COST REPORTS

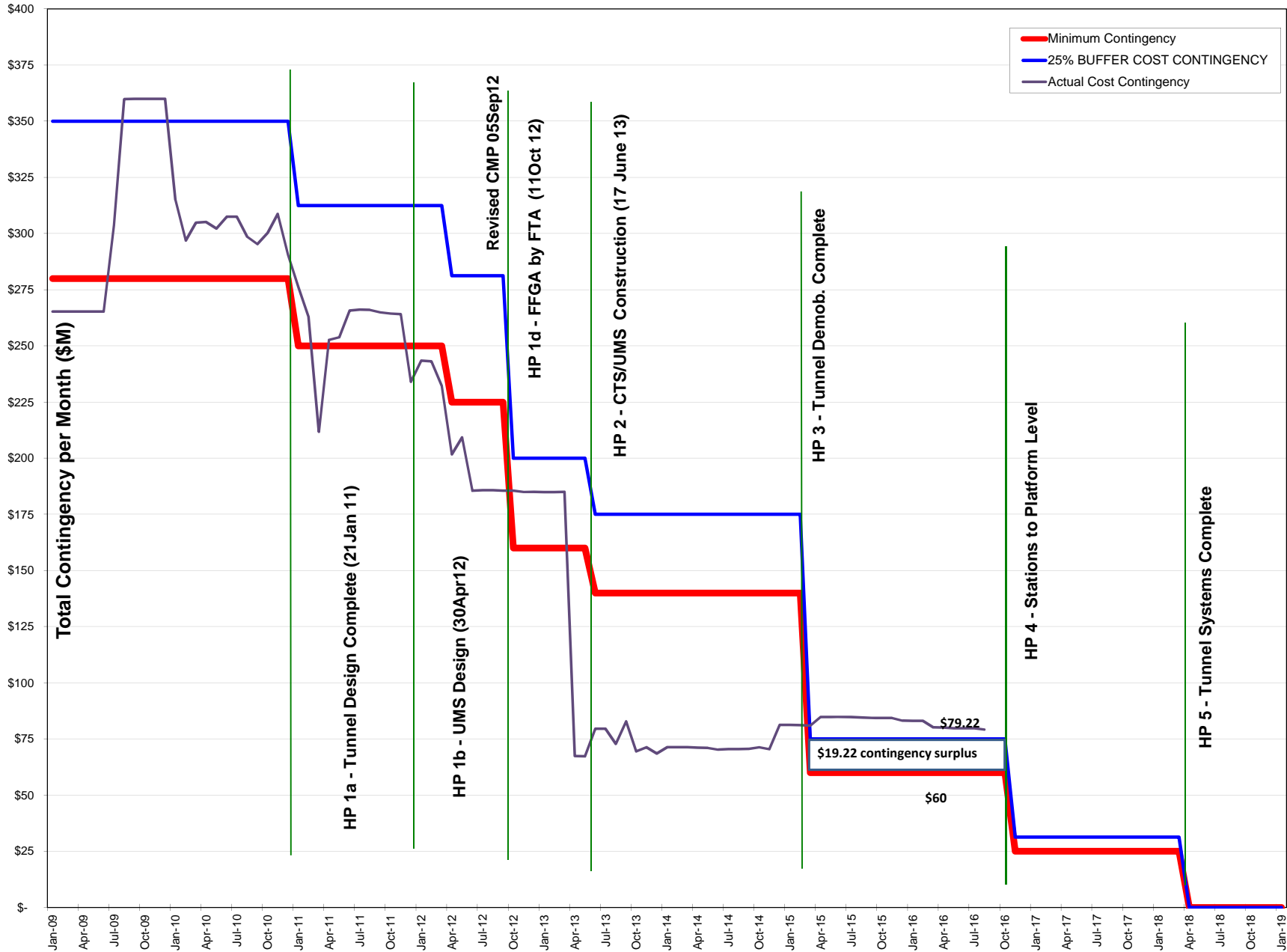
- 7.1 Program Project Budget
- 7.2 Earned Value Cash Flow
- 7.3 Contingency Drawdown Curve
- 7.4 Summary Contingency Management Trend Report
- 7.5 Detail Contingency Usage Report
- 7.6 Budget Revisions: Report sorted by Construction Packages & Soft Costs
- 7.7 Project Budget & Expenditure Report: Sorted by SCC Summary
- 7.8 Budget & Expenditure Report: Sorted by SCC Details
- 7.9 Detail Monthly Expenditure Report: grouped by Project Phase
- 7.10 Cost Report Notes

A. Central Subway Project							
Project	Name	Amount	PM	Funding Source	Reporting	Cost Report	Notes
1	CPT544 Central Subway Project	\$1,578,300,000	J. Funghi	62% Fed, 30% State, 8% Local	yes		1
Total:		\$1,578,300,000					
B. Related SFMTA Capital Improvement Projects							
Project	Name	Amount	PM	Funding Source	Reporting		
2	CPT690 TBM Retrieval Shaft Relocation	\$9,700,000	Funghi/Magyar	MTA Operating Funds	no		2
3	CPT718 Chinatown Metro Plaza	\$6,980,000	J. Funghi	Transbay Redevelopment	no		3
4	CPT665 Central Subway Project - Goodwill	\$2,367,750	K. Magyar	I-Bond Interest	no		4
5	CPT705 MOH - Broadway/Sansome	\$8,000,000	K. Magyar	MTA Operating Funds	no		5
Total:		\$27,047,750					
C. Central Subway Project - Project Offset Credits							
	From	Amount	Index	Notes	Reporting		
1	2009-2016 Utility Co. - Form B Reimbursement	\$12,227,954	--	Construction contracts	yes		6
2	2017-2019 PG&E - Power Feed Reimbursement	\$7,624,540	--	Not yet bill PG&E	yes		7
3	6/26/2013 BART Elevator	\$90,000	68CPT544135B	Not yet rec'd BART Funds	yes		8
4	11/6/2013 Tutor Perini - CAD Files	\$2,500	68CPT5441236	Deposit to Design Index	yes		9
5	1/27/2014 SFPUC - Sewer Main	\$2,925,296	68W251	Certified in Contract 1300	yes		10
6	8/27/2014 SFMTA Traffic Effectiveness Project funded	\$694,651	68W324/686D42	Contract 1252 CMod #40	yes		11
7	9/27/2014 SFPUC - 24" Water Main	\$328,857	68CPT544135A	Contract 1252 CMod #41	yes		12
8	2/15/2015 Chinatown Plaza Construction Estimate	\$75,000	68CPT7181341	Contract 1300 CMod #6	yes		13
9	3/27/2015 SFPUC - 24" Water Main Additional Work	\$112,102	68W409	Contract 1252 CMod #48	yes		14
10	3/15/2016 Support for North Beach Restoration, OCS and Streetlighting	\$155,468	68T7373342D2/D3	Contract 1252 CMod #51	yes		14a
Total:		\$24,236,368					

Earned Value Cash Flow Curve



Cost Contingency Drawdown



COST ELEMENT	CONTRACT COST					CONTINGENCY					BUDGET	VARIANCE	Cost Report Notes	
	ORIGINAL CONTRACT VALUE / September 2013 SUPPLEMENTAL BUDGET	APPROVED CHANGES	CURRENT CONTRACT VALUE	POTENTIAL CHANGES	ESTIMATE AT COMPLETION (EAC)	ORIGINAL CONTINGENCY / Sep 2013 SUPPLEMENTAL CONTINGENCY (Exclude CN 1250 & CN1251)	CONTINGENCY ADJUSTMENT TRANSFERS	REVISED AUTHORIZED CONTINGENCY (Exclude CN1250 & CN1251)	REMAINING CONTINGENCY AFTER APPROVED CHANGES DEDUCTED [h - b]	REMAINING CONTINGENCY AFTER POTENTIAL CHANGES DEDUCTED [i - d]	ORIGINAL CONTRACT VALUE + REVISED AUTHORIZED CONTINGENCY [a + h]	BUDGET - ESTIMATE AT COMPLETE [j - e]		
	a	b	c [a + b]	d	e [c + d]	f	g	h	i	j	j	k		
SCC 10-50 CONSTRUCTION CONTRACT PACKAGES														
1250	UTILITY RELOCATION PACKAGE #1 <i>Contract 1250 Department of Technology</i>	9,273,939 166,756	2,694,211	11,968,150 166,756		11,968,150 166,756	1,953,377	740,834	2,694,211		11,968,150 166,756		15	
1251	UTILITY RELOCATION PACKAGE #2 <i>Contract 1251 Department of Technology</i>	16,832,550 75,615	3,962,032	20,794,582 75,615		20,794,582 75,615	5,367,297	(1,405,265)	3,962,032		20,794,582 75,615		16	
1252	GUIDEWAY TUNNEL	233,584,015	1,814,428	235,398,443	170,654	235,569,097	23,658,464	(21,328,979)	2,329,485	515,057	344,403	235,913,500	344,403	17
1300	STATIONS <i>1253 UNION SQUARE/MARKET ST STATION [UMS]</i> <i>1254 CHINA TOWN STATION [CTS]</i> <i>1255 YERBA BUENA/ MOSCONE STATION [YBM]</i> <i>1256 SURFACE TRACKWORK & SYSTEMS [STS]</i>	839,676,400 294,030,590 247,567,810 158,089,000 139,989,000	3,770,307 244,995 2,850,957 (645,932) 1,320,286	843,446,707 294,275,585 250,418,767 157,443,068 141,309,286	21,289,367 13,939,250 3,737,772 2,890,127 722,218	864,736,073 308,214,835 254,156,539 160,333,195 142,031,504	40,000,000 20,000,000 10,000,000 5,000,000 5,000,000		39,925,000 20,000,000 9,925,000 5,000,000 5,000,000	36,154,693 19,755,005 7,074,043 5,645,932 3,679,714	14,865,327 5,815,755 3,336,271 2,755,805 2,957,496	879,601,400 314,030,590 257,492,810 163,089,000 144,989,000	14,865,327 5,815,755 3,336,271 2,755,805 2,957,496	18 19
OTHER		31,233,501		31,233,501	0	31,233,501	1,160,000		1,160,000	1,160,000	1,160,000	32,393,501	1,160,000	
SCC 10 - 50 Construction Sub-total		1,130,842,776	12,240,977	1,143,083,753	21,460,021	1,164,543,774	64,818,464	(21,403,979)	43,414,485	37,829,750	16,369,730	1,180,913,503	16,369,730	
SCC 60-80 SOFT COSTS PACKAGES														
60	ROW, LAND, EXISTING IMPROVEMENTS	36,511,799	(4,265,478)	32,246,321		32,246,321	1,000,000	4,265,478	1,000,000	5,265,478	5,265,478	37,511,799	5,265,478	20
70	VEHICLES	24,108,712	(10,799,712)	13,309,000		13,309,000	2,276,941	10,799,712	13,076,653	13,076,653	13,076,653	26,385,653	13,076,653	21
80	PROFESSIONAL SERVICES	310,518,041		310,518,041		310,518,041	18,221,079		18,221,079	18,221,079	18,221,079	328,739,120	18,221,079	
SCC 60 - 80 Construction Sub-total		371,138,552	(15,065,190)	356,073,362		356,073,362	21,498,020	15,065,190	32,297,732	36,563,210	36,563,210	392,636,572	36,563,210	
SCC 90	UNALLOCATED CONTINGENCY						3,845,945	21,134,447	24,980,392	4,749,924	4,749,924	4,749,924	4,749,924	22
TOTAL		1,501,981,328	(2,824,213)	1,499,157,115	21,460,021	1,520,617,136	90,162,429	14,795,658	100,692,609	79,142,884	57,682,863	1,578,299,999	57,682,863	
											Total Project Budget	1,578,300,000	23	
											Estimate At Completion	1,520,617,136	24	
											Variance	57,682,863	25	

Note #17 - Adjusted Contract 1252 Guideway Tunnel contingency "column g" to reflect construction contract modifications #20, #40, #41, #48 and #51 were funded by other funding sources.

Contract Modification/Trend Log - Contract 1252 Tunnel

Awarded NTE Amount: \$233,584,015.00
Substantial Completion: 4/15/2015

Revised NTE Amount \$235,398,442.67
Revised Substantial Completion 4/15/2015

Contract Modifications			Amount	
No.	Description	COR/PCC No.	CMod	Contract NTE
1	Amendment of Insurance Requirements	n/a	\$0.00	\$233,584,015.00
2	Amendment of General Liability Insurance Requirements	n/a	(\$3,040,713.00)	\$230,543,302.00
3	Relocation of PG&E Ductbank & Removal/Reinstall TODCO Scaffolding	CORs 10 & 29	\$6,633.37	\$230,549,935.37
4	Investigate 48" Pipe at UMS S. Headwall	PCC 9	\$75,000.00	\$230,624,935.37
5	LB/MOS Oil Filled Pipe Abatement and MOS Asbestos Pipe Abatement	CORs 7, 22 & 27	\$23,912.54	\$230,648,847.91
6	Arch. Support at MOS Station N. Headwall	COR 12	\$16,892.96	\$230,665,740.87
7	Revisions to MOS N. Headwall Elevation	PCC 1	\$20,358.23	\$230,686,099.10
8	UMS Reduced Duration	COR 21	\$0.00	\$230,686,099.10
9	48" Pipe Investigation and Removal at the UMS Headwalls	PCC 11	\$150,000.00	\$230,836,099.10
10	PG&E Impacts to Launch Box and Flagger	CORs 6 & 41	\$8,618.96	\$230,844,718.06
11	Bart Annex Wall	PCC 7	\$15,500.00	\$230,860,218.06
12	LB - Concrete Manhole and Slurry Pipe Removal	COR 38	\$3,820.84	\$230,864,038.90
13	Retrieval Shaft - Pipe/Duct Bank Removal	CORs 31, 47, 50, 58, 66	\$9,908.04	\$230,873,946.94
14	MOS - 16"Pipe Removal @ N/S Headwalls	CORs 39, 44	\$4,551.99	\$230,878,498.93
15	MOS - S. Headwall Asbestos Pipe Abatement	COR 26	\$27,629.64	\$230,906,128.57
16	UMS Utility Removal/Construct Wall	CORs 46, 48, 68	\$21,150.28	\$230,927,278.85
17	MOS – 16" Steel Pipe Removal at N/S Headwalls/Enlarge Tie-In Hole	CORs 73, 76, 81	\$5,056.63	\$230,932,335.48
18	SFWD Support Work	PCC 13	\$20,000.00	\$230,952,335.48
19	Additional BART Instrumentation	PCC 6	\$307,860.75	\$231,260,196.23
20	Relocation of Retrieval Shaft	PCC 10	\$5,150,000	funded by CPT690
21	Columbus Ave. Restoration	NA	\$261,057.00	\$231,521,253.23
22	LB - Jet Grout Quantity Overrun	COR 070	\$599,900.00	\$232,121,153.23
23	Old Navy - Comp. Grout DSCs	COR 079	\$259,373.00	\$232,380,526.23
24	SFWD Excavation, Support and Backfilling	PCC 13	\$13,982.00	\$232,394,508.23
25	Cross Passage 5 - VECP	COR 087	(\$2,674.00)	\$232,391,834.23
26	Various CORs	CORs 9, 24, 37, 45, 51, 61, 71, 77, 83, 99	\$73,700.00	\$232,465,534.23
27	MOS – Reroute traffic signal lines / Equipment Standby	CORs 11, 17	\$80,719.00	\$232,546,253.23
28	PCC 12 - Comp.Grout Mtls., Accel. at R. Shaft, Haz. Mtls @ R.Shaft, Obst. at CSM panel W6 (NTE Amount CMod)	FA COs 006, 007, 008, 009	\$81,937.00	\$232,628,190.23
29	AWSS Conflict with Water Line, AWSS Restraining at Launch Box, AT&T Vault Conflict with Sewer Main	COR 1, 2, 3	\$73,045.00	\$232,701,235.23

Connecting people. Connecting communities.				
30	MOS Headwall End-stops	COR 018	\$144,000.00	\$232,845,235.23
31	Launch Box - Slurry Wall Obstructions	COR 32, 34, 42, 43, 62, 65, 67	\$234,438.00	\$233,079,673.23
32	COR 8 + Markup - Associated w/ COR 001, COR 002, and COR 003	COR 1, 2, 3, 8	\$168,000.00	\$233,247,673.23
33	LB - Pre-Excavation for Slurry Walls	COR 015	\$125,000.00	\$233,372,673.23
34	Modifications to Tunnel Alignment at Market Street - Initial Design Costs	PCC 012 Part 1	\$39,930.00	\$233,412,603.23
35	Deleted AWSS Work at Union St. and Columbus Ave.	PCC 015	(\$187,181.00)	\$233,225,422.23
36	Curb Ramp Work at NE Corner of 4th and Harrison Streets (Force Account)	FA CO 011	\$5,023.00	\$233,230,445.23
37	Staging Yard Hazardous Material	COR 30, 54, 75	\$401,933.00	\$233,632,378.23
38	Modifications to AWSS Facilities at 4th and Bryant Streets	PCC 014	\$35,925.00	\$233,668,303.23
39	MOS N. Headwall Impacts, LB – Jet Grout Overrun	COR 85, 70	\$240,333.00	\$233,908,636.23
40	Culvert, Street & Sidewalk Restoration in N.Beach (includes QC testing)	PCC 20	\$694,651.00	funded by TEP
41	Install Water Main in North Beach	PCC 20	\$328,860.00	funded by SFPUC
42	UMS - Shoring Impacts due to 48" ATT Pipe at SW Headwall	COR 069	\$29,463.00	\$233,938,099.23
43	UMS Tangent Pile SRB-H DSC, UMS Jet Grout Column No. 18 DSC	COR 096, COR 102	\$60,870.00	\$233,998,969.23
44	Subcontractor Substitution	NA	\$0.00	\$233,998,969.23
45	Modifications to Tunnel Alignment - Construction Costs	PCC 12	\$883,693.00	\$234,882,662.23
46	MOS 20" Conflict with 16" AWSS @ South Headwall	COR 082	\$28,160.00	\$234,910,822.23
47	CDF Encasement on 42" RCP	COR 005	\$95,000.00	\$235,005,822.23
48	North Beach Water Main Additional Work	PCC 20	\$112,102.20	funded by SFPUC
49	Various CORS and FACOS; FACO 13-15, 17-19; COR 116, 118-121	FACO 13-15, 17-19; COR 116, 118-121	\$180,010.41	\$235,185,832.64
50	Contract Milestone Changes		\$0.00	\$235,185,832.64
51	PCC 020 - Supp 1 - North Beach Restoration, OCS and Streetlighting		\$155,468.17	funded by TEP
52	PCC 19 - Piping, Conduit and Casing at 4th Street Portal		\$11,678.00	\$235,197,510.64
53	COR 117 - Retrieval Shaft Impacts		\$30,278.08	\$235,227,788.72
54	Material Hardness at the Retrieval Shaft		\$166,182.81	\$235,393,971.53
55	PCC 020 - Supp 2 - North Beach Sewer Work		\$19,730.14	\$235,413,701.67
56	Deleted Water Utility Replacement/Water, Sewer, MRY and AWSS Design Changes	PCC 24, 28	(\$15,259.00)	\$235,398,442.67
Sub Total:			\$1,814,427.67	

Pending Contract Modifications		COR/PCC No.	Estimated	
Description			Amount	Time
I	Other Pending Contract Modifications	Cmod # TBD	\$170,653.95	
Sub Total:			\$170,653.95	

Contract Modification/Trend Log - Contract 1300 Stations

Awarded NTE Amount	\$839,676,400	\$843,446,707
Substantial Completion	2/10/2018	2/10/2018

	UMS	CTS	YBM	STS	COST REPORT NOTES
Potential Changes	13,939,250	3,737,772	2,890,127	722,218	26
Forecasted - Trends	3,879,690	1,697,682	595,172	213,829	
Negotiation					
Change of date range to receive art	(10,001)				
UMS Geoprobe Credit	(15,600)				
UMS Powell St. Elevator Site Hazmat	16,028				
STS Traffic Signal and SL Changes				298,307	
STS Comm and Elec Cabinets Relocati				67,221	
YBM Delete Instrument & Monitoring			(50,195)		
STS - OCS Pole Changes				12,706	
UMS - PCC 027 (Escalator Barricade)	(9,227)				
CTS-FACO#39- Soil Testing cmplly OAB		5,478			
UMS-FACO #32 8" Waterline Conf NDSC	75,291				
UMS FACO #31 NDSC Incomplete PGE DB	165,944				
UMS-FACO #25 OCS Pole@ Market/Ellis	3,349				
UMS-FACO #19 Street Light at Stock.	2,361				
STS-FACO #48 Work Related St Lght				2,051	
UMS FACO #34 Becho Obstruct J Piles	7,435				
UMS FACO #45 Blocked Stubs PG&E	7,500				
CTS-PCC40 Plaza Surface Slab Pntrtn		5,525			
YBM PCC 41 Install #7 Box Clementin			38,501		
UMS Sewer Line Conflict	744,465				
UMS MRY Duct Bank-East Conflict	41,038				
UMS FACO #52 NDSC Transite Pipe	4,497				
CTS-Delete Tree Planting		(3,967)			
USG COR 222 Shaft Wall Connections	170,189				
USG COR 261 8" Wall at Grid 11B	5,632				
UMS COR 250 Macy's DSC	75,001				
USG COR 274 Shear Wall at Grid 10	68,945				
STS - Deletion of ARS (Revision 1)				(4,689,000)	
YBM COR 63 Buried objects in P-86			24,648		
STS COR 100 DSC Zayo PVC Conf w SW				80,082	
STS COR 210 WD pit size increase				21,988	
CTS PCC 001 Delete DB on Stockton		(84,018)			
STS COR #92 PG&E Vault Conf 12 AWSS				78,074	
UMS - PCC #29 (Concrete Wale Suppt)	41,424				
YBM COR 086 Side SW lat util conf			78,296		

Contract Modification/Trend Log - Contract 1300 Stations

Awarded NTE Amount	\$839,676,400	\$843,446,707
Substantial Completion	2/10/2018	2/10/2018

	UMS	CTS	YBM	STS	COST REPORT NOTES
CTS COR 041 FACO #4 JT Unid. Cond		28,026			
CTS COR 040 FACO 002 Connect (E) DB		16,936			
YBM COR 49 Buried timber piles			88,230		
CTS-COR#201 Swr Line & Station Roof		46,046			
YBM COR 115 C1250 unencased JT			9,843		
STS COR 272 Pier AWSS Conflict				24,287	
STS COR 258 WD/PVC Conflict				87,511	
STS COR 211 SW conf AWSS 4th/Freelo				4,561	
STS COR #229 Multi E Util Conf w N				8,284	
UMS COR 232 S.Walk Hatches RFI 240	744				
STS COR #88 Modify CBs and Culverts				4,395	
STS COR 101 Cleaning for non-78" SW				58,906	
STS COR 113 DSC PVC Conduit/WL Conf				11,997	
STS COR 164 DSC 8" AWSS Lat Conf 78				5,750	
STS COR #187 DB Conf w N 12 Water				37,012	
CTS COR 231 Sidewalk Hatches		9,917			
STS COR #241 2-In WD Line w Offset				5,386	
STS COR 254 DSC 4" Confl w 36" FM				25,197	
STS COR 290 E CB PVC Confl WD AWSS				3,329	
STS COR #296 Mult E Util Confl AWSS				44,610	
UMS COR 301 Concr. Overpour and PVC	14,792				
UMS Remove Fire Hose Valve	(2,016)				
UMS - DCW and Hose Bibbs	29,159				
CTS PCC 061 Escalator Pit Provision		46,809			
STS PCC 063 Del ATT/TSIC/PGE on 4th				(36,495)	
STS COR 073 AWSS Incorrectly Shown				35,134	
STS COR 067 FACO #41 GW Lead Filter				22,695	
STS COR 037 FACO 037 30" FM Removal				63,972	
STS COR 090 Subsurface Obstrc				20,452	
STS COR 091 PG&E Vault Conf 16" Wtr				40,001	
STS COR 093 12" Water Conf 12" Tee				5,001	
STS COR 094 Unknown DB Conf 12" Wtr				20,001	
STS COR 074 AWSS Offset/Sewer Demo				108,384	
USG COR 360 Column on Grid Line B	4,132				
STS COR 198 NDSC Out of Spec Cover				8,123	
STS COR 367 DSC Conf w/ CP and FM				36,256	
STS COR 371 Conflicts w/ 12" AWSS				8,402	

Contract Modification/Trend Log - Contract 1300 Stations

Awarded NTE Amount	\$839,676,400	\$843,446,707
Substantial Completion	2/10/2018	2/10/2018

	UMS	CTS	YBM	STS	COST REPORT NOTES
UMS Roof Deck - Schedule Recovery	80,841				
STS PCC 066 Add CS ATCS Emer Stop B				200,208	
STS COR 387 Oil Line Confl SW MH				6,124	
YBM COR 385 PG&E Damage to Work			0		
UMS COR 391 AT&T Duct Bank Conflict	25,001				
USG COR 365 Elevator Hoist Beam	50,001				
UMS COR 389 4" Steel line asbestos	20,001				
YBM COR 390 RFI 1654 Chip Headwall			55,602		
STS COR 392 Util at 4th-Town SW MH				13,246	
YBM Sidewalk Bulb-out at Stair#1			83,937		
USG COR 384 Slab on Grade Depressio	100,001				
STS COR 401 AWSS Layout 4th/King				31,866	
STS COR 409 Conc in confl w/ (N) FM				3,722	
STS COR 416 Conc DB/wall/lines conf				81,783	
USG COR 435 Work Stoppage	50,001				
USG Hydrant Relocation on Geary St.	65,163				
USG COR 415 Glass Roof Steel Elev.	10,001				
STS COR 454 Addtl Conflicts w/ 8" W				21,586	
USG Door Openings in Shear Walls	41,774				
UMS COR 466 Asbestos Conduit	21,344				
UMS COR 417 Redesigned Manhole	14,385				
STS COR 469 SW Cleaning 4th/King				1,836	
YBM COR 485 Issues with N S Headwal			30,001		
USG Removal of Existing Column	4,116				
YBM PCC 79 Install 12in WM to Howar			209,888		
YBM COR 95 UST Removal Folsom			196,191		
YBM COR 249 Utility conflicts Folso			93,425		
UMS FACO #29 BART Hazmat Abatement	22,884				
STS COR 089 Extra Sewer Work				29,468	
USG PCC 73 Acceleration of Work	102,203				
YBM COR 50 Contam soil along H Line			39,560		
STS COR 480 Conc confl w (N) FM				886	
YBM COR 248 Transite pipe Folsom			1,501		
YBM COR 267 Design SW hatch Stair 5			10,848		
UMS COR 493 Steel Shape Inside Pile	25,001				
UMS COR 510 6" VCP Side Sewer	19,696				
STS COR 519 Re-pressure test WD				2,739	

Contract Modification/Trend Log - Contract 1300 Stations

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Substantial Completion	2/10/2018	2/10/2018

	UMS	CTS	YBM	STS	COST REPORT NOTES
STS COR 523 AV & GV on 8" WD Main				1,601	
STS COR 530 Tunnel Track Machine				20,064	
STS COR 536 Util in Conf w 36" FM				66,287	
STS PCC 084 Removal of ATT DB & Vau				69,361	
UMS Grade 50 Steel in Lieu Grade 70	572,884				
UMS COR 565 GEN Emer Vent Design	500,001				
CTS COR 171 Mismarked gas line		14,581			
UMS COR 578 Elev./Escalator Monitor	25,001				
UMS COR 579 Elevators 1-4 SFFD	25,001				
STS COR 584 Debris confl w/ culvert				11,569	
UMS COR 591 Escalators and Stairs	100,001				
UMS COR 600 Hydrocarbon Odor	50,001				
CTS PCC#25 Stairs 5, 6, 7 Mods		21,672			
YBM COR 116 Archaeological Discovery			124,268		
YBM PCC 82 Delete Scope Due to Hote			39,025		
STS PCC #51 Inventor Tmp Crossover				23,420	
STS Track Switch Machine Change				391,909	
UMS COR 539 Elev.3/4 Machine Pocket	10,001				
STS COR 447 Added Exc for SFWD POC				29,423	
STS COR 520 Lat Conf AWSS				3,867	
STS COR 522 Thickened Str. Stan				7,046	
STS COR 533 Conc wall confl util				39,170	
YBM COR 566 WD Tie-In @ Folsom			6,302		
STS COR 601 Conc Enc in cn. w W lin				2,827	
YBM COR 620 Openings in Conc. Deck			10,001		
STS COR 621 Additional WD Exc. per				26,293	
STS COR 623 Unkn Con Structure				2,966	
STS COR 641 SW delay due to conf ut				10,025	
STS COR 650 Asbestos conf w/ ATT DB				4,518	
YBM PCC 76 AWSS, SSFM, WM Design			627,854		
CTS - PCC #036 (CMU Wall Bracing)		19,552			
UMS COR 636 Bi-Fold Door Tube Steel	24,911				
USG COR 654 Plaza Level Tie-In	10,001				
UMS COR 652 Elev. 1 & 2 Cond. Shaft	10,001				
UMS COR 661 Card Acc & Dr alm Pnts	10,001				
YBM COR 45 Abandoned sewer not slur			7,492		
UMS COR 673 Elation System Changes	10,001				

Contract Modification/Trend Log - Contract 1300 Stations

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	UMS	CTS	YBM	STS	COST REPORT NOTES
UMS COR 674 1.5 in Vertical Drain	10,001				
STS COR 657 Tunnel Ca Bsn Dsn Chang				20,001	
UMS COR 678 Drain Piping Size	10,001				
UMS COR 696 Use 2.5 Inch Conduit	10,001				
CTS COR 695 Mod to Grondng System		10,001			
STS COR 664 Threaded Dowels				10,001	
UMS COR 689 Pil Ovstrss at Outgrigs	10,001				
UMS COR 712 Utltly Undrping Moniting	10,001				
YBM COR 711 Conduit for AC Swichgea			10,001		
UMS COR 710 Alrm Sys Dsgn & Asso Mt	10,001				
UMS COR 709 Potbl Gen Tap Box	10,001				
CTS COR 706 Glas Canpy Trench Drain		10,001			
YBM COR 624 CMU Wall Supports			56,209		
CTS COR 625 Added Cane Detc Rail		1,889			
CTS COR 626 Ad HSS & Plt @ H Beam		5,130			
CTS COR 627 Add HSS Col @ Tickt Mac		3,404			
STS COR 634 Forc Mn Dsn PCO 329				132,332	
CTS COR 640 East Leg swer Spline		61,264			
STS COR 644 Wod Pier Cnf w AWSS				13,258	
STS COR 655 Unkn Cnc St / cn w cbsn				1,224	
STS COR 662 Foo cnf with catch bsn				1,257	
STS COR 666 Ukn Con Vlt/con N36"				10,118	
STS COR 685 DSC 8" Con unslab St Sc				13,075	
UMS COR 687 Trant Pipe at St 134+00	10,001				
UMS COR 707 Prc /Ins Gas by PG& E	(9,680)				
USG PCC 68 Ramp Barriers Handrail	33,214				
UMS COR 726 Beam 213 Stair Opening	10,001				
UMS COR 730 Pltf Strt Lvl Stn & Elv	10,001				
UMS COR 729 N&S Uti House Adv Ltbox	10,001				
USG COR 734 Casing Column Conflict	10,001				
YBM COR 727 Elev Unts 1&2 Mach Room			10,001		
UMS COR 738 Unfishd Sur @ Seis Jnts	10,001				
UMS COR 612 4" Top Ovr Ligt Wt Conc	10,001				
UMS COR 743 Ovrhd Bi-Fold Door SDs	10,001				
STS COR 744 Roadway Luminaire				10,001	
USG COR 745 14 line wall Tube Suprt	10,001				
UMS COR 748 Emsl Seismic Joint Seal	10,001				

Contract Modification/Trend Log - Contract 1300 Stations

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	UMS	CTS	YBM	STS	COST REPORT NOTES
USG COR 293 Water on N.Concourse	10,001				
STS COR 716 Unkn Brik Ctch Bsn				4,512	
STS PCC 096 4th Street SW Slip Lini				966,687	
UMS COR 762 Bm Fram at Strs 3 & 4	10,001				
YBM COR 758 Delge Vlve Access Door			10,001		
UMS COR 667 Writ Prtst -Ass Liq Dmg	9,000				
USG COR 760 Hor Rebr 7" Em Col B10	3,001				
UMS COR 770 Fiil Void Sp with CDF	10,001				
UMS COR 775 Con W Plac of St Lt Pol	3,001				
UMS COR 774 Inadqte Conc Demolition	4,001				
UMS COR 773 Excava Support Design	5,001				
STS COR 755 AWSS Material Delay				1,495	
STS COR 757 Unkn Dbrs Remv & Hauld				519	
STS COR 772 Reconn Unkn Prop Drn Ln				2,200	
USG COR 259 Lead Paint on Columns	69,821				
USG PCC 31 HVAC Trench Mod.	186,511				
YBM PCC 139 Changes to Handrails			5,384		
UMS PCC 146 FD-1 Floor Drain	6,291				
STS COR 787 AT&T Dct Bnk InCw N 36"				35,001	
UMS-Relocation of Traffic Signal Co	32,275				
UMS PCC 149 Solar/Low-e Coating	23,290				
UMS COR 690 Fire Alarm Annunciator	10,001				
USG COR 635 Plza Lvl Bm to Cnc Wall	10,001				
USG COR 735 Geary St Wall Footing	10,001				
USG COR 279 SFMTA CSP Field	10,001				
USG COR 776 Conceptual Seelve Plan	10,001				
UMS COR 781 Utilty Trench Supp Slab	100,001				
UMS COR 782 Utilty Trench @ Stairs 3	10,001				
USG COR 784 Tem Sho for Fan Lvl Str	10,001				
UMS COR 789 Strs 3&4 SOE Shotcrete	10,001				
CTS COR 795 Rmvl of Emr Push Buttns		10,001			
STS COR 803 SFMTA External IT Feeds				10,001	
STS COR 804 Facility SCADA Sys Arch				10,001	
USG COR 805 Fotngs 16A As Built Dim	10,001				
YBM COR 806 Trans for Con ret revis			10,001		
STS COR 807 Tunnel Invret Slab				10,001	
YBM COR 809 Brushed SS Panel			10,001		

Contract Modification/Trend Log - Contract 1300 Stations

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	UMS	CTS	YBM	STS	COST REPORT NOTES
USG COR 509 Soil Nail Shotcrete Wal	606,533				
Proposed Contract Change (PCC)					
YBM_CTS PCC 33 Platform Gate Revisio			47,057		
CTS - Removal of Bus Bulb		15,997			
CTS Delete PGE Work at Vault 732		(50,000)			
UMS MRY Duct Bank-West	54,981				
Deletion of ARS (Part II)				(52,409)	
YBM PCC 56 OCS Pole Foundations			551		
CTS PCC 067 Wall Shift North Access		(1,097)			
CTS PCC 050 Chinatown Plaza		0			
UMS Preload Rqmt for Concourse Level	10,001				
UMS Jet Grout at O'Farrell	35,001				
YBM PCC 37 SFAC Node Sculpture			50,797		
YBM PCC 59 Pavers Basis of Design			7,516		
UMS Locate PG&E Conduits	20,001				
CTS PCC 083 Switchgear Breakers		30,001			
YBM PCC 85 Ticketing Hall Changes			(14,034)		
UMS PCC 86 Headwall Soffit Pile	20,001				
USG PCC 87 Tiebacks, L2, GL 14-15	40,001				
USG PCC 89 E. Light Pole Foundation	2,501				
UMS PCC 71 Rerouting of Slab Drains	0				
YBM PCC 91 Concourse Beam Revision			0		
UMS PCC 93 Ellis Deck Seismic Joint	10,001				
STS PCC 095 Frame/Grate Change				(50,001)	
YBM PCC 97 Change Concourse Opening			30,001		
YBM PCC 099 Glass Art Changes			30,000		
CTS PCC 98 Slurry Wall Wr Proofing		20,001			
USG PCC 101 Elev. Machine Rooms	0				
UMS PCC 103 PG&E S.light at Maiden	0				
CTS PCC 104 Water Leak NE HH		20,001			
STS PCC 114 Stdpipe & Cond @ Portal				30,001	
UMS PCC 115 Decking Support WT	0				
USG PCC 116 Demo Column, Const Beam	0				
UMS PCC 118 Elevator Hoist Beams	0				
CTS PCC 120 Prov Cond-Lft-net Systm		0			
STS PCC 121 PG&E Pnts Streetlightig				0	
UMS PCC 122 Drain Piping Grout Dtls	250,001				

Contract Modification/Trend Log - Contract 1300 Stations

Awarded NTE Amount
Substantial Completion

\$839,676,400
2/10/2018

\$843,446,707
2/10/2018

	UMS	CTS	YBM	STS	COST REPORT NOTES
STS PCC 117 PDS Signs Moun Brack				0	
USG PCC 123 South Wall Ground Beams	4,001				
USG PCC 124 Irrigation Main	10,000				
USG PCC 127 Footing Elev Suvey Diff	0				
USG PCC 128 Dowls of Rbar Conn Dtls	0				
USG PCC 129 Escalator Work Point	0				
YBM PCC 130 HVAC VRV's			(23)		
USG PCC 133 Sheet Metal HVAC Duct	0				
USG PCC 134 Temp South Wall Support	40,001				
UMS PCC 136 Transformer Vault 3138	8,199				
UMS PCC 94 Clean out MRY Ductbank	0				
USG PCC 102 Fire & Life Safety	0				
USG PCC105 Adj to Row X and Row Y	10,001				
USG PCC 106 Edge of Slab for 8" Conn	10,001				
USG PCC 107 Light Pole Footings	10,001				
USG PCC 108 Con Wok chges du to DSC	10,001				
USG PCC 109 Rein Dtls for Struc Con	10,001				
USG PCC 110 Term of Built Up Colus	10,001				
USG PCC 111 Conc Cemo & Rev Con Dtl	10,001				
USG PCC 112 Glass Walk Roof System	10,001				
USG PCC 113 Elv/Esc Pit Floor Slope	10,001				
USG PCC 125 Foot,SOG & Str St Chang	50,000				
YBM PCC 126 Kiosks/Removal of PESBs			0		
YBM PCC 131 Add Rec Room CN316			693		
YBM PCC 132 Raise Pit Floor Elev 4			0		
YBM PCC 138 Add Rec Room SU310			781		
YBM PCC 140 Stair 3, Escalators 1&2			0		
UMS USG PCC 141 Wtrproof & Drn at P	88,585				
USG PCC 142 Storage Light & Elect.	22,000				
USG PCC 144 Conc. Finishing/Repairs	54,500				
YBM PCC 145 Stair 7/Escalators 3, 4			0		
USG PCC 147 Geary Streetlight	10,000				
YBM PCC 148 Elev.3, 4 Hoist Beam MP			0		
UMS PCC 150 South HW Wale Connection	15,000				
YBM PCC 152 Ventilation Rooms			0		
USG PCC 155 16-D Footing Demolition	18,000				

Contract Modification/Trend Log - Contract 1300 Stations

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	UMS	CTS	YBM	STS	COST REPORT NOTES
UMS PCC 153 Geary Catch Basin	(5,000)				
Change Order Request (COR)					
UMS-FACO #30 NDSC Inadequate CDF	157,462				
STS- AT&T Conduit Changes				22,290	
STS COR 145 DSC PVC Conduits Conf				1,276	
UMS COR 189 NDSC Pipe Conflict	5,938				
CTS COR 255 Additional Instruments		429,777			
STS COR 270 SW AWSS Conflict				8,280	
STS COR 271 DB AWSS Conflict				891	
STS COR 297 TC for Track Work at 4t				150,001	
UMS COR 110 DSC Obst. at JG Columns	1,945,520				
UMS COR 300 14 HDPE at Geary	(307)				
CTS COR 372 DSC Potholing @ N Acces		9,846			
STS COR 402 3x5 SW Confl w/ VCP SW				10,001	
UMS COR 403 Waterproofing at BART	63,285				
STS COR 404 Contam Soil in MRY DB				12,303	
STS COR 406 Addtl TC at 4th/King				675,001	
STS COR 411 Delay 8" WL miss serv				7,501	
CTS COR 405 Hydrocarbons in HH		17,485			
CTS COR 408 MSX Termination		191,291			
CTS COR 437 Unanticipated Elec @ Sh		50,001			
CTS COR 445 3x5 w/ HDPE/PVC Inside		10,001			
CTS COR 450 Switchgear Cubicles		100,001			
STS COR 451 PGE Vlt WD Line Confl				30,001	
STS COR 455 Conflicts w/ 8" WD Line				10,001	
STS COR 456 Conflicts w/ MRY Poles				20,001	
UMS COR 393 Wrap 14" Sewer Line	4,131				
STS COR 461 WD Confl w/ Mult Utilit				100,001	
STS COR 475 Removal of Fiber Optic				1,101	
STS COR 476 Conc DB Conf w/ FH				25,001	
STS COR 500 Tunnel Monuments				5,001	
CTS COR 526 Connection b/t Wall & D		25,001			
CTS COR 527 Connection b/t Wall/Dec		25,001			
STS COR 531 ATT Confl w/ 36" FM				25,001	
STS COR 558 CB in conf w AWSS				3,001	
STS COR 491 Util in conf w/ 10" VCP				10,001	
CTS COR 529 Rock Mass at Elev 37'		25,001			

Contract Modification/Trend Log - Contract 1300 Stations

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	UMS	CTS	YBM	STS	COST REPORT NOTES
YBM COR 564 Concrete Encased PG&E			0		
CTS COR 568 CMOD 019 Reservations		300,001			
CTS COR 574 Escalator supports		1			
STS COR 559 Temporary Trolley Pole				15,001	
STS COR 567 Loct of Plinth Breaks				10,001	
YBM COR 572 Kiosks on Platform Lvl			8,060		
CTS COR 582 Monitor at Agent Booth		25,001			
CTS COR 583 Elev 1-4 SFFD monitor		25,001			
STS COR 604 Duct Bank in conf. w se				10,001	
STS COR 615 Sump Pump Pit Cover				1,044	
UMS COR 617 Material North of Geary	50,001				
STS COR 639 Util conf w/ 12" AWSS				10,001	
UMS COR 646 Add. Floor Drain	6,504				
CTS COR 629 Abrsv Nosng at Con Strs		3,494			
STS COR 632 Omit Unist & add Anr Bt				6,293	
CTS COR 659 Add Grts at Sta Ag glas		1,537			
UMS COR 660 Added Grommets at Booth	1,134				
USG COR 663 Water Flow from Roof	50,001				
STS COR 665 Ov prd Lit Pl fdn in				3,001	
CTS COR 670 Kitn Sink Piping		10,001			
STS COR 675 Dct bnk & 3" Gas Line				10,001	
STS COR 677 Fir Supsion stnpip Mat				3,895	
CTS COR 679 GI Cutos & Add Sp hds		23,450			
STS COR 682 Shtdown #1 Rail Mods				9,432	
STS COR 683 Gas Ln & unkn Duct Bank				5,001	
STS COR 688 Ukn Piers /conct N 12"				5,001	
UMS COR 693 Add Crysized Glass Panel	40,823				
STS COR 694 Tr Rts in con/car pipes				5,001	
STS COR 699 Dct Bnk in Cnf w 36"FM				20,001	
STS COR 702 Brk Cs Bsn cnct w N Cuv				5,001	
STS COR 703 Tunl Wakway Expn jnts				3,412	
YBM COR 705 Del bems & Embds/Ven sh			(17,893)		
UMS COR 714 Acc Con Alr Ponts	10,001				
CTS COR 628 Add Beams and Embeds		8,331			
CTS COR 200 Dr & Dr Hrdwre for GFRC		7,797			
UMS COR 701 End of Platform Gates	24,492				
UMS COR 511 NDSC 10" VCP Culvert	8,320				

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	UMS	CTS	YBM	STS	COST REPORT NOTES
UMS COR 715 Drainge Pipe System	0				
USG COR 718 Infill Voids at 14 line	0				
YBM COR 719 Elv 1&2 frm Trcn to Hyd			0		
CTS COR 723 Strc Glc Asse Add Desig		0			
YBM COR 724 Anti-Graffiti Film			0		
UMS COR 725 Seis Qu Cer for St Glze	10,738				
UMS COR 731 Drain Piping Grout	10,001				
STS COR 736 Addition Rein Handholes				5,905	
UMS COR 747 NDSC Unidnfied Con Pile	50,001				
CTS COR 722 Elv 1-4 OH Str Host Bms		23,823			
STS COR 737 Dct Bnk infc w AT&T Rem				5,001	
UMS COR 653 Comp. Grout Holes	75,750				
STS COR 751 Db in coflct 36" FM& MH				10,001	
UMS COR 763 NDSC Trans in Vlt 3138	50,001				
STS COR 766 Wood Pir inCon w N Pils				25,001	
STS COR 303 8" WL Con w MRY Ducbnk				15,001	
STS COR 304 Black Transite Ductban				20,001	
YBM COR 588 Stair 3, Escalators 1&2			0		
YBM COR 589 Stair 7, Escalators 3&4			0		
YBM COR 692 Rebar to Base Plate TOD			13,118		
YBM COR 704 Vert Pickets@ Plumb Ris			(827)		
CTS COR 780 Aded Frit on Roof Panls		19,229			
STS COR 785 Unkn Con STin con W Swr				15,001	
CTS COR 794 Stn Clean Agnt Supp Sys		447,320			
STS COR 796 Woodn Piers Woodn Beams				10,001	
UMS COR 798 Comp Grout Damage	50,001				
YBM COR 800 Escalator Submittals			0		
USG COR 801 Column 14A Demolition	0				
UMS COR 808 Dimensions at Escalator	0				
Change Order - Pending					
CTS-Plaza Surface Level Struct Mods		(10,337)			
UMS FACO #53 NDSC Transite Pipe	61,055				
STS COR #236 E WD Config at Freelon				55,902	
YBM COR 240 Headhouse Contam. Mtl.			106,923		
UMS COR 407 8 inch SS Line Conflict	4,647				
USG - PCC #030 (US Garage Underpin)	732,157				
STS COR 443 AWSS Procurement Delay				5,777	

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	UMS	CTS	YBM	STS	COST REPORT NOTES
UMS PG&E Additional Conduits	36,657				
UMS COR 079 DSC Storage Tanks	97,817				
STS COR 468 Buried Debris in Subgra				2,027	
YBM COR 230 Change load Bilco hatch			15,119		
YBM COR 459 AWSS / WD Conflict			75,154		
STS COR 562 WD offsets around basin				25,143	
STS COR 570 Slab confl w/ AWSS				6,122	
STS COR 571 Brk struc confl w/ AWSS				1,648	
YBM COR 586 Concrete Block @ Folsom			16,709		
STS COR 613 Unknown Duct Bank				1,572	
STS COR 618 Add Exc added 4" Gate				11,838	
STS COR 619 Added WD POC Excavation				5,113	
STS COR 614 Conflict of installation				2,011	
UMS PCC 100 Waterproofing and Drain	178,873				
UMS COR 645 Notch Wales at N. HWall	98,841				
STS COR 535 LPW Main POC				42,312	
STS COR 753 Unknn Con in Conf N 36"				1,414	
Contract Modification					
CMod #035 STS PCC 077				11,147	
CMod #037 CTS Various CORs		8,886			
CMod #038 STS PCC 008				107,285	
CMod #039 STS Various CORs				50,170	
CMod #41 YBM Analytical Soil Testing			3,655		
CMod #42 YBM Class 2 Conta. Material			40,250		
CMod #36 YBM COR 54			14,484		
CMod #40 UMS Various CORs	23,271				
Approved	244,995	2,850,957	(645,932)	1,320,286	
Contract Modification					
CMod #1 BART Elevator Option 1 @ Pow	90,000				
CMod #3 CTS Work Safely Ard Power Po		25,956			
CMod #5 YBM Deletion of Comp Groutin			(1,833,869)		
CMod #6 CTS Plaza Constr Supt Servi		75,000			
CMod#7 STS FACOs 016, 017 & COR 009				80,170	
CMod #4 CTS-Force Account Change Or		130,000			
Cmod #10 YBM PCC 042			64,287		
CMod#8 STS PCC 006 ATT MH, PB&Trench				225,208	
CMod #9 YBM COR 10,15,16,18,20,25			126,663		

Contract Modification/Trend Log - Contract 1300 Stations

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	UMS	CTS	YBM	STS	COST REPORT NOTES
CMod #11 UMS PCC 002	12,997				
CMod #12 STS Traffic Control				1,032,302	
CMod # 14 YBM COR 036, 078			58,526		
CMod #15 YBM COR 196			3,178		
CMod #16 UMS COR 184	8,261				
CMod #017 CTS CORs 001 053 & 069		54,322			
CMod #018 CTS PCC 012		60,248			
CMod #13 CTS COR 006		57,707			
CMod #19 CTS COR 007, 026		2,274,225			
CMod #20 YBM PCC 047 and 45			364,562		
CMod #021 STS CORs 48/52/114/233/252				18,221	
CMod #22 UMS PCC 045, 046	16,198				
CMod #23 UMS PCC 058	63,838				
CMOD 24 STS PCC 23				108,053	
CMod #026 YBM COR 072			84,509		
CMod #025 - Various CORs			59,113		
CMod #027 UMS PCC 092	0				
CMod #028 CTS PCC 017.1		97,743			
CMod #029 STS PCC 009.1				(143,668)	
CMod #30 YBM Various CORs			334,165		
CMod #31 UMS COR 595	53,701				
CMod #32 YBM Various PCCs			92,934		
CMod #033 CTS Various CORs		56,422			
CMod #034 CTS Various CORs		19,334			
Grand Total	14,184,245	6,588,729	2,244,195	2,042,504	

7.6 BUDGET REVISIONS: REPORT SORTED BY CONSTRUCTION PACKAGES & SOFT COSTS

Report Period: August 2016

		July 2016			August 2016				
Group by Contract & SCC	CATEGORY ITEM	July 2016 Base	July 2016 Allocated Contingency	July 2016 Base + Allocated Contingency (YOE)	August 2016 Base	August 2016 Allocated Contingency	August 2016 Base + Allocated Contingency (YOE)	BUDGET TRANSFERS [August 2016] vs. [July 2016]	Cost Report Notes
10-50	CONSTRUCTION CONTRACT PACKAGES	1,142,498,122	18,415,381	1,160,913,503	1,143,008,753	37,904,750	1,180,913,503	20,000,000	
1250	UTILITY RELOCATION PACKAGE #1	12,134,906		12,134,906	12,134,906		12,134,906	0	
	Contract 1250 Form B Credit	(2,275,419)		(2,275,419)	(2,275,419)		(2,275,419)	0	
1251	UTILITY RELOCATION PACKAGE #2	20,870,197		20,870,197	20,870,197		20,870,197	0	
	Contract 1251 Form B Credit	(7,618,412)		(7,618,412)	(7,618,412)		(7,618,412)	0	
1252	GUIDEWAY TUNNEL	235,398,443	515,057	235,913,500	235,398,443	515,057	235,913,500	0	27
	Contract 1252 Form B Credit	(254,050)		(254,050)	(254,050)		(254,050)	0	
1300	CN1300 STATIONS TOTAL	842,861,076	16,740,324	859,601,400	843,371,707	36,229,693	879,601,400	20,000,000	28
1253: UMS	UNION SQUARE/MARKET STREET STATION [UMS]	294,221,884	4,808,706	299,030,590	294,275,585	19,755,005	314,030,590	15,000,000	
	UMS 1253 Form B Credit	(528,370)		(528,370)	(528,370)		(528,370)	0	
1254: CTS	CHINA TOWN STATION [CTS]	250,170,268	2,322,542	252,492,810	250,343,767	7,149,043	257,492,810	5,000,000	
	CTS 1254 Form B Credit	(451,703)		(451,703)	(451,703)		(451,703)	0	
1255: YBM	YERBA BUENA/ MOSCONE STATION [YBM]	157,015,969	6,073,032	163,089,001	157,443,068	5,645,933	163,089,001	0	
	YBM 1255 Form B Credit	(100,000)		(100,000)	(100,000)		(100,000)	0	
1256: STS	SURFACE TRACKWORK & SYSTEMS [STS]	141,452,954	3,536,045	144,988,999	141,309,286	3,679,713	144,988,999	0	
	STS 1256 SFPUC SEWER MAIN CREDIT	(2,925,296)		(2,925,296)	(2,925,296)		(2,925,296)	0	
	STS 1256 Form B Credit	(1,000,000)		(1,000,000)	(1,000,000)		(1,000,000)	0	
OTHER	OTHER CONSTRUCTION TOTAL	46,386,751	1,160,000	47,546,751	46,386,751	1,160,000	47,546,751	0	
40.06	PUBLIC ART PROGRAM	8,175,555	1,160,000	9,335,555	8,175,555	1,160,000	9,335,555	0	
40.02	MISC. CONSTR CONTRCT WK (TRACTION POWER FOR 1251)	258,202		258,202	258,202		258,202	0	
40.01	CONTRACT 1300 SOIL PROCESS	500,000		500,000	500,000		500,000	0	29
50.01	TEMPORARY LICENSE AGREEMENT (ATCS CENTRAL CONTROL)	487,972		487,972	487,972		487,972	0	
50.06	MTA FARE COLLECTION EQUIPMENT	5,400,000		5,400,000	5,400,000		5,400,000	0	
50.06	BART FARE COLLECTION EQUIPMENT	700,000		700,000	700,000		700,000	0	

7.6 BUDGET REVISIONS: REPORT SORTED BY CONSTRUCTION PACKAGES & SOFT COSTS

Report Period: August 2016

		July 2016			August 2016				
Group by Contract & SCC	CATEGORY ITEM	July 2016 Base	July 2016 Allocated Contingency	July 2016 Base + Allocated Contingency (YOE)	August 2016 Base	August 2016 Allocated Contingency	August 2016 Base + Allocated Contingency (YOE)	BUDGET TRANSFERS [August 2016] vs. [July 2016]	Cost Report Notes
40.02	JOB ORDER CONTRACTS (JOCS) - CONSTRUCTION	117,255		117,255	117,255		117,255	0	
40.08	AON RISK INSURANCE	18,088,750		18,088,750	18,088,750		18,088,750	0	
40.02	PUBLIC AGENCIES UTILITY COORDINATION	3,713,215		3,713,215	3,713,215		3,713,215	0	
40.02	DEPARTMENT OF PARKING AND TRAFFIC (DPT)	1,200,000		1,200,000	1,200,000		1,200,000	0	
50.03	UNION SQUARE/ MARKET STREET STATION POWER FEED	2,959,826		2,959,826	2,959,826		2,959,826	0	
50.03	UNION SQUARE/ MARKET STREET STATIONS PERMANENT POWER	(2,350,000)		(2,350,000)	(2,350,000)		(2,350,000)	0	
50.03	CHINATOWN STATION POWER FEED	2,959,826		2,959,826	2,959,826		2,959,826	0	
50.03	CHINATOWN STATION PERMANENT POWER	(2,350,000)		(2,350,000)	(2,350,000)		(2,350,000)	0	
50.03	YERBA BUENA/ MOSCONE STATION [YBM] POWER FEED	3,125,222		3,125,222	3,125,222		3,125,222	0	
50.03	YERBA BUENA/ MOSCONE STATION [YBM] PERMANENT POWER	(2,368,540)		(2,368,540)	(2,368,540)		(2,368,540)	0	
50.03	SURFACE STATION POWER FEED	11,839		11,839	11,839		11,839	0	
50.04	COMMUNICATION CONNECTION COSTS	5,757,629		5,757,629	5,757,629		5,757,629	0	
60	ROW, LAND, EXISTING IMPROVEMENTS	32,246,321	5,265,478	37,511,799	32,246,321	5,265,478	37,511,799	0	
60.01	PURCHASE OR LEASE OF REAL ESTATE	30,065,810	5,265,478	35,331,288	30,065,810	5,265,478	35,331,288	0	30
60.02	RELOCATION OF EXISTING HOUSEHOLDS	2,180,511		2,180,511	2,180,511		2,180,511	0	
70	VEHICLES	13,309,000	13,076,653	26,385,653	13,309,000	13,076,653	26,385,653	0	
70.01	LIGHT RAIL	13,309,000	13,076,653	26,385,653	13,309,000	13,076,653	26,385,653	0	31
70.07	SPARE PARTS								
80	PROFESSIONAL SERVICES	310,518,041	18,221,079	328,739,120	310,518,041	18,221,079	328,739,120	0	
80.01	PRELIMINARY ENGINEERING	46,202,674		46,202,674	46,202,674		46,202,674	0	

7.6 BUDGET REVISIONS: REPORT SORTED BY CONSTRUCTION PACKAGES & SOFT COSTS

Report Period: August 2016

		July 2016			August 2016				
Group by Contract & SCC	CATEGORY ITEM	July 2016 Base	July 2016 Allocated Contingency	July 2016 Base + Allocated Contingency (YOE)	August 2016 Base	August 2016 Allocated Contingency	August 2016 Base + Allocated Contingency (YOE)	BUDGET TRANSFERS [August 2016] vs. [July 2016]	Cost Report Notes
80.02	FINAL DESIGN	61,322,751		61,322,751	61,322,751		61,322,751	0	
80.03	PROJECT MANAGEMENT FOR DESIGN & CONSTRUCTION	89,021,634	13,905,845	102,927,479	89,021,634	13,905,845	102,927,479	0	
80.04	CONSTRUCTION ADMINISTRATION & MANAGEMENT	91,037,791	2,956,812	93,994,603	91,037,791	2,956,812	93,994,603	0	
80.05	INSURANCES	6,800,000		6,800,000	6,800,000		6,800,000	0	
80.06	LEGAL: PERMITS. REVIEW FEES BY OTHER AGENCIES	8,258,184		8,258,184	8,258,184		8,258,184	0	
80.07	SURVEYS, TESTING, INVESTIGATION. INSPECTION	883,100		883,100	883,100		883,100	0	
80.08	START-UP	6,991,907	1,358,422	8,350,329	6,991,907	1,358,422	8,350,329	0	
	ALL SCC CATEGORIES 10 TO 80	1,498,571,484	54,978,591	1,553,550,076	1,499,082,115	74,467,960	1,573,550,076	20,000,000	
90	UNALLOCATED CONTINGENCIES			24,749,927			4,749,927	(20,000,000)	32
	TOTAL PROJECT COST 10 TO 100			1,578,300,002			1,578,300,002		
	TOTAL CONTINGENCY			79,728,518			79,217,887		
	CONTINGENCY MINIMUM			60,000,000			60,000,000		
	BELOW OR ABOVE MINIMUM			19,728,518			19,217,887		

COST STATUS BY CATEGORY	SCC CODES	BUDGET July 2016	BUDGET TRANSFERS	BUDGET August 2016	August 2016 CTD	August 2016 EAC
CONSTRUCTION	SCC 010 - 050	1,160,913,504	20,000,000	1,180,913,504	706,272,872	1,164,543,774
REAL ESTATE	SCC 060	37,511,799	0	37,511,799	30,731,457	32,246,321
VEHICLES	SCC 070	26,385,653	0	26,385,653	2,147,782	13,309,000
PRELIM ENGINEERING	SCC 080.01	46,202,674	0	46,202,674	46,202,675	46,202,674
FINAL DESIGN	SCC 080.02	61,318,331	0	61,318,331	61,576,939	61,322,751
PM FOR DESIGN & CONSTRUCTION	SCC 080.03 - 080.04	196,922,082	0	196,922,082	118,829,267	180,059,425
OTHER PROF SRVCS	SCC 080.05 - 080.08	24,296,033	0	24,296,033	12,044,438	22,933,191
UNALLOC CONTINGENCY	SCC 090	24,749,924	(20,000,000)	4,749,924		
Grand Total		1,578,300,000	0	1,578,300,000	977,805,430	1,520,617,136

SCC DESCRIPTION	August 2016 BUDGET	August 2016 CTD
010 - GUIDEWAY & TRACK ELEMENTS	285,712,152	203,103,816
020 - STATIONS, STOPS, TERMINALS, INTERMODAL	593,203,715	294,345,356
040 - SITEWORK & SPECIAL CONDITIONS	206,626,538	188,295,711
050 - SYSTEMS	95,371,099	20,527,989
060 - ROW, LAND, EXISTING IMPROVEMENTS	37,511,799	30,731,457
070 - VEHICLES (number)	26,385,653	2,147,782
080 - PROFESSIONAL SERVICES (applies to Cats. 10-50)	328,739,120	238,653,319
090 - UNALLOCATED CONTINGENCY	4,749,924	
Grand Total	1,578,300,000	977,805,430

SCC DESCRIPTION	August 2016 BUDGET	August 2016 CTD
010.02-Guideway: At grade semi-exclusive (allows cross-traffic)	2,860,000	145,000
010.06-Guideway: Underground cut & cover	70,833,126	61,323,677
010.07-Guideway: Underground tunnel	200,808,300	136,368,623
010.09-Track: Direct fixation	6,761,089	2,647,916
010.12-Track: Special (switches, turnouts)	4,449,637	2,618,600
020.01-At-grade station, stop, shelter, mall, terminal, platform	6,673,138	1,529,775
020.02-Aerial station, stop, shelter, mall, terminal, platform	3,679,713	0
020.03-Underground station, stop, shelter, mall, terminal, platform	561,149,027	289,321,873
020.07-Elevators, escalators	21,701,837	3,493,708
040.01-Demolition, Clearing, Earthwork	11,282,452	10,715,171
040.02-Site Utilities, Utility Relocation	57,269,608	60,006,845
040.03-Haz. mat'l, contam'd soil removal/mitigation, ground water treatments	7,301,393	4,136,986
040.04-Environmental mitigation, e.g. wetlands, historic/archeologic, parks	1,020,165	641,366
040.05-Site structures including retaining walls, sound walls	2,706,431	2,706,431
040.06-Pedestrian / bike access and accommodation, landscaping	9,755,506	2,226,933
040.07-Automobile, bus, van accessways including roads, parking lots	6,967,874	2,151,465
040.08-Temporary Facilities and other indirect costs during construction	110,323,109	105,710,514
050.01-Train control and signals	28,031,423	6,876,113
050.02-Traffic signals and crossing protection	12,584,529	8,227,891
050.03-Traction power supply: substations	21,487,073	4,128,353
050.04-Traction power distribution: catenary and third rail	12,441,113	1,226,628
050.05-Communications	12,062,374	69,003
050.06-Fare collection system and equipment	6,100,000	0
050.07-Central Control	2,664,586	1
060.01-Purchase or lease of real estate	35,331,288	28,322,027
060.02-Relocation of existing households and businesses	2,180,511	2,409,430
070.01-Light Rail	26,385,653	2,147,782
080.01-Preliminary Engineering	46,202,674	46,202,675
080.02-Final Design	61,318,331	61,576,939
080.03-Project Management for Design and Construction	102,927,479	62,657,752
080.04-Construction Administration & Management	93,994,603	56,171,515

SCC DESCRIPTION	August 2016 BUDGET	August 2016 CTD
080.05-Professional Liability and other Non-Construction Insurance	6,800,000	6,340,196
080.06-Legal; Permits; Review Fees by other agencies, cities, etc.	8,262,604	4,872,536
080.07-Surveys, Testing, Investigation, Inspection	883,100	13,740
080.08-Start up	8,350,329	817,966
090.00-Unallocated Contingency	4,749,924	
Grand Total	1,578,300,000	977,805,430

[A] Cost Account Description	BUDGET	ACTUAL COSTS				[G] VARIANCE (B - F)	COST REPORT NOTES
	[B] August 2016 Budget (YOE)	[C] PRIOR MONTH Total	[D] PRIOR MONTH Monthly	[E] CURRENT Monthly	[F] CURRENT Total		
TOTAL PRELIMINARY ENGINEERING	46,542,061	46,542,061	0	0	46,542,061	0	33
11 - SFMTA PROJECT MANAGEMENT	8,828,359	8,253,957	0	0	8,253,957	574,403	34
12 - SFMTA ENGINEERING SERVICES	11,425,594	11,425,594	0	0	11,425,594	0	35
16 - DEPARTMENT OF PARKING AND TRAFFIC (DPT)	935,451	802,883	0	0	802,883	132,568	
21 - ARTS COMMISSION	1,500,570	1,500,570	0	0	1,500,570	1	36
22 - FIRE DEPARTMENT	33,825	33,825	0	0	33,825	0	
23 - CITY ATTORNEY'S OFFICE	1,234,754	1,234,754	0	0	1,234,754	0	
24 - RISK MANAGEMENT	0	0	0	0	0	0	
26 - PLANNING	99,604	99,604	0	0	99,604	0	
27 - DEPARTMENT OF PUBLIC HEALTH (DPH)	4,420	4,420	0	0	4,420	0	
29 - CITY AUDITOR	308,540	315,616	0	1,694	317,310	(8,770)	
32 - DPW - IDC ENGINEERING (HYDRAULIC)	3,322,887	3,714,064	0	0	3,714,064	(391,177)	
34 - DPW - IDC CONSTRUCTION (CAPITAL)	17,462	17,462	0	0	17,462	0	
36 - DPW - BSM INFRASTRUCTURE (MAPPING)	76,549	76,549	0	0	76,549	0	
39 - DPW - PCS SITE ASSESSMENT & REMEDIATION (SAR)	13,993	13,993	0	0	13,993	0	
51 - 821 HOWARD STREET	1,005,653	1,005,653	0	0	1,005,653	0	
55 - 651 BRANNAN	2,294,910	2,294,910	0	0	2,294,910	0	37
63 - CENTRAL SUBWAY PARTNERSHIP - AECOM-EPC JV CONTRACT	26,793,234	26,793,234	0	0	26,793,234	0	38
66 - ANIL VERMA	395,204	395,204	0	0	395,204	0	39
67 - HILL INTERNATIONAL CONTRACT 156	6,716,294	6,716,294	0	0	6,716,294	(920,555)	
68 - ARTHUR GALLAGER & CO. CS 164	6,800,000	6,340,196	0	0	6,340,196	459,804	
71 - TUNNEL/UTILITIES - CONTRACT # CONTRACT 155-1	5,469,336	5,469,336	0	0	5,469,336	0	40
72 - STATIONS - CONTRACT # CONTRACT 155-2	26,220,609	26,220,609	0	0	26,220,609	0	41
73 - SYSTEMS/INTEGRATION - CONTRACT 155-3	11,432,312	11,432,312	0	0	11,432,312	(0)	42
331 - BAY AREA RAPID TRANSIT (BART)	146,427	146,427	0	0	146,427	0	
332 - SAN FRANCISCO COUNTY TRANSPORTATION AUTHORITY (SFC)	0	0	0	0	0	0	
TOTAL FINAL DESIGN	115,075,987	114,307,465	0	1,694	114,309,159	(153,726)	
11 - SFMTA PROJECT MANAGEMENT	15,589,933	7,530,278	494,819	1,030	7,531,308	8,058,625	

[A] Cost Account Description	BUDGET	ACTUAL COSTS				[G] VARIANCE (B - F)	COST REPORT NOTES
	[B] August 2016 Budget (YOE)	[C] PRIOR MONTH Total	[D] PRIOR MONTH Monthly	[E] CURRENT Monthly	[F] CURRENT Total		
1.3.011.01.080.03 - CM:SFMTA LABOR-PROJECT MANAGEMENT	15,589,933	7,530,278	494,819	1,030	7,531,308	8,058,625	
12 - SFMTA ENGINEERING SERVICES	905,264	2,375,339	15,447	10,160	2,385,499	(461,917)	
1.3.012.02.080.04 - CM: SFMTA LABOR-ENGINEERING CONTRACT 1252	123,582	57,648	0	0	57,648	65,934	
1.3.012.06.080.04 - CM: SFMTA LABOR-ENGINEERING CONTRACT 1300	1,800,000	2,317,691	15,447	10,160	2,327,851	(527,851)	
13 - SFMTA CONSTRUCTION MANAGEMENT	44,075,375	11,070,193	432,766	(6,933)	11,063,260	31,993,796	
1.3.013.01.080.04 - CM:SFMTA LABOR-CONSTR. MANAGEM	43,057,057	11,070,193	432,766	(6,933)	11,063,260	31,993,796	
16 - DEPARTMENT OF PARKING AND TRAFFIC (DPT)	3,588,074	1,714,794	7,355	80,113	1,794,906	2,037,382	
1.3.016.01.080.04 - DPT CONTRACT 1300 SUPPORT UMS	299,600	85,019	0	0	85,019	214,581	
1.3.016.01.080.04 - DPT CONTRACT 1300 SUPPORT CTS	274,900	72,919	1,842	0	72,919	201,981	
1.3.016.01.080.04 - DPT CONTRACT 1300 SUPPORT YBM	238,400	114,006	0	0	114,006	124,394	
1.3.016.01.080.04 - DPT CONTRACT 1300 SUPPORT STS	876,876	187,764	5,513	7,701	195,465	681,411	
1.3.016.02.040.08 - DPT: FIELD OPS TUNNEL [B84]	0	1,464	0	0	1,464	(1,464)	
1.3.016.02.040.08 - DPT: FIELD OPS TUNNEL [B86]	0	204,261	0	0	204,261	(204,261)	
1.3.016.06.040.02 - DPT:DPT TRAFFIC SHOP CONTRACT 1300	1,200,000	0	0	0	0	1,200,000	
1.3.016.07.080.04 - DPT:SSD DS/CN: 1UTL	38,450	0	0	0	0	38,450	
1.3.016.08.040.08 - DPT:PCOS:2UTL [68A]	400,728	400,728	0	0	400,728	0	
1.3.016.08.040.08 - DPT:SSD CN:2UTL	0	108,020	0	0	108,020	(108,020)	
1.3.016.08.080.04 - DPT:SSD [1326]	259,120	235,252	0	17,284	252,536	6,584	
1.3.016.08.080.04 - DPT:SSD [13BN]	0	23,302	0	0	23,302	(23,302)	
1.3.016.08.080.04 - DPT:SSD [13CN]	0	963	0	0	963	(963)	
1.3.016.08.080.04 - DPT:SSD [B85]	0	92,008	0	0	92,008	(92,008)	
1.3.016.03.040.08 - PCOS:1300/UMS [68CPT544132W.CPT544132W]	0	78,408	0	55,128	133,536	(133,536)	
1.3.016.09.040.08 - PCOS:1300/STS [68CPT544132Z.CPT544132Z]	0	110,679	0	0	110,679	(110,679)	
17 - MOTIVE POWER	2,195	0	0	0	0	2,195	
1.3.017.07.040.02 - PWR:SFMTA-MOTIVE POWER-UTL.REL	2,195	0	0	0	0	2,195	
18 - SFMTA OPERATIONS	400,000	39,586	0	(91)	39,495	286,974	
1.3.018.04.040.02 - OPS:SUPPORT TO CONTRACT 1300/CTS	100,000	26,469	0	0	26,469	73,531	
1.3.018.06.080.07 - OPS:SUPPORT TO CONTRACT 1300/UMS	300,000	13,117	0	(91)	13,026	286,974	
19 - OTHER SFMTA	700,000	1,505,407	0	(560,578)	944,829	(244,829)	
1.3.019.01.080.07 - OTH.MTA SURVEY AND TESTING	500,000	0	0	0	0	500,000	
1.3.019.07.080.07 - OTH.MTA SFMTA-SURVEY; TSTG [6840]	0	714	0	0	714	(714)	
1.3.019.08.040.08 - OTH.MTA 1251 MATERIALS	150,000	126,149	0	0	126,149	23,851	
1.3.019.08.080.08 - OTH.MTA OPERATION SUPPORT DUR	50,000	1,378,544	0	(560,578)	817,966	(767,966)	

[A] Cost Account Description	BUDGET	ACTUAL COSTS				[G] VARIANCE (B - F)	COST REPORT NOTES
	[B] August 2016 Budget (YOE)	[C] PRIOR MONTH Total	[D] PRIOR MONTH Monthly	[E] CURRENT Monthly	[F] CURRENT Total		
21 - ARTS COMMISSION	12,010,885	2,767,242	86,207	16,940	2,784,182	9,226,703	
1.3.021.01.040.06 - ARTS:CTYCO-ARTS COMMISSION CONSTRUCTION	4,772,555	0	0	0	0	4,772,555	
1.3.021.01.080.03 - ARTS:CTYCO-ARTS COMMISSION [1227]	2,030,147	388,167	0	0	388,167	1,641,980	43
1.3.021.01.080.04 - ARTS:CTYCO-ARTS COMMISSION [PWE335MPFUNA	21,000	11,386	0	0	11,386	9,614	
1.3.021.06.080.03 - ARTS:CTYCO-ARTS COMMISSION PM [285MC.132J]	158,970	482,248	43,533	4,761	487,008	(328,038)	
1.3.021.01.080.03 - ARTS:CTYCO-ARTS COMMISSION [PWA335MPFUNA	0	4,389	0	0	4,389	(4,389)	
1.3.021.01.080.03 - ARTS:CTYCO-ARTS COMMISSION [PWE335MPFUNA	0	2,648	691	0	2,648	(2,648)	
1.3.021.06.040.06 - ARTS:CTYCO-ARTS COMMISSION [68CPT5441327.CP	1,500,000	1,393,660	0	0	1,393,660	106,340	
1.3.021.06.040.06 - ARTS:CTYCO-ARTS COMMISSION [285MCPFUNA.CP	1,903,000	484,743	42,938	12,180	496,923	1,406,077	
1.3.021.01.080.03 - ARTS:CTYCO-ARTS COMMISSION [132J]	465,213	0	0	0	0	465,213	
1.3.021.97.040.06 - ARTS:ARTS COMMISSION ALLOC CO	1,160,000	0			0	1,160,000	
23 - CITY ATTORNEY'S OFFICE	2,171,781	1,749,611	193,865	101,483	1,851,093	320,688	
1.3.023.01.080.06 - ATTY:CN LEGAL-CITY ATTORNEY OF	2,171,781	1,749,611	193,865	101,483	1,851,093	320,688	
25 - PUBLIC UTILITIES COMMISSION SEWER	(2,925,296)	0	0	0	0	(2,925,296)	
1.3.025.09.040.02 - STS.1256: SITE UTILITIES SFPUC SEWER MAIN	(2,925,296)					(2,925,296)	
26 - PLANNING	137,062	23,277	0	2,119	25,395	111,667	
1.3.026.01.080.06 - CM:CTYCO-PLANNING DEPARTMENT	137,062	23,277	0	2,119	25,395	111,667	
28 - PUBLIC UTILITIES COMMISSION WATER	4,242,012	2,765,491	75,434	(15,288)	2,750,203	1,491,809	
1.3.028.02.040.02 - CM:CTYCO-PUBLIC UTIL COMM. (PUC)	0	4,745	0	0	4,745	(4,745)	
1.3.028.02.040.08 - PUC: FIELD OPERATIONS TUNNEL	398,400	538,233	0	(29,433)	508,800	(110,400)	
1.3.028.02.080.04 - PUC:MTA CSP CN1252 [470465]	105,000	91,587	0	0	91,587	13,413	
1.3.028.03.040.02 - PUC:CDD CONTRACT 1300/UMS SUPPORT	606,354	108,380	214	108	108,489	497,865	
1.3.028.03.080.04 - PUC:CMB CONTRACT 1300/UMS INSPECTION	230,000	30,363	0	0	30,363	199,637	
1.3.028.04.040.02 - PUC:CDD CONTRACT 1300/CTS SUPPORT	271,755	169,496	495	63	169,559	102,196	
1.3.028.04.080.04 - PUC:CMB CONTRACT 1300/CTS INSPECTION	115,000	7,203	248	0	7,203	107,797	
1.3.028.05.040.02 - PUC:CDD CONTRACT 1300/YBM SUPPORT	450,282	202,006	8,917	4,810	206,816	243,466	
1.3.028.05.080.04 - PUC:CMB CONTRACT 1300/YBM INSPECTION	184,000	21,914	13,280	2,143	24,058	159,942	
1.3.028.06.040.02 - PUC:CMB CONTRACT 1300/SFWD AWSS MATERIAL	0	224,244	0	0	224,244	(224,244)	
1.3.028.07.040.02 - PUC:PUC CDD WATER CONNECTION CONTRACT 12	248,035	291,501	0	0	291,501	(43,466)	
1.3.028.07.080.04 - PUC:PUC CMB INSPECTION CONTRACT 1250	74,468	113,844	0	0	113,844	(39,376)	
1.3.028.08.040.02 - PUC:PUC CDD WATER CONNECTION CONTRACT 12	565,389	318,130	0	0	318,130	247,259	
1.3.028.08.080.04 - PUC:PUC CMB INSPECTION CONTRACT 1251	266,252	289,424	0	0	289,424	(23,172)	
1.3.028.09.040.02 - PUC:CMB CONTRACT 1300/STS SUPPORT	520,077	245,856	24,477	5,352	251,208	268,869	

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	[B] August 2016 Budget (YOE)	[C] PRIOR MONTH Total	[D] PRIOR MONTH Monthly	[E] CURRENT Monthly	[F] CURRENT Total		
1.3.028.09.080.04 - PUC:CMB CONTRACT 1300/STS INSPECTION	207,000	108,566	27,804	1,668	110,234	96,766	
32 - DPW - IDC ENGINEERING (HYDRAULIC)	1,150,459	(186,908)	13,588	2,625	(184,283)	1,199,443	
1.3.032.01.080.04 - CM:DPW:1424J-BUREAU OF ENGINEERING (BOE) [A	60,000	(285,405)	0	0	(285,405)	345,405	
1.3.032.03.080.04 - DPW IDC HYDRAULIC CN1300 UMS SUPPORT	297,938	14,537	3,370	0	14,537	283,401	
1.3.032.04.080.04 - DPW IDC HYDRAULIC CN1300 CTS SUPPORT	295,639	17,545	4,422	0	17,545	278,094	
1.3.032.05.080.04 - DPW IDC HYDRAULIC CN1300 YBM SUPPORT	301,882	24,339	1,041	0	24,339	277,543	
1.3.032.06.080.04 - DPW: BOE: 1300 DSDC	6,000	0	0	0	0	6,000	
1.3.032.08.080.04 - DPW.HYRDDPW-BOE IDC ENG SVC DC	9,000	0	0	0	0	9,000	
1.3.032.09.080.04 - DPW IDC HYDRAULIC CN1300 STS SUPPOR	180,000	42,076	4,755	2,625	44,700	135,300	
34 - DPW - IDC CONSTRUCTION (CAPITAL)	6,695,348	5,004,220	102,719	34,399	5,038,619	1,656,729	
1.3.034.02.080.04 - DPW:CONSTR:1252 CM [CD12]	730,000	1,207,603	0	0	1,207,603	(477,603)	
1.3.034.02.080.04 - DPW:CONSTR:1252 CM [13AC12]	206,000	138,397	0	0	138,397	67,603	
1.3.034.01.080.04 - DPW:BCM LABOR [2113]	2,140,142	2,140,142	0	0	2,140,142	0	
1.3.034.06.080.04 - DPW:CONSTR:1300 CM [13CP12]	3,619,206	1,518,078	102,719	34,399	1,552,477	2,066,729	
36 - DPW - BSM INFRASTRUCTURE (MAPPING)	465,562	110,294	0	1,447	111,741	434,821	
1.3.036.01.080.04 - DPW:MPPG:DPW-BUREAU OF ST USE	417,129	32,680	0	0	32,680	384,449	
1.3.036.02.080.04 - DPW:MPPG:1300-DPW-BUREAU OF ST USE	0	33,084	0	0	33,084	(33,084)	
1.3.036.02.080.06 - DPW:MPPG:DPW-BUREAU OF ST USE	90,000	44,530	0	1,447	45,977	44,023	
1.3.036.06.080.06 - DPW:MPPG:BSM PERMIT REVIEW	39,433	0	0	0	0	39,433	
37 - DPW - PCS MATERIAL TESTING LABORATORY	83,100	0	0	0	0	83,100	
1.3.037.01.080.07 - DPW.MTL.LABDPW-MATERIAL TESTIN	83,100	0	0	0	0	83,100	
39 - DPW - PCS SITE ASSESSMENT & REMEDIATION (SAR)	622,474	942,284	0	0	942,284	(319,810)	
1.3.039.01.080.04 - DPW:SITE ASSESSMENT & REMEDIATION (SAR) [13	8,621	506,858	0	0	506,858	(498,237)	
1.3.039.01.080.04 - DPW:SITE ASSESSMENT & REMEDIATION (SAR) [22	92,459	92,459	0	0	92,459	0	
1.3.039.01.080.04 - DPW:SITE ASSESSMENT & REMEDIATION (SAR) [22	78,400	78,400	0	0	78,400	0	
1.3.039.01.080.04 -DPW:SITE ASSESSMENT & REMEDIATION (SAR) [225	151,515	151,515	0	0	151,515	0	
1.3.039.01.080.04 - DPW:SITE ASSESSMENT & REMEDIATION (SAR) [23	24,343	24,343	0	0	24,343	0	
1.3.039.01.080.04 - DPW:SITE ASSESSMENT & REMEDIATION	58,757	10,109	0	0	10,109	48,648	
1.3.039.01.080.04 - DPW:SITE ASSESSMENT & REMEDIATION (SAR) [CE	31,367	31,367	0	0	31,367	0	
1.3.039.01.080.04 - DPW:SITE ASSESSMENT & REMEDIATION (SAR) [CH	100,000	8,621	0	0	8,621	91,379	
1.3.039.01.080.04 - DPW:SITE ASSESSMENT & REMEDIATION (SAR)	17,000	0	0	0	0	17,000	
1.3.039.02.080.04 - DPW: SITE ASSESSMENT & REMEDIATION (SAR) - C	18,632	16,880	0	0	16,880	1,753	
1.3.039.02.080.04 - DPW: SITE ASSESSMENT & REMEDIATION (SAR) - C	41,379	21,732	0	0	21,732	19,647	

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46 - MACY'S WEST - SFPUC SEWER WORK	258,202	258,202	0	0	258,202	0	
1.3.046.08.040.02 - MCY.SWRC. CONTRACT: MACY'S-SEW	258,202	258,202	0	0	258,202	0	
51 - 821 HOWARD STREET	4,690,481	612,061	0	0	612,061	4,078,420	
1.3.051.01.080.03 - ODC.HWRD:ODCs - 821 HOWARD STR	4,625,481	595,176	0	0	595,176	4,030,305	
1.3.051.02.080.04 - ODC.HWRD:ODCs - TUNNEL CONTRACT 1252	10,000	1,056	0	0	1,056	8,944	
1.3.051.06.080.04 - ODC.HWRD:ODCs - STATION CONTRACT 1300	55,000	15,829	0	0	15,829	39,171	
55 - 651 BRANNAN	0	10,348	0	0	10,348	(10,348)	
1.3.055.01.080.03 - CM:ODCs - 651 BRANNAN STREET	0	10,348	0	0	10,348	(10,348)	44
63 - CENTRAL SUBWAY PARTNERSHIP - AECOM-EPC JV CONTRACT	42,373,401	29,545,295	418,346	780,961	30,326,256	12,047,145	
1.3.063.01.080.03 - CM:PM:AECOM.CS149 OM-EPC JV CS149-PM	9,507,939	6,192,552	(201,851)	312,384	6,504,937	3,003,002	45
1.3.063.01.080.04 - CM:AECOM.CS149OM-EPC JV CS-149 [3B]	5,218,630	3,731,336	(302,777)	468,577	4,199,913	1,018,717	
1.3.063.01.080.04 - CM:AECOM.CS149OM-EPC JV CS-149 [3E]	7,000,000	7,982,813	0	0	7,982,813	(982,813)	
1.3.063.01.080.04 - CM:AECOM.CS149OM-EPC JV CS-149 [3F]	0	4,546,775	842,862	0	4,546,775	(4,546,775)	
1.3.063.01.080.04 - CM:AECOM.CS149OM-EPC JV CS-149 [9B]	11,042	11,042	0	0	11,042	(0)	
1.3.063.01.080.04 - CM:AECOM.CS149OM-EPC JV CS-149 [9D]	550,000	515,694	0	0	515,694	34,306	
1.3.063.01.080.04 - CM:AECOM.CS149OM-EPC JV CS-149 [9E]	600,000	523,943	0	0	523,943	76,057	
1.3.063.01.080.04 - CM:AECOM.CS149OM-EPC JV CS-149 [9F]	0	461,196	80,112	0	461,196	(461,196)	
1.3.063.01.080.04 - FD:CM:EPC JV CS49-PM [123A]	5,579,945	5,579,945	0	0	5,579,945	0	
1.3.063.97.080.03 - AECOM.CS149 ALLOCAT CONTING	13,905,845					13,905,845	
67 - HILL INTERNATIONAL CONTRACT 156	11,000,000	2,080,479	71,329	59,361	2,139,840	8,860,160	
1.3.067.01.080.03 - HILL.CS156:HILL INTL. CS-156 [1336]	600,000	920,426	0	0	920,426	(320,426)	
1.3.067.01.080.03 - HILL.CS156:HILL INTL. CS-156 [1337]	8,479,445	1,032,793	71,329	(238,232)	794,561	7,684,884	
1.3.067.01.080.03 - HILL.CS156:HILL INTL. [1330]	1,000,000	127,261	0	0	127,261	872,739	
1.3.067.01.080.03 - HILL INTERNATIONAL CS156 AWP 2016 [68CPT54413]	920,555	0	0	297,593	297,593	622,962	
69 - BAYLAND SOIL PROCESS CONTRACT 175	500,000	187,961	13,087	0	187,961	312,039	46
1.3.069.06.040.01 - BAYLAND.CS175:BAYLAND SOIL PROCESS [133K]	500,000	187,961	13,087	0	187,961	312,039	
71 - TUNNEL/UTILITIES - CONTRACT # CONTRACT 155-1	1,358,950	1,952,415	42,364	21,969	1,974,384	(615,434)	
1.3.071.01.080.04 - CM: CS155.1 DESIGN SUPPORT DURING CM [1232]	0	(87,201)	0	0	(87,201)	87,201	47
1.3.071.02.080.04 - CM: CS155.1 DESIGN SUPPORT DURING CM [1332]	1,358,950	2,039,616	42,364	21,969	2,061,585	(702,635)	
72 - STATIONS - CONTRACT # CONTRACT 155-2	8,752,240	6,605,204	507,848	364,579	6,969,783	1,782,457	
1.3.072.01.080.04 - CM: CS155.2 DESIGN SUPPORT DURING CM [1233]	50,000	51,351	0	0	51,351	(1,351)	48
1.3.072.01.080.04 - CM: CS155.2 DESIGN SUPPORT DURING CM [1333]	8,702,240	6,553,854	507,848	364,579	6,918,432	1,783,808	
73 - SYSTEMS/INTEGRATION - CONTRACT 155-3	4,828,269	2,489,147	111,329	23,784	2,512,931	2,315,338	

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1.3.073.01.080.04 - CM: CS155.3 DESIGN SUPPORT DURING CM [1236]	90,000	89,791	0	0	89,791	209	
1.3.073.01.080.04 - CM: CS155.3 DESIGN SUPPORT DURING CM [1334]	4,738,269	2,399,356	111,329	23,784	2,423,140	2,315,129	
81 - UTILITIES RELOCATION #1 (PORTAL & MOS) - CONTRACT 1250	11,968,150	11,968,150	0	0	11,968,150	0	
1.3.081.07.040.01 - UR1.CONTRACT 1250:SITWORK: DEMOLIT	167,458	167,458	0	0	167,458	0	
1.3.081.07.040.02 - UR1.CONTRACT 1250:SITWORK: UTILITI	10,099,341	10,099,341	0	0	10,099,341	0	
1.3.081.07.040.03 - UR1.CONTRACT 1250:SITWORK:HAZMAT	453,321	453,321	0	0	453,321	0	
1.3.081.07.040.08 - UR1.CONTRACT 1250:SITWORK:TEMPORAR	1,248,030	1,248,030	0	0	1,248,030	0	
82 - UTILITIES RELOCATION #2 (UMS) - CONTRACT 1251	20,794,582	20,794,582	0	0	20,794,582	0	49
1.3.082.08.040.01 - UR2.CONTRACT 1251:SITWORK: DEMOLIT	752,240	752,240	0	0	752,240	0	
1.3.082.08.040.02 - UR2.CONTRACT 1251:SITWORK:UTILITI	10,328,044	10,328,044	0	0	10,328,044	0	
1.3.082.08.040.03 - UR2.CONTRACT 1251:SITWORK:HAZMAT	172,712	172,712	0	0	172,712	0	
1.3.082.08.040.05 - UR2.CONTRACT 1251:SITWORK: STRUCTU	2,706,431	2,706,431	0	0	2,706,431	0	
1.3.082.08.040.06 - UR2.CONTRACT 1251:SITWORK:PEDESTRA	319,317	319,317	0	0	319,317	0	
1.3.082.08.040.07 - UR2.CONTRACT 1251:SITWORK:AUTO/BUS	190,362	190,362	0	0	190,362	0	
1.3.082.08.040.08 - UR2.CONTRACT 1251:SITWORK:TEMP FAC	6,325,476	6,325,476	0	0	6,325,476	0	
GUIDEWAY TUNNELS TOTAL	235,913,500	233,793,900	(1,087,497)	0	233,793,900	2,119,601	
83 - GUIDEWAY TUNNELS - CONTRACT # 1252 BASE	233,584,015	231,897,864	(1,459,208)	0	231,897,864	1,686,151	50
1.3.083.02.010.06 - CONTRACT 1252:GUIDEWAY:UNDERGRN'D CUT	60,446,425	60,446,425	(931,453)	0	60,446,425	0	
1.3.083.02.010.07 - CONTRACT 1252:GUIDEWAY:UNDERGROUND	105,423,090	105,164,927	0	0	105,164,927	258,163	
1.3.083.02.020.03 - CONTRACT 1252: STATIONS: UNDERGROUND	21,685,000	21,685,000	0	0	21,685,000	0	
1.3.083.02.040.01 - CONTRACT 1252:SITWORK:DEMO CLEARING	2,440,000	2,440,000	0	0	2,440,000	0	
1.3.083.02.040.02 - CONTRACT 1252:SITWORK:UTILITIES & RE	10,895,000	10,487,676	0	0	10,487,676	407,324	
1.3.083.02.040.03 - CONTRACT 1252:SITWORK:HAZMAT&MITIGAT	200,000	0	(200,000)	0	0	200,000	
1.3.083.02.040.04 - CONTRACT 1252:SITWORK:ENVIRON. MITIG	300,000	54,292	(0)	0	54,292	245,708	
1.3.083.02.040.06 - CONTRACT 1252:SITWORK:PED/BIKE ACCES	50,000	4,532	(45,468)	0	4,532	45,468	
1.3.083.02.040.07 - CONTRACT 1252:SITWORK:AUTO/BUS ACCES	1,345,000	1,345,000	0	0	1,345,000	0	
1.3.083.02.040.08 - CONTRACT 1252:SITWORK:TEMP FACILITIE	30,799,500	30,270,012	(282,287)	0	30,270,012	529,488	
83 - GUIDEWAY TUNNELS - CONTRACT # 1252 CMODs	1,814,428	1,896,036	371,711	0	1,896,036	(81,608)	51
1.3.083.83.010.06 - CONTRACT 1252: CONTRACT MOD	112,251	112,251	21,334	0	112,251	0	
1.3.083.83.010.07 - CONTRACT 1252: CONTRACT MOD	1,941,810	1,891,810	116,183	0	1,891,810	50,000	
1.3.083.83.020.03 - CONTRACT 1252: CONTRACT MOD	1,004,156	1,004,156	50,551	0	1,004,156	0	
1.3.083.83.040.02 - CONTRACT 1252: CONTRACT MOD	1,035,588	935,588	118,156	0	935,588	100,000	
1.3.083.83.040.03 - CONTRACT 1252: CONTRACT MOD	453,475	453,475	0	0	453,475	0	

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1.3.083.83.040.08 - CONTRACT 1252: CONTRACT MOD	(2,732,852)	(2,501,244)	65,487	0	(2,501,244)	(231,608)	
1.3.083.93.010.07 - CONTRACT 1252: TUNNEL ALLOC CONTING	515,057	0	0	0	0	515,057	52
CONTRACT 1300 - STATIONS, TRACKWORK AND SYSTEMS TOTAL	879,601,400	399,017,997	6,844,330	13,356,123	412,374,120	467,227,280	53
84 - UNION SQUARE/MARKET STREET STATION (UMS) - WORK PACK	294,030,590	160,515,981	1,995,095	4,286,139	164,802,120	129,228,470	18
1.3.084.03.020.03 - UMS.1253: UNDERGROUD STATION	253,822,452	137,737,586	1,687,095	3,470,938	141,208,524	112,613,928	
1.3.084.03.020.07 - UMS.1253: ELEVATORS ESCALATOR	9,465,694	1,374,096	0	0	1,374,096	8,091,598	
1.3.084.03.040.01 - UMS.1253: DEMOLITION CLEARING	6,071,588	5,794,089	0	30,000	5,824,089	247,499	
1.3.084.03.040.02 - UMS.1253: SITE UTILITIES UTIL	3,971,620	2,448,209	253,000	168,300	2,616,509	1,355,111	
1.3.084.03.040.03 - UMS.1253: HAZARDOUS MATERIALS	550,000	317,888	0	0	317,888	232,112	
1.3.084.03.040.04 - UMS.1253: ENVIRONMENTAL MITIGA	244,500	231,010	0	0	231,010	13,490	
1.3.084.03.040.06 - UMS.1253: PEDESTRIAN/BIKE	18,969	12,501	0	0	12,501	6,468	
1.3.084.03.040.07 - UMS.1253: AUTOMOBILE BUS ACCE	1,547,185	48,001	0	6,200	54,201	1,492,984	
1.3.084.03.040.08 - UMS.1253: TEMPORARY FACILITIES	10,398,701	9,674,761	5,000	104,451	9,779,212	619,489	
1.3.084.03.050.02 - UMS.1253: TRAFFIC SIGNALS AND	4,773,076	2,796,800	50,000	506,250	3,303,050	1,470,026	
1.3.084.03.050.03 - UMS.1253: TRACTION POWER SUPPL	1,815,534	10,001	0	0	10,001	1,805,533	
1.3.084.03.050.04 - UMS.1253: TRACTION POWER DISTR	216,957	66,038	0	0	66,038	150,919	
1.3.084.03.050.05 - UMS.1253: COMMUNICATIONS	1,134,314	5,001	0	0	5,001	1,129,313	
84 - UNION SQUARE/MARKET STREET STATION (UMS) CMODs	244,995	111,258	0	53,701	164,959	80,036	
1.3.084.84.020.03 - CMOD:UMS.1253: UNDERGROUD STATION	37,456	21,258	0	0	21,258	16,198	
1.3.084.84.020.07 - CMOD:UMS.1253: ELEVATORS, ESCALATORS	90,000	90,000	0	0	90,000	0	
1.3.084.84.040.01 - CMOD:UMS.1253: DEMOLITION CLEARING	53,701	0	0	53,701	53,701	0	
1.3.084.84.040.02 - CMOD:UMS.1253: SITE UTILITIES UTIL	63,838	0	0	0	0	63,838	
1.3.084.94.020.03 - UMS.1253: AC: ALLOC CONTING	19,755,005	0	0	0	0	19,755,005	54
85 - CHINATOWN STATION (CTS) - WORK PACKAGE 1254	247,567,810	106,631,489	2,683,060	5,187,229	111,818,718	135,749,092	
1.3.085.04.010.07 - CTS.1254: GUIDEWAY: UNDERGROUND TUNNEL	76,417,579	22,183,435	2,388,640	4,622,769	26,806,204	49,611,375	
1.3.085.04.020.03 - CTS.1254: UNDERGROUND STATION	133,001,053	60,949,570	161,439	175,149	61,124,719	71,876,334	
1.3.085.04.020.07 - CTS.1254: ELEVATORS ESCALATOR	6,812,856	1,164,501	52,000	60,500	1,225,001	5,587,855	
1.3.085.04.040.01 - CTS.1254: DEMOLITION CLEARING	400,000	400,000	0	0	400,000	0	
1.3.085.04.040.02 - CTS.1254: SITE UTILITIES UTIL	6,001,718	4,624,436	38,850	38,150	4,662,586	1,339,132	
1.3.085.04.040.03 - CTS.1254: HAZARDOUS MATERIALS	350,000	12,500	0	0	12,500	337,500	
1.3.085.04.040.04 - CTS.1254: ENVIRONMENTAL MITIGA	325,665	206,064	0	0	206,064	119,601	
1.3.085.04.040.06 - CTS.1254: PEDESTRIAN/BIKE	15,000	0	0	0	0	15,000	
1.3.085.04.040.07 - CTS.1254: AUTOMOBILE BUS ACCE	225,677	27,500	0	0	27,500	198,177	

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	[B] August 2016 Budget (YOE)	[C] PRIOR MONTH Total	[D] PRIOR MONTH Monthly	[E] CURRENT Monthly	[F] CURRENT Total		
1.3.085.04.040.08 - CTS.1254: TEMPORARY FACILITIES	16,571,322	15,804,762	0	103,476	15,908,238	663,084	
1.3.085.04.050.02 - CTS.1254: TRAFFIC SIGNALS AND	1,599,593	977,480	20,891	127,185	1,104,665	494,928	
1.3.085.04.050.03 - CTS.1254: TRACTION POWER SUPPL	4,063,927	227,500	0	60,000	287,500	3,776,427	
1.3.085.04.050.04 - CTS.1254: TRACTION POWER DISTRIBUTION	124,481	51,240	21,240	0	51,240	73,241	
1.3.085.04.050.05 - CTS.1254: COMMUNICATIONS	1,658,938	2,500	0	0	2,500	1,656,438	
85 - CHINATOWN STATION (CTS) CMODs	2,775,957	1,147,452	97,743	260,440	1,407,892	1,368,065	55
1.3.085.85.040.01 - CMOD:CTS.1254: POWER POLE	155,956	148,212	0	0	148,212	7,744	
1.3.085.85.040.02 - CMOD:CTS.1254: SITE UTILITIES UTIL	288,069	212,313	97,743	69,921	282,234	5,835	
1.3.085.85.040.03 - CMOD:CTS.1254: HAZARDOUS MATERIALS	2,274,225	729,220	0	190,519	919,739	1,354,486	
1.3.085.85.040.08 - CMOD:CTS.1254: TEMPORARY FACILITIES	57,707	57,707	0	0	57,707	0	
1.3.085.95.020.03 - CTS.1254: AC: ALLOC CONTING	7,149,043	0	0	0	0	7,149,043	56
86 - YERBA BUENA MOSCONE STATION (YBM) - WORK PACKAGE 12	158,089,000	87,492,894	895,672	493,589	87,986,483	70,102,517	
1.3.086.05.020.03 - YBM.1255: UNDERGROUND STATION	119,048,929	63,657,254	226,655	620,961	64,278,215	54,770,713	
1.3.086.05.020.07 - YBM.1255: ELEVATORS ESCALATOR	5,333,287	804,611	0	0	804,611	4,528,676	
1.3.086.05.040.01 - YBM.1255: DEMOLITION CLEARING	657,000	657,000	0	0	657,000	0	
1.3.086.05.040.02 - YBM.1255: SITE UTILITIES UTIL	6,520,189	6,163,179	221,760	0	6,163,179	357,011	
1.3.086.05.040.03 - YBM.1255: HAZARDOUS MATERIALS	2,629,439	1,676,936	0	(71,430)	1,605,506	1,023,933	
1.3.086.05.040.04 - YBM.1255: ENVIRONMENTAL MITIGA	100,000	84,476	0	15,524	100,000	0	
1.3.086.05.040.06 - YBM.1255: PEDESTRIAN/BIKE	16,665	1	0	0	1	16,664	
1.3.086.05.040.07 - YBM.1255: AUTOMOBILE BUS ACCE	1,542,725	495,401	4,000	0	495,401	1,047,324	
1.3.086.05.040.08 - YBM.1255: TEMPORARY FACILITIES	15,564,753	12,591,812	307,669	(117,299)	12,474,513	3,090,240	
1.3.086.05.050.02 - YBM.1255: TRAFFIC SIGNALS AND	1,726,492	1,267,423	111,588	33,333	1,300,756	425,736	
1.3.086.05.050.03 - YBM.1255: TRACTION POWER SUPPL	3,708,425	84,800	24,000	12,500	97,300	3,611,125	
1.3.086.05.050.05 - YBM.1255: COMMUNICATIONS	1,241,096	10,001	0	0	10,001	1,231,095	
86 - YERBA BUENA MOSCONE STATION (YBM) CMODs	(645,932)	745,949	0	419,117	1,165,066	(1,810,998)	
1.3.086.86.040.01 - CMOD:YBM.1255: DEMOLITION CLEARING	84,509	84,509	0	0	84,509	0	
1.3.086.86.040.02 - CMOD:YBM.1255: SITE UTILITIES UTIL	1,100,250	658,262	0	419,117	1,077,379	22,871	
1.3.086.86.040.08 - CMOD:YBM.1255: TEMPORARY FACILITIES	(1,830,691)	3,178	0	0	3,178	(1,833,869)	
1.3.086.96.020.03 - YBM.1255: AC: ALLOC CONTING	5,645,933	0	0	0	0	5,645,933	57
87 - SURFACE TRACKWORK AND SYSTEMS -WORK PACKAGE 1256	139,989,000	41,150,975	1,120,760	2,532,908	43,683,883	96,305,117	
1.3.087.09.010.02 - STS.1256: GUIDEWAY: AT-GRADE SEMI-EXCLUSIVE	2,860,000	145,000	0	0	145,000	2,715,000	
1.3.087.09.010.06 - STS.1256: GUIDEWAY: UNDERGROUND CUT & CVR	10,274,450	607,001	10,000	158,000	765,001	9,509,449	
1.3.087.09.010.07 - STS.1256: GUIDEWAY: UNDERGROUN	16,510,764	2,014,983	266,250	490,700	2,505,683	14,005,081	

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1.3.087.09.010.09 - STS.1256: TRACK DIRECT FIXATION	6,761,089	2,636,871	10,000	11,045	2,647,916	4,113,174	
1.3.087.09.010.12 - STS.1256: TRACK: SPECIAL	4,449,637	2,618,600	0	0	2,618,600	1,831,037	
1.3.087.09.020.01 - STS.1256: AT-GRADE STATION	6,673,138	1,516,775	97,607	13,000	1,529,775	5,143,363	
1.3.087.09.040.02 - STS.1256: SITE UTILITIES, UTILITY RELOCA	17,509,893	10,323,178	453,540	758,340	11,081,518	6,428,375	
1.3.087.09.040.03 - STS.1256: HAZARDOUS MATERIALS	200,000	183,626	0	0	183,626	16,374	
1.3.087.09.040.04 - STS.1256: ENVIRONMENTAL MITIGATION	50,000	50,000	0	0	50,000	0	
1.3.087.09.040.07 - STS.1256: AUTOMOBILE BUS ACCE	2,116,925	39,001	0	0	39,001	2,077,924	
1.3.087.09.040.08 - STS.1256: TEMPORARY FACILITIES	13,850,984	10,639,373	235,870	225,900	10,865,273	2,985,711	
1.3.087.09.050.01 - STS.1256: TRAIN CONTROL AND SIGNALS	27,543,451	6,032,403	0	793,710	6,826,113	20,717,338	
1.3.087.09.050.02 - STS.1256: TRAFFIC SIGNALS AND	4,485,368	2,460,027	19,778	59,393	2,519,420	1,965,948	
1.3.087.09.050.03 - STS.1256: TRACTION POWER SUPPL	9,911,014	983,640	0	0	983,640	8,927,374	
1.3.087.09.050.04 - STS.1256: TRACTION POWER DISTRIBUTION	6,099,675	848,996	27,715	22,820	871,816	5,227,859	
1.3.087.09.050.05 - STS.1256: COMMUNICATIONS	8,028,025	51,500	0	0	51,500	7,976,525	
1.3.087.09.050.07 - STS.1256: CENTRAL CONTROL	2,664,586	1	0	0	1	2,664,585	
87 - SURFACE TRACKWORK AND SYSTEMS (STS) CMODs	1,320,286	1,221,999	52,000	123,000	1,344,999	(24,713)	
1.3.087.89.040.02 - CMOD:STS.1256: SITE UTILITIES, UTILITY RELOCA	269,763	413,431	0	0	413,431	(143,668)	
1.3.087.89.040.03 - CMOD:STS.1256: HAZARDOUS MATERIALS	18,221	18,219	0	0	18,219	2	
1.3.087.89.040.08 - CMOD:STS.1256: TEMPORARY FACILITIES	1,032,302	790,349	52,000	123,000	913,349	118,953	
1.3.087.99.020.01 - STS.1256: AC: ALLOC CONTING	3,679,713	0	0	0	0	3,679,713	58
141 - CONSTRUCTION ADMINISTRATION	2,956,812	0	0	0	0	2,956,812	
1.3.141.97.080.04 - CONSTR.ADMIN:ALLOC CONTING	2,956,812					2,956,812	
142 - LEGAL/PERMITS	2,014,204	0	0	0	0	2,014,204	
1.3.142.01.080.06 - LGL.PRMTSF:LEGAL; PERMITS	2,014,204	0	0	0	0	2,014,204	
144 - STARTUP	8,300,329	0	0	0	0	8,300,329	
1.3.144.01.080.08 - STRT: STARTUP	6,941,907	0	0	0	0	6,941,907	
1.3.144.97.080.08 - STRTA: AC STARTUP ALLOC CONTIN	1,358,422					1,358,422	
151 - TEMPORARY LICENSE AGREEMENT	17,000	0	0	0	0	17,000	
1.3.151.01.080.06 - TEMP.LICPORARY LICENSE AGREEME	17,000	0	0	0	0	17,000	
170 - COMMUNICATIONS CONNECTIONS	5,757,629	0	0	0	0	5,757,629	
1.3.170.01.050.04 - COMM.CONNN:COMMUNICATION CONN	5,757,629	0	0	0	0	5,757,629	
181 - AON RISK INSURANCE CS 163	18,113,750	18,798,132	0	0	18,798,132	(684,382)	
1.3.181.01.040.08 - AON.CS163 AON RISK INS.	18,088,750	18,773,132	0	0	18,773,132	(684,382)	
1.3.181.01.080.03 - AON.CS171 AON RISK INS. STUDY	25,000	25,000	0	0	25,000	0	

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191 - FARE COLLECTION CONTRACTOR	5,400,000	0	0	0	0	5,400,000	
1.3.191.01.050.06 - FARE.CONSUL:FARE COLLECTION	5,400,000	0	0	0	0	5,400,000	
192 - THALES T&S CENTRAL CONTROL	487,972	50,000	0	0	50,000	437,972	
1.3.192.01.050.01 - THALES T&S ATCS	487,972	50,000	0	0	50,000	437,972	
202 - JOC2-022.0	63,938	0	0	0	0	63,938	
1.3.202.01.040.02 - JOC2-022:15&22 POTHOLING UTILI LGHT FNDS	63,938	0	0	0	0	63,938	
203 - JOC2-029.0	53,317	0	0	0	0	53,317	
1.3.203.07.040.02 - JOC0292-029: RELOCATE VAULTS-S	53,317	0	0	0	0	53,317	
302 - PG&E	1,988,173	2,749,912	0	0	2,749,912	(761,739)	
1.3.302.03.050.03 - PGE PERMANENT POWER UMS	(2,350,000)	0	0	0	0	(2,350,000)	
1.3.302.03.050.03 - PGE POWER FEED UMS	2,959,826	115,690	0	0	115,690	2,844,136	
1.3.302.04.050.03 - PGE PERMANENT POWER CTS	(2,350,000)	0	0	0	0	(2,350,000)	
1.3.302.04.050.03 - PGE POWER FEED CTS	2,959,826	0	0	0	0	2,959,826	
1.3.302.05.050.03 - PGE PERMANENT POWER YBM	(2,368,540)	0	0	0	0	(2,368,540)	
1.3.302.05.050.03 - PGE POWER FEED YBM	3,125,222	2,634,222	0	0	2,634,222	491,000	
1.3.302.09.050.03 - PGE POWER FEED STS	11,839	0	0	0	0	11,839	
331 - BAY AREA RAPID TRANSIT (BART)	951,356	60,455	0	0	60,455	890,901	
1.3.331.01.080.04 - CM:SFMTA LABOR-ENG SVCS-IRP/BART/SF	0	33,152	0	0	33,152	(33,152)	
1.3.331.01.080.06 - CM: BAY AREA RAPID TRANSIT (BART) [122A]	951,356	27,304	0	0	27,304	924,052	
333 - AMERICAN PUBLIC TRANSP. ASSOCIATION (APTA) CS-APTA	146,500	54,612	0	7,500	62,112	84,388	
1.3.333.01.080.03 - APTA:APTA - IRP [2G]	46,500	27,304	0	3,750	31,054	15,446	
1.3.333.01.080.03 - APTA:APTA - IRP [2C]	100,000	27,308	0	3,750	31,058	68,942	
334 - BART FARE COLLECTION SYSTEM	700,000	0	0	0	0	700,000	
1.3.334.01.050.06 - BART:BART FARE COLLECTION EQP	700,000	0	0	0	0	700,000	
401 - ECONOMIC AND WORKFORCE DEVELOPMENT (EWD)	17,600	17,600	0	0	17,600	0	
1.3.401.01.080.04 - EWD: MAYORS OFFICE ECON DEV	17,600	17,600	0	0	17,600	0	
402 - DEPARTMENT OF TECHNOLOGY	242,371	237,534	0	0	237,534	4,837	
1.3.402.07.050.04 - DT:1UTL:COMM. CONNECTIONS	166,756	166,179	0	0	166,179	577	
1.3.402.08.050.04 - DT:2UTL:COMM.CONNECTIONS	75,615	71,354	0	0	71,354	4,261	
404 - DEPARTMENT OF BUILDING INSPECTION (DBI)	1,204,081	1,204,081	0	0	1,204,081	0	
1.3.404.01.080.06 - DPT OF BUILDING INSPECTION	1,204,081	1,204,081	0	0	1,204,081	0	
491 - FORM B - REIMBURSEMENT	(12,227,954)	0	0	0	0	(12,227,954)	
1.3.491.02.040.02 - FORMB - CONTRACT 1252 UTILITY REIMBUR	(254,050)	0			0	(254,050)	59

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1.3.491.03.040.02 - FORMB - UMS:CONTRACT 1300 UTILITY REIMBURSE	(528,370)	0			0	(528,370)	60
1.3.491.04.040.02 - FORMB - CTS:CONTRACT 1300 UTILITY REIMBURSE	(451,703)	0			0	(451,703)	61
1.3.491.05.040.02 - FORMB - YBM:CONTRACT 1300 UTILITY REIMBURSE	(100,000)	0			0	(100,000)	62
1.3.491.07.040.02 - FORMB - CONTRACT 1250 UTILITY REIMBUR	(2,275,419)	0			0	(2,275,419)	64
1.3.491.08.040.02 - FORMB - CONTRACT 1251 UTILITY REIMBUR	(7,618,412)	0			0	(7,618,412)	65
1.3.491.09.040.02 - FORMB - STS:CONTRACT 1300 UTILITY REIMBURSE	(1,000,000)	0			0	(1,000,000)	66
TOTAL CONSTRUCTION PHASE	1,348,140,480	769,899,172	8,343,337	14,281,703	784,180,876	564,075,989	
1.4.091.01.070.01 - LRVS: LIGHT RAIL VEHICLES RFP [34B]	1,325,000	1,319,773	0	0	1,319,773	5,227	
1.4.091.01.070.01 - LRVS: LIGHT RAIL VEHICLES PROJECT MGT [68E]	827,132	828,051	0	(42)	828,009	(877)	
1.4.091.01.070.01 - LRVS: LRV PROCUREMENT ODC	25,000	0	0	0	0	25,000	
1.4.091.01.070.01 - LRVS: LRV PROCUREMENT	11,131,868	0	0	0	0	11,131,868	
1.4.091.97.070.01 - LRVA:AC: VEHICLES ALLOC CONTI	13,076,653					13,076,653	21
TOTAL VEHICLES	26,385,653	2,147,824	0	(42)	2,147,782	24,237,871	
1.5.015.01.060.01 - RE: EASEMENT ACQUISIT	400,000	322,939	0	0	322,939	77,061	
1.5.015.01.060.01 - RE: REAL EST SITE ACQ	16,523,400	14,307,667	0	0	14,307,667	2,215,733	
1.5.015.01.060.01 - RE: REAL ESTATE	750,000	766,272	0	0	766,272	(16,272)	
1.5.015.01.060.01 - RE: REC & PARK MOU	6,987,624	6,987,624	0	0	6,987,624	0	
1.5.015.01.060.01 - RE:-DEPT OF TRANSPOR	2,686,000	2,686,000	0	0	2,686,000	0	
1.5.015.01.060.01 - RE:-LICENSES FEES	400,000	381,311	0	0	381,311	18,689	
1.5.015.97.060.01 - RE:A:AC: RE: REAL ESTATE ALLO	5,265,478					5,265,478	
1.5.023.01.060.01 - ATTY:REAL ES	2,212,882	2,756,776	77,918	7,534	2,764,310	(551,428)	
1.5.101.01.060.02 - RES.RELO: RELOCATION COST	1,275,200	1,289,701	0	0	1,289,701	(14,501)	
1.5.102.01.060.02 - COMM.RELO-RELOC COMMERCIAL	905,311	1,119,729	0	0	1,119,729	(214,418)	
TOTAL ROW, LAND, EXISTING IMPROVEMENTS	37,405,895	30,618,019	77,918	7,534	30,625,553	6,780,342	
90 - CONTINGENCY	79,217,885					79,217,885	
1.7.500.91.090.00 - UNALLOCATED CONTINGENCY	4,749,924					4,749,924	67
TOTAL ALLOCATED CONTINGENCY	74,467,961					74,467,961	
TOTAL PROJECT COST	1,578,300,001	963,514,541	8,421,254	14,290,889	977,805,430	599,690,400	

7.1 Program Project Budget	
1	The Central Subway Project (CSP) (SFMTA Capital Program CPT 544) is defined in the FTA-SFMTA October 2012 Full Funding Grant Agreement with a budget of \$1.578 billion.
2	The TBM Retrieval Shaft Relocation (SFMTA Capital Program CPT 690) is one of four capital projects that is related to CSP. These projects are reported for background information as needed outside of the main body of the Project Monthly Progress Report.
3	The Chinatown Plaza (CPT 718) is for Chinatown Station enhancement capital project. The project has funding outside of the Central Subway Project. The construction is carried out in Contract 1300 Contract Modifications.
4	CPT 665 is a Real Estate project to relocation in compliance with California regulations for business relocations but outside of the Central Subway Project as defined by the FTA FFGA.
5	CPT 705 is an SFMTA capital improvement between the Agency and community stakeholders outside of the Central Subway Project.
6	Utility company reimbursements (Form B) result in funds received for work carried out on behalf of utilities concurrent to CSP work to achieve efficiencies.
7	PG&E Power Feed reimbursement funds are the refunds from PG&E when completion of Stations construction and switch to permanent power.
8	BART Elevator funds are reimbursements for work carried out on behalf of BART to install BART Powell Street Station elevator.
9	The Tutor Perini - CAD Files funds are the result of payments by the contractor for project documentation not included in the contract.
10	SFPUC Sewer Main funds are reimbursements for work carried out on behalf of San Francisco PUC (includes 10% construction contingency).
11	Traffic Effectiveness Project funded Contract Modification #40 for Culvert, Street & Sidewalk Restoration in North Beach are reimbursements for work carried out in Contract 1252 on behalf of SFMTA SSD.
12	SFPUC 24" Water Main funds are reimbursements for work carried out in Contract 1252 Contract Modification #41 on behalf of San Francisco PUC (includes construction management cost).
13	SFPUC North Beach 24" Water Main Additional Work funds are reimbursements for work carried out in Contract 1252 Contract Modification #48 on behalf of San Francisco PUC (includes construction management cost).
14	CN1300 Contract Modification #6 is funded by Chinatown Plaza (CPT 718) project.
14a	Traffic Effectiveness Project funded Contract Modification #51 for support for North Beach Restoration, OCS and Streetlighting which are reimbursements for work carried out in Contract 1252.

7.4 Contingency Management Trend Report	
15	In Oct 2014 Report, updated Contract 1250 contract cost to segregate contract amount and contract modification amount. Note that September 2013 Supplemental Authorized Contingency "column f" did not include completed contract.

16	In Oct 2014 Report, updated Contract 1251 contract cost to segregate contract amount and contract modification amount. Note that September 2013 Supplemental Authorized Contingency "column f" did not include completed contract.
17	Contract 1252 Original Contract Value "column a" and Original Contingency "column f" did not match September 2013 Supplemental due to Supplemental were used the revised value to reflect Contract Modifications #3-#18. Reduced Contract 1252 contingency to reflect CMod #20 for retrieval shaft relocation cost \$5.15M funded by CPT690, CMod #40 for Culvert, Street & Sidewalk Restoration cost \$694,651 funded by Traffic Effectiveness Project (TEP), and CMod #41 for install 24" Water Main in North Beach cost \$328,860 funded by SFPUC. In August 2015 report, release \$15M CN1252 Tunnel assigned contingency to program unallocated contingency. In March 2106 report, reduced Contract 1252 contingency by \$377,435 cost to reflect certification of five CMODS. CMod#49, #52 and #53 total \$221,967 are funded by CPS. CMod#51 Support for North Beach Restoration, OCS and Streetlighting cost of \$155,468 is being funded by TEP. Released \$155,468 CN1252 allocated contingency to program's unallocated contingency. In May 2016 report, reduced Contract 1252 contingency by \$185,913 cost to reflect certification of two CMODS. In July 2016 report, increased Contract 1252 contingency by \$15,259 cost to reflect certification of one CMOD.
18	BART Elevator scope and SFPUC Sewer Main scope is in Contract 1300; effort will be funded by BART. In January 2015 Report, corrected Station Contract value to match awarded amount.
19	In March 2016 Report, lowered Contract 1300 Stations CTS contingency by \$75,000 because Contract Modification #6 was funded by Project CPT718. The \$75,000 has been transferred to program's unallocated contingency.
20	In April 2015 report, real estate budget stated in RAMP Rev5 is \$36.7M, including \$1M contingency. The cost workbook ROW & contingency budget reflects this with \$36,511,799 and \$1,000,000 respectively. Revised costbook ROW budget & contingency to be \$37,511,799. The \$4,265,478 Caltrans lease savings is allocated to ROW allocated contingency.
21	In Dec 2014 Report, redistributed LRV budget to reflect recent firm bid cost per vehicle (\$3,327,250/unit) from vehicle procurement contract award. (SFMTA Board meeting 15JUL14, calendar item #11). Vehicle line item total budget remains unchanged, redistributed fund by reducing base amount to \$13,309,000, column "c" and increased allocated contingency column "h", by same amount.
22	In Oct 2014 Report, made two corrections: i) revised Professional Services, Original Contract Value "column a" from \$310,518,041 to \$310,618,041, ii) revised Original Congency. "column f" unallocated contingency from \$3,883,481 to \$3,845,945. In April 2015 report, used \$500K program contingency for CS-175 Bayland Soil Process contract. In August 2015 Report, added \$15M from Contract 1252. In March 2016 Report, the \$155,468 costs funded by other project offset credits and \$75K funded by CPT718 added to program's unallocated contingency. In August 2016 Report, moved \$15M to UMS contingency and \$5M to CTS contingency.
23	The total Central Subway Project budget of \$1.578 billion, based on the October 2012 FFGA with the FTA, is the primary MPR report reference.
24	Estimate at Completion is shown at Column "e".
25	Estimate at Completion vs. Budget variance is shown at Column "k".

7.5 Contract Modification/Trend Log - Contract 1300 Stations

26	Reported all trend cost for Contractor Change Order Requests and Proposed Contract Change and applied probability to forecasted trends. In April 2016 Report, reviewed probability formula and adjusted cost.
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7.6 Budget Revisions: Report Sorted by Construction Packages

27	In Dec 2014 Report, reduced CN1252 allocated contingency by \$28K to execute Contract Modification #46, see Report 7.5 Detail Contingency Usage Report. In August 2015 report, release \$15M CN1252 Tunnel assigned contingency to program unallocated contingency. In March 2016 report, five contract modifications certified totaling \$377,435 of which \$155,468 is using another source of funding. Released \$155,648 from CN1252 Tunnel assigned contingency to program unallocated contingency. In May 2016 Report, reduced CN1252 allocated contingency by \$186K to execute Contract Modification #54 and #55, see Report 7.5 Detail Contingency Usage Report.
28	In March 2016 Report, lowered Contract 1300 Stations CTS contingency by \$75,000 because Contract Modification #6 was funded by Project CPT718. The \$75,000 has been transferred to program's unallocated contingency. Refer to Report Note #19
29	Budget for Soil Process contract.
30	Program contingency increased by \$5,265,478. Refer to Report Note #20.
31	In Dec 2014 Report, redistributed LRV budget to reflect recent firm bid cost per vehicle (\$3,327,250/unit) from vehicle procurement contract award. (SFMTA Board meeting 15JUL14, calendar item #11). Vehicle line item total budget remains unchanged, redistributed fund by reducing base amount to \$13,309,000 and increased allocated contingency by same amount.
32	In April 2015 report, program contingency decreased by \$500,000. Refer to Reports #20. In August 2015 report, release \$15M CN1252 Tunnel assigned contingency to program unallocated contingency. In March 2016 report, released \$155,468 from Contract 1252 Tunnel assigned contingency and \$75,000 from Contract 1300 Stations assigned contingency totaling \$230,956. In August 2016, released a total of \$20M unassigned contingency to assigned contingency; \$15M to CN1300 UMS station and \$5M to CTS station.

7.9 Detail Monthly Expenditure Report

Phase 1 Preliminary Engineering

33	Famis cost for Preliminary Engineering (PE) is \$48,210,903.71. Cost Report for Preliminary Engineering (PE) is \$46,542,060. Some Design cost reported in Famis were moved to Design Phase, refer to Notes #19 and #20.
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Phase 2 Design Phase

34	Famis cost adjustment to transfer Project Management cost from July 2013 to Phase 3 Construction Phase.
35	Famis Phase 1 PE Index Code: 357906.CPT5441112 cost is \$10,222,939 \$8,949,300 is reported in Cost Report Phase 1 PE and the balance of \$1,273,639 is reported in Phase 2 Design.

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36	1.2.021.01.080.03 - FD:CTYCO-ARTS COMMISSION [357909ART001.CPT5441227]: FAMIS: \$1,425,167 Cost Report: \$1,425,167 cost is reported in Phase 2 Design, 1.2.021.01.080.03 Cost Transfer: Remaining cost is reported in Phase 3 Construction, 1.3.021.01.080.03 - ARTS:CTYCO-ARTS COMMISSION [357909ART001.CPT5441227]
37	1.2.055.01.080.02 - FD:ODCs - 651 BRANNAN STREET [35CPT5441241.CPT5441241]: FAMIS: \$2,294,910 Cost Report: \$2,294,910 1.2.055.01.080.02 Cost Transfer: Future costs to be allocated to 1.3.055.01.080.02 - FD:ODCs - 651 BRANNAN STREET [35CPT5441241.CPT5441241]
38	1.2.063.01.080.03 - AECOM.CS149 OM-EPC JV CS149-PM [68CPT544133D.CPT544133D]: FAMIS: \$4,698,167 Cost Report: \$4,698,167 on 1.2.063.01.080.03 Cost Transfer: Future costs to 1.3.063.01.080.03 - AECOM.CS149 OM-EPC JV CS149-PM [68CPT544133D.CPT544133D]
39	AVA Cost \$395,204 is reported in Phase 2 Final Design 1.2.066.01.080.03
40	1.2.071.01.080.02 - FD:FINAL DESIGN-DP1 [35CPT5441232.CPT5441232]: FAMIS: \$5,608,147 Cost Report: \$5,469,336 Cost Transfer: \$138,811 to 1.3.071.01.080.04 - FD:FINAL DESIGN-DP1 [35CPT5441232.CPT5441232]
41	1.2.072.01.080.02 - FD:FINAL DESIGN-DP2 [35CPT5441233.CPT5441233]: FAMIS: \$26,268,511 COST REPORT: \$26,220,609 COST TRANSFER: \$47,902 to 1.3.072.01.080.04 - FD:FINAL DESIGN-DP2 [35CPT5441233.CPT5441233]
42	1.2.073.01.080.02 - FD:FINAL DESIGN-DP3 [35CPT5441236.CPT5441236]: FAMIS: \$11,502,372 COST REPORT: \$11,432,312 COST TRANSFER: \$70,060 to 1.3.073.01.080.04 - CM: DP3 [35CPT5441236.CPT5441236]

Phase 3 CONSTRUCTION PHASE	
43	1.3.021.01.080.03 - ARTS:CTYCO-ARTS COMMISSION [357909ART001.CPT5441227]: FAMIS: \$1,525,982 Cost Report: \$1,425,167 1.2.021.01.080.03 Cost Transfer: any future costs to 1.3.021.01.080.03
44	1.3.055.01.080.02 - FD:ODCs - 651 BRANNAN STREET [35CPT5441241.CPT5441241]: FAMIS: \$2,294,910 Cost Report: \$2,294,910 1.2.055.01.080.02 - FD:ODCs - 651 BRANNAN STREET [35CPT5441241.CPT5441241] Cost Transfer: Future costs to be allocated to 1.3.055.01.080.02

45	1.3.063.01.080.03 - AECOM.CS149 OM-EPC JV CS149-PM [68CPT544133D.CPT544133D]: FAMIS: \$4,698,167 Cost Report: \$4,698,167 on 1.2.063.01.080.03 Cost Transfer: Future costs to 1.3.063.01.080.03 - AECOM.CS149 OM-EPC JV CS149-PM [68CPT544133D.CPT544133D]
46	Used \$500K program contingency for CS-175 Bayland Soil Process contract. Refer to Report Notes #20.
47	1.3.071.01.080.04 - FD:FINAL DESIGN-DP1 [35CPT5441232.CPT5441232]: FAMIS: \$5,608,147 Cost Report: \$5,469,336 Cost Transfer: \$138,811 to 1.3.071.01.080.04 - FD:FINAL DESIGN-DP1 [35CPT5441232.CPT5441232]
48	1.3.072.01.080.04 - FD:FINAL DESIGN-DP2 [35CPT5441233.CPT5441233]: FAMIS: \$26,268,511 COST REPORT: \$26,220,609 COST TRANSFER: \$47,902 to 1.3.072.01.080.04 - FD:FINAL DESIGN-DP2 [35CPT5441233.CPT5441233]
49	Contract 1251 Final cost is \$20,794,582.
50	In March 2016 and July 2016, contract 1252 modifications budget and actuals have been realigned and adjusted to reflect actuals costs.
51	In March 2016 and July 2016, contract 1252 modifications budget and actuals have been realigned and adjusted to reflect actuals costs.
52	In July 2015 Report, used Contract 1300 Contractor schedule to report budget and actual cost. The Standard Cost Categories (SCC) allocation changed from previous reports. In August 2015 Report, adjusted some of Contract 1300 Contractor SCC assignment to match most of previous SCC assignment. In March 2016, \$75,000 Cmod#6 subtracted from CN1300 Stations contingency (using CPT718 funding) and transferred to Program contingency; this lead to the total CN1300 Station budget being lowered.
53	Revised Contract 1252 allocated contingency SCC code from 040.08 to 010.07.
54	Revised Contract 1300/UMS allocated contingency SCC code from 040.08 to 020.03.
55	In March 2016 Report, reduced Contract 1252 contingency by \$377,435 cost to reflect certification of five CMODS.
56	Revised Contract 1300/CTS allocated contingency SCC code from 040.08 to 020.03.
57	Revised Contract 1300/YBM allocated contingency SCC code from 040.08 to 020.03.
58	Revised Contract 1300/STS allocated contingency SCC code from 040.08 to 020.01.
59	Revised Form B Reimbursements SCC code from 900.01 to 040.02
60	Revised Form B Reimbursements SCC code from 900.01 to 040.02
61	Revised Form B Reimbursements SCC code from 900.01 to 040.02
62	Revised Form B Reimbursements SCC code from 900.01 to 040.02
63	Revised Form B Reimbursements SCC code from 900.01 to 040.02
64	Revised Form B Reimbursements SCC code from 900.01 to 040.02
65	Revised Form B Reimbursements SCC code from 900.01 to 040.02
66	Revised Form B Reimbursements SCC code from 900.01 to 040.02

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67	<p>Increase Program contingency \$1,023,508. Refer to Report Notes #11 and #12. In April 2015 report, program contingency decreased by \$500,000. Refer to Report Notes #20. In August 2015 report, release \$15M CN1252 Tunnel assigned contingency to program unallocated contingency. In March 2016 report, program unallocated contingency increased by \$230,468. In August 2016, released \$20M to CN1300 Construction assigned contingency from program unallocated contingency.</p>
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Appendix B

DETAIL SCHEDULE REPORTS

SCHEDULE HIGHLIGHTS

The Master Project Schedule (MPS) below includes progress through August 2016. The August 2016 Schedule Update submittal from Contract 1300 Contractor is rejected due to incorrect schedule logic. The Contract 1300 schedule represented in this report is based on the SFMTA August 2016 Schedule Update.

The MPS shows a forecast Revenue Service Date of August 2019.

The controlling critical (longest) path of the MPS runs through CTS Excavation succeeded by STS Startup & Testing, Commissioning and Pre-Revenue Activities to the Baseline Finish and Revenue Service Date. See Appendix B – Longest Path. The latest schedule shows the longest path running through the Chinatown Station (CTS). Contractor is required to implement a Recovery Schedule to put the Project back on schedule.

Schedule Contingency is fully utilized on the critical path of the MPS, which is below the Minimum Schedule Contingency level of 6 months. A schedule re-evaluation will be performed, utilizing the updated Contract 1300 Schedule. Recovery options are being implemented in key areas as work proceeds. SFMTA continues to meet with Contractor to discuss all schedule concerns and comments. Excavation and support of the Lower Left & Right Sidedrifts began in early August after additional excavation to reach the appropriate working elevation was achieved. TPC continues to experience lower production rates during their mining activity that resulted in a loss of 7 calendar days in month of August. Mitigation measures were continued by TPC in an effort to recover some lost time, extending workday shifts to two-twelve hours shifts, and progressing breakout of the remaining bottom center slurry wall. Contract 1300 Schedule delay in August results in the forecast Revenue Service Date slipping to August 1, 2019.

Contract 1300 Contractor submitted twenty-one (21) Schedule Updates from December 2014 to August 2016. SFMTA rejected eleven (11) Schedule Updates from September 2015 to April 2016 and June to August 2016. SFMTA approved as noted the May 2016 Schedule Update. Contractor has been directed to develop a Recovery Schedule as required by Contract and correct out-of-sequence and Retained Logic driving many of the forecast dates. Review of schedule updates as well as identifying recovery options is ongoing.

Contract 1300 - WP1253 UMS / WP1254R CTS / WP1255 YBM / WP1256 STS:

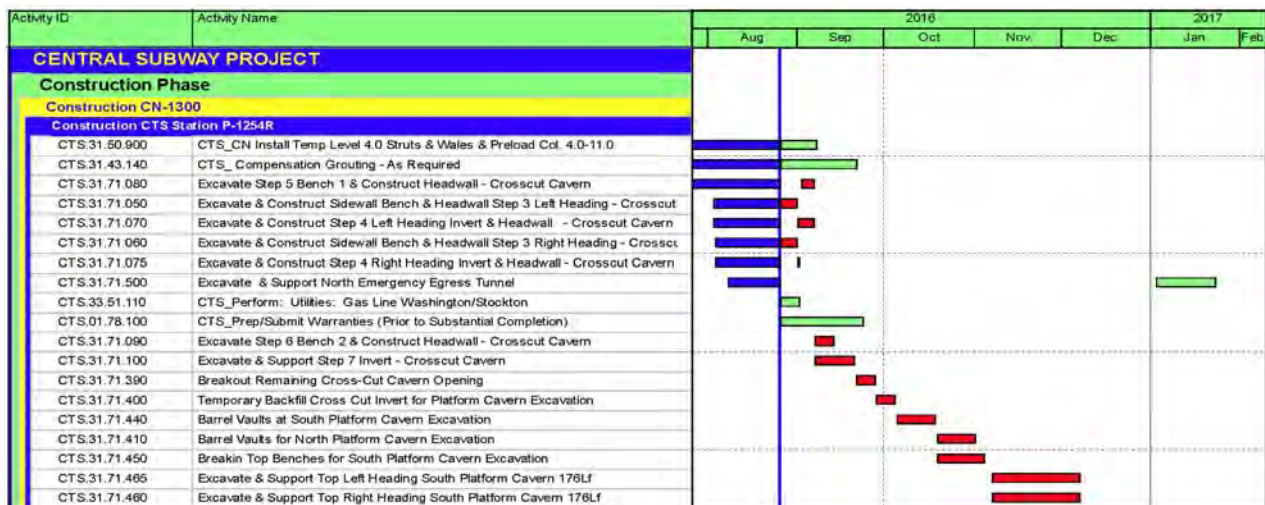
The Contractor, Tutor Perini Corporation's (TPC) baseline schedule is incorporated into the master program schedule. The preliminary SFMTA Contract 1300 August 2016 schedule is used within the August Report. The SFMTA Contract 1300 August 2016 schedule is based on the approved baseline schedule logic with adjustments made for fixing retained logic and lags. The SFMTA will continue to use the SFMTA Contract 1300 schedule update as a forecasting tool going forward until the Contract 1300 Contractor submits an acceptable schedule that addresses all of SFMTA scheduling concerns.

Work Package P-1254R (CTS) has performed the following work this month:

- Head house- excavated to 6' below Temp Level 5.0 and install walers and struts
- Cross Cut Cavern: Approximately 75% done with CCC under Stockton Street
- Incidental street work (minor), ongoing monitoring and surveying
- North Emergency Egress (NEE) Shaft completed; NEE Tunnel approximately 50% completed

Work Package P-1254R (CTS) will perform the following work next month

- Crosscut Cavern: complete Crosscut Cavern under Stockton Street
- Begin and complete barrel vault pipe drilling for North and South Platform Tunnels



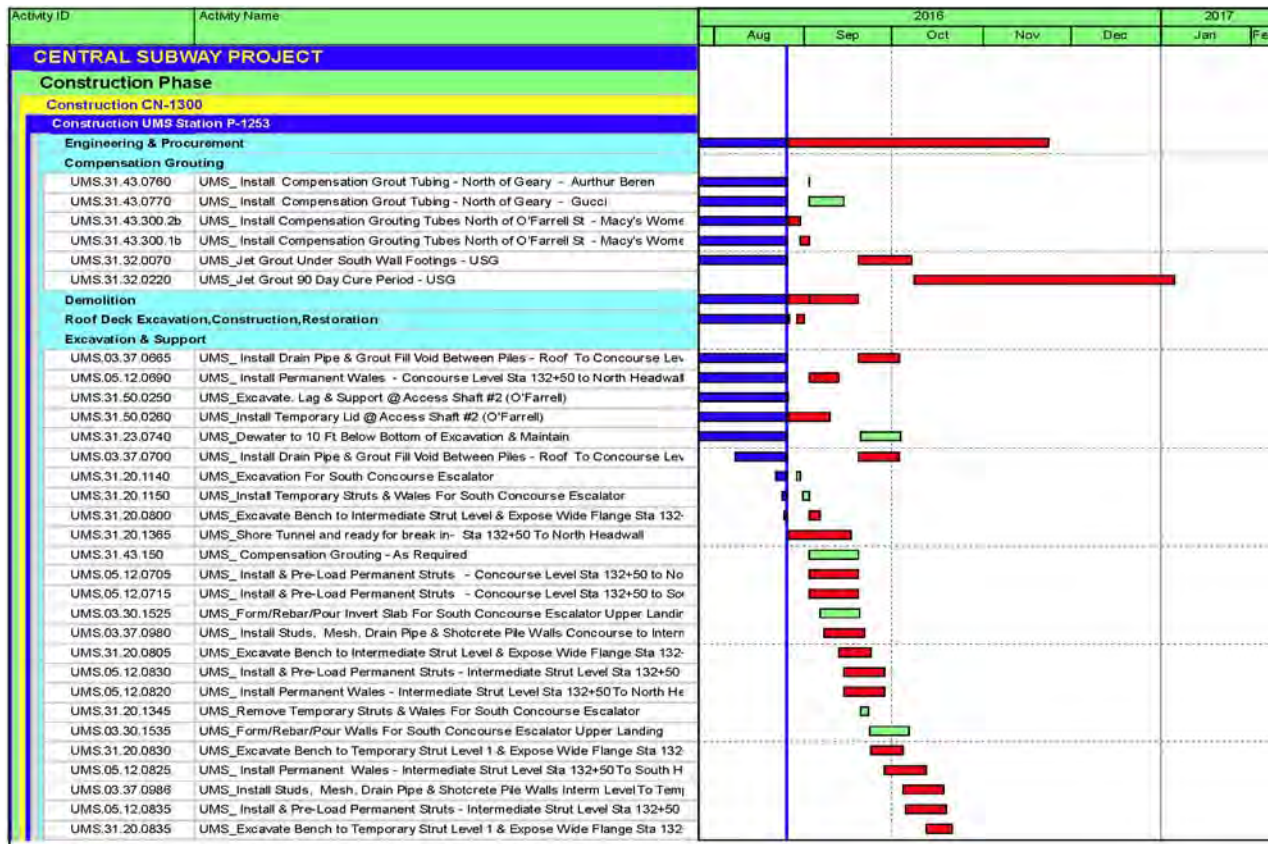
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Work Package P-1253 (UMS) has performed the following work this month:

- Restored one lane on Stockton Street between Post and Geary Streets
- North Concourse: Installed traffic signal conduits, sidewalk, and ADA ramps; completed backfill and installed paving. Below deck: installed vertical drain pipe as well as lookouts, starpacks, and shotcrete benches. All North Concourse level wales and struts were previously installed with the exception of wales 9 and 10, which jog around the north headwall
- Platform Station: Completed drilling and installing compensation grout pipes and performed preconditioning of compensation grout pipes. Installed all remaining Concourse level wales with the exception of wales 9 and 10 and began installing struts. Installed most vertical drain pipes. Performed coring for jet grout verification. At roof deck pour 13-1A, performed pile repairs and installed steel decking to partially construct a portion of roof deck 1A, the remainder of which is being left open for access. Installed AWSS pipe at night
- South Concourse: Performed preconditioning of compensation grout pipes, primarily on the west side of the station. Excavated to expose escalator piles, chipped concrete to expose beams, and began installation of the temporary escalator wale/strut supports.
- Ellis: Installed rebar for the west side structural invert slab and repaired escalator shaft walls. Installed sewer culvert
- UMS Garage: Completed PCC030 Underpinning along GL-16. Commence excavation, formwork and rebar installation for the pile caps. Completed installation of new column footings along south wall of the garage. Continue FRP new shear walls along GL10, and B-line on Level 1

Work Package P-1253 (UMS) will perform the following work next month:

- Nighttime lane closures expected for jet grout coring and utility work
- North Concourse: Below deck: install wales 9 and 10; installed shotcrete and steel for pile repair. Excavate North Concourse Fan Level
- Platform Station: Install wales 9 and 10. Complete preconditioning of compensation grout pipes. Installed lookouts, benches and wales. Install rebar and concrete for Roof Deck 13-1A. Complete installation of AWSS pipe at night. Continue excavation and support of Stairs 3&4 shaft on O'Farrell Street
- South Concourse: Complete preconditioning of compensation grout pipes. Continue to excavate and install support wales/struts at the escalator piles to reach the invert elevation of the pit
- Ellis Annex: Install and test seismic roof deck joint. Install waterproofing and lightweight concrete over roof deck joint. Backfill and restore
- UMS Garage: Begin excavation and construction of escalator pits. Install steel framework along the Geary South Wall. Tie-in remaining micropiles to existing footings. Install underpinning below the 16.3 to 17 line and install soil nail wall. Continue installing formwork, waterproofing, and install rebar and concrete for the plaza level deck

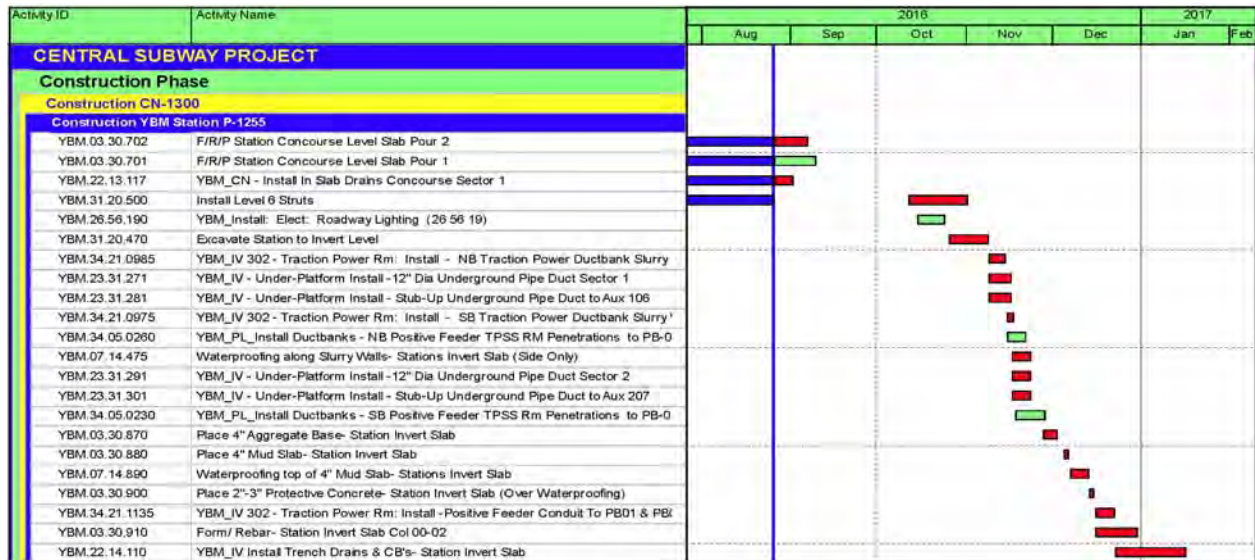


Work Package P-1255 (YBM) has performed the following work this month:

- Continue utility installation on east side of 4th Street north of the north headwall
- Shotcrete for interior walls in Concourse is in progress
- Placing cast-in-place concrete walls in Mezzanine is complete
- Excavation and preparation for Vent Shaft floor placement in progress

Work Package P-1255 (YBM) will perform the following work next month:

- Continue utility installation on east side of 4th Street north of the north headwall
- Continue placing shotcrete interior perimeter walls in Concourse
- Complete Vent Shaft floor waterproofing and concrete slab
- Begin breakthrough of tunnel liner and excavation beneath Concourse slab in Station box
- Preparation for placement of stairs within station box
- Preparation for CMU interior wall placement in Mezzanine Level

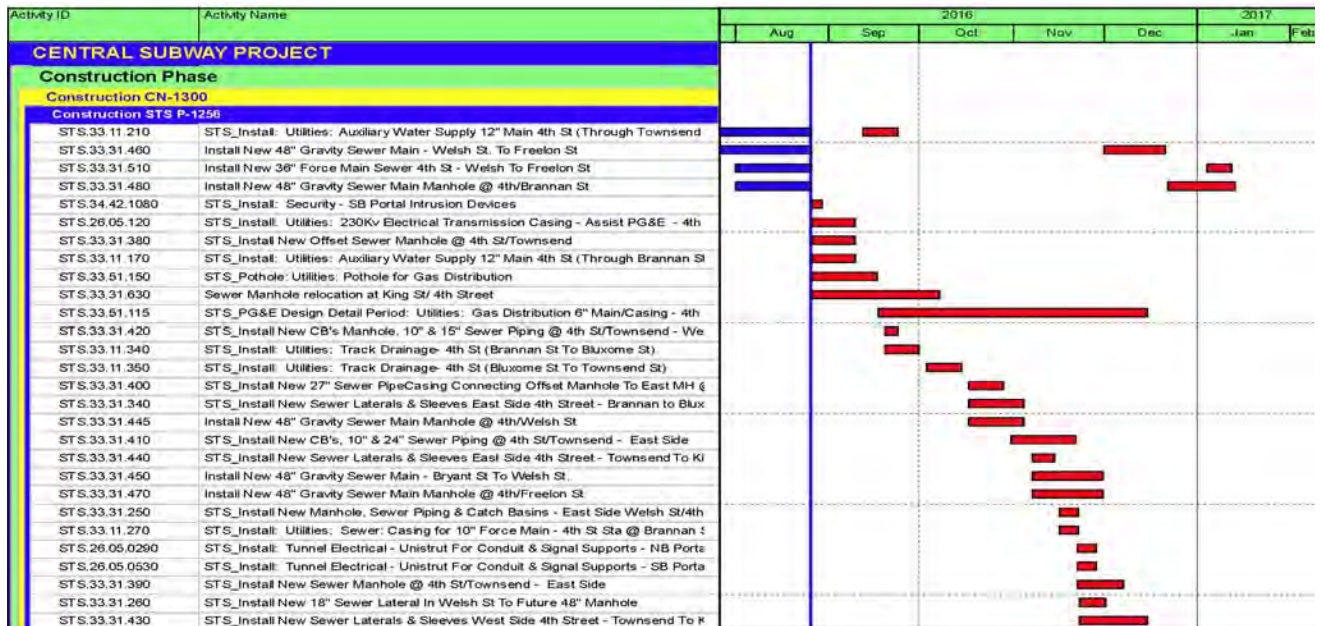


Work Package P-1256 (STS) has performed the following work this month:

- Started work on the 78" sewer offset manhole at 4th/Bluxome
- 78" Sewer slip-line work on 4th between Townsend and King is ongoing. Sewer is being prepped to receive the 66" HOBAS fiberglass pipes
- Auxiliary Water Supply System (AWSS) work on 4th between Townsend and King is on hold pending completion of 78" sewer slipline work
- Continue to install 48" sewer and 36" force main on 4th Freelon and Welsh
- Started MRY ductbank at 4th/Brannan intersection
- Continuing to install tunnel drain and pour tunnel invert between UMS and YBM

Work Package P-1256 (STS) will perform the following work next month:

- Continue 78" sewer rehabilitation
- Continue 36" sewer force main installation
- Continue 48" sewer force main installation
- Continue MRY ductbank installation
- Resume AWSS installation
- Continue track drain and tunnel invert pour



SCHEDULE REVISIONS

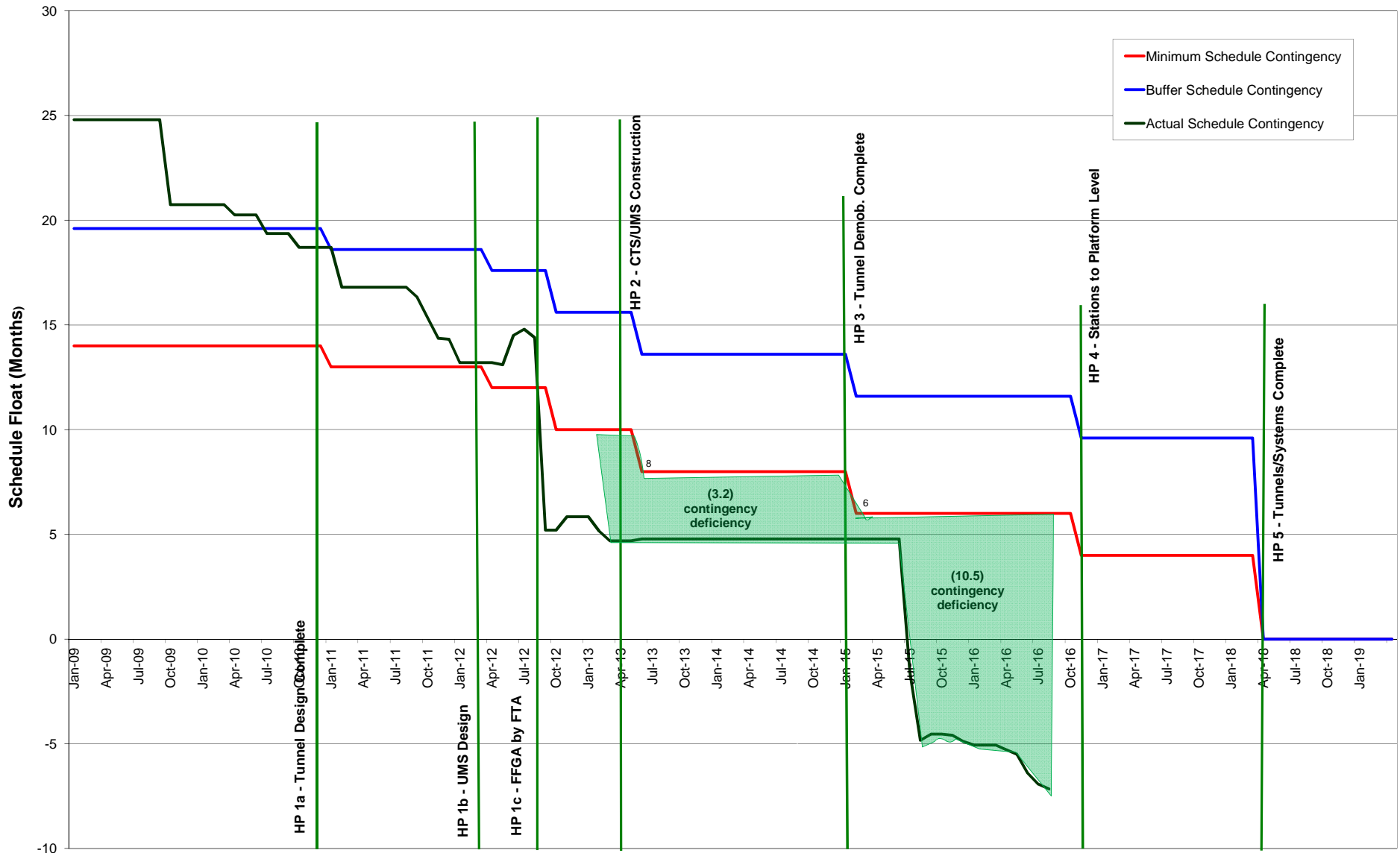
The SFMTA Contract 1300 August 2016 schedule update was added this period to the Central Subway Project Master Schedule.

LIST OF SCHEDULE REPORTS

- 1.1. Schedule Contingency Drawdown
- 1.2. Master Summary Schedule
- 1.3. Program Critical Path Schedule
- 1.4. Construction Contract Summary Schedule
- 1.5. Detail Schedule for Remaining Work

CENTRAL SUBWAY PROJECT Schedule Contingency Drawdown

August, 2016



Activity ID	Activity Name	Original Duration	Start	Finish	2016		2017				2018				2019			2020		
					Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2
CENTRAL SUBWAY PROJECT					▼ CENTRAL SUBWAY PROJECT															
Program Level Milestones					▼ Program Level Milestones															
PJD1000	Central Subway Project Start	0	06/03/03 A																	
MS0004A	Tunnel Excavation Complete - Project Milestone #4A	0		09/05/14 A																
MS0019	Baseline Finish Date: 12-26-2018	0		08/01/19*	◆ Baseline Finish Date: 12-26-2018															
MS0009	CSP Revenue Service Date	0		08/01/19*	◆ CSP Revenue Service Date															
Preliminary Engineering Phase																				
Final Design																				
Light Rail Vehicles					▼ Light Rail Vehicles															
Real Estate					▼ Real Estate															
Construction Phase					▼ Construction Phase															
Construction Support and Costs					▼ Construction Support and Costs															
Construction Utility Contract #1- MOS & Portal CN-1250																				
Construction Utility Contract #2 - UMS CN-1251																				
Construction Tunnels CN-1252					▼ Construction Tunnels CN-1252															
Construction CN-1300					▼ Construction CN-1300															
CN- 1300 Milestone					▼ CN- 1300 Milestone															
Construction UMS Station P-1253					▼ Construction UMS Station P-1253															
Construction CTS Station P-1254R					▼ Construction CTS Station P-1254R															
Construction YBM Station P-1255					▼ Construction YBM Station P-1255															
Construction STS P-1256					▼ Construction STS P-1256															
Project Start Up					▼ Project Start Up															
Unallocated Contingency					▼ Unallocated Contingency															
CO1.700	Cost Activity Unallocated Contingency (LOE) - 1.7.500.99.090.00 - Contingency	115	02/19/19	07/31/19	▼ Cost Activity Unallocated Contingency (

Activity ID	Activity Name	Original Duration	Start	Finish	Total Float	2016		2017				2018				2019				2020				2021	
						Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2
CENTRAL SUBWAY PROJECT																									
Program Level Milestones						115	02/19/19	08/01/19																	
HidPt5	Systems Complete	0		02/19/19	-150																				
MS0019	Baseline Finish Date: 12-26-2018	0		08/01/19*	-150																				
MS0009	CSP Revenue Service Date	0		08/01/19*	-150																				
Construction Phase						781	06/22/16 A	08/01/19																	
Construction CN-1300						728	06/22/16 A	05/19/19																	
CN- 1300 Milestone						64	02/18/19	05/19/19																	
STS1500	CN 1300 Substantial Completion	0		02/18/19	-222																				
MS-10	Substantial Completion - 1,700 Calendar Days (SP-4.B) { 10-Feb-18 }	0		02/18/19*	-374																				
BUF1017	STS Buffer Float- (0)	0	02/19/19	02/19/19	-150																				
C.Punch	Closeout Punchlist/Remaining Work	90	02/19/19	05/19/19	-373																				
MS-20	Final Completion - 1,790 Calendar Days (SP-4C)	0		05/19/19*	-373																				
Construction CTS Station P-1254R						664	06/22/16 A	02/18/19																	
CTS.31.71.080	Excavate Step 5 Bench 1 & Construct Headwall - Crosscut Cavern	5	06/22/16 A	09/06/16	-265																				
CTS.31.71.050	Excavate & Construct Sidewall Bench & Headwall Step 3 Left Heading - Crossc	21	08/03/16 A	08/31/16	-265																				
CTS.31.71.070	Excavate & Construct Step 4 Left Heading Invert & Headwall - Crosscut Cavern	14	08/03/16 A	09/06/16	-265																				
CTS.31.71.060	Excavate & Construct Sidewall Bench & Headwall Step 3 Right Heading - Cros	19	08/04/16 A	08/31/16	-265																				
CTS.31.71.075	Excavate & Construct Step 4 Right Heading Invert & Headwall - Crosscut Cavern	12	08/04/16 A	09/01/16	-265																				
CTS.31.71.090	Excavate Step 6 Bench 2 & Construct Headwall - Crosscut Cavern	5	09/07/16	09/13/16	-265																				
CTS.31.71.100	Excavate & Support Step 7 Invert - Crosscut Cavern	10	09/07/16	09/20/16	-265																				
CTS.31.71.390	Breakout Remaining Cross-Cut Cavern Opening	5	09/21/16	09/27/16	-265																				
CTS.31.71.400	Temporary Backfill Cross Cut Invert for Platform Cavern Excavation	5	09/28/16	10/04/16	-265																				
CTS.31.71.440	Barrel Vaults at South Platform Cavern Excavation	10	10/05/16	10/18/16	-265																				
CTS.31.71.450	Breakin Top Benches for South Platform Cavern Excavation	13	10/19/16	11/04/16	-265																				
CTS.31.71.465	Excavate & Support Top Left Heading South Platform Cavern 176Lf	20	11/07/16	12/07/16	-265																				
CTS.31.71.460	Excavate & Support Top Right Heading South Platform Cavern 176Lf	20	11/07/16	12/07/16	-265																				
CTS.31.71.550	Excavate & Support Top Right Bench South Platform Cavern 176Lf	10	12/08/16	12/21/16	-265																				
CTS.31.71.560	Excavate & Support Top Left Bench South Platform Cavern 176Lf	10	12/08/16	12/21/16	-265																				
CTS.31.71.580	Excavate & Support Top Left Step 3 Invert South Platform Cavern 176Lf	14	12/22/16	01/12/17	-265																				
CTS.31.71.570	Excavate & Support Top Right Step 3 Invert South Platform Cavern 176Lf	14	12/22/16	01/12/17	-265																				
CTS.31.71.590	CTS- Install Temporary Bracing - Sidewalls (Platform Cavern)	6	01/13/17	01/20/17	-265																				
CTS.31.71.600	Excavate & Support Top Center Drift Step 4 South Platform Cavern 176Lf	18	01/23/17	02/15/17	-265																				
CTS.31.71.610	Excavate & Support Center Bench Step 5 South Platform Cavern 176Lf	11	02/16/17	03/02/17	-265																				
CTS.31.71.620	Excavate & Construct Invert Step 6 South Platform Cavern 176Lf	10	03/03/17	03/16/17	-265																				
CTS.31.71.630	Demo Sidewalls & Repair Headwall South Platform Cavern 176Lf	10	03/17/17	03/30/17	-265																				
CTS.31.71.640	Stage Equipment & Construct Ramp For Crossover Breakin	5	03/31/17	04/06/17	-265																				
CTS.31.71.650	Break-in Crossover Cavern	1	04/07/17	04/07/17	-265																				
CTS.31.71.660	Excavate & Construct Left Sidewall & Headwall 268 Lf	55	04/10/17	06/26/17	-265																				
CTS.31.71.670	Excavate & Construct Right Sidewall & Headwall 268 Lf	55	04/10/17	06/26/17	-265																				
CTS.31.71.680	Install Temporary Support Struts	10	06/27/17	07/11/17	-265																				
CTS.31.71.690	Install Ramp For Center Drift	2	07/12/17	07/13/17	-265																				
CTS.31.71.700	Excavate & Support Center Drift	35	07/14/17	08/31/17	-265																				
CTS.31.71.710	Remove Crossover Excavation Ramp	2	09/01/17	09/05/17	-265																				
CTS.31.71.720	Excavate & Support Center Bench - Crossover	10	09/06/17	09/19/17	-265																				
CTS.31.71.730	Excavate & Construct Invert - Crossover	7	09/20/17	09/28/17	-265																				
CTS.31.71.740	Demo Sidewalls, Repair Headwall & Top Joint - Crossover	10	09/29/17	10/12/17	-265																				
CTS.31.71.750	Repair Invert Joint - Crossover	10	10/13/17	10/26/17	-265																				

Activity ID	Activity Name	Original Duration	Start	Finish	Total Float	2016		2017				2018				2019				2020				2021	
						Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2
CTS.31.74.700	Place Smoothing Concrete - Final Lining invert - Crossover Cavern	15	10/27/17	11/14/17	-319																				
CTS.31.74.705	Install Waterproofing - Final Lining invert - Crossover Cavern	15	11/15/17	12/04/17	-319																				
CTS.31.74.715	Install Rebar & Grout Piping - Final Lining invert - Crossover Cavern	20	12/05/17	12/28/17	-319																				
CTS.31.74.710	Place Smoothing Concrete - Final Lining Invert -Cross Cut Cavern	6	12/29/17	01/05/18	-319																				
CTS.31.74.725	Place Concrete - Final Lining invert - Crossover Cavern	20	12/29/17	01/22/18	-319																				
CTS.31.74.960	Install Waterproofing & Grout Pipes - Final Lining Invert -Cross Cut Cavern	5	01/06/18	01/11/18	-319																				
CTS.31.74.970	Install Rebar - Final Lining Invert -Cross Cut Cavern	10	01/12/18	01/23/18	-319																				
CTS.31.74.980	Place Concrete - Final Lining Invert -Cross Cut Cavern	10	01/24/18	02/03/18	-319																				
CTS.31.74.990	Install Waterproofing - Final Lining Arches - Crosscut Cavern	15	02/05/18	02/21/18	-319																				
CTS.31.74.10	Install Rebar & Grout Piping - Final Lining Arches - Crosscut Cavern	15	02/10/18	02/27/18	-319																				
CTS.31.74.350	Shotcrete Final Lining Arches - Crosscut Cavern	20	02/21/18	03/15/18	-319																				
CTS.03.30.760	Shore/Rebar/Form Pour Track Slab - Crosscut Cavern	15	03/16/18	04/05/18	-266																				
CTS.03.30.780	Rebar/Form/ Pour Platforms - Crosscut Cavern	20	03/30/18	04/26/18	-266																				
CTS.03.30.770	Shore/Rebar/Form Pour Concourse Level Slab - Crosscut Cavern	15	04/13/18	05/03/18	-266																				
CTS.09.83.664	CTS_PL 05 Spray - Set Up Scaffold- Platform Level	5	05/04/18	05/10/18	-266																				
CTS.09.83.544	CTS_PL 05 Spray - Acoustical Vermiculite Wall Plaster NB - Sector 2	6	05/11/18	05/18/18	-266																				
CTS.09.83.554	CTS_PL 05 Spray - Acoustical Vermiculite Wall Plaster SB - Sector 2	6	05/21/18	05/28/18	-266																				
CTS.34.21.122	CTS_PL Station Platform: Install - Traction Power Box PS01 @ SB Track - Sec	3	05/29/18	05/31/18	-266																				
CTS.34.05.160	CTS_PL Install Conduit SB Positive Feeder Box PS01 to PS05 (Traction Power)	5	06/01/18	06/07/18	-266																				
CTS.34.05.170	CTS_PL Install Conduit SB Positive Feeder Box PS01 to PS07 (Traction Power)	5	06/08/18	06/14/18	-266																				
CTS.34.05.180	CTS_PL Install:Conduit SB Positive Feeder Box PS02 to PS08 (Traction Power)	5	06/15/18	06/21/18	-266																				
CTS.03.46.980	CTS_PL Station Platform: Install - GFRC Perforated Wall Panel System SB - S	15	06/22/18	07/12/18	-266																				
CTS.03.46.164	CTS_PL Station Platform: Install - GFRC Perforated Ceiling Panels - Sector 2	15	07/13/18	08/02/18	-266																				
CTS.14.31.265	Set Escalator Trusses 1 & 2 (Concourse to Platform) - Crosscut	2	08/03/18	08/06/18	-266																				
CTS.14.31.275	CTS_PL Assemble Components Escalator #1	15	08/07/18	08/27/18	-266																				
CTS.14.31.415	CTS_PL Assemble Components Escalator #2	15	08/28/18	09/17/18	-266																				
CTS.09.66.616	CTS_PL Station Platform Cross-Cut Cavern: Install - Terrazzo Flooring Sector 2	5	09/18/18	09/24/18	-266																				
CTS.09.66.618	CTS_PL Station Platform Cross-Cut Cavern: Install - Terrazzo Cove Base Sect	5	09/25/18	10/01/18	-266																				
CTS.09.66.620	CTS_PL Station Platform Cross-Cut Cavern: Grind & Polish - Terrazzo Floorin	10	10/02/18	10/15/18	-266																				
CTS.14.24.255	CTS-PL 05: Assemble Elevator #1	15	10/16/18	11/05/18	-266																				
CTS.14.24.265	CTS-PL 05: Assemble Elevator #2	15	11/06/18	11/26/18	-266																				
CTS.14.24.275	CTS-PL 05: Install Elevator Power & Controls	10	11/27/18	12/10/18	-266																				
CTS.08.44.265	CTS_PL Install Elevators 1 & 2 Glass Enclosure - Crosscut Platform Level	10	11/27/18	12/10/18	-266																				
CTS.08.44.580	Install Elevators 1 & 2 Glass Enclosure - Crosscut Concourse Level	10	11/27/18	12/10/18	-266																				
CTS.14.24.285	CTS-PL 05: Startup & Test Elevators 1&2	5	12/11/18	12/17/18	-266																				
CTS.14.24.295	CTS-PL 05:Inspections - Elevators 1&2	1	12/18/18	12/18/18	-266																				
CTS.01.80.00	CTS- Building Systems Start-up & Testing	44	12/19/18	02/18/19	-266																				
Project Start Up		163	02/19/19	08/01/19	-217																				
STU1010	S&S Certification / Pre-Revenue Activities	115	02/19/19	07/31/19	-150																				
BUF0018	Muni Float	0	08/01/19	08/01/19	-150																				

█ S&S Certification / Pre-Revenue Activities
 █ Muni Float

Activity Name	Original Duration	Start	Finish	2012				2013				2014				2015				2016				2017				2018				2019			
				Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4
CENTRAL SUBWAY PROJECT	2234	06/08/11 A	05/19/19																																
Construction Phase	2234	06/08/11 A	05/19/19																																
Construction Tunnels CN-1252	1799	06/08/11 A	05/15/15 A																																
1252 Tunnel Contract BIH	1799	06/08/11 A	05/15/15 A																																
Contract Milestones	1437	06/08/11 A	05/15/15 A																																
General Conditions	1752	08/01/11 A	05/15/15 A																																
4th & Bryant St TBM Launch Box Construction	686	03/30/12 A	06/02/14 A																																
Moscone Station Headwalls	430	05/14/12 A	09/20/13 A																																
UMS Station Headwalls	425	07/24/12 A	11/22/13 A																																
UMS - Remove Geary to Ellis OCS	5	07/24/12 A	07/26/12 A																																
UMS - Setup Traffic Control for Headwall Construction	1	07/30/12 A	07/30/12 A																																
North Headwall	237	02/27/13 A	11/22/13 A																																
South Headwall	404	07/31/12 A	11/22/13 A																																
4th St and Market Compensation Grouting	707	01/28/13 A	04/30/15 A																																
Southbound Tunneling	451	04/27/13 A	10/13/14 A																																
Ellis St Compensation Grouting	561	07/31/12 A	05/09/14 A																																
Green St Compensation Grouting	320	08/05/13 A	06/30/14 A																																
Retrieval Shaft	1070	10/31/11 A	03/20/15 A																																
Cross Passage 1-5	339	03/22/14 A	04/16/15 A																																
Cross Passage 1	79	06/14/14 A	09/13/14 A																																
Cross Passage 2	105	05/10/14 A	09/09/14 A																																
Cross Passage 3	127	03/31/14 A	08/28/14 A																																
Cross Passage 4	114	03/22/14 A	07/31/14 A																																
Cross Passage 5	277	05/31/14 A	04/16/15 A																																
Portal Structure	196	09/02/14 A	04/15/15 A																																
Contract Close Out	307	03/03/14 A	05/15/15 A																																
Construction CN-1300	1706	06/03/13 A	05/19/19																																
CN- 1300 Milestone	1545	06/17/13 A	05/19/19																																
Construction UMS Station P-1253	1706	06/17/13 A	02/19/19																																
Construction CTS Station P-1254R	1525	06/17/13 A	02/19/19																																
Construction YBM Station P-1255	1642	06/10/13 A	02/19/19																																
Construction STS P-1256	1532	06/03/13 A	04/30/19																																

Activity ID	Activity Name	Original Duration	Start	Finish	Total Float	2016		2017				2018				2019				2020				2021						
						Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3				
CENTRAL SUBWAY PROJECT																														
Light Rail Vehicles																														
Real Estate																														
Construction Phase																														
Construction Support and Costs																														
Construction Tunnels CN-1252																														
Construction CN-1300																														
CN- 1300 Milestone																														
No 13-Disp																														
Construction UMS Station P-1253																														
Preconstruction																														
Engineering & Procurement																														
Site Work / Utility Relocation																														
Compensation Grouting																														
Demolition																														
Roof Deck Excavation,Construction,Restoration																														
Excavation & Support																														
UMS.05.12.0900	UMS_Install Permanent Struts & Wales - Concourse Level	15	04/14/16 A	09/26/16	-52																									
UMS.03.37.0665	UMS_Install Drain Pipe & Grout Fill Void Between Piles - Roof To Concourse Level Sta 132+50 to North Headwall	10	04/25/16 A	10/03/16	-233																									
UMS.05.12.0690	UMS_Install Permanent Wales - Concourse Level Sta 132+50 to North Headwall	10	05/16/16 A	09/12/16	-259																									
UMS.31.50.0260	UMS_Install Temporary Lid @ Access Shaft #2 (O'Farrell)	10	06/17/16 A	09/09/16	-254																									
UMS.31.23.730	UMS_Dewatering Maintainance	213	06/20/16 A	05/23/17	-129																									
UMS.03.37.0700	UMS_Install Drain Pipe & Grout Fill Void Between Piles - Roof To Concourse Level Sta 132+50 to South Headwall	10	08/08/16 A	10/03/16	-259																									
UMS.31.20.1140	UMS_Excavation For South Concourse Escalator	2	08/22/16 A	08/30/16	26																									
UMS.31.20.1150	UMS_Install Temporary Struts & Wales For South Concourse Escalator	3	08/24/16 A	09/02/16	26																									
UMS.31.20.0800	UMS_Excavate Bench to Intermediate Strut Level & Expose Wide Flange Sta 132+50 to North Headwall	1	08/25/16 A	09/06/16	-258																									
UMS.31.43.150	UMS_Compensation Grouting - As Required	10	09/02/16	09/19/16	17																									
UMS.05.12.0705	UMS_Install & Pre-Load Permanent Struts - Concourse Level Sta 132+50 to North Headwall - 13ea	10	09/02/16	09/19/16	-259																									
UMS.05.12.0715	UMS_Install & Pre-Load Permanent Struts - Concourse Level Sta 132+50 to South Headwall - 13ea	10	09/02/16	09/19/16	-260																									
UMS.03.30.1525	UMS_Form/Rebar/Pour Invert Slab For South Concourse Escalator Upper Landing	10	09/06/16	09/19/16	26																									
UMS.03.37.0980	UMS_Install Studs, Mesh, Drain Pipe & Shotcrete Pile Walls Concourse to Intermediate Level Sta 132+50 to North Headwall	10	09/07/16	09/21/16	-258																									
UMS.31.20.0805	UMS_Excavate Bench to Intermediate Strut Level & Expose Wide Flange Sta 132+50 to South Headwall	9	09/12/16	09/23/16	-260																									
UMS.05.12.0830	UMS_Install & Pre-Load Permanent Struts - Intermediate Strut Level Sta 132+50 To North Headwall	10	09/14/16	09/28/16	-258																									
UMS.05.12.0820	UMS_Install Permanent Wales - Intermediate Strut Level Sta 132+50 To North Headwall	10	09/14/16	09/28/16	-258																									
UMS.31.20.1345	UMS_Remove Temporary Struts & Wales For South Concourse Escalator	3	09/20/16	09/22/16	26																									
UMS.03.30.1535	UMS_Form/Rebar/Pour Walls For South Concourse Escalator Upper Landing	10	09/23/16	10/06/16	26																									
UMS.31.20.0830	UMS_Excavate Bench to Temporary Strut Level 1 & Expose Wide Flange Sta 132+50 To North Headwall	7	09/23/16	10/04/16	-260																									
UMS.05.12.0825	UMS_Install Permanent Wales - Intermediate Strut Level Sta 132+50 To South Headwall	10	09/28/16	10/12/16	-258																									
UMS.03.37.0986	UMS_Install Studs, Mesh, Drain Pipe & Shotcrete Pile Walls Interm Level To Temp Strut Lvl 1 Sta 132+50 to North Headwall	10	10/04/16	10/18/16	-260																									
UMS.05.12.0835	UMS_Install & Pre-Load Permanent Struts - Intermediate Strut Level Sta 132+50 To South Headwall	10	10/05/16	10/19/16	-258																									
UMS.31.20.0835	UMS_Excavate Bench to Temporary Strut Level 1 & Expose Wide Flange Sta 132+50 To South Headwall	7	10/12/16	10/21/16	-258																									
UMS.31.50.0850	UMS_Install & Pre-Load Temporary Struts - Level 1 Sta 132+50 to North Headwall	34	10/18/16	12/08/16	-260																									
UMS.03.37.0985	UMS_Install Studs, Mesh, Drain Pipe & Shotcrete Pile Walls Concourse to Intermediate Level Sta 132+50 to South Headwall	10	10/19/16	11/02/16	-237																									
UMS.31.20.0860	UMS_Excavate Bench to Mezzanine Level & Expose Wide Flange Sta 132+50 to North Headwall	7	10/25/16	11/03/16	-260																									
UMS.05.12.0880	UMS_Install Permanent Wales - Mezzanine Level Sta 132+50 To North Headwall	10	11/01/16	11/16/16	-260																									
UMS.03.37.0987	UMS_Install Studs, Mesh, Drain Pipe & Shotcrete Pile Walls Interm Level To Temp Strut Lvl 1 Sta 132+50 to South Headwall	10	11/02/16	11/17/16	-237																									
UMS.05.12.0890	UMS_Install & Pre-load Permanent Struts - Mezzanine Level Sta 132+50 To North Headwall	34	11/16/16	01/09/17	-260																									
UMS.31.20.0865	UMS_Excavate Bench to Mezzanine Level & Expose Wide Flange Sta 132+50 to South Headwall	7	11/17/16	11/30/16	-237																									
UMS.31.50.0855	UMS_Install & Pre-Load Temporary Struts - Level 1 Sta 132+50 to South Headwall	11	11/17/16	12/06/16	-237																									

Activity ID	Activity Name	Original Duration	Start	Finish	Total Float	2016		2017				2018				2019				2020			2021					
						Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3		
UMS.05.12.0885	UMS_Install Permanent Wales - Mezzanine Level Sta 132+50 To South Headwall	11	11/28/16	12/13/16	-237																							
UMS.05.12.0895	UMS_Install & Pre-Load Permanent Struts - Mezzanine Level Sta 132+50 To South Headwall	11	12/05/16	12/20/16	-237																							
UMS.03.37.0877	UMS_Install Studs, Mesh Drain Pipe & Shotcrete Pile Walls Temp Strut Level 1 to Mezzanine Sta	10	12/20/16	01/05/17	-237																							
UMS.31.20.0970	UMS_Excavate Bench to Temporary Strut Level 2 & Expose Wide Flange Sta 132+50 To North H	6	01/09/17	01/17/17	-253																							
UMS.03.37.0870	UMS_Install Studs, Mesh Drain Pipe & Shotcrete Pile Walls Temp Strut Level 1 to Mezzanine Sta	13	01/09/17	01/26/17	-260																							
UMS.31.20.0975	UMS_Excavate Bench to Temporary Strut Level 2 & Expose Wide Flange Sta 132+50 To South H	8	01/17/17	01/27/17	-253																							
UMS.03.37.0875	UMS_Install Studs, Mesh Drain Pipe & Shotcrete Pile Walls Mezzanine Level to Temp Strut Leve	10	01/26/17	02/09/17	-260																							
UMS.03.37.0880	UMS_Install Studs, Mesh Drain Pipe & Shotcrete Pile Walls Mezzanine Level to Temp Strut Level	10	01/27/17	02/10/17	-253																							
UMS.31.50.0990	UMS_Install & Preload Temporary Struts - Level 2 Sta 132+50 To North Headwall	10	02/09/17	02/23/17	-260																							
UMS.31.50.0995	UMS_Install & Preload Temporary Struts - Level 2 Sta 132+50 To South Headwall	10	02/10/17	02/24/17	-253																							
UMS.03.37.0995	UMS_Install Studs, Mesh Drain Pipe & Shotcrete Pile Walls Temp Strut Level 2 To Platfrm Strut Le	10	02/10/17	02/24/17	-205																							
UMS.31.20.0980	UMS_Excavate Bench to Platform Strut Level & Expose Wide Flange Sta 132+50 To North Head	8	02/23/17	03/07/17	-260																							
UMS.05.12.1200	UMS_Install Permanent Wales Platform Strut Level Sta 132+50 To North Headwall	10	03/02/17	03/16/17	-260																							
UMS.31.20.0985	UMS_Excavate Bench to Platform Strut Level & Expose Wide Flange Sta 132+50 To South Head	8	03/07/17	03/17/17	-260																							
UMS.05.12.1220	UMS_Install & Preload Permanent Struts Platform Strut Level Sta 132+50 To North Headwall	10	03/09/17	03/23/17	-260																							
UMS.31.41.0205	UMS_Install Sheet Piling at Garage Base Slab	10	03/10/17	03/23/17	-175																							
UMS.05.12.1210	UMS_Install Permanent Wales Platform Strut Level Sta 132+50 To South Headwall	10	03/14/17	03/28/17	-260																							
UMS.05.12.1230	UMS_Install & Preload Permanent Struts Platform Strut Level Sta 132+50 To South Headwall	10	03/16/17	03/30/17	-260																							
UMS.03.37.0990	UMS_Install Studs, Mesh Drain Pipe & Shotcrete Pile Walls Temp Strut Level 2 To Platfrm Strut Le	10	03/23/17	04/06/17	-251																							
UMS.31.20.1030	UMS_Excavate/Demo Liner to Temporary Strut Level 3 & Expose Wide Flange Sta 132+50 To No	10	03/23/17	04/06/17	-260																							
UMS.31.20.1120	UMS_Excavate to Bottom of North Concourse Slab (Intermediate Strut Level)	5	03/24/17	03/30/17	-112																							
UMS.31.50.1050	UMS_Install Temporary Struts - Level 3 Sta 132+50 To North Headwall	10	03/30/17	04/13/17	-255																							
UMS.31.20.1040	UMS_Excavate/Demo Liner to Temporary Strut Level 3 & Expose Wide Flange Sta 132+50 To So	10	03/30/17	04/13/17	-260																							
UMS.31.20.1355	UMS_Excavate HVAC Chase Beneath North Concourse Slab (Intermediate Strut Level)	5	03/31/17	04/06/17	-112																							
UMS.03.37.1020	UMS_Install Studs, Mesh Drain Pipe & Shotcrete Pile Walls Platfrm Strut Level To Temp Strut Leve	10	04/06/17	04/20/17	-251																							
UMS.05.12.2250	UMS_Install W-Section Piles Reinforcement - (Intermediate Strut Level)	10	04/07/17	04/20/17	-99																							
UMS.31.20.1060	UMS_Excavate to Bottom of Invert Slab & Expose Wide Flange Sta 132+50 To North Headwall	3	04/13/17	04/18/17	-260																							
UMS.31.50.1220	UMS_Install Temporary Struts - Level 3 Sta 132+50 To South Headwall	10	04/13/17	04/27/17	-255																							
UMS.03.37.1030	UMS_Install Studs, Mesh Drain Pipe & Shotcrete Pile Walls Platfrm Strut Level To Temp Strut Leve	10	04/13/17	04/27/17	-239																							
UMS.05.12.2130	UMS_Attach 60 ea Platform Level Slab Stub Beam Assemblies To West Side Piles - Sta 132+50	15	04/18/17	05/09/17	-260																							
UMS.05.12.1360	UMS_Attach 60 ea Platform Level Slab Stub Beam Assemblies To East Side Piles - Sta 132+50 To	15	04/18/17	05/09/17	-260																							
UMS.03.37.1040	UMS_Install Studs, Mesh Drain Pipe & Shotcrete Pile Walls Temp Strut Level 3 To Invert Sta 132+	10	04/25/17	05/09/17	-254																							
UMS.31.20.1130	UMS_Excavate to Bottom of Invert Slab & Expose Wide Flange Sta 132+50 To South Headwall	3	04/27/17	05/02/17	-255																							
UMS.05.12.1370	UMS_Attach 40 ea Platform Level Slab Stub Beam Assemblies To West Side Piles - Sta 132+50 To	10	05/09/17	05/23/17	-260																							
UMS.05.12.1365	UMS_Attach 40 ea Platform Level Slab Stub Beam Assemblies To East Side Piles - Sta 132+50 To	10	05/09/17	05/23/17	-260																							
UMS.03.37.1050	UMS_Install Studs, Mesh Drain Pipe & Shotcrete Pile Walls Temp Strut Level 3 To Invert Sta 132+	10	05/16/17	05/31/17	-252																							
UMS.31.20.0260	UMS_Excavate to Intermediate Level In Garage	5	07/06/17	07/12/17	-174																							
Concrete/Shotcrete		547	01/27/16 A	05/25/18	-85																							
Structural Steel		323	01/27/17	05/03/18	-215																							
Masonry		373	01/10/17	06/25/18	-182																							
Mechanical		454	01/10/17	10/16/18	-197																							
Electrical		494	03/16/17	02/05/19	-257																							
Electrical - Transportation		343	05/19/17	09/21/18	-191																							
Architectural Finishes		499	12/23/16	11/21/18	-218																							
Conveyances		295	08/17/17	10/10/18	-173																							
Stairs		514	10/17/16	10/22/18	-196																							
Startup & Testing		61	10/29/18	01/22/19	-257																							
No 13-Disp		1577	01/13/14 A	02/18/19	64																							
Construction CTS Station P-1254R		1392	07/10/14 A	02/19/19	63																							
Preconstruction		15	01/16/18	02/05/18	4																							

Activity ID	Activity Name	Original Duration	Start	Finish	Total Float	2016		2017				2018				2019				2020				2021					
						Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3			
Site Work / Utility Relocation						584	08/26/16	11/21/18	127																				
Excavation & Support						597	03/10/16 A	07/02/18	-101																				
CTS.31.50.900	CTS_CN Install Temp Level 4.0 Struts & Wales & Preload Col. 4.0-11.0	8	03/10/16 A	09/07/16	4	CTS_CN Install Temp Level 4.0 Struts & Wales & Preload Col. 4.0-11.0																							
CTS.31.43.140	CTS_ Compensation Grouting - As Required	10	06/12/16 A	09/21/16	350	CTS_ Compensation Grouting - As Required																							
CTS.31.20.320	Excavate to 4' Below Level 6 Struts EL 7.5' Col 4.0-11.0	9	07/21/17 A	10/05/17	-264	Excavate to 4' Below Level 6 Struts EL 7.5' Col 4.0-11.0																							
CTS.31.50.330	Install Temp Level 6 Struts & Wales & Preload	10	10/04/17	10/17/17	-264	Install Temp Level 6 Struts & Wales & Preload																							
CTS.31.20.335	Excavate to 3' Below Level 7 Struts EL -7.5 Col 4.0-11.0	15	10/06/17	10/26/17	-264	Excavate to 3' Below Level 7 Struts EL -7.5 Col 4.0-11.0																							
CTS.31.50.370	Install Temp Level 7 Struts & Wales & Preload	10	10/16/17	10/27/17	-264	Install Temp Level 7 Struts & Wales & Preload																							
CTS.31.20.380	Excavate Headhouse to Invert Slab EL -18.67	12	10/30/17	11/15/17	-264	Excavate Headhouse to Invert Slab EL -18.67																							
CTS.31.50.070	Remove Level 6 & 7 Struts & Wales	5	02/07/18	02/13/18	-259	Remove Level 6 & 7 Struts & Wales																							
CTS.31.50.110	CTS_CN Remove Level 5 Struts & Wales	5	02/07/18	02/13/18	-264	CTS_CN Remove Level 5 Struts & Wales																							
CTS.31.50.150	CTS_CN Remove Level 4 Struts & Wales	5	03/28/18	04/03/18	-221	CTS_CN Remove Level 4 Struts & Wales																							
CTS.31.50.195	Remove Level 3 Struts & Wales	5	05/24/18	05/30/18	-257	Remove Level 3 Struts & Wales																							
CTS.31.50.235	Remove Level 2 Struts & Wales	5	05/31/18	06/06/18	-247	Remove Level 2 Struts & Wales																							
CTS.31.50.240	Remove Level 1 Struts & Wales	5	06/26/18	07/02/18	-258	Remove Level 1 Struts & Wales																							
Tunnel / Cavern Mining						296	06/22/16 A	10/12/17	-133																				
CTS.31.71.080	Excavate Step 5 Bench 1 & Construct Headwall - Crosscut Cavern	5	06/22/16 A	09/06/16	-265	Excavate Step 5 Bench 1 & Construct Headwall - Crosscut Cavern																							
CTS.31.71.070	Excavate & Construct Step 4 Left Heading Invert & Headwall - Crosscut Cavern	14	08/03/16 A	09/06/16	-265	Excavate & Construct Step 4 Left Heading Invert & Headwall - Crosscut Cavern																							
CTS.31.71.075	Excavate & Construct Step 4 Right Heading Invert & Headwall - Crosscut Cavern	12	08/04/16 A	09/01/16	-265	Excavate & Construct Step 4 Right Heading Invert & Headwall - Crosscut Cavern																							
CTS.31.71.500	Excavate & Support North Emergency Egress Tunnel	15	08/08/16 A	01/23/17	40	Excavate & Support North Emergency Egress Tunnel																							
CTS.31.71.090	Excavate Step 6 Bench 2 & Construct Headwall - Crosscut Cavern	5	09/07/16	09/13/16	-265	Excavate Step 6 Bench 2 & Construct Headwall - Crosscut Cavern																							
CTS.31.71.100	Excavate & Support Step 7 Invert - Crosscut Cavern	10	09/07/16	09/20/16	-265	Excavate & Support Step 7 Invert - Crosscut Cavern																							
CTS.31.71.390	Breakout Remaining Cross-Cut Cavern Opening	5	09/21/16	09/27/16	-265	Breakout Remaining Cross-Cut Cavern Opening																							
CTS.31.71.400	Temporary Backfill Cross Cut Invert for Platform Cavern Excavation	5	09/28/16	10/04/16	-265	Temporary Backfill Cross Cut Invert for Platform Cavern Excavation																							
CTS.31.71.440	Barrel Vaults at South Platform Cavern Excavation	10	10/05/16	10/18/16	-265	Barrel Vaults at South Platform Cavern Excavation																							
CTS.31.71.410	Barrel Vaults for North Platform Cavern Excavation	10	10/19/16	11/01/16	-64	Barrel Vaults for North Platform Cavern Excavation																							
CTS.31.71.450	Breakin Top Benches for South Platform Cavern Excavation	13	10/19/16	11/04/16	-265	Breakin Top Benches for South Platform Cavern Excavation																							
CTS.31.71.465	Excavate & Support Top Left Heading South Platform Cavern 176Lf	20	11/07/16	12/07/16	-265	Excavate & Support Top Left Heading South Platform Cavern 176Lf																							
CTS.31.71.460	Excavate & Support Top Right Heading South Platform Cavern 176Lf	20	11/07/16	12/07/16	-265	Excavate & Support Top Right Heading South Platform Cavern 176Lf																							
CTS.31.71.550	Excavate & Support Top Right Bench South Platform Cavern 176Lf	10	12/08/16	12/21/16	-265	Excavate & Support Top Right Bench South Platform Cavern 176Lf																							
CTS.31.71.560	Excavate & Support Top Left Bench South Platform Cavern 176Lf	10	12/08/16	12/21/16	-265	Excavate & Support Top Left Bench South Platform Cavern 176Lf																							
CTS.31.71.420	Breakin Top / Bench Sidewalls for North Platform Cavern Excavation	12	12/08/16	12/23/16	-87	Breakin Top / Bench Sidewalls for North Platform Cavern Excavation																							
CTS.31.71.580	Excavate & Support Top Left Step 3 Invert South Platform Cavern 176Lf	14	12/22/16	01/12/17	-265	Excavate & Support Top Left Step 3 Invert South Platform Cavern 176Lf																							
CTS.31.71.570	Excavate & Support Top Right Step 3 Invert South Platform Cavern 176Lf	14	12/22/16	01/12/17	-265	Excavate & Support Top Right Step 3 Invert South Platform Cavern 176Lf																							
CTS.31.71.425	Complete Top / Bench Invert Sidewalls & Headwall Left Side for North Platform Cavern Excavation	33	12/27/16	02/10/17	-87	Complete Top / Bench Invert Sidewalls & Headwall Left Side for North Platform Cavern Excavation																							
CTS.31.71.435	Complete Top / Bench Invert Sidewalls & Headwall Right Side for North Platform Cavern Excavation	33	12/27/16	02/10/17	-87	Complete Top / Bench Invert Sidewalls & Headwall Right Side for North Platform Cavern Excavation																							
CTS.31.71.800	TB-4 SEM Additional Flashcrete	30	01/20/17	03/02/17	-57	TB-4 SEM Additional Flashcrete																							
CTS.31.71.810	TB-5 SEM Additional Shotcrete	30	01/20/17	03/02/17	-57	TB-5 SEM Additional Shotcrete																							
CTS.31.71.820	TB-6 SEM Additional Lattice Girders	30	01/20/17	03/02/17	-57	TB-6 SEM Additional Lattice Girders																							
CTS.31.71.830	TB-7 SEM Additional Steel Arches	30	01/20/17	03/02/17	-57	TB-7 SEM Additional Steel Arches																							
CTS.31.71.840	TB-8 SEM Additional Face Bolts	30	01/20/17	03/02/17	-57	TB-8 SEM Additional Face Bolts																							
CTS.31.71.850	TB-9 SEM Additional Metal Sheets	30	01/20/17	03/02/17	-57	TB-9 SEM Additional Metal Sheets																							
CTS.31.71.860	TB-10 SEM Additional Probe Holes	30	01/20/17	03/02/17	-57	TB-10 SEM Additional Probe Holes																							
CTS.31.71.870	TB-11 SEM Additional Grout Holes	30	01/20/17	03/02/17	-57	TB-11 SEM Additional Grout Holes																							
CTS.31.71.880	TB-12 SEM Additional Permeation Grouting	30	01/20/17	03/02/17	-57	TB-12 SEM Additional Permeation Grouting																							
CTS.31.71.890	TB-13 SEM Additional Pocket Excavation	30	01/20/17	03/02/17	-57	TB-13 SEM Additional Pocket Excavation																							
CTS.31.71.900	TB-14 SEM Additional Drilled Gravity Dewatering Pipes/ Gravity Well Points	30	01/20/17	03/02/17	-57	TB-14 SEM Additional Drilled Gravity Dewatering Pipes/ Gravity Well Points																							
CTS.31.71.910	TB-15 SEM Additional Vacuum Well Points	30	01/20/17	03/02/17	-57	TB-15 SEM Additional Vacuum Well Points																							
CTS.31.71.780	TB-2 SEM Additional Grouted Pipe Spiles	30	01/20/17	03/02/17	-57	TB-2 SEM Additional Grouted Pipe Spiles																							
CTS.31.71.790	TB-3 SEM Additional Barrel Vault Pipes	30	01/20/17	03/02/17	-57	TB-3 SEM Additional Barrel Vault Pipes																							

Activity ID	Activity Name	Original Duration	Start	Finish	Total Float	2016		2017				2018				2019				2020				2021		
						Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3
CTS.31.71.770	TB-1 SEM Additional Rebar Spiles	30	01/20/17	03/02/17	-57																					
CTS.31.71.600	Excavate & Support Top Center Drift Step 4 South Platform Cavern 176Lf	18	01/23/17	02/15/17	-265																					
CTS.31.71.520	Initial Excavation & Support - South Emergency Egress Tunnel	10	01/24/17	02/06/17	40																					
CTS.31.71.530	Complete Excavation & Support - South Emergency Egress Tunnel	5	02/07/17	02/13/17	40																					
CTS.31.71.610	Excavate & Support Center Bench Step 5 South Platform Cavern 176Lf	11	02/16/17	03/02/17	-265																					
CTS.31.71.455	Excavation / Support Top Center Drift & Construct Headwall for North Platform Cavern Excavation	13	02/17/17	03/07/17	-87																					
CTS.31.71.620	Excavate & Construct Invert Step 6 South Platform Cavern 176Lf	10	03/03/17	03/16/17	-265																					
CTS.31.71.475	Excavation / Support Center Bench Invert & Construct Headwall for North Platform Cavern Excavation	9	03/17/17	03/29/17	-94																					
CTS.31.71.630	Demo Sidewalls & Repair Headwall South Platform Cavern 176Lf	10	03/17/17	03/30/17	-265																					
CTS.31.71.485	Demo Sidewall, Repair Headwall for North Platform Cavern Excavation	10	03/30/17	04/12/17	-94																					
CTS.31.71.640	Stage Equipment & Construct Ramp For Crossover Breakin	5	03/31/17	04/06/17	-265																					
CTS.31.71.650	Break-in Crossover Cavern	1	04/07/17	04/07/17	-265																					
CTS.31.71.660	Excavate & Construct Left Sidewall & Headwall 268 Lf	55	04/10/17	06/26/17	-265																					
CTS.31.71.670	Excavate & Construct Right Sidewall & Headwall 268 Lf	55	04/10/17	06/26/17	-265																					
CTS.31.71.690	Install Ramp For Center Drift	2	07/12/17	07/13/17	-265																					
CTS.31.71.700	Excavate & Support Center Drift	35	07/14/17	08/31/17	-265																					
CTS.31.71.720	Excavate & Support Center Bench - Crossover	10	09/06/17	09/19/17	-265																					
CTS.31.71.730	Excavate & Construct Invert - Crossover	7	09/20/17	09/28/17	-265																					
CTS.31.71.740	Demo Sidewalls, Repair Headwall & Top Joint - Crossover	10	09/29/17	10/12/17	-265																					
Cavern Lining		336	01/24/17	05/08/18	-240																					
Concrete/Shotcrete		439	02/21/17	10/26/18	-230																					
Structural Steel		104	06/04/18	10/25/18	-242																					
Masonry		104	03/21/18	08/13/18	-228																					
Mechanical		258	11/21/17	11/15/18	-244																					
Misc Metal		259	11/21/17	11/21/18	-247																					
Electrical		263	12/05/17	12/06/18	-249																					
Electrical - Transportation		84	05/11/18	09/05/18	-236																					
Architectural Finishes		282	11/06/17	12/10/18	-216																					
Conveyances		98	08/03/18	12/18/18	-267																					
Startup & Testing		44	12/19/18	02/18/19	-266																					
CTS.01.80.00	CTS- Building Systems Start-up & Testing	44	12/19/18	02/18/19	-266																					
No 13-Disp		1392	07/10/14 A	02/19/19	63																					
Construction YBM Station P-1255		1492	01/10/14 A	02/18/19	64																					
Preconstruction		15	01/16/18	02/05/18	4																					
Excavation & Support		536	04/20/16 A	05/30/18	-103																					
Concrete/Shotcrete		579	03/04/16 A	06/06/18	247																					
Mechanical		5	09/27/17	10/03/17	-4																					
Electrical		398	11/15/16	05/24/18	-138																					
Electrical - Transportation		345	11/09/16	03/06/18	-81																					
Conveyances		222	08/29/17	07/11/18	-169																					
Startup & Testing		61	07/23/18	10/15/18	-176																					
No 13-Disp		1492	01/10/14 A	02/18/19	64																					
Construction STS P-1256		1485	02/24/14 A	04/30/19	13																					
Preconstruction		15	01/16/18	02/05/18	4																					
Site Work / Utility Relocation		104	07/27/16 A	01/26/17	-64																					
Concrete/Shotcrete		70	01/27/17	05/04/17	149																					
Tunnel Concrete		371	04/18/16 A	06/28/18	-114																					
Structural Steel		15	06/30/17	07/21/17	-38																					
Electrical		201	12/08/17	09/17/18	-200																					
Electrical - Transportation		21	05/17/18	06/14/18	-204																					

Activity ID	Activity Name	Original Duration	Start	Finish	Total Float	2016		2017				2018				2019				2020				2021										
						Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3								
Trackwork		372	01/25/17	06/28/18	-114																													
Track System Work		668	05/16/16 A	04/30/19	-252																													
Startup & Testing		84	10/16/18	02/08/19	-264																													
STS.34.42.425	Startup & Testing - Tunnel & ATSC Systems	84	10/16/18	02/08/19	-264																													
No 13-Disp		1480	02/24/14 A	02/18/19	64																													
Unallocated Contingency		115	02/19/19	07/31/19	-150																													

Appendix C

PROJECT SCOPE AND FUNDING OVERVIEW

Project Overview

The Central Subway Project will construct a modern, efficient light-rail line that will improve public transit in San Francisco. This new 1.7-mile extension of Muni's T Third Line will provide direct connections to major retail, sporting and cultural venues while efficiently transporting people to jobs, educational opportunities and other amenities throughout the city.

The Central Subway Project is Phase 2 of the San Francisco Municipal Transportation Agency's (SFMTA) Third Street Light Rail Transit Project. Phase 1 of the project constructed a 5.1-mile light-rail line along the densely populated 3rd Street corridor. It began revenue service in April 2007, restoring light-rail service to a high transit-ridership area of San Francisco for the first time in 50 years.

The Central Subway Project will extend the T Third Line from the 4th Street Caltrain Station to Chinatown, providing a direct, rapid transit link from the Bayshore and Mission Bay areas to SoMa, Union Square and downtown.

Four new stations will be built along the 1.7-mile project alignment—an above-ground station at 4th and Brannan streets and three underground stations at Moscone Center, Union Square and Chinatown.

The Central Subway will run through the burgeoning technology and digital-media hub in SoMa, where dozens of companies have taken up residence along the 4th Street corridor. Increased



Project Overview - continued

transit options will attract new employers – the Central Subway makes travel more convenient throughout the corridor and improves connections to downtown, local and regional rail and the Muni bus system.

The Central Subway Project will contribute to San Francisco’s economic competitiveness and help secure the city’s status of a regional, national and global hub. It will provide a pollution-free transit option that will reduce the environmental impact of transportation in the city, save natural resources, reduce traffic congestion and improve public transit for thousands of San Franciscans.

Funding Overview

The Central Subway Project is funded by the federal government, the State of California, the Metropolitan Transportation Commission, the San Francisco County Transportation Authority (SFCTA) and the City and County of San Francisco.

The majority of funding for the Central Subway Project is expected to be provided by the Federal Transit Administration’s (FTA) New Starts program, with a total commitment over the life of the project of \$942.2 million. To date, \$41 million in Department of Transportation Congestion Mitigation and Air Quality Improvement Program funds have been committed and expended.

With the addition in the December 2013 MPR of work to relocate the retrieval site for two tunnel boring machines (TBMs), the SFMTA’s baseline budget for the Central Subway Project is \$1.588 billion. In total, about half of the Third Street Light Rail Transit Project’s funding is from federal sources, with the remaining half from state and local sources. This is in line with the expectations of the FTA for New Starts-financed programs.

The table below summarizes the local, state and federal fund sources for both phases of the T Third Line including with the addition of the retrieval shaft to the Phase 2 totals.

	T Third (Phase 1)	Central Subway (Phase 2 + Retrieval Shaft Relocation)	Total (Phase 1 + Phase 2 + Retrieval Shaft Relocation)	Percentage of Total
Federal	\$123.380	\$983.225	\$1,106.605	49.5%
State	\$160.700	\$471.100	\$631.800	28.2%
Local	\$364.380	\$133.675	\$498.055	22.3%
Total	\$648.460	\$1,588.000	\$2,236.460	100.0%

All amounts in millions of dollars

The six charts that follow summarize use of fund sources by phase and with the addition of the retrieval shaft relocation additional budget and funding:

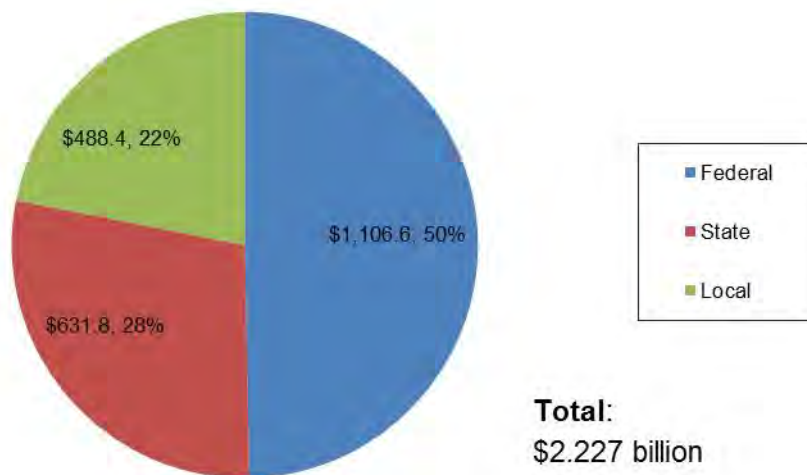
- Phase 1 + Phase 2 of the T Third Line federal, state and local funding percentages previous to the addition of the retrieval shaft relocation budget and funding in December 2013.

Funding Overview - continued

- Phase 2 Central Subway Project only total funding source percentages previous to the addition of the retrieval shaft relocation budget and funding.
- Phase 2 Central Subway Project only detail of the six State and Local funding sources previous to the addition of the retrieval shaft relocation.
- The next three charts that follow are the above three data sets above with the retrieval shaft relocation budget and funding added to the overall presentation.

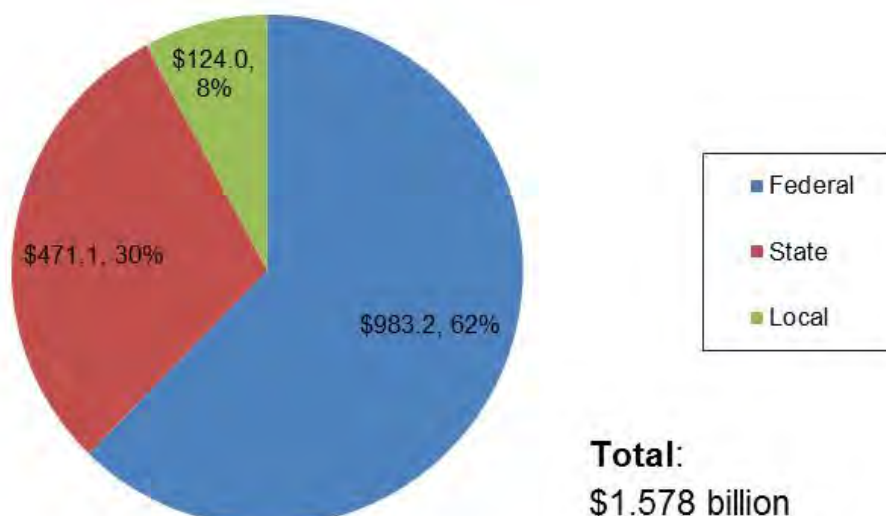
Third Street Light Rail Transit Project Funding

Phase 1 + Phase 2
(\$ in millions)



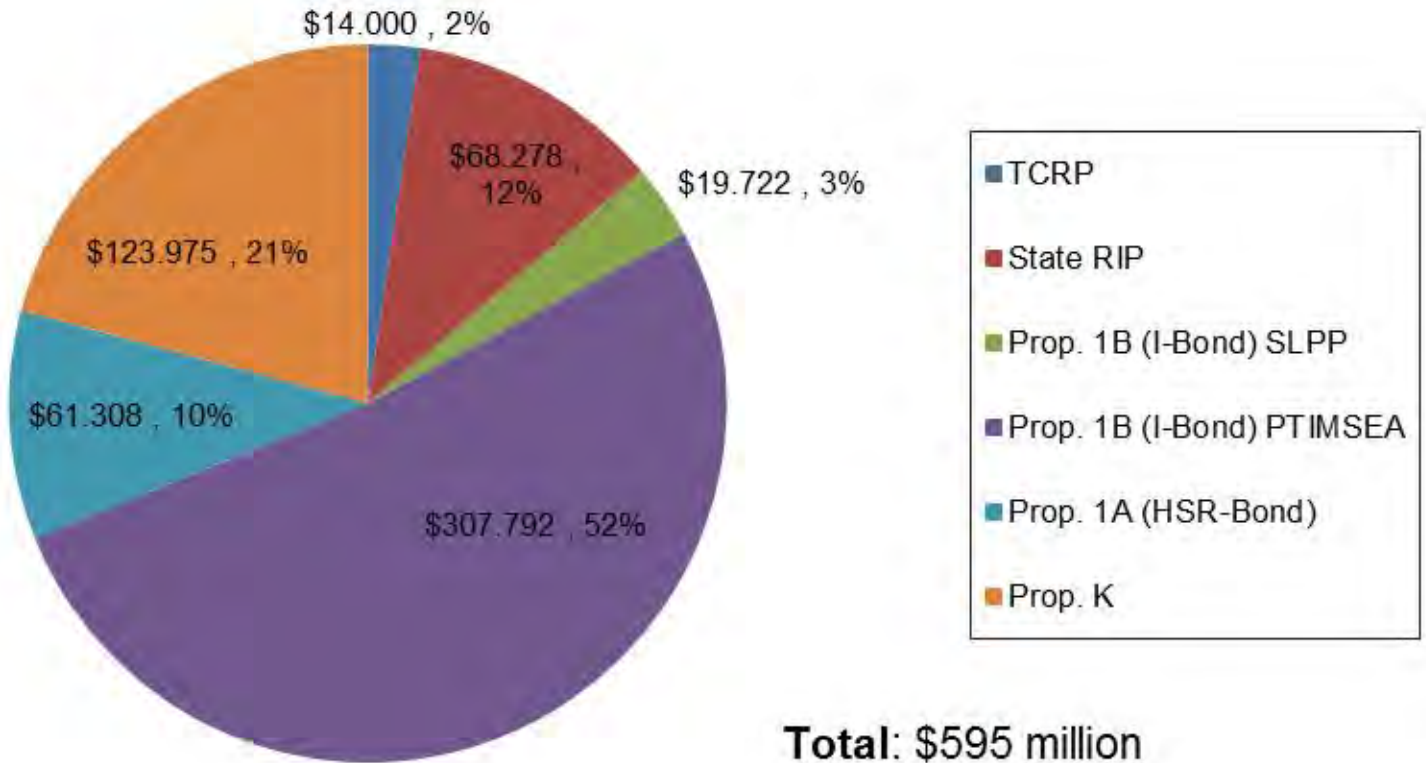
Central Subway Project Funding

Phase 2
(\$ in millions)



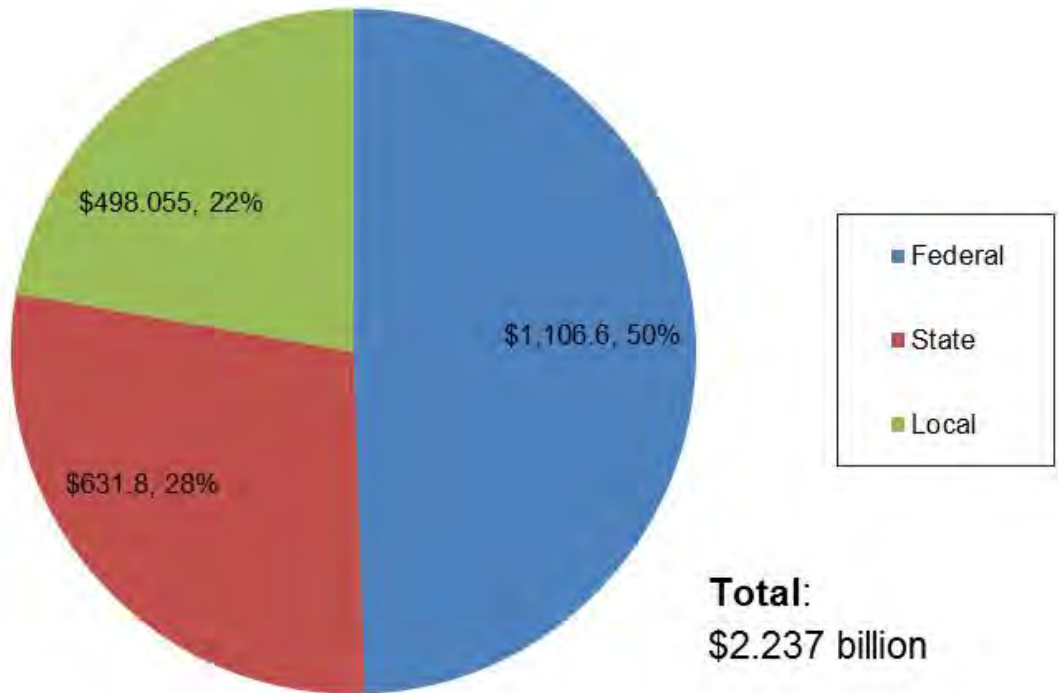
State and Local Funding

Phase 2
(\$ in millions)



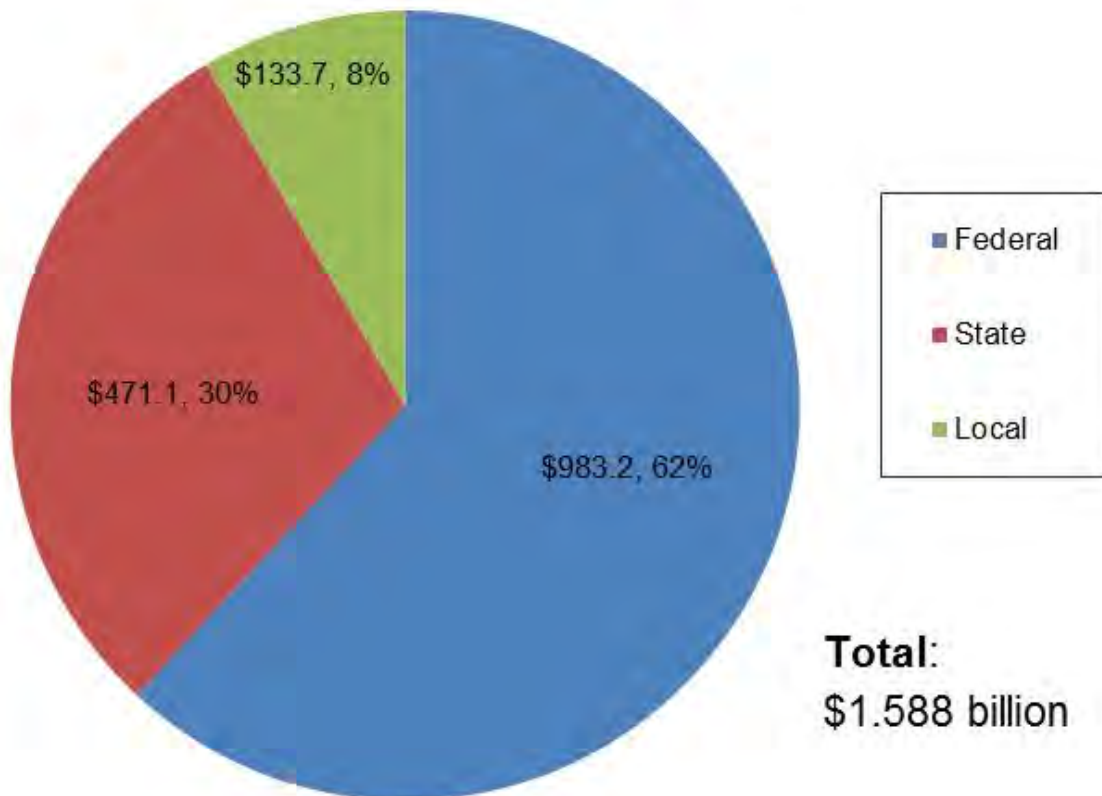
Third Street Light Rail Transit Project Funding

Phase 1 + Phase 2 + Retrieval Shaft Relocation
(\$ in millions)

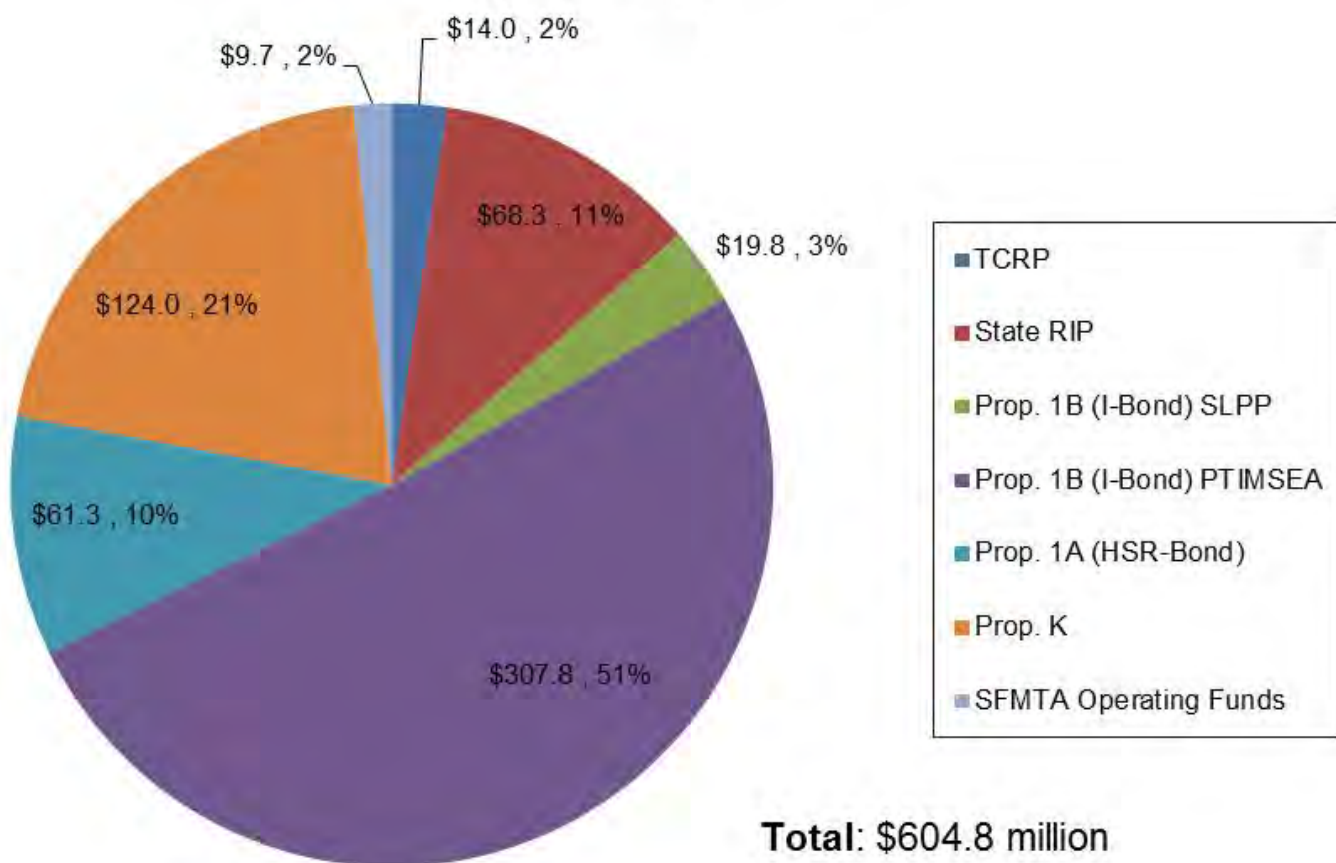


Central Subway Project Funding

Phase 2 + Retrieval Shaft Relocation
(\$ in millions)



State and Local Funding Phase 2 + Retrieval Shaft Relocation (\$ in millions)



Appendix D

COMPLETED CONTRACTS

Moscone Station and Portal Utility Relocation

Contract 1250

Contractor: Synergy Project Management, Inc.

Budget/Expenditures

Category	Amount
Original Budget	\$11,227,316
Expenditures Final	\$11,968,150
Utility Reimbursements	(\$2,275,419)
Final Program Cost	\$9,692,731
Budget Impact (Underrun)	(\$1,534,585)

Contract Details

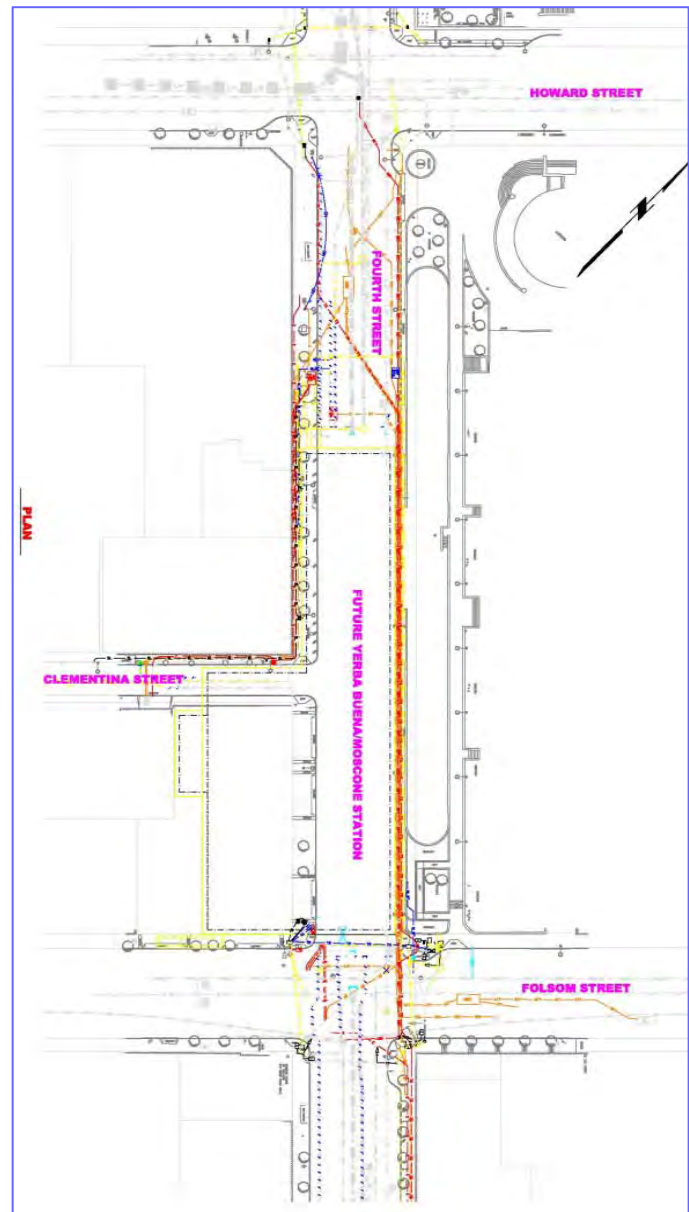
Contract Awarded:	November 17, 2009
Notice to Proceed:	January 4, 2010
Substantial Completion:	June 23, 2011
Contract Award Value:	\$ 9,273,939
Modifications Final :	\$ 2,694,211
Final Contract Value:	\$11,968,150

Status

- Work complete
- Project closeout administration and documentation
- Final Completion Date: June 23, 2011

Description

This project relocates utilities within the footprint of the proposed Yerba Buena/Moscone Station and the 4th Street Portal where the tunnel boring machines will descend underground. Also included is installation of building protections and monitoring of buildings adjacent to utility trenches.



Union Square/Market Street Station Utility Relocation

Contract 1251

Contractor: Synergy Project Management, Inc.

Budget/Expenditures

Category	Amount
Original Budget	\$22,199,847
Expenditures Final	\$20,794,581
Utility Reimbursements	(7,413,510)
Final Program Costs	\$13,176,169
Budget Impact (Underrun)	(\$9,023,678)

Contract Details

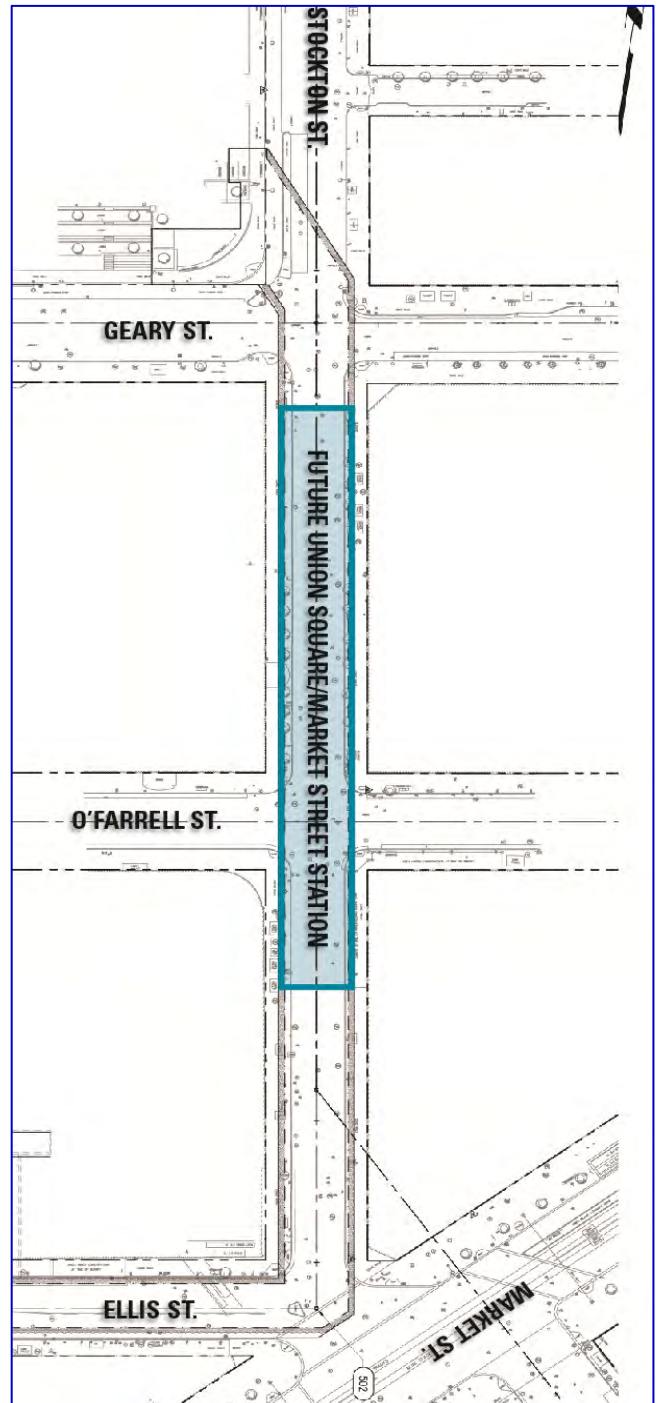
Contract Awarded:	December 7, 2010
Notice to Proceed:	January 12, 2011
Substantial Completion:	August 16, 2012
Contract Award Value:	\$16,832,550
Modifications Final:	\$3,962,031
Final Contract Value:	\$20,794,581

Status

- Final completion date October 15, 2012.
- Completed punch list work
- Project Final Acceptance by the SFMTA Board of Directors

Description

This project relocates utilities for the Union Square/Market Street Station and temporarily reroutes existing trolley coach lines around the construction.



Central Subway Pagoda Palace Demolition

Contract 1277 Contractor: MH Construction

Work Description

Demolish and clear the former Pagoda Theater for use the site to recover the tunnel boring machines when tunnels are completed in 2015. Locate and supply contractor facilities and installations. Obtain permits and approvals and coordinate work with City agencies and utility companies. Furnish and install signs and distribute notices to the local community prior to commencing with construction, cleanup and remove of debris from the site.

- Work was substantially completed Sept. 24, 2013.
- Administrative closeout in progress.
- Completed administrative closeout in June 2016.

Budget/Expenditures	
Category	Amount
Current Budget	\$648,976
Expenditures to Date	\$648,976

Contract Details	
Contract Awarded:	June 12, 2013
Notice to Proceed:	July 15, 2013
Substantial Completion:	Sept. 24, 2013
Contract Award Value:	\$498,995
Modifications to Date:	\$149,981
Current Contract Value:	\$648,976



Central Subway Tunneling

Contract 1252 Contractor: Barnard Impregilo Healy Joint Venture

Description of Work

1.5-mile twin bore tunnels from Hwy I-80 to North Beach using two tunnel boring machines (TBMs). Contractor procurement and installation of the TBMs; construction of the TBM launch box and retrieval shaft excavation support; Yerba Buena/Moscone Station and Union Square/Market Street Station end walls; tunnel excavation and installation of precast segmental lining, the 4th Street portal transition to the surface and cross passages. Throughout, settlement monitoring and protection of existing utilities, buildings and BART tunnels.

Status

- Final Completion Date: May 15, 2015
- Administrative closeout in progress

Budget/Expenditures	
Category	Amount
Current Budget	\$235,913,500
Other Project Budget	\$5,150,000
Other Offset Credits	\$1,291,078
Expenditures to Date	\$233,793,900

Contract Details	
Contract Awarded:	June 28, 2011
Notice to Proceed 1:	January 27, 2012
Notice to Proceed 2:	March 14, 2012
Partial NTP 3:	April 12, 2012
Notice to Proceed 3:	October 15, 2012
Substantial Completion:	April 13, 2015
Contract Award Value:	\$233,584,015
Modifications to Date:	\$8,255,506
Current Contract Value:	\$241,839,521



Appendix E

SBE PARTICIPATION

Quarterly Report

Current Report: April 2016 – June 2016

PROGRAM SUPPORT CONTRACTS – SBE PARTICIPATION

Appendix E presents the Central Subway Program Small Business Enterprise or SBE goals and the actual SBE participation achieved to date – as of June 30, 2016.¹

CS Program SBE Summary Table for Professional Services and Construction Contracts

The summary compares the dollar value of the Base Contracts, the SBE Contract Goals, the percent and dollar value expended to date and the SBE actual participation to date.

Contract No.	Contractor	Services/Segment	A	B	C	D	E	F	G	
			Contract Amount	SFMTA SBE Contract Goal	Contract Expenditure to Date (Est.)	SBE Actual to Date	SBE Contract \$ = A * B	SBE Amount to Date = C * D	Contractor's SBE Goal (in Bid)	
A Project Professional Services Contracts			<i>millions</i>		<i>millions</i>		<i>millions</i>		<i>millions</i>	
1	149	CS Partnership	Project Management	\$85.14	30%	\$54.38	35%	\$25.54	\$19.25	31.4%
2	156	Hill International	Project Controls Task 1	\$17.11	26%	\$9.22	29%	\$4.45	\$2.68	26.0%
3	155-1	PB Telemon	Tunnels Design	\$7.94	30%	\$7.76	30%	\$2.38	\$2.30	31.6%
4	155-2	CS Design Group	Stations Design	\$36.52	30%	\$32.21	42%	\$10.96	\$13.58	36.4%
5	155-3	HNTB, Inc. - B&C	Systems, Track & Surface Station Design	\$17.23	30%	\$25.12	28%	\$5.17	\$7.03	30.0%
Subtotal Professional Services				\$163.94		\$128.69		\$48.50	\$44.84	
B Project Construction Contracts			<i>millions</i>		<i>millions</i>		<i>millions</i>		<i>millions</i>	
1	1250	Synergy Inc	Utility Relocation 1	\$11.97	20%	\$11.97	97.2%	\$2.39	\$11.63	96.4%
2	1251	Synergy Inc	Utility Relocation 2	\$20.79	20%	\$20.79	87.4%	\$4.16	\$18.18	94.9%
3	1252	BIH	Tunnels and Portal - in Construction	\$241.29	6%	\$234.88	5.8%	\$14.48	\$13.73	6.1%
4	1277	MH Construction	Pagoda Demolition	\$0.65	100%	\$0.65	100.0%	\$0.65	\$0.65	100.0%
5	1300	Tutor-Perini	Stations/Track/Systems - in Construction	842.94	20%	\$392.17	17.2%	\$168.59	\$67.60	25.5%
Subtotal Construction Contracts				\$1,117.64		\$660.47		\$190.27	\$111.79	
Contract	Contractor	Services/Segment	Base Contract	SFMTA Goal	Expenditures	SBE Actual	= A * B	= C * D	Bid Goal	
			A	B	C	D	E	F	G	

SBE Summary Table Notes and Sources:

- a) Column A is the base contract amount awarded. Column B is the Agency SBE goal percent for each contract awarded.

The SFMTA SBE Contract Goals are also on the Central Subway web site under the listing of on-going contracts – see “**Closed and Awarded Contracts**” at this link:

<http://centralsubwaysf.com/content/closed-and-awarded-contracts>

- b) Column C shows each contract’s current amount expended to date (estimated) including accruals. Column D is the actual SBE percent level of each contract based on payments to date.

Column E is the expected SBE dollar amount when the contract amount is completed and the SFMTA SBE goal achieved using this calculation: Columns A * B = Column E, the SBE Expected \$ Amount.

Column F is the actual SBE dollar amount out of the total contract expenditure to date:

Columns C * D = Column F, the SBE Expended \$ Amount.

The source of the SBE Actual percent to date and dollar amounts are Progress Payment Applications and Contractor’s monthly submittals that may include the current estimated

¹ An SBE is a for-profit, small business concern with a three (3) year average gross revenue not exceeding \$14 million or \$12 million, depending on the scope of work to be performed, that is certified under any of the following programs: the State of California’s Small Business Program with the Department of General Services (“State Program”), the City and County of San Francisco’s LBE Program (“City Program”), or the California Unified Certification Program (“Federal DBE program”).

accruals. The BIH SBE percent is from the contractor's progress payment #39, Form 6.

- c) Column G, the Contractor's SBE Goal in the submitted bid, is background information that is not calculated in the table. The table source of the Contractor's SBE Goals is from the SFMTA Contract Compliance Office. A Contractor's SBE goal in the bid is one source used by SFMTA Contract Compliance to assess and propose the Agency's SBE goal for a contract.
- d) The three constructions contracts shown in **bold type, 1250, 1251 and 1277**, with gray background, are completed contracts. Little to no changes will be shown in future reports.
- e) The SBE Hill International Actual to Date SBE participation is 28.9% for the overall SFMTA contract. The Hill International data is for the Central Subway Task 1 portion of the Hill International contract to provide SFMTA Project Controls services and systems.
- f) The SBE SFMTA goal for Contract 1300 Tutor-Perini is 20% SBE with a provision of 50% for trucking.

The 1300 Tutor-Perini SBE percent Actual is based on the SBE data provided in Progress Payment #30, June 2016, SFMTA SBE FORM No. 6.

- g) The SBE SFMTA goal for Contract 1277 MH Construction was based on an SBE set-aside.

SBE Participation Details

The two tables that follow present the Central Subway's professional services and construction contract amounts, expenditures and SBE levels with additional details.

Active Professional Services Contracts - SBE Participation Details

		As of:	6/30/2016
Contract:	Project Management and Construction management		
Contract No.	CS-149 Central Subway Partnership		
Status:	On-going		
	Base Contract Value		\$85,139,092
	Approved Change Orders		-0-
	Current Contract Value		\$85,139,092
	Expended to Date (est.)		\$54,378,411
	% Expended		63.9%
	SBE Participation		35.4%
Contract:	Project Controls Cost and Schedule Support		
Contract No.	CS 156 Hill International Task 1*		
Status:	On-going		
	Base Contract Value		\$17,112,873
	Approved Change Orders		-0-
	Current Contract Value		\$17,112,873
	Expended to Date (est.)		\$9,223,658
	% Expended		53.9%
	SBE Participation		29.1%
Contract:	Design Package 1 for CNs 1250, 1251 and 1252 Tunnels*		
Contract No.	CS-155-1 PB / Telemon		
Status:	Design is completed. Construction support ongoing		
	Base Contract Value		\$5,795,000
	Approved Change Orders (7)		\$2,145,159
	Current Contract Value		\$7,940,159
	Expended to Date (est.)		\$7,756,940
	% Expended		97.7%
	SBE Participation		29.7%
Contract:	Design Package 2 for 1253 UMS, 1254 CTS, 1255 YBM Stations.		
Contract No.	CS-155-2 Central Subway Design Group		
Status:	Design is completed. Construction support ongoing		
	Base Contract Value		\$35,059,252
	Approved Change Orders (1)		\$1,460,360
	Current Contract Value		\$36,519,612
	Expended to Date (est.)		\$32,210,651
	% Expended		88.2%
	SBE Participation		41.1%
Contract:	DP 3 Systems, Track work, Surface station.		
Contract No.	CS-155-3 HNTB-B&C		
Status:	Design is completed. Construction support ongoing		
	Base Contract Value		\$16,822,238
	Approved Change Orders (5)		\$312,814
	Current Contract Value		\$17,232,252
	Expended to Date (est.)		\$25,123,582
	% Expended		145.8%
	SBE Participation		27.4%

* denote accrual

Active and Completed Construction Contracts - SBE Participation Details

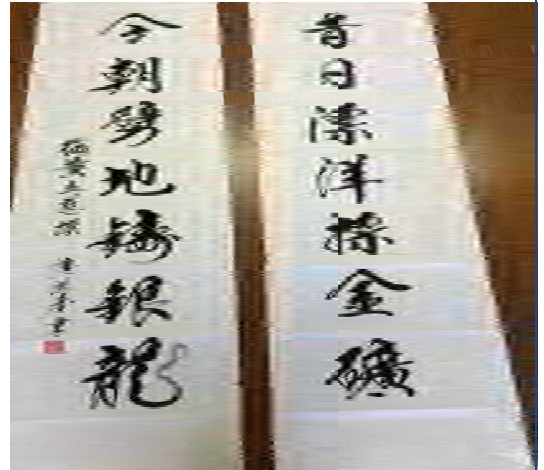
		<i>Data as of:</i>	6/30/2016
Contract:	Synergy Inc Utility Relocation 1 YBM & Launch Box		
Contract No.	1250		
Status:	Contract is completed and closed out		
	Base Contract Value		\$9,273,939
	Approved Change Orders		\$2,694,211
	Final Contract Value		\$11,968,150
	% Expended		100%
	SBE SFMTA Goal		20%
	SBE Participation To Date		97.2%
Contract:	Synergy Inc Utility Relocation 2 UMS		
Contract No.	1251		
Status:	Contract is completed and closed out		
	Base Contract Value		\$16,832,550
	Approved Change Orders		3,962,031
	Final Contract Value		\$20,794,581
	% Expended		100%
	SBE SFMTA Goal		20.0%
	SBE Participation To Date		87.4%
Contract:	Pagoda Palace Demolition / MH Construction		
Contract No.	1277		
Status:	Contract is completed and closed out		
	Base Contract Value		\$498,995
	Approved Change Orders		\$149,981
	Current Contract Value		\$648,976
	Expended to Date (est.)		\$648,976
	% Expended		100.00%
	SBE SFMTA Goal		100.0%
	SBE Participation To Date		100.0%
Contract:	Tunnels Barnard/Impregilo/Haley		
Contract No.	1252*		
Status:	Construction is underway and ongoing		
	Base Contract Value		\$233,584,015
	Approved Change Orders		\$8,270,765
	Current Contract Value		\$241,854,780
	Expended to Date (est.)		\$234,881,397
	% Expended		97.1%
	SBE SFMTA Goal		6.0%
	SBE Participation To Date		5.8%
Contract:	Stations and Systems / Tutor Perini		
Contract No.	1300*		
Status:	Construction is underway and ongoing		
	Base Contract Value		\$839,676,400
	Approved Change Orders		\$3,259,676
	Current Contract Value		\$842,936,076
	Expended to Date (est.)		\$392,173,667
	% Expended		46.5%
	SBE SFMTA Goal		20.0%
	SBE Participation To Date		18.5%
* Contract 1252 and Contract 1300 Mar cost are accrual.			

Photos on the next page:

In April 2016, the Central Subway Project and their partners at the Chinatown Community Development Center began a search for a couplet, a form of Chinese poetry with two lines of verse that have a joined meaning and follow a list of exacting rules, to be turned into art that will adorn the entryway of the Chinatown Plaza at the Chinatown Station.

central subway

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