

central subway

Station Excavation

Work to excavate and construct all three subway stations is in full swing.



Progress Report
July 2015



SFMTA
Municipal
Transportation
Agency

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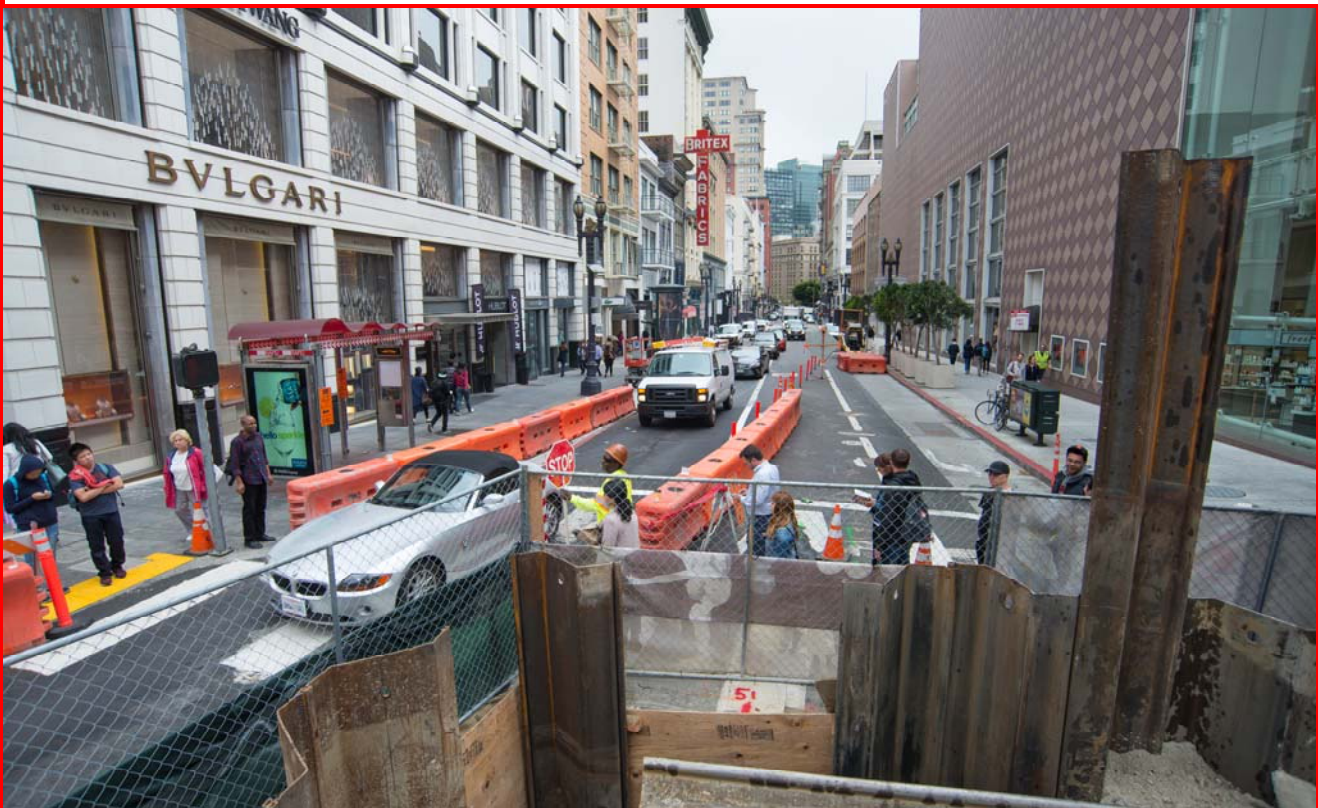
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Cover photo: To begin excavation of the Chinatown Station headhouse, workers broke through a temporary “rat” slab below the permanent roof slab, excavated a pit, and lowered a bulldozer. Workers then began hauling out soil from underneath the roof slab. This excavated soil is then lifted up using an excavator and loaded onto waiting dump trucks. **More photos of this construction can be found starting on page 33.**

Above photos: In order to build sections of station roof decking across the first of two active intersections affected by Union Square/Market Street Station construction, traffic lanes were diverted to the north side of Geary, while steel sheet piles were installed prior to excavation.

See the Appendix E final page for CS websites hyperlinks and public outreach on line resources. The Project main web site is at: <http://www.centralsubwaysf.com/>



Steel cylinders called “walers” are used as temporary supports during excavation of the Yerba Buena/Moscone Station headhouse.

Executive Summary

The excavations now underway at YBM, UMS and CTS are opening space for installing the concrete roof of the future stations.

Chinatown Station - Backfill surface slab at Washington Street. Completed pothole dewatering walls on Stockton Street. Completed inclinometers on Stockton Street. Began permanent utility relocation under Washington Street. Continue relocating ductbank on Stockton Street.

Union Square/Market Street Station - Continued welding beams in the Ellis Annex and in the North Concourse. Continued selective demolition Union Square Garage levels #4, #3, and #1 and 2. Continued production and test jet grouting on the east and west sides of Stockton Street.

Yerba Buena/Moscone Station - Traffic flowing on west side of 4th Street with east side work area closed for roof and utility construction. East side surface slab construction is in progress (three segments, TPC sections 4, 5, 6 south to north) – backfill of sections 4 and 5 is complete with additional utility installation pending, rebar installation in section 6 is in progress. Backfill over portion of headhouse beneath Clementina Street is in progress. Utility installation at 4th and Howard (sewer mains) and at 4th and Folsom Street (culvert) is in progress. Headhouse temporary bracing strut level 1 installation is in progress. Archaeological monitoring by spot checking is in progress.

Surface, Track and Systems Continue Muni ductbank installation. Continue 78” sewer rehabilitation. Continue 36” sewer force main. Started Auxiliary Water Supply System (AWSS) installation.

Tunnel - achieved Final Completion on May 15, 2015. Contract administrative closeout is ongoing to reconcile contract cost.

Total project costs to date are \$818.77 million, an increase of \$12.39 million over last month. The total cost to date equals 51.88% of the total project budget of \$1.578 billion. The Master Project Schedule forecast the Revenue Service Date of February 2019.

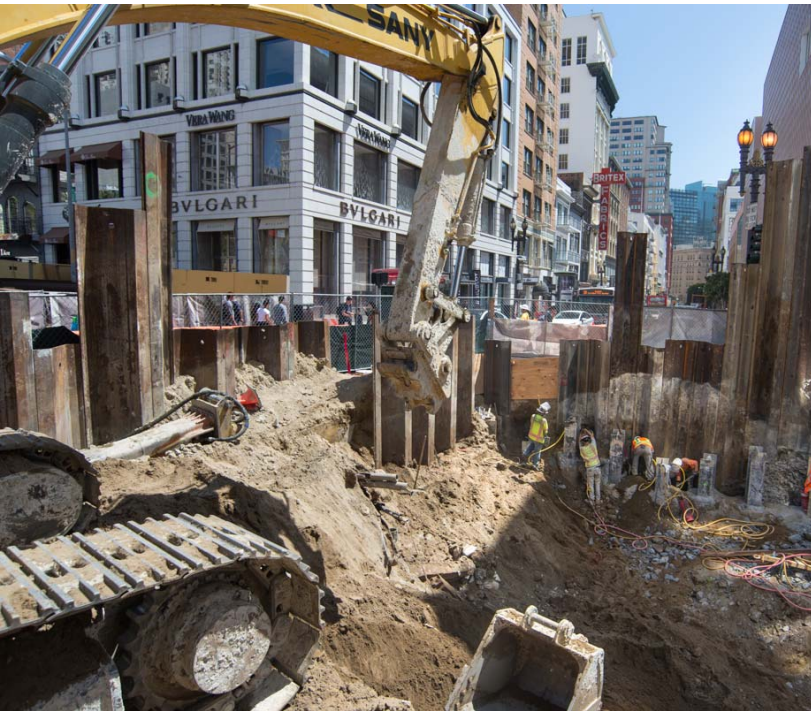
The Stations Contractors’ Safety Reports show no recordable accidents took place this month and the rates of work site accident incidents by the man hours worked continue to be below industry standards - see tables on page 28.

Key Milestones

Construction of Roof Deck Section for UMS North Concourse at Geary

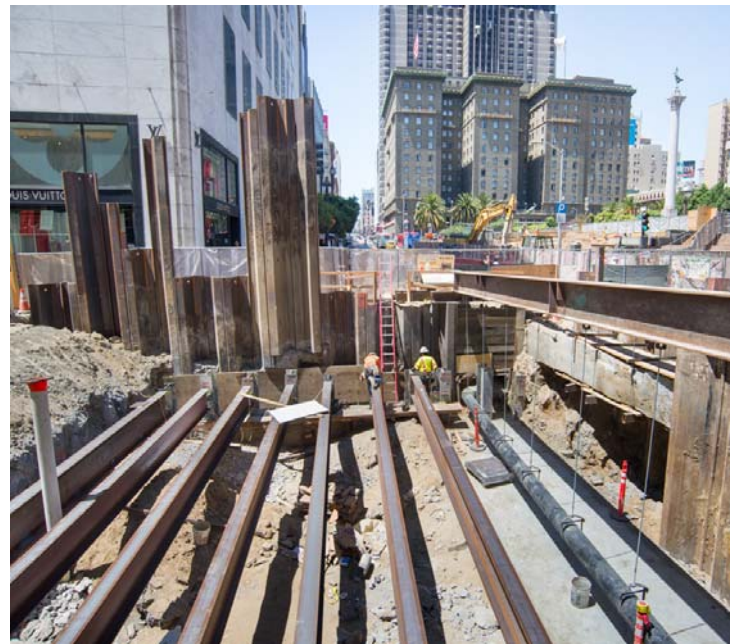


1 Traffic lanes are shifted over and steel sheet piles are installed



2 Workers excavate the area reinforced by the steel sheet piles

| MILESTONE | DATE EXPECTED |
|--|----------------------|
| General | |
| Revenue Service | December 2018 |
| Contract 1252 Tunnels | |
| Substantial Completion | April 15, 2015 (A) |
| Contract 1300 Stations, Surface, Track, Systems | |
| Notice to Proceed (NTP 1) | June 17, 2013 (A) |
| Notice to Proceed (NTP 2) | January 12, 2014 (A) |
| Substantial Completion | February 10, 2018 |



3 Roof beams are attached to reinforced concrete piles already installed

Costs and Schedule

Costs (See Appendix A for Details)

The Current Cost Estimate (CCE) for the Central Subway Project is **\$1.578 billion** in year of expenditure dollars (\$YOE). This total project cost is shown at the top of Report 7.1, Program Project Budget. This capital cost projection incorporates allocated and un-allocated contingencies to cover the risks associated with the project completion.

Total net incurred costs for the project are \$818.77 million, a \$12.39 million increase over last month. The cost to date figure reflects expenditures through FAMIS 786 Report (\$775.66 million) plus the utilities joint trench Form B Reimbursement payment (\$10.03 million), invoices currently being processed (\$20.03 million) and estimates of outstanding pay requests (\$13.06 million). This incurred amount equals 51.88% of the total project budget of \$1.578 billion.

The current funding level to date is \$1,029.79 million. This represents 65% of the total project budget.

Earned Value Analysis

In July 2015 Report, Central Subway Project Master Schedule has re-baselined construction phase and adopted Contract 1300 Station contractor baseline schedule. In this report, Contract 1300 Station has used accepted February 2015 schedule update and July 2015 actual cost. Earned Value Analysis and Earned Value Cash Flow Curve, Report 7.2 are not included in July Report.

Schedule Highlights - Continued

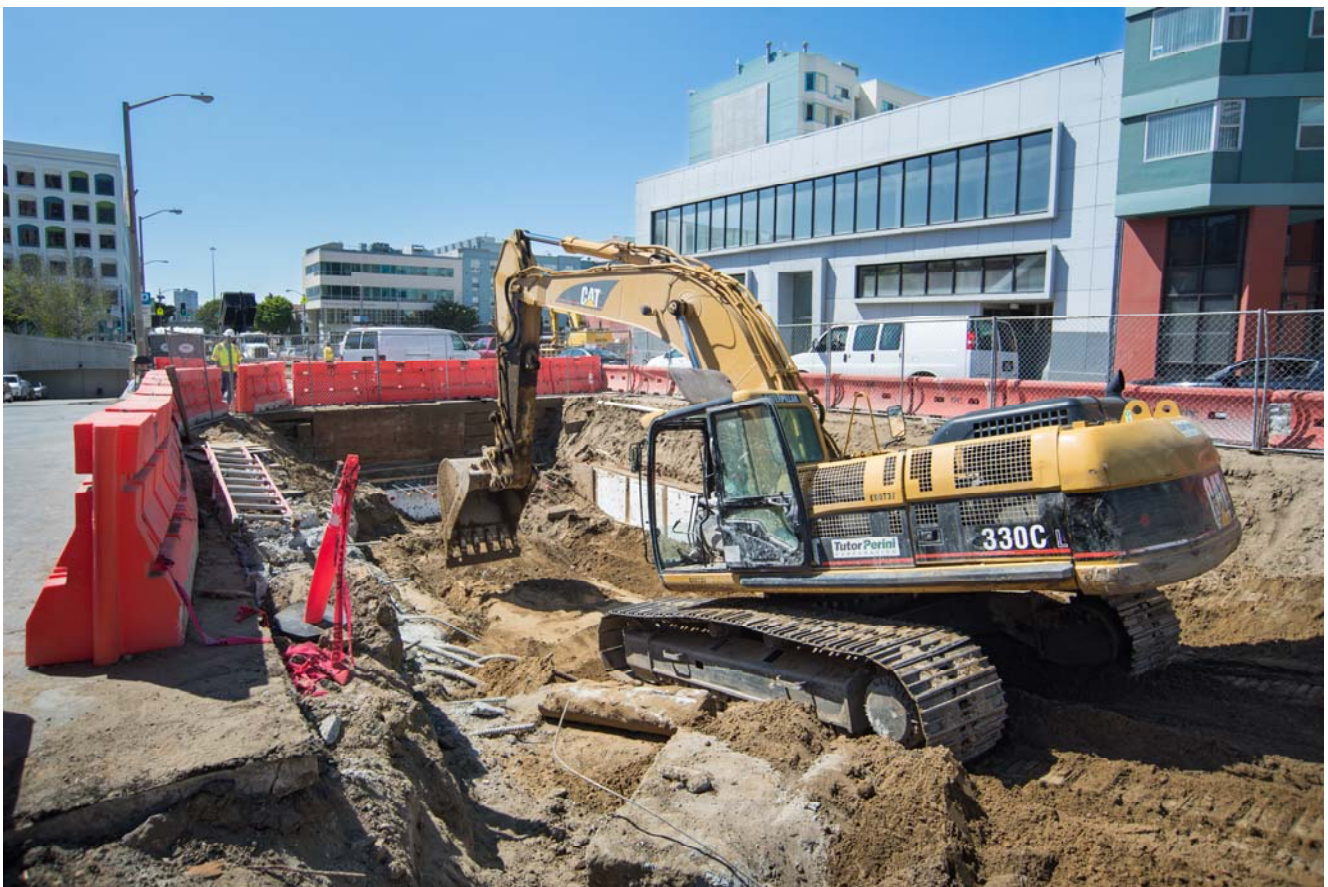
The Master Project Schedule (MPS) below includes progress through July 31, 2015.

The MPS shows the forecast the Revenue Service Date of February 2019 due to 6 Months delay shown in Contractor February Update Schedule.

The controlling critical (longest) path of the MPS runs through CTS succeeded by Commissioning and Pre-Revenue Activities to the Baseline Finish and Revenue Service Date. See Appendix B – Longest Path. The later schedule shows the longest path has changed from CTS to UMS. UMS delay is due to Jet Grout production rate reduced from 5.31 columns per day to 2.30 columns per day.

The Schedule Contingency is reduced to zero on the critical path of the MPS, which is below the Minimum Schedule Contingency level of 6 months. A re-evaluation is now required, utilizing the updated Contract 1300 Schedule to justify any revisions to current minimums.

The 1300 Contractor submitted eight (8) Schedule Update from December 2014 to July 2015. SFMTA has approved December 2014 to February 2015 monthly updates. The February Schedule Update shows 6 Months slippage of progress on the Critical Path. Contractor has been directed to develop a recovery schedule as required by Contract. The 1300 Contractor continues address key critical activities to recover lost time and improve schedule performance. Review of schedule update as well as identifying recovery options is ongoing.



An excavator waits to remove soil from a portion of 4th Street where the last section of reinforced concrete roof slab will be constructed for Yerba Buena/Moscone Station.

Schedule Highlights - Continued

Contract 1300 Stations, Surface, Track and Systems

The Contractor, Tutor Perini Corporation's (TPC) baseline schedule is incorporated into the master program. The Contractor, Tutor Perini Corporation's (TPC) February schedule is used in July Report. Anticipated to use Contractor, Tutor Perini Corporation's (TPC) August schedule for August Report.

Contract 1300 has several interface points with Contract 1252. The timing of these interface points correlate to when the NTP was issued for Contract 1300. This table represents the current status of these interface points with respect to the Contract 1252 update. The 1300 Contractor is not ready to access the tunnel used by Contract 1252. The tunnel access from Contract 1252 is not expected to impact Contract 1300. The current completion date of the Contract 1252 is shown in the table below.

| Interface Points | CN1300 Requirement | CN 1252 | Variance | |
|--------------------------|--------------------|-----------------|----------|----|
| | | Mar 2015 Update | | |
| Tunnel Portal Completion | 24-Sep-15 | 15-May-15 | 132 | CD |

Work Package P-1254R CTS performed the following work this month:

- Backfill Surface Slab at Washington Street
- Complete pothole dewatering wells on Stockton Street
- Complete inclinometers on Stockton Street
- Began permanent utility relocation under Washington Street
- Continue relocating ductbank on Stockton Street

Work Package P-1253 UMS performed the following work this month:

- Continued setting and welding beams in the Ellis Annex and in the North Concourse
- Continued production and test jet grouting on the east and west sides of Stockton Street

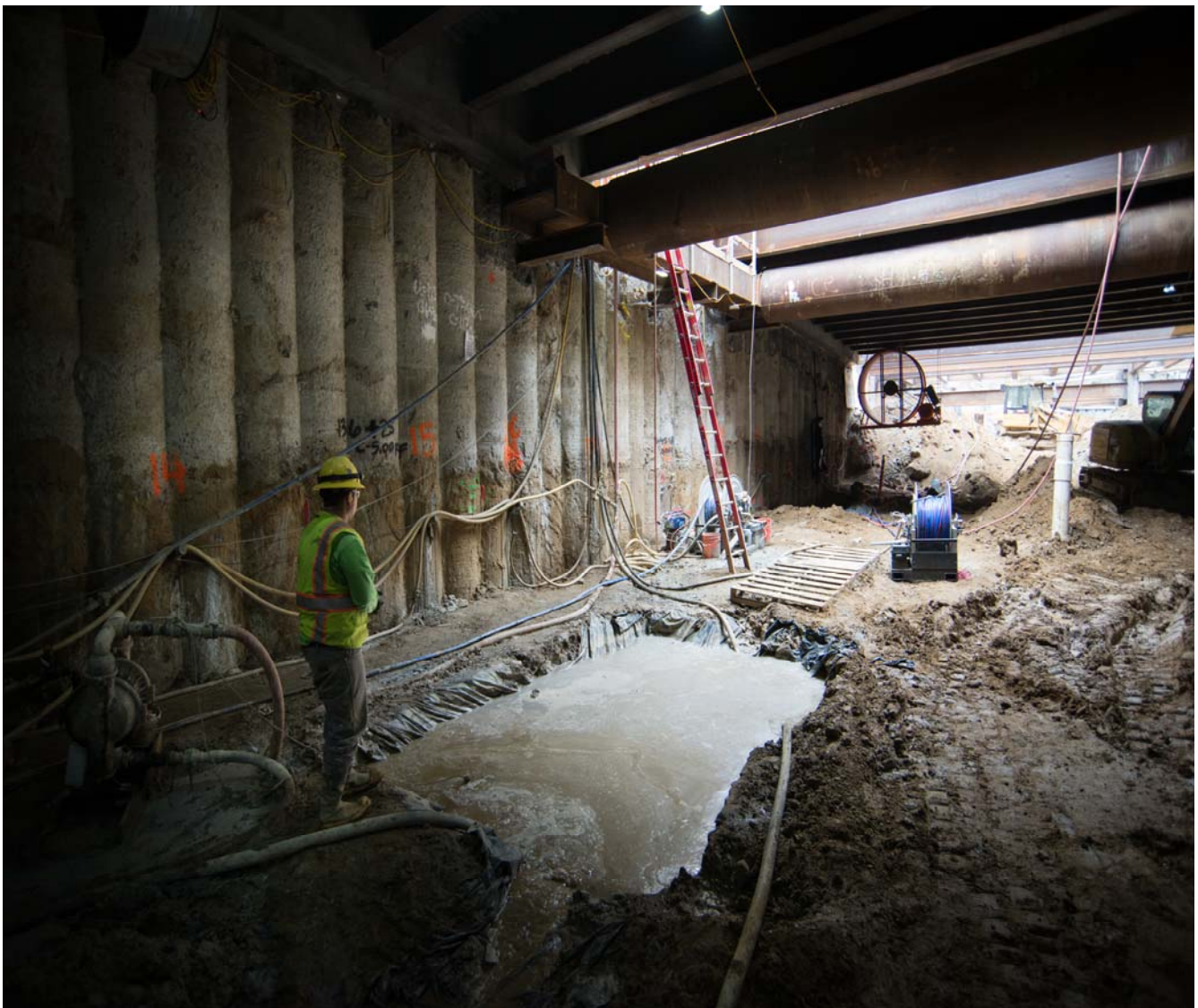
Work Package P-1255 YBM performed the following work this month:

- Traffic flowing on west side of 4th Street with east side work area closed for roof and utility construction
- East side surface slab construction is in progress (three segments, TPC sections 4, 5, 6 south to north) – roof slab in sections 4 and 5 is complete with utility installation and backfill in progress, roadway section removal and excavation in section 6 is in progress
- Backfill over portion of headhouse beneath Clementina Street is in progress
- Utility installation at 4th and Howard (sewer mains) and at 4th and Folsom (culvert) is in progress
- Headhouse dewatering and excavation to temporary bracing level 1 is in progress
- Archaeological monitoring is in progress

Schedule Highlights - Continued

Work Package P-1256 STS performed the following work this month:

- Continue Muni ductbank installation
- Continue 78" sewer rehabilitation
- Continue 36" sewer force main
- Started Auxiliary Water Supply System (AWSS) installation



A worker monitors a drilling muck pit during compensation grouting efforts inside the future south concourse of Union Square/Market Street Station.

Master Project Schedule

| Activity ID | Activity Name | Original Duration | Start | Finish | 2015 | | | | | | | | | | | | 2016 | | | | | | | | | | | | 2017 | | | | | | | | | | | | 2018 | | | | | | | | | | | | 2019 | | | | | | | | | | | | 2020 | | | | | | | | | | | |
|-------------------------------|---|-------------------|-------------|-------------|------------------------|----|----|----|----|----|----|----|----|----|----|----|------|----|----|----|----|----|----|----|----|----|----|----|------|----|----|----|----|----|----|----|----|----|----|----|------|----|----|----|--|--|--|--|--|--|--|--|------|--|--|--|--|--|--|--|--|--|--|--|------|--|--|--|--|--|--|--|--|--|--|--|
| | | | | | Q1 | Q2 | Q3 | Q4 | Q1 | Q2 | Q3 | Q4 | Q1 | Q2 | Q3 | Q4 | Q1 | Q2 | Q3 | Q4 | Q1 | Q2 | Q3 | Q4 | Q1 | Q2 | Q3 | Q4 | Q1 | Q2 | Q3 | Q4 | Q1 | Q2 | Q3 | Q4 | Q1 | Q2 | Q3 | Q4 | Q1 | Q2 | Q3 | Q4 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| CENTRAL SUBWAY PROJECT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| PJ01000 | Central Subway Project Start | 4074 | 03-Jun-03 A | 02-May-19 | CENTRAL SUBWAY PROJECT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| MS0004A | Tunnel Excavation Complete - Project Milestone #4A | 4060 | 03-Jun-03 A | 12-Feb-19 | CENTRAL SUBWAY PROJECT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| MS0019 | Baseline Finish Date: 12-26-2018 | 0 | 03-Jun-03 A | 05-Sep-14 A | CENTRAL SUBWAY PROJECT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| MS0009 | CSP Revenue Service Date | 0 | | 12-Feb-19* | CENTRAL SUBWAY PROJECT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | Preliminary Engineering Phase | 2661 | 03-Jun-03 A | 07-Jan-10 A | CENTRAL SUBWAY PROJECT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | Final Design | 1811 | 08-Jan-10 A | 17-Jun-13 A | CENTRAL SUBWAY PROJECT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | Light Rail Vehicles | 2060 | 15-Apr-13 A | 04-Dec-18 | CENTRAL SUBWAY PROJECT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | Real Estate | 3130 | 01-Aug-08 A | 31-Jul-15 | CENTRAL SUBWAY PROJECT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | Construction Phase | 2384 | 04-Jan-10 A | 02-May-19 | CENTRAL SUBWAY PROJECT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | Construction Support and Costs | 2671 | 04-Jan-10 A | 02-May-19 | CENTRAL SUBWAY PROJECT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | Construction Utility Contract #1- MOS & Portal CN-1250 | 505 | 04-Jan-10 A | 23-May-11 A | CENTRAL SUBWAY PROJECT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | Construction Utility Contract #2 - UMS CN-1251 | 643 | 12-Jan-11 A | 15-Oct-12 A | CENTRAL SUBWAY PROJECT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | Construction Tunnels CN-1252 | 1127 | 08-Jun-11 A | 01-Aug-15 | CENTRAL SUBWAY PROJECT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | Construction CN-1300 | 1512 | 03-Jun-13 A | 22-Nov-18 | CENTRAL SUBWAY PROJECT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | CN- 1300 Milestone | 1368 | 17-Jun-13 A | 22-Nov-18 | CENTRAL SUBWAY PROJECT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | Construction UMS Station P-1253 | 1388 | 17-Jun-13 A | 25-Aug-18 | CENTRAL SUBWAY PROJECT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | Construction CTS Station P-1264R | 1315 | 17-Jun-13 A | 25-Aug-18 | CENTRAL SUBWAY PROJECT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | Construction YBM Station P-1255 | 1512 | 10-Jun-13 A | 27-Aug-18 | CENTRAL SUBWAY PROJECT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | Construction STS P-1256 | 1440 | 03-Jun-13 A | 07-Nov-18 | CENTRAL SUBWAY PROJECT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | Project Start Up | 115 | 24-Aug-18 | 12-Feb-19 | CENTRAL SUBWAY PROJECT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | Unallocated Contingency | 115 | 24-Aug-18 | 12-Feb-19 | CENTRAL SUBWAY PROJECT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| CO1700 | Cost Activity Unallocated Contingency (LOE) - 1.7,500,99,090.00 - Contingency | 115 | 24-Aug-18 | 12-Feb-19 | CENTRAL SUBWAY PROJECT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |

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Required Revenue Service Date: 26-Dec-18
Data Date: 28-Feb-15

SFMTA Central Subway Project
Master Project Schedule
Summary Schedule - July 2015



Broken concrete marks where demolition efforts have begun on the bottom level of the Union Square Garage. The southeast corner of the garage will be demolished to make room for a station entrance structure.

Contracts & Construction

Construction Contracts In Progress

Contract 1300: Combined Work Packages 1253, 1254, 1255, 1256

- Contractor: Tutor - Perini Corporation
- Amount: \$838.24 million
- Contract Status: 34.53% complete construction

Contracts Completed

[See Appendix D](#)

Contract 1250: Moscone Station and Portal Utilities Relocation

Contract 1251: Union Square/Market Street Station Utility Relocation

Contract 1277: Pagoda Theater Site Demolition (Funded separately from the CS Project budget)

Contract 1252: Central Subway Tunneling

Contract SBE Participation (Updated Quarterly) [See Appendix E](#)

Stations, Surface, Track and Systems

Contract 1300 Contractor: Tutor-Perini Corporation

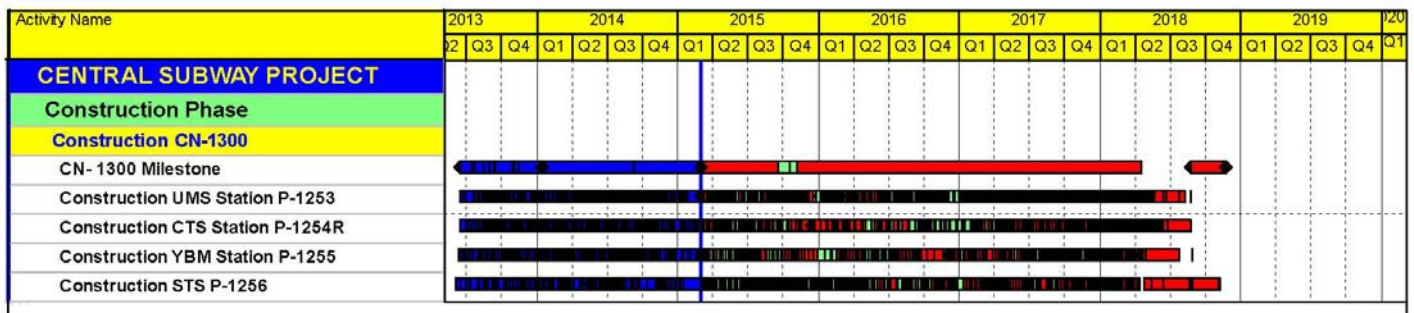
Description of Work

The Contract 1300 scope is to construct the Central Subway's three subway stations, one surface station, construct the 2,000 feet of surface track, and install track and operating systems throughout the new alignment. The separate station and systems work packages are presented in the following pages.

Work includes station finishes, AC and DC substations, elevators, escalators, lighting, emergency ventilation fans, HVAC fire alarm/ suppression/ protection, Cutter Soil Mixing, secant pile bottom up and Sequential Excavation Method construction, settlement monitoring, building protection, connecting to and modifying the BART Powell Street Station, PA, CCTV, signage, installation of fare collection equipment and station start-up and commissioning.

| Contract Details | | Budget/Expenditures▲ | |
|--------------------------|-------------------|----------------------|---------------|
| Contract Awarded: | May 21, 2013 | Current Budget | \$859,676,400 |
| Notice to Proceed: | June 17, 2013 | Expenditures to Date | \$277,044,309 |
| Substantial Completion: | February 10, 2018 | | |
| Contract Award Value: | \$839,676,400 | | |
| Modifications to Date: | (\$1,432,743) | | |
| Current Contract Value : | \$838,243,657 | | |

1300 Summary Schedule



Chinatown Station

Contract 1300 - Work Package 1254R



Current Work Status

- Backfill Surface Slab at Washington Street
- Completed pothole dewatering wells on Stockton Street
- Completed inclinometers on Stockton Street
- Began permanent utility relocation under Washington Street
- Continue relocating ductbank on Stockton Street

Work Expected Next Month

- Install dewatering wells on Stockton Street
- Deliver and install temp bracing beams
- Install service crane
- Install MBX instrumentation on Stockton Street
- Continue PG&E duck bank work on Stockton Street
- Excavate to El 70, install compensation grout tubes, pre grout

Description of Work

This Work Package is to construct one subway station. Includes station finishes, AC and DC Traction Power substations elevators, escalators, lighting, emergency ventilation fans, HVAC fire alarm/ suppression/ protection, slurry wall top-down construction, settlement monitoring, building protection, PA, CCTV, signage, installation of fare collection equipment and station start-up and commissioning.



Three Month Look Ahead

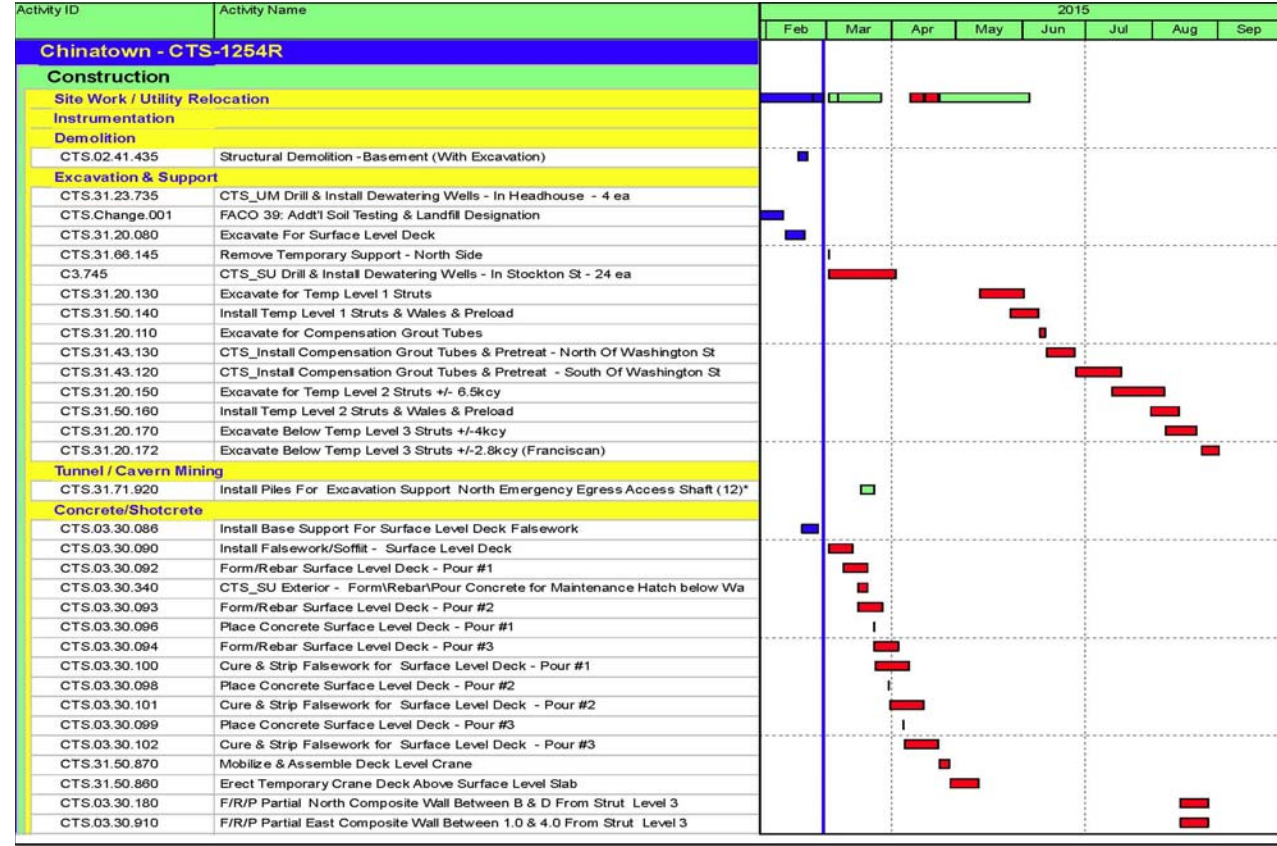
- Finish installing compensation grout tubes, pre grout
- Slip line brick sewer on Stockton Street
- Drill dewatering wells on Stockton Street
- Excavate inside head house, install temp bracing

Chinatown Station Construction Status - Continued

| Contract Details | |
|--------------------------------|--------------------------|
| Contract Awarded: | May 21, 2013 |
| Notice to Proceed: | June 17, 2013 |
| Substantial Completion: | February 10, 2018 |
| Contract Award Value: | \$247,567,810 |
| Modifications to Date: | \$230,956 |
| Current Contract Value: | \$247,798,766 |

| Budget/Expenditures | |
|-----------------------------|----------------------|
| Current Budget | \$252,567,810 |
| Expenditures to Date | \$62,555,555 |

CTS Three Month Schedule



Union Square/Market Street Station

Contract 1300 Work Package 1253

Description of Work

This Work Package is to construct one subway station. Includes station finishes, AC and DC Traction Power substations elevators, escalators, lighting, emergency ventilation fans, HVAC fire alarm/ suppression/ protection, slurry wall top-down construction, settlement monitoring, building protection, PA, CCTV, signage, installation of fare collection equipment and station start-up and commissioning.

Current Status This Month

- Continued welding beams in the Ellis Annex and in the North Concourse
- Continued production and test jet grouting on the east and west sides of Stockton Street

Work Expected Next Month

- Street closures: Stockton Street between Ellis and Post and Ellis halfway, Stockton to Powell. Alternate lane configuration on O'Farrell at Stockton. Narrowing of Geary to 1 lane at Stockton
- Union Square Garage – demolition and abatement
- North Concourse - install decking, concrete for roof deck and reroute of ductbanks and waterline
- Continue jet grouting operations on East and West sides of Stockton Street between Geary and O'Farrell Streets
- Complete compensation grouting pipe installation and preconditioning at South Concourse
- Demolition and shotcrete installation operations at Ellis Street Annex
- Geary Street portion of North concourse sewer notch and roof installation should be complete



Three Month Look Ahead

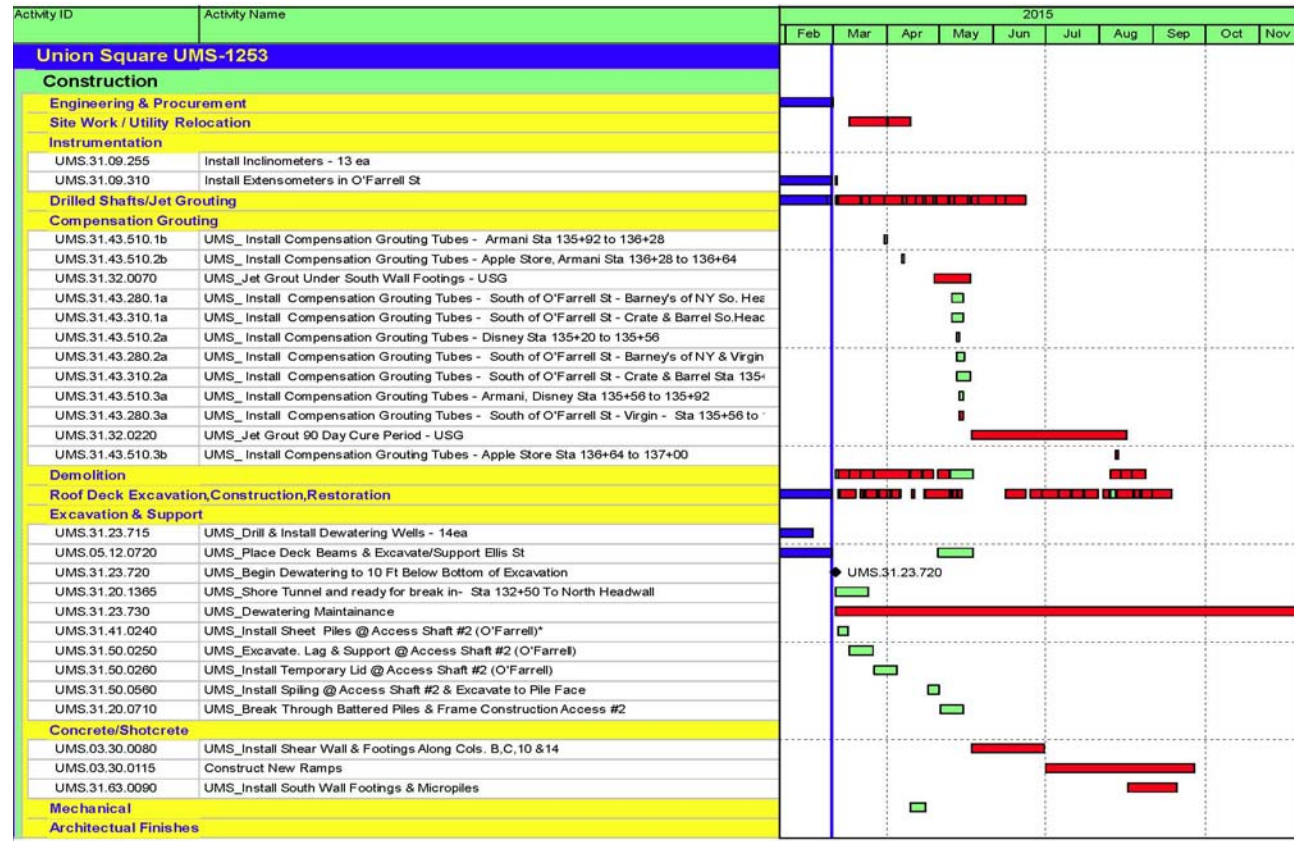
- Progress I-beam, roof deck and waterproofing installation for roof deck construction
- Continue demolition operations at Ellis Street on remaining BART structure to be removed
- Install new roof on Ellis for BART Station entrance to UMS station
- Continue working on Union Square Garage demolition
- Continue jet-grouting operations on Stockton Street between Geary and O'Farrell Streets
- Install new roof on Station between Maiden Lane and Geary Street

Union Square Market Street Station Construction - Continued

| Contract Details | |
|--------------------------------|--------------------------|
| Contract Awarded: | May 21, 2013 |
| Notice to Proceed: | June 17, 2013 |
| Substantial Completion: | February 10, 2018 |
| Contract Award Value: | \$294,030,590 |
| Modifications to Date: | \$90,000 |
| Current Contract Value: | \$294,120,590 |

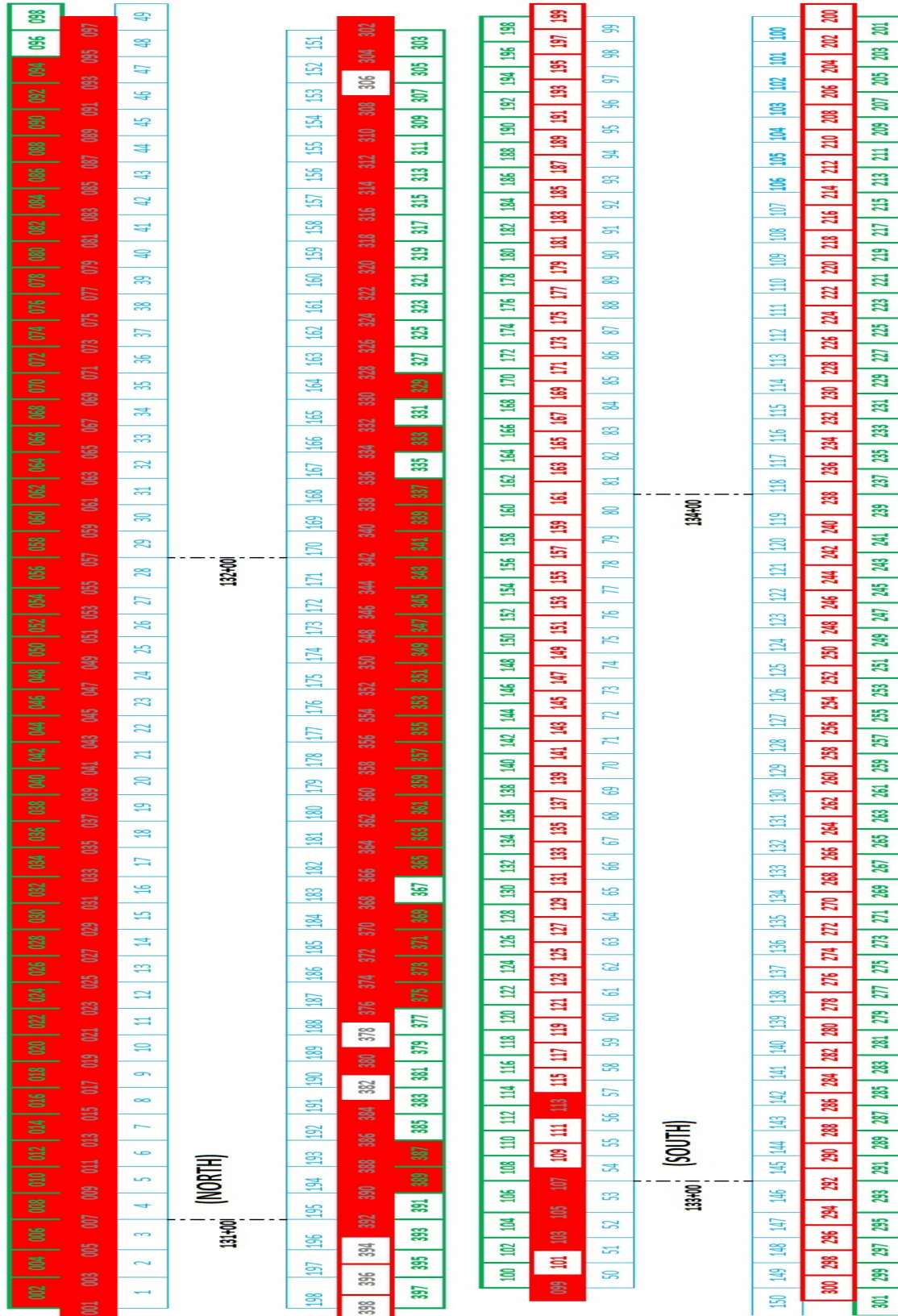
| Budget/Expenditures ▲ | |
|-----------------------------|----------------------|
| Current Budget | \$299,030,590 |
| Expenditures to Date | \$120,861,345 |

UMS Three Month Schedule



UMS North End and South End Jet Grouting

Jet Grouting - 167 of 398 Completed



Yerba Buena/Moscone Station

Contract 1300 - Work Package 1255



Description of Work

This Work Package is to construct one subway station. Includes station finishes, AC and DC Traction Power substations elevators, escalators, lighting, emergency ventilation fans, HVAC fire alarm/ suppression/ protection, slurry wall top-down construction, settlement monitoring, building protection, PA, CCTV, signage, installation of fare collection equipment and station start-up and commissioning.

Current Status

- Traffic flowing on west side of 4th Street with east side work area closed for roof and utility construction
- East side surface slab construction is in progress (three segments, TPC sections 4, 5, 6 south to north) – backfill of sections 4 and 5 is complete with additional utility installation pending, rebar installation in section 6 is in progress
- Backfill over portion of headhouse beneath Clementina Street is in progress
- Utility installation at 4th and Howard (sewer mains) and at 4th and Folsom (culvert) is in progress
- Headhouse temporary bracing strut level 1 installation is in progress
- Archaeological monitoring by spot checking is in progress

Work Expected Next Month

- Complete east side surface slab construction (three segments, TPC sections 4, 5, 6 south to north), by concrete placement in section 6 and waterproofing
- Complete utility installation on east side of station box roof
- Continue Installation of temporary bracing in headhouse excavation



- Continue headhouse excavation and breakthrough beneath station roof on 4th Street

Three Month Look Ahead

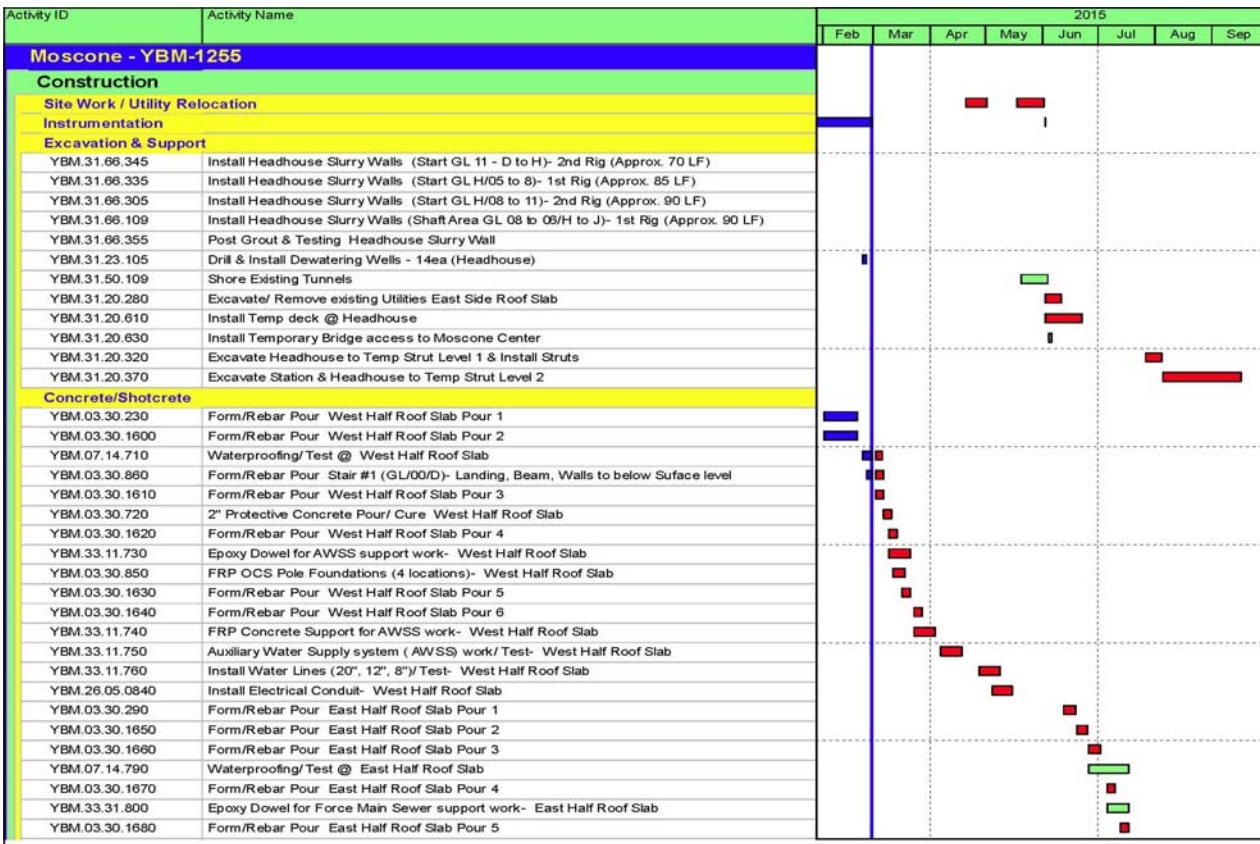
- East side station box roof slab concrete placement
- Utility Installation above east side station box roof slab
- Restoration of traffic lanes on 4th Street
- Continuation of headhouse excavation, install excavation temporary bracing
- Archaeological Monitoring
- Utility installation at intersections of 4th Street with Howard and Folsom Streets

Yerba Buena Moscone Station Construction - Continued

| Contract Details | |
|--------------------------------|--------------------------|
| Contract Awarded: | May 21, 2013 |
| Notice to Proceed: | June 17, 2013 |
| Substantial Completion: | February 10, 2018 |
| Contract Award Value: | \$158,089,000 |
| Modifications to Date: | (\$1,833,869) |
| Current Contract Value: | \$156,255,131 |

| Budget/Expenditures ▲ | |
|-----------------------------|----------------------|
| Current Budget | \$163,089,000 |
| Expenditures to Date | \$70,261,970 |

YBM Three Month Schedule



Systems, Trackwork, & Surface Station

Contract 1300 - Work Package 1256

Description of Work

This Work Package is to construct one Surface Station. Includes light rail track and systems, track invert, track safety walkways; light rail track and systems constructed on the 2,000 foot surface for the alignment from the tunnel portal, south to the tie-in to the existing Muni T-Line at Fourth and King Streets; and the surface Fourth and Brannan Street (FBS) Station.

Current Status

- Continue Muni ductbank installation
- Continue 78" sewer rehabilitation
- Continue 36" sewer force main
- Started Auxiliary Water Supply System (AWSS) installation

Work Expected Next Month

- Continue 78" sewer rehabilitation
- Continue MRY ductbank and vault installation
- Continue 36" sewer force main
- Continue AWSS installation
- Start waterline installation



Three Month Look Ahead

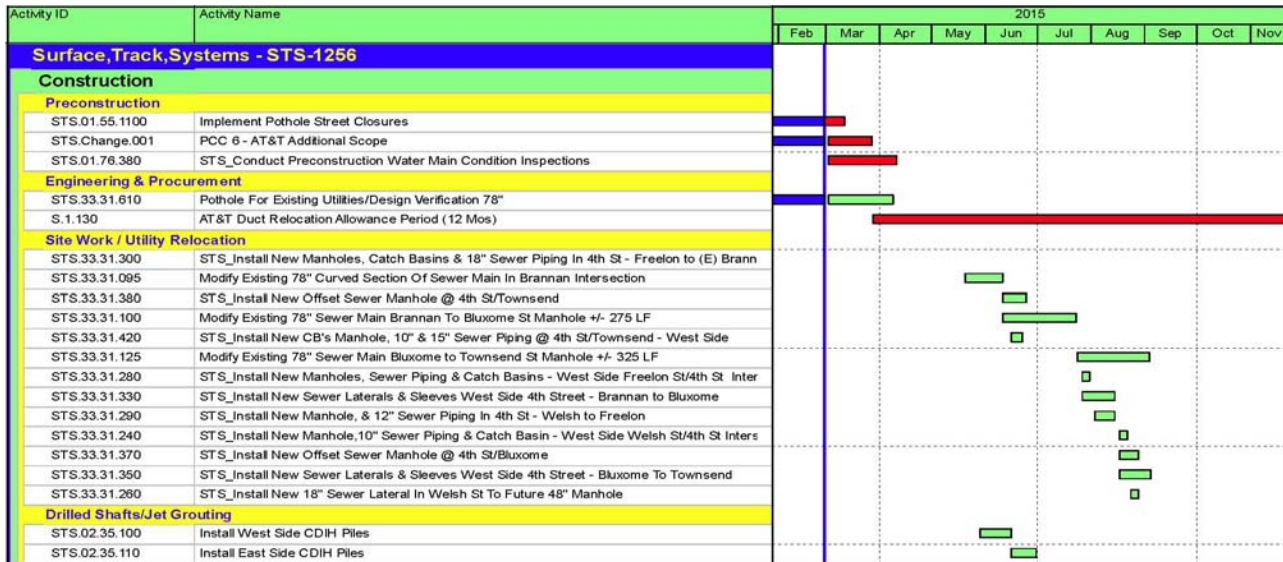
- Waterline installation
- AWSS installation
- Muni ductbank installation
- 36" sewer force main installation
- 78" Sewer rehabilitation
- Start 4th/King Special Trackwork

Systems, Trackwork, & Surface Station Construction - Continued

| Contract Details | |
|-------------------------|-------------------|
| Contract Awarded: | May 21, 2013 |
| Notice to Proceed: | June 17, 2013 |
| Substantial Completion: | February 10, 2018 |
| Contract Award Value: | \$139,989,000 |
| Modifications to Date: | \$80,170 |
| Current Contract Value: | \$140,069,170 |

| Budget/Expenditures | |
|----------------------|---------------|
| Current Budget | \$144,989,000 |
| Expenditures to Date | \$23,365,439 |

Systems, Track and Surface Station Three Month Schedule



Program Components

Community Outreach

Outreach public information, events and presentations for July 2015 include:

Developing outreach information and messaging for construction at 4th & King

Prepare and disseminate construction notice for dewatering well and outreach to Chinatown businesses and residential buildings along 800 & 900 block of Stockton Street

Conducted meetings with Hotel Utah Bar and Grand Nightclub regarding issues on 4th Street between Bryant and Welsh

Update FTA quarterly presentation

Outreach in Support of Mitigation and Monitoring

Team members participated in weekly progress and traffic meetings to address neighborhood concerns

Outreach and communication efforts continue in Chinatown, Union Square, and SOMA

Weekly photo documentation of project work and editing

Weekly construction update emails sent to list of approximately 600 residents and stakeholders
Distributed monthly construction update to STS and CTS neighborhood,; CTS flier are bi-lingual

Media Coverage

| Central Subway Media Coverage | | | |
|-------------------------------|--|--------------------|-----------------|
| Date | Title (with link to story) | Source | Reporter/Writer |
| 07/07/15 | Kilroy Realty Acquires Fully Entitled Development Site | Marketwatch | unknown |
| 07/16/15 | Chinese Cultural Foundation Unveils Public Art Installation Celebrating Central Subway Coming to Chinatown | PRWEB | Kimberly Stenk |
| 7/30/15 | S200 SF Light Rail Vehicle. San Francisco. California. United States of America | Railway-Technology | unknown |

Quality Assurance

Quality Assurance monthly activity of oversight, surveillance, audits, proactive feedback and QA records actively involves the Project construction management staff, the resident engineers, the prime construction contractor and their subcontractors.

Stations and Systems Contract CN1300 Quality Assurance Monitoring – On Going

- UMS roof beam to pile WF installation – Installation of steel is proceeding as joint details are painstakingly reviewed for approval, through numerous meetings and subsequent revised/supplemental erection submittals – primarily only fit-up (tack welding) was performed this month
- UMS Water Proofing Subcontractor – Waterproofing Experts
- UMS Jet Grout Subcontractor, continuation of Jet Grout
- YBM Slurry Wall Panels to roof slab and subsequent interior slabs
- YBM Water Proofing Subcontractor – Waterproofing Experts
- Daily Inspection Reports
- Smith Emery - TPC's laboratory and Inspection Services – including Special Inspections)
- Preparatory, Initial and Additional Initial Phase Meetings as required by Specification Section 01 45 00. Attendance/participation of these vital meetings (Contractor is not allowed to start work until such time that all prerequisite requirements have been met which continues to greatly facilitates all work being done to the strict requirements of the Contract Documents, the “first time”)
- Bi-Weekly (resumed as a result of the last partnering meeting) and ad hoc Quality Task Force Meetings are attended primarily to assist and support the Contractor's QC effort, when conducted
- Daily Instrumentation/Monitoring and Construction Management Task Force (CMTF) Meeting
- Progress Meetings for C1300, STS, YBM, UMS and CTS when conducted and as time constraints allow

Document comment and review:

- Contractor's submittals, e.g., review of welding, concrete and other Quality related submittals/comments as requested to support the RE's and CM, and RFIs related to quality
- QA Staff continues random/spot checks of the 1300 Contractor's Field Testing lab results; items requiring further action/investigation (missing or inconsistent data, compressive strength results that appear to have a very broad range of values and such) are brought to the attention of the Contractor
- Review of updated CNCR logs and CNCRs that provide guidance to REs regarding content, accuracy of the disposition and confirmation close-out is correct – TPC QC has been asked to correct the CNCR Log that will result in accurate descriptions and status of each CNCR. TPC QC is to fulfill the 24 hour initial generation of a CNCR upon becoming aware of a non-

Quality Assurance - Continued

conforming condition, through the issuance of the “Initial Issue” of each CNCR. The Initial Issue of each CNCR will provide the “what” and “where” but not necessarily the “why” or how to prevent repeating CNCRs. The accuracy and completeness of the CNCR Log continues to be a subject of discussion at each C1300 Progress Meeting and is tracked through the associated Meeting Minutes. TPC QC is now posting CNCRs to CM13, using the Noncompliance module of CM13. Currently, existing (completed by “hand”) CNCRs are posted as attachments, eventually, TPC QC will utilize CM13 to generate/process each CNCR which will mitigate the log accuracy/timeliness issues.

QA Issues:

- TPC’s Program Executive has issued CAR #4 Revisions 3, the issue of TPC Management not honoring the QCM’s authority to Stop Work remains unresolved. Note that the 01 45 00 *Contractor Quality Control* Specification Section requires that the CQM has the ability to issue SWNs). CAR #4 is included in Quality Assurance Audit (QAA) QAA 025, which was posted to the EDMS and transmitted to the Contractor for responses to six (6) findings.

QA Concerns:

- TPC QCM not having the authority to exercise Stop Work to prevent non-conforming work from being incorporated into the permanent Work. (CAR #4)
- Necessity of using both Reinforcing Steel Design Drawings and approved Reinforcing Steel Shop Drawings to inspect/accept rebar placement



Quality Assurance - Continued

- Incomplete/confusing shop drawing submittals for UMS structural steel resulting in QC and or QA stopping TPC from making welded connections upon discovery that approved details are missing, continues
- Welding inspection and associated documentation of CWI acceptance of all welded joints, including tack welding, at UMS. Numerous meetings have been held with the result being that TPC QC/Smith Emery will create a spreadsheet that will facilitate the verification that all welds to be embedded in concrete have been CWI accepted or CNCRs generated and closed, prior to final sign-offs on each concrete placement
- Use of CM13 as the document depository related to a user having the ability to delete submittal attachments after a submittal has been approved without any record or trail of the deletion (this was a finding included in QAA 025 Audit Report)
- Contractor's Quality Control Manager's (CQM) attendance

Other Program QA Practices Implemented

- Close-out of Corrective Action Requests: Close outs continued from Quality Assurance staff's Audits, Surveillances and PMOC Quarterly Reviews. The status is tracked in the Corrective Action Log that is provided to the project team and the FTA PMOC
- On-going indoctrination to the CSP Quality Program continues on a one-to-one basis as new personnel are added to the PM/CM Staff.
- Completed and posted to the EDMS Quality Assurance Audit QAS 025 of the implementation and effectiveness of some components of the C1300 Contractor's approved Contractor Quality Program (CQP). The Contractor's response to the 6 audit findings should be provided mid-August 2015
- Revision 11 of the Contractor's CQP was reviewed with an approval status of Reject and Re-submit due to revisions made that are not per the requirements stated in Specification 01 45 00, Quality Control related to Contractor Non Conformance Reports (CNCRs). Revision 12 of the CQP is anticipated to be submitted for approval mid-August 2015
- Quality Assurance Surveillance's QAS 064 - TPC's performance of audits, QAS 066 CWI's performing welding inspection acceptance with required documents at UMS, QAS 067 mill certification for structural steel at UMS, and QAS 068 receipt inspection, were posted to the EDMS and transmitted to the Contractor for action/information

Quality Assurance - Continued

Risk Management

Risk Mitigation Management Meeting No. 72 was held on July 9, 2015. The Risk Assessment Committee reviewed and discussed Risks that include Construction Risks with ratings above 6; Remaining Requirement and Design Risks; and, any New Risk Assessment and Mitigations identified to date.

Program Safety & Security

The San Francisco Municipal Transportation Agency is committed to the highest practical level of safety and security standards and practices in the public transit industry. The Safety and Security Management Plan (SSMP) components are reported on below as appropriate including, Safety and Security Committee, the Fire Life Safety and Security Committee the Construction Conformance Verification and Documentation and Contractor Safety and Security.

Safety Summary for Stations Systems Track Construction

July was an interesting month in that the local Cal-OSHA enforcement office showed up on the project. There now appears to be some territorial issues between the local Cal-OSHA office and Mining & Tunneling.

UMS did not have any recordable or first aid incidents. SFMTA requested that air sampling be conducted in the Union Square Garage. Samples were taken for particulates and for lead. TPC is awaiting the results of the testing.

YBM did not have any recordable or first aid incidents. Efforts at this station are focusing on getting 4th street back into full operation by Labor Day. They are also starting work at the head house performing excavation.

CTS continues excavating the head house area. Layne is starting to mob in and will perform compensation grouting in late August. Viking has been drilling wells at night. It is a very noisy operation. SFMTA has handed out ear protection (plugs) to the local residents to try and help until this task is completed.

CTS continues excavating the head house area. Layne is starting to mob in and will perform compensation grouting in late August. Viking has been drilling wells at night. It is a very noisy operation. SFMTA has handed out ear protection (plugs) to the local residents to try and help until this task is completed.

Table 1300 Stations Construction Safety Record

Table 1300 below summarizes the Month to Date and Project to Date for the Stations, Systems and Track Construction contractor and subcontractors. Table 1300 shows that no recordable accidents took place in July for the work underway at the station sites and surface line.

Next Month Look Ahead

1300 Contract

Program Safety & Security - continued

1. AT CTS, additional excavation down will allow for the installation of the stairway for access purposes.
2. UMS station is seeing the beginning of the demolition of the eastern portion of the Union Square Garage. Close monitoring of the air will be done to ensure no hazardous substances leave the work zone area. This will continue. Additional attention will be given to all underground work at UMS – North, Central and South.
3. AT YBM, we will closely monitoring traffic controls. They are close to completing work on 4th street. Excavation is taking place at the head house and close monitoring will be done.
4. The critical work at STS this month is the closure at 4ht and King Streets. Close supervision will be conducted.

Project Safety Record - Contract 1300

SAFETY GOALS

OSHA Recordable Accidents, <3.4
Lost Time Cases, <1.6

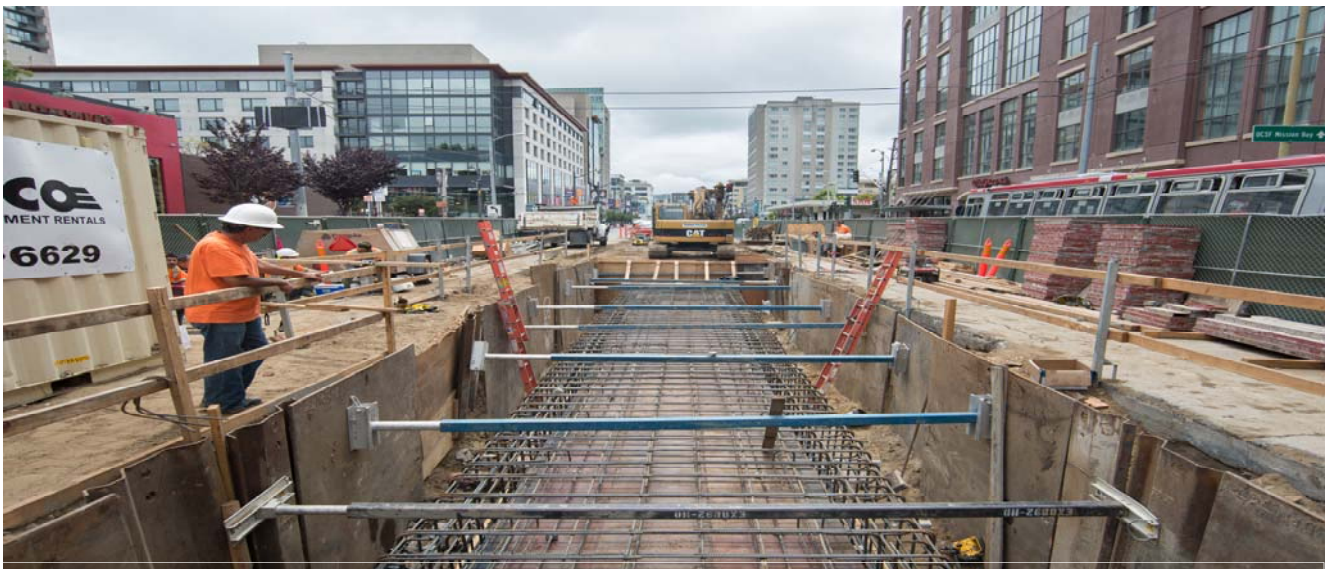
Through Month End -July 2015

| JOB TO DATE | Tutor | Subs | Total Project | Rate* |
|--|---------|---------|---------------|-------|
| OSHA Recordable Accidents | 1 | 0 | 1 | 0.32 |
| Job Transfer or Restricted Duty Cases | 0 | 0 | 0 | 0.00 |
| Lost Time Cases | 0 | 0 | 0 | 0.00 |
| Total Project Incidents | 1 | 0 | 1 | 0.32 |
| Man Hours Worked Through M/E July 2015 | 252,195 | 377,220 | 629,415 | |

| YEAR TO DATE (Month ,Day, Year to Month, Day, Year) | Tutor | Subs | Total Project | Rate* |
|--|---------|---------|---------------|-------|
| OSHA Recordable Accidents | 1 | 0 | 1 | 0.84 |
| Job Transfer or Restricted Duty Cases | 0 | 0 | 0 | 0.00 |
| Lost Time Cases | 0 | 0 | 0 | 0.00 |
| Total Project Incidents | 1 | 0 | 1 | 0.84 |
| Man Hours Worked Through M/E July 2015 | 116,425 | 121,003 | 237,428 | |

* Rate is calculated based on number of incidents divided by total number of man hours worked multiplied by 200,000 man hours.

OSHA Recordable Accidents - 2008 Construction Industry Rate for Highway, Street, and Bridge Construction = 3.9



A worker inspects rebar work completed ahead of pouring concrete on 4th Street near Bluxome. This work is to reinforce a 78” sewer so it may withstand the weight of trains passing overhead.

Technical Capacity

No Project positions are currently open or unfilled.



Dust is kept down during floor slab demolition inside the Union Square Garage by spraying a light mist of water onto material being broken apart.

Staffing

The Central Subway Staffing Table shows Planned and Actual full-time equivalent staff (FTEs) working on the Program by organizational function and responsibility.

| | May-2015 | | Jun-2015 | | Jul-2015 | |
|--------------------------------|--------------|--------------|--------------|--------------|--------------|--------------|
| | Planned | Actual | Planned | Actual | Planned | Actual |
| Project Management | | | | | | |
| Program Management | 7.10 | 6.00 | 7.10 | 6.00 | 7.10 | 6.00 |
| Quality Assurance | 1.80 | 2.30 | 1.80 | 2.30 | 1.80 | 2.30 |
| Contract Administration | 1.40 | 1.40 | 1.40 | 1.40 | 1.40 | 1.40 |
| Community Outreach | 5.50 | 5.00 | 5.50 | 5.10 | 5.50 | 5.10 |
| Finance | 2.00 | 2.00 | 2.00 | 2.00 | 2.00 | 2.00 |
| Project Controls | 5.80 | 6.10 | 5.80 | 6.10 | 5.80 | 5.80 |
| Subtotal | 23.60 | 22.80 | 23.60 | 22.90 | 23.60 | 22.60 |
| Construction Management | | | | | | |
| CM - CN 1252 | 4.00 | 2.00 | 4.00 | 2.00 | 1.00 | 1.00 |
| CM - CN 1300 | 30.00 | 27.70 | 30.00 | 28.20 | 30.00 | 30.00 |
| Design Support - CN 1252 | 0.90 | 0.10 | 0.90 | 0.10 | 0.00 | 0.10 |
| Design Support - CN 1300 | 12.40 | 12.00 | 12.40 | 12.00 | 12.40 | 12.00 |
| Subtotal | 47.30 | 41.80 | 47.30 | 42.30 | 43.40 | 43.10 |
| Start Up | | | | | | |
| Start Up / Safety & Security | 3.00 | 2.10 | 3.00 | 2.10 | 3.00 | 2.10 |
| Subtotal | 3.00 | 2.10 | 3.00 | 2.10 | 3.00 | 2.10 |
| Total | 73.90 | 66.70 | 73.90 | 67.30 | 70.00 | 67.80 |

Third-Party Agreements

In January 2015, the court issued a decision on the amount payable to the property for the single outstanding access license, for compensation grouting, at 19 Stockton Street, adjacent to the Union Square / Market Street Station. The City Attorney's Office is working with Project Staff to release payment of the Court ordered amount to the property owner.

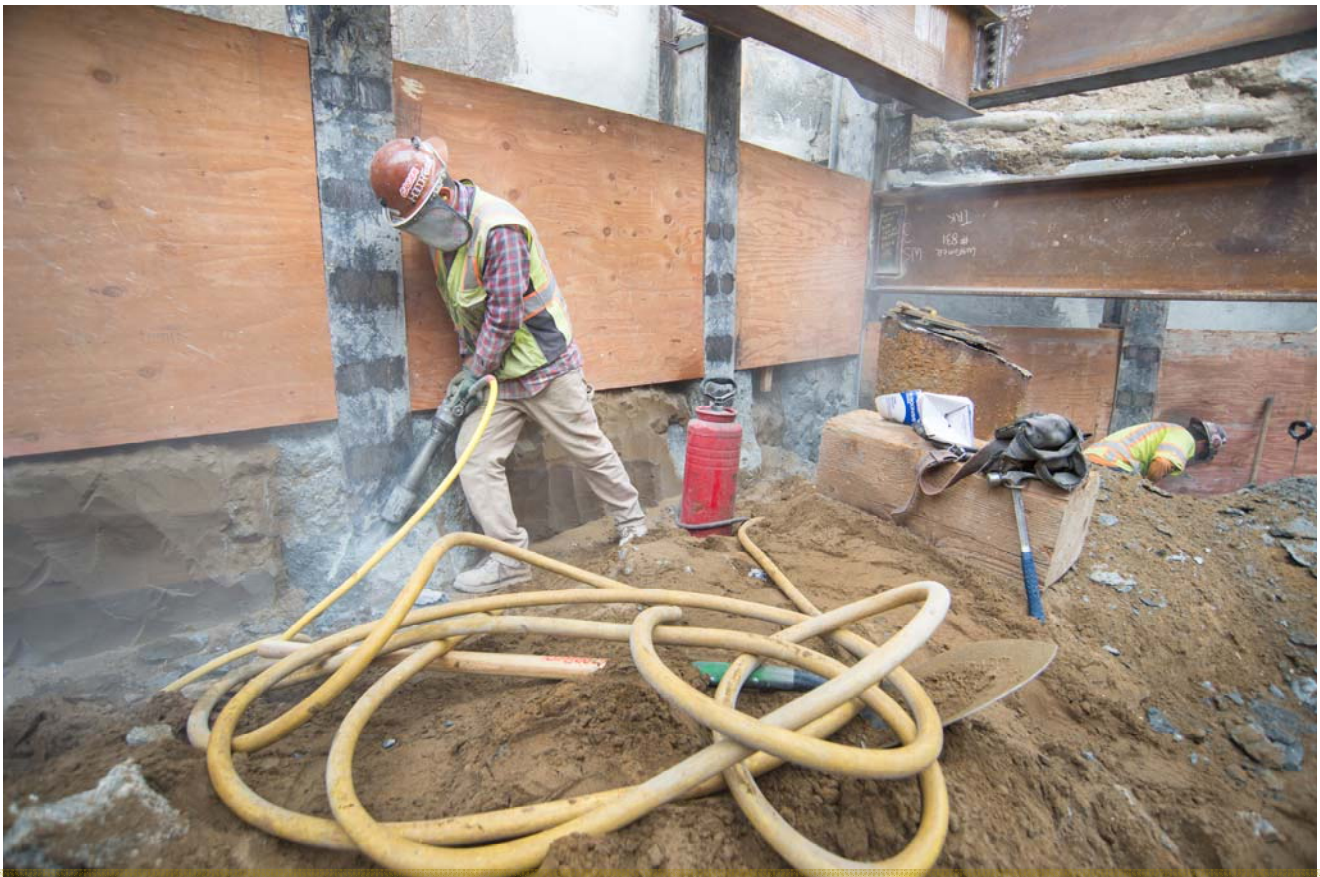
Installation of instrumentation within the building is complete. Installation of the grout tubes commenced late June and continued into July. Project staff continue to keep the Property Owner informed of the status of the work.

LRV Procurement

The SFMTA light rail vehicle procurement continues to schedule the 24 LRVs that will be part of the Central Subway start up to be ready for service testing and training by mid 2018 in order to begin use at the start of Central Subway service in December 2018.

The SFMTA Board authorized award of the contract on July 15, 2014 to Siemens for up to 260 cars, which includes a base contract of 175 cars for a total of \$648 million, with an option to acquire up to 85 more, bringing the overall total to 260 cars. On September 19, 2014 Mayor Edwin Lee approved the contract.

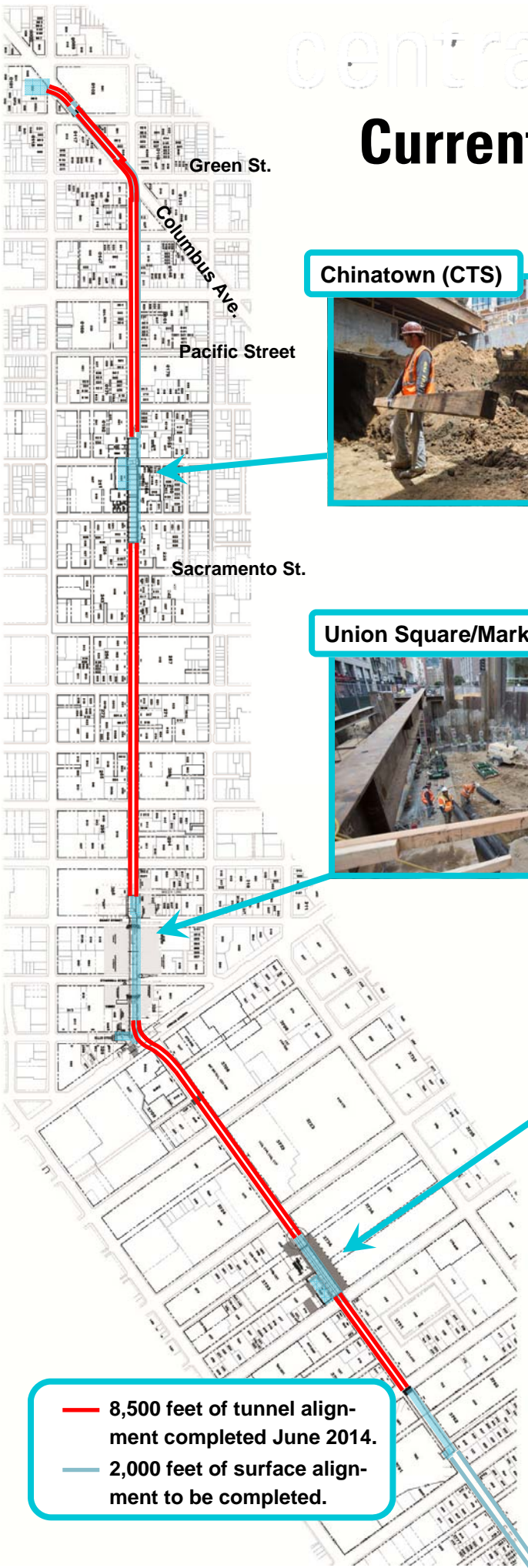
LRV task date/month milestones will be further developed and integrated into CS Master Project Schedule, the Rail Activation Plan and the startup schedule in 2015.



A worker uses a small jackhammer to remove concrete from around a vertical column inside the south concourse of the Union Square/Market Street Station.

Central T subway

Current Construction Activity



Chinatown (CTS)



Union Square/Market Street (UMS)



Yerba Buena/Moscone (YBM)



4th St. Surface Track, Systems (STS)





A bulldozer brings out soil and other materials removed from under the Chinatown Station roof slab, while an excavator pulls it up and fills a waiting dump truck.



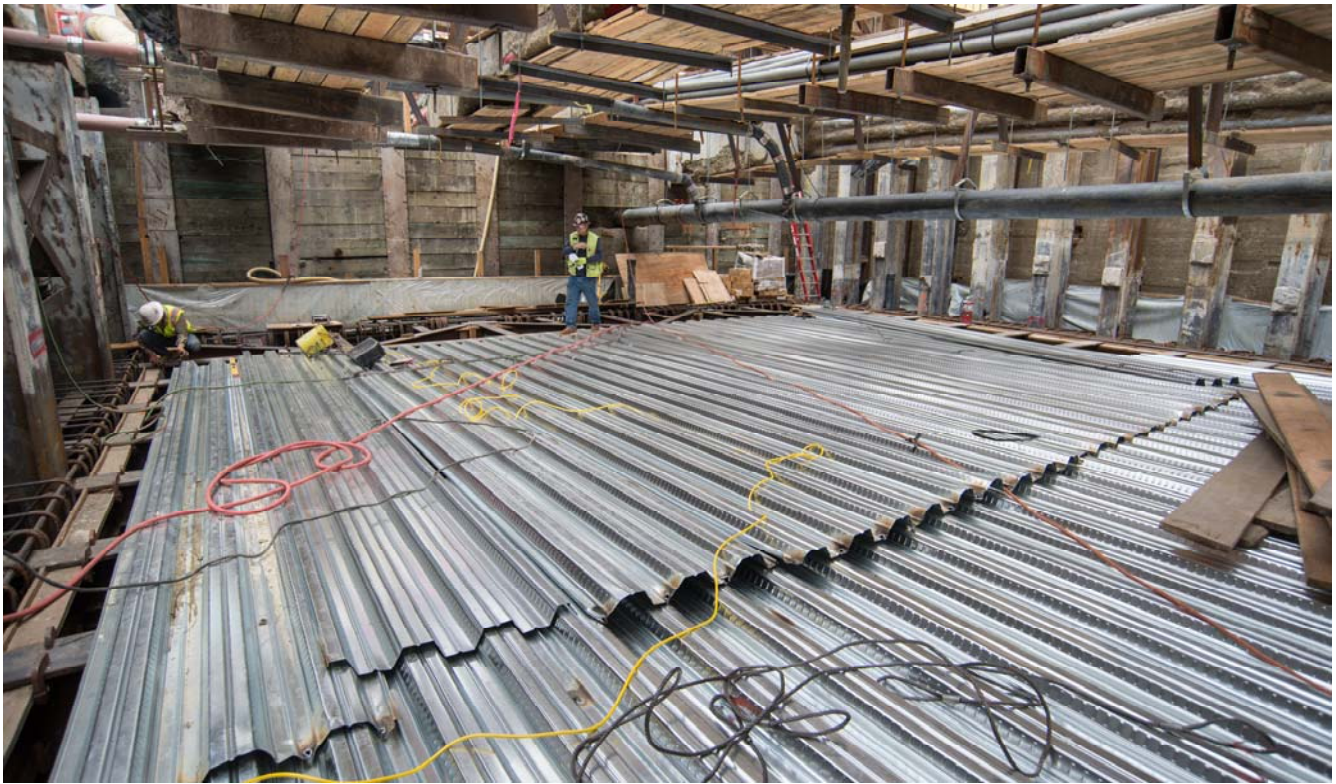
A worker removes boards left over from concrete forms under the roof slab.



After the first level of excavation was completed under the roof slab, workers then removed what concrete forms had been exposed and left behind.



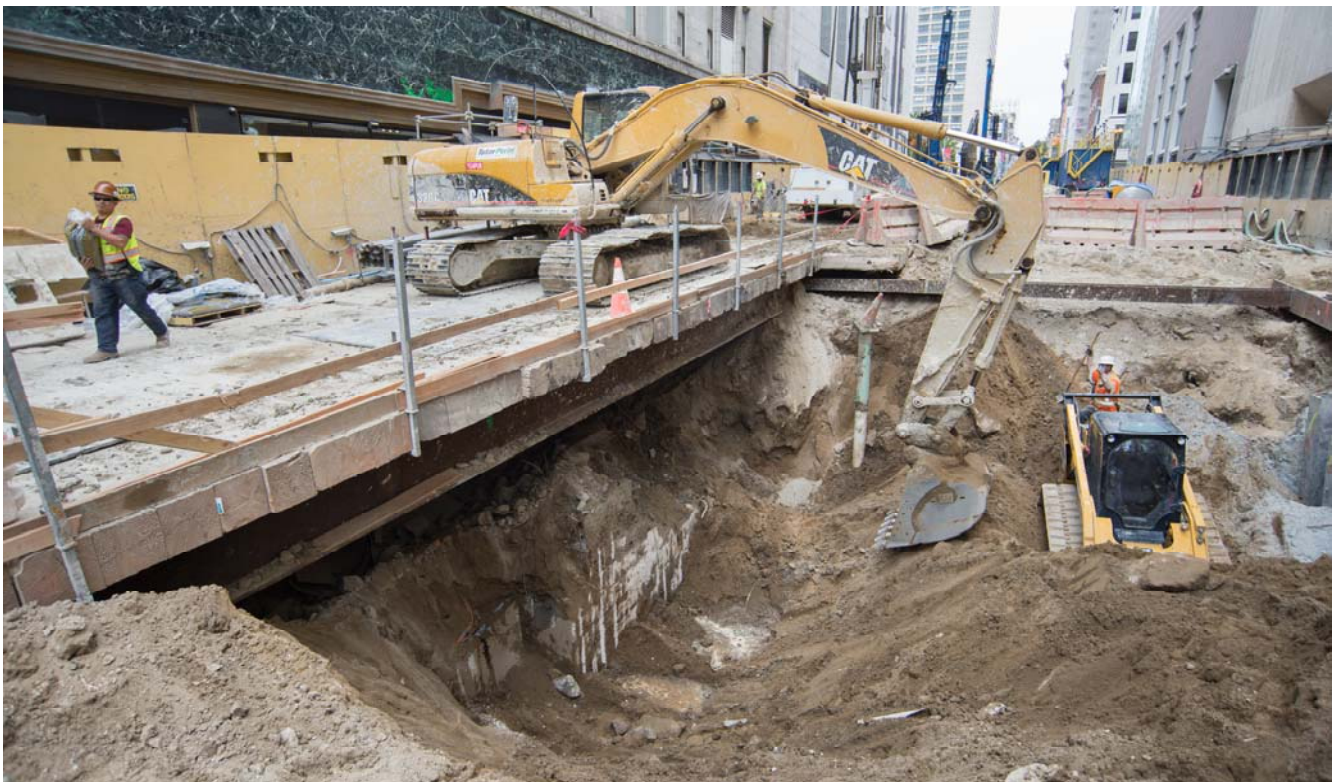
Workers begin installation of a sewer line on the south side of the intersection at Geary and Stockton prior to station concourse roof installation.



Following the installation of structural beams, workers install corrugated metal sheets before laying rebar and pouring concrete for the concourse roof deck.



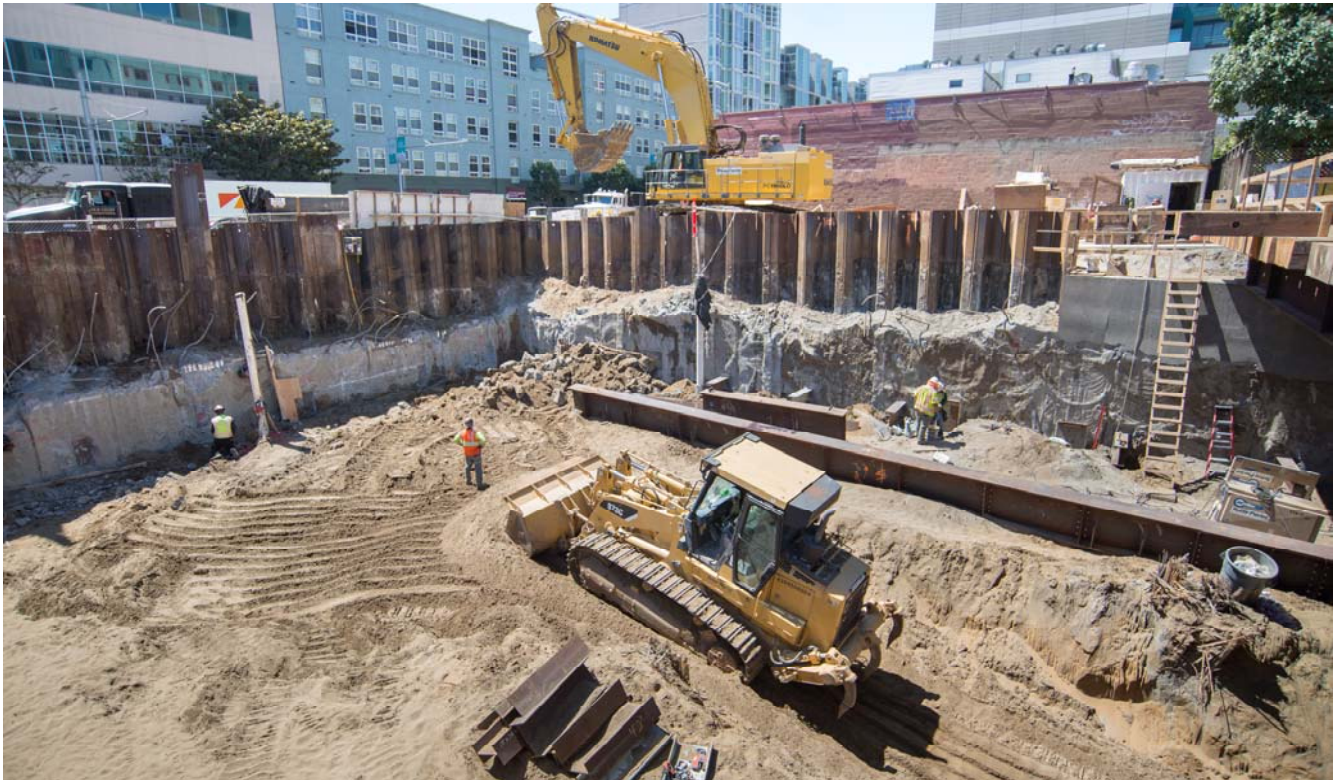
Portions of the southeast corner of the Union Square Garage are being demolished, prior to the construction of the north station entrance, there.



A temporary steel and wooden “bridge” has been constructed over an area of excavation north of O’Farrell on Stockton where a section of roof deck is being constructed.



An excavator waits while the last section of roof deck for the station platform structure is being constructed in the background.

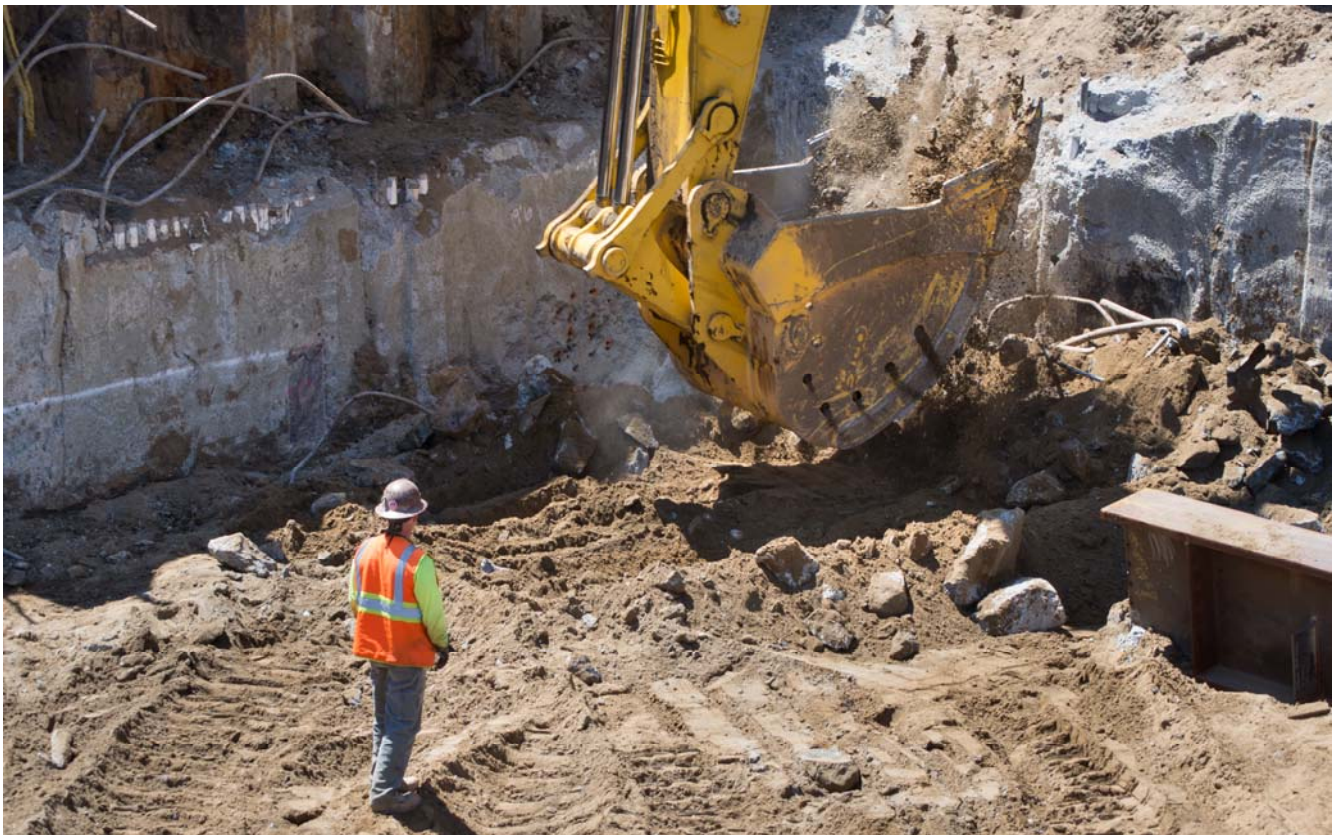


A bulldozer pushes excavated soil to a transfer pile at the southwest corner of the work site. This excavation will eventually become the station headhouse.

YBM - continued



Steel supports called “walers” are temporarily installed during headhouse excavation.



A worker observes an excavator bucket shake out loose soil separating it from concrete removed from the headhouse excavation site.

STS



Sewer pipes waiting to be installed overlook utility work near the intersection of 4th and King.



Trenching work on 4th Street just north of King, temporarily covered by steel plates.

STS—Continued



Sheet pile installation for trenching operations just north of Welsh Alley on 4th Street in SoMa.



An exposed rebar cage prior to pouring concrete for a sewer upgrade on 4th Street south of Bluxome.

Appendix A

DETAIL COST REPORTS

1. PROJECT COST

The Current Cost Estimate (CCE) for the Central Subway Project is **\$1.578 billion** in year of expenditure dollars (\$YOE). This total project cost is shown at the top of Report 7.1, Program Project Budget. This capital cost projection incorporates allocated and unallocated contingencies to cover the risks associated with the project completion.

Total net incurred costs for the project are \$818.77 million, a \$12.39 million increase over last month. The cost to date figure reflects expenditures through FAMIS 786 Report (\$775.66 million) plus the utilities joint trench Form B Reimbursement payment (\$10.03 million), invoices currently being processed (\$20.03 million) and estimates of outstanding pay requests (\$13.06 million). This incurred amount equals 51.88% of the total project budget of \$1.578 billion.

The current funding level to date is \$1,029.79 million. This represents 65% of the total project budget.

| CONTRACT | PP NO | PP PERIOD TO | PROG PYMT AMOUNT | CONTRACT | PP NO | PP PERIOD TO | PROG PYMT AMOUNT |
|----------|-------|--------------|------------------|-----------------|-------|--------------|-------------------------|
| CS155.1 | 52 | 3/31/2014 | \$ 35,640.00 | CS155.2* | 65 | 7/31/2015 | \$ 196,468.00 |
| CS155.1 | 53 | 4/30/2014 | \$ 20,527.00 | CS155.3 | 60 | 3/31/2015 | \$ 71,356.82 |
| CS155.1 | 54 | 5/31/2014 | \$ 30,702.00 | CS155.3 | 61 | 4/30/2015 | \$ 63,948.37 |
| CS155.1 | 55 | 6/30/2014 | \$ 21,712.00 | CS155.3 | 62 | 5/31/2015 | \$ 65,130.00 |
| CS155.1 | 56 | 7/31/2014 | \$ 38,474.00 | CS155.3* | 63 | 6/30/2015 | \$ 53,964.00 |
| CS155.1 | 57 | 8/31/2014 | \$ 42,393.00 | CS155.3* | 64 | 7/31/2015 | \$ 47,059.00 |
| CS155.1 | 58 | 9/30/2014 | \$ 39,232.00 | Contract 1252 | 38 | 3/31/2015 | \$ 1,386,601.62 |
| CS155.1 | 59 | 10/31/2014 | \$ 36,256.00 | Contract 1252 | 39 | 4/30/2015 | \$ 1,174,889.06 |
| CS155.2 | 60 | 11/30/2014 | \$ 22,204.00 | Contract 1252 | 40 | 5/31/2015 | \$ 236,920.00 |
| CS155.1 | 61 | 12/31/2014 | \$ 19,820.00 | Contract 1300 | 17 | 5/31/2015 | \$ 7,965,803.00 |
| CS155.1 | 62 | 1/31/2015 | \$ 20,970.00 | Contract 1300 | 18 | 6/30/2015 | \$ 5,976,041.00 |
| CS155.1 | 63 | 2/28/2015 | \$ 19,070.00 | Contract 1300* | 19 | 7/31/2015 | \$ 11,231,362.55 |
| CS155.1 | 64 | 3/31/2015 | \$ 16,373.00 | CS149 | 76 | 5/31/2015 | \$ 483,285.82 |
| CS155.1 | 65 | 4/30/2015 | \$ 16,369.00 | CS149* | 77 | 6/30/2015 | \$ 680,009.51 |
| CS155.1 | 66 | 5/31/2015 | \$ 2,121.00 | CS149* | 78 | 7/31/2015 | \$ 650,000.00 |
| CS155.1* | 67 | 6/30/2015 | \$ 1,180.00 | CS156.1 | 48 | 1/31/2015 | \$ 81,713.15 |
| CS155.1* | 68 | 7/31/2015 | \$ 1,181.00 | CS156.1 | 49 | 2/28/2015 | \$ 79,068.45 |
| CS155.2 | 60 | 2/28/2015 | \$ 124,730.00 | CS156.1 | 50 | 3/31/2015 | \$ 87,287.88 |
| CS155.2 | 61 | 3/31/2015 | \$ 152,981.50 | CS156.1* | 51 | 4/30/2015 | \$ 78,239.81 |
| CS155.2 | 62 | 4/30/2015 | \$ 200,556.00 | CS156.1* | 52 | 5/31/2015 | \$ 62,700.74 |
| CS155.2 | 63 | 5/31/2015 | \$ 191,448.00 | CS156.1* | 53 | 6/30/2015 | \$ 78,665.12 |
| CS155.2* | 64 | 6/30/2015 | \$ 196,467.00 | CS156.1* | 54 | 7/31/2015 | \$ 53,950.08 |
| | | | | other accruals* | | 7/31/2015 | \$ 404,131.32 |
| | | | | | | | \$ 33,085,343.74 |

* Estimated Amount

2. CONTINGENCY ALLOCATIONS AND USAGE

The current Total Project Contingency is **\$84.74 million**, which is a \$24.74 million favorable balance against the current Minimum Contingency level of \$60 million. The Contingency Drawdown Curve is shown in Report 7.3. Follows by Report 7.4 Contingency Management Trend Report with the Remaining Contingency after Approved Changes Deducted contingency items in column "i".

In this reporting period, Contract 1252 Tunnel and Contract 1300 Stations had not processed any contract modifications. Refer to Report 7.5 for approved contract modifications and potential changes.

3. BUDGET TRANSFERS

No budget transfers in this reporting period.

4. FORM B

The Utilities Joint Trench Form B Details is listed in the Table A2 below. Total utilities joint trench Form B Reimbursement payment to three construction contracts is \$10.03 million.

| TABLE A2: UTILITIES JOINT TRENCH FORM B DETAILS* | [A] Apr 2015 BUDGET | [B] EXPENDED TO DATE | Associated Cost Account |
|---|---------------------------|----------------------------|---|
| 1.3.491.07.040.02 - FORM B - CN1250 UTILITY REIMBURSEMENT | (2,275,419) | 2,463,325 | 1.3.081.07.040.02 - 1UTL:SITWORK: UTILITIES & RELOC |
| 1.3.491.08.040.02 - FORM B - CN1251 UTILITY REIMBURSEMENT | (7,618,412) | 3,608,217 | 1.3.082.08.040.02 - 2UTL:SITWORK:UTILITIES&RELOCATE |
| 1.3.491.02.040.02 - FORM B - CN1252 UTILITY REIMBURSEMENT | (254,050) | 3,958,658 | 1.3.083.02.040.02 - TUNN:Sitework:Utilities & Relocate |
| 1.3.491.04.040.02 - FORM B - CTS: CN1300 UTILITY REIMBURSEMENT | (451,703) | | |
| 1.3.491.09.040.02 - FORM B - STS: CN1300 UTILITY REIMBURSEMENT | (1,000,000) | | |
| 1.3.491.03.040.02 - FORM B - UMS: CN1300 UTILITY REIMBURSEMENT | (528,370) | | |
| 1.3.491.05.040.02 - FORM B - YBM: CN1300 UTILITY REIMBURSEMENT | (100,000) | | |
| TOTAL | (12,227,954) | 10,030,200 | |

Note: *Revised Form B Reimbursements SCC code from 900.01 to 040.02.

5. EARNED VALUE (EV) ANALYSIS

In July 2015 Report, Central Subway Project Master Schedule has re-baselined construction phase and adopted Contract 1300 Station contractor baseline schedule. In this report, Contract 1300 Station has used accepted February 2015 schedule update and July 2015 actual cost. Earned Value Analysis and Earned Value Cash Flow Curve, Report 7.2 are not included in July Report.

Earned Value Analysis and Definitions

SPI is a measure of schedule efficiency on a project. It is the ratio of earned value (EV) to planned value (PV). A SPI equal to or greater than one indicates more work was completed than planned and a value of less than one indicates less work was completed than planned. A value of less than 0.9 is unfavorable.

CPI is a measure of cost efficiency on a project. It is the ratio of earned value (EV) to actual cost value (AC). A CPI equal to or greater than one indicates a cost under run and a value of less than one indicates a cost overrun. A value of less than 0.9 is unfavorable.

The following earning rules are established for each of the phase:

| Cost Element Group | Planned Value (Primavera) | Earned Value (Primavera) | Actual Cost (SFMTA Cost Accounting (SAP)) |
|----------------------------|--|--|--|
| Prelim. Engineering | Expenditure Plan Level of Effort (LOE) | Equals to Planned Value (LOE) | Time Keeping; Vendor Accruals and Invoices |
| Final Design | Expenditure Plan Level of Effort (LOE) | Equals to Planned Value (LOE) | Time Keeping; Vendor Accruals and Invoices |
| Procurement | Planned Delivery Date | Actual Delivery Date | Time Keeping; Vendor Accruals and Invoices |
| Real Estate | Expenditure Plan Level of Effort (LOE) | Equals to Planned Value (LOE) | Time Keeping; Vendor/ Material Accruals and Invoices |
| Construction | Schedule of Work | % Complete* x Budget at Completion (BAC) | Vendor Accruals and Invoices |
| Sub-Total | Performance Measurement Baseline (PMB) | Total Earned Value | Total Actual Cost |
| Below the Line | + Contingency | | |
| Total | Approved Budget | | |

6. FUNDING SUMMARY

The Funding Available Table below shows the total awarded funds to date vs. the total committed funds from the Project's seven funding sources.

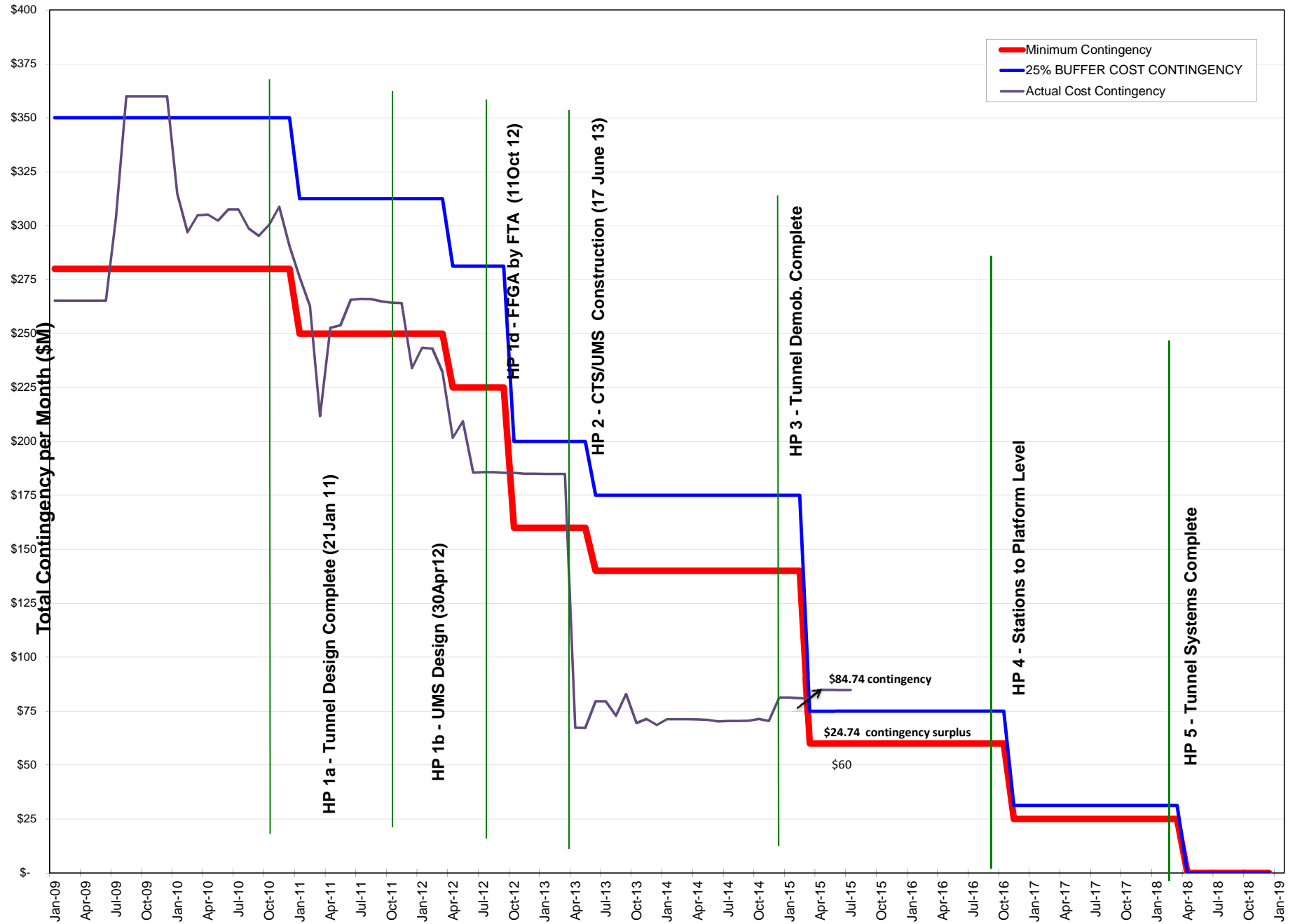
| Funding Available Table | | |
|--------------------------------|----------------------------------|------------------------------------|
| | Funding | |
| | Committed Funding Sources | Total Awarded Funds to Date |
| Federal | | |
| Sect. 5309-NS | \$942,200 | \$469,196 |
| CMAQ | \$41,025 | \$41,025 |
| Federal Subtotal | \$983,225 | \$510,221 |
| State | | |
| TCRP | \$14,000 | \$14,000 |
| State RIP | \$88,000 | \$12,498 |
| Prop. 1B (I-Bond) PTIMSE | \$307,792 | \$307,792 |
| Prop. 1A (HSR-Bond) | \$61,308 | \$61,308 |
| State Subtotal | \$471,100 | \$395,598 |
| Local | | |
| Prop. K | \$123,975 | \$123,975 |
| Local Subtotal | \$123,975 | \$123,975 |
| CPT 544 Total | \$1,578,300 | \$1,029,794 |

7. LIST OF COST REPORTS

- 7.1 Program Project Budget
- 7.2 Earned Value Cash Flow – NOT Issued
- 7.3 Contingency Drawdown Curve
- 7.4 Summary Contingency Management Trend Report
- 7.5 Detail Contingency Usage Report
- 7.6 Budget Revisions: Report sorted by Construction Packages & Soft Costs
- 7.7 Project Budget & Expenditure Report: Sorted by SCC Summary
- 7.8 Budget & Expenditure Report: Sorted by SCC Details
- 7.9 Detail Monthly Expenditure Report: grouped by Project Phase
- 7.10 Cost Report Notes

| A. Central Subway Project | | | | | | |
|---|--|------------------------|---------------|------------------------------|-----------|-------------------|
| Project | Name | Amount | PM | Funding Source | Reporting | Cost Report Notes |
| 1 | CPT544 Central Subway Project | \$1,578,300,000 | J. Funghi | 62% Fed, 30% State, 8% Local | yes | 1 |
| Total: | | \$1,578,300,000 | | | | |
| B. Related SFMTA Capital Improvement Projects | | | | | | |
| Project | Name | Amount | PM | Funding Source | Reporting | |
| 2 | CPT690 TBM Retrieval Shaft Relocation | \$9,700,000 | Funghi/Magary | MTA Operating Funds | no | 2 |
| 3 | CPT718 Chinatown Metro Plaza | \$6,980,000 | J. Funghi | Transbay Redevelopment | no | 3 |
| 4 | CPT665 Central Subway Project - Goodwill | \$2,367,750 | K. Magary | I-Bond Interest | no | 4 |
| 5 | CPT705 MOH - Broadway/Sansome | \$8,000,000 | K. Magary | MTA Operating Funds | no | 5 |
| Total: | | \$27,047,750 | | | | |
| C. Central Subway Project - Project Offset Credits | | | | | | |
| | From | Amount | Index | Notes | Reporting | |
| 1 | 2009-2016 Utility Co. - Form B Reimbursement | \$12,227,954 | -- | Construction contracts | yes | 6 |
| 2 | 2017-2019 PG&E - Power Feed Reimbursement | \$7,624,540 | -- | Not yet bill PG&E | yes | 7 |
| 3 | 6/26/2013 BART Elevator | \$90,000 | 68CPT544135B | Not yet rec'd BART Funds | yes | 8 |
| 4 | 11/6/2013 Tutor Perini - CAD Files | \$2,500 | 68CPT5441236 | Deposit to Design Index | yes | 9 |
| 5 | 1/27/2014 SFPUC - Sewer Main | \$2,925,296 | 68W251 | Certified in Contract 1300 | yes | 10 |
| 6 | 9/27/2014 SFPUC - 24" Water Main | \$328,857 | 68CPT544135A | Contract 1252 CMod #41 | yes | 11 |
| 7 | 8/27/2014 SFMTA Traffic Effectiveness Project funded | \$694,651 | -- | Contract 1252 CMod #40 | yes | 12 |
| 8 | 3/27/2015 SFPUC - 24" Water Main Additional Work | \$112,102 | 68W409 | Contract 1252 CMod #48 | yes | 13 |
| Total: | | \$24,005,900 | | | | |

Cost Contingency Drawdown



| COST ELEMENT | CONTRACT COST | | | | | CONTINGENCY | | | | BUDGET | VARIANCE | Cost Report Notes | |
|--|---|----------------------|------------------------|-----------------------|------------------------------|---|----------------------------------|--|---|--|---------------------------------------|-------------------|----|
| | ORIGINAL CONTRACT VALUE / September 2013 SUPPLEMENTAL BUDGET | APPROVED CHANGES | CURRENT CONTRACT VALUE | POTENTIAL CHANGES | ESTIMATE AT COMPLETION (EAC) | ORIGINAL CONTINGENCY / Sep 2013 SUPPLEMENTAL CONTINGENCY (Exclude CN 1250 & CN1251) | CONTINGENCY ADJUSTMENT TRANSFERS | REVISED AUTHORIZED CONTINGENCY (Exclude CN1250 & CN1251) | REMAINING CONTINGENCY AFTER APPROVED CHANGES DEDUCTED [h - b] | ORIGINAL CONTRACT VALUE + REVISED AUTHORIZED CONTINGENCY [a + h] | BUDGET - ESTIMATE AT COMPLETE [j - e] | | |
| | a | b | c | d | e | f | g | h | i | j | k | | |
| SCC 10-50 CONSTRUCTION CONTRACT PACKAGES | | | | | | | | | | | | | |
| 1250 | UTILITY RELOCATION PACKAGE #1 Contract 1250 Department of Technology | 9,273,939 166,756 | 2,694,211 | 11,968,150 166,756 | | 11,968,150 166,756 | 1,953,377 | 740,834 | 2,694,211 | 11,968,150 166,756 | | 14 | |
| 1251 | UTILITY RELOCATION PACKAGE #2 Contract 1251 Department of Technology | 16,832,550 75,615 | 3,962,032 | 20,794,582 75,615 | | 20,794,582 75,615 | 5,367,297 | (1,405,265) | 3,962,032 | 20,794,582 75,615 | | 15 | |
| 1252 | GUIDEWAY TUNNEL | 233,584,015 | 1,421,807 | 235,005,822 | (77,798) | 234,928,024 | 23,658,464 | (6,173,511) | 17,484,953 | 16,063,145 | 251,068,968 | 16,140,943 | 16 |
| 1300 | STATIONS | 839,676,400 | (1,432,743) | 838,243,657 | 11,573,150 | 849,816,807 | 20,000,000 | | 20,000,000 | 21,432,743 | 859,676,400 | 9,859,593 | 17 |
| | 1253 UNION SQUARE/MARKET ST STATION [UMS] | 294,030,590 | 90,000 | 294,120,590 | 5,959,070 | 300,079,660 | 5,000,000 | | 5,000,000 | 4,910,000 | 299,030,590 | (1,049,070) | |
| | 1254 CHINA TOWN STATION [CTS] | 247,567,810 | 230,956 | 247,798,766 | 6,285,863 | 254,084,629 | 5,000,000 | | 5,000,000 | 4,769,044 | 252,567,810 | (1,516,819) | |
| | 1255 YERBA BUENA/ MOSCONE STATION [YBMI] | 158,089,000 | (1,833,869) | 156,255,131 | 2,832,841 | 159,087,972 | 5,000,000 | | 5,000,000 | 6,833,869 | 163,089,000 | 4,001,028 | |
| | 1256 SURFACE TRACKWORK & SYSTEMS [STS] | 139,989,000 | 80,170 | 140,069,170 | (3,504,623) | 136,564,547 | 5,000,000 | | 5,000,000 | 4,919,830 | 144,989,000 | 8,424,453 | |
| OTHER | | 31,233,501 | | 31,233,501 | | 31,233,501 | 1,160,000 | | 1,160,000 | 1,160,000 | 32,393,501 | 1,160,000 | 20 |
| SCC 10 - 50 Construction Sub-total | | 1,130,842,776 | 6,645,307 | 1,137,488,083 | 11,495,352 | 1,148,983,435 | 44,818,464 | (6,173,511) | 38,644,953 | 38,655,888 | 1,176,143,971 | 27,160,536 | |
| SCC 60 - 80 SOFT COSTS PACKAGES | | | | | | | | | | | | | |
| 60 | ROW, LAND, EXISTING IMPROVEMENTS | 36,511,799 | (4,265,478) | 32,246,321 | | 32,246,321 | 1,000,000 | 4,265,478 | 1,000,000 | 5,265,478 | 37,511,799 | 5,265,478 | 18 |
| 70 | VEHICLES | 24,108,712 | (10,799,712) | 13,309,000 | | 13,309,000 | 2,276,941 | 10,799,712 | 13,076,653 | 13,076,653 | 26,385,653 | 13,076,653 | 19 |
| 80 | PROFESSIONAL SERVICES | 310,518,041 | | 310,518,041 | | 310,518,041 | 18,221,079 | | 18,221,079 | 18,221,079 | 328,739,120 | 18,221,079 | |
| SCC 60 - 80 Construction Sub-total | | 371,138,552 | | 356,073,362 | | 356,073,362 | 21,498,020 | 15,065,190 | 32,297,732 | 36,563,210 | 392,636,572 | 36,563,210 | |
| SCC 90 | UNALLOCATED CONTINGENCY | | | | | | 3,845,945 | 6,173,511 | 10,019,456 | 9,519,456 | 9,519,456 | 9,519,456 | 20 |
| TOTAL | | 1,501,981,328 | 6,645,307 | 1,493,561,445 | 11,495,352 | 1,505,056,797 | 70,162,429 | 15,065,190 | 80,962,141 | 84,738,554 | 1,578,299,999 | 73,243,202 | |
| | | | | | | | | | | | Total Project Budget | 1,578,300,000 | 21 |
| | | | | | | | | | | | Estimate At Completion | 1,505,056,797 | 22 |
| | | | | | | | | | | | Variance | 73,243,202 | 23 |
| Note #15 - Adjusted Contract 1252 Guideway Tunnel contingency "column g" to reflect construction contract modifications #20, #40, #41 and \$48 were funded by other funding sources. | | | | | | | | | | | | | |

Contract Modification/Trend Log - Contract 1300 Stations

7.5 DETAIL CONTINGENCY USAGE REPORT

Awarded NTE Amount **\$839,676,400**
Substantial Completion **2/10/2018**

\$838,243,657
2/10/2018

| | UMS | CTS | YBM | STS | COST REPORT NOTES |
|-------------------------------------|--------------------|--------------------|--------------------|----------------------|-------------------|
| Potential Changes | \$5,959,070 | \$6,285,863 | \$2,832,841 | (\$3,504,623) | 24 |
| Forecasted - Trends | \$3,174,154 | \$5,421,436 | \$973,554 | (\$1) | |
| Negotiation | | | | | |
| Grade 50 Steel inLieu ofA913 Grad70 | \$595,197 | | | | |
| Modification to UMS Roof Composite | \$14,336 | | | | |
| STS-FACO #20 Add'l Soil Testing Req | | | | \$5,000 | |
| CTS-FACO #5 Unidentif 3" Metal Pipe | | \$350 | | | |
| CTS-FACO #4 Joint Trench Unid. Cond | | \$25,000 | | | |
| CTS-FACO #2 Connecting (E) Ductbank | | \$10,000 | | | |
| UMS BART Entry Hazmat Abatement | \$33,600 | | | | |
| CTS- AWSS Wk PGE relocation schedul | | \$60,309 | | | |
| Additional AT&T Manhole, Pullboxes, | | | | \$225,208 | |
| STS Traffic Signal and SL Changes | | | | \$298,307 | |
| STS Comm and Elec Cabinets Relocati | | | | \$67,221 | |
| CTS-PCC#17 FH at Jade Galore | | \$106,350 | | | |
| CTS-Plaza Surface Level Struct Mods | | \$15,701 | | | |
| STS - OCS Pole Changes | | | | \$12,706 | |
| CTS-FACO#39- Soil Testing cmply OAB | | \$5,000 | | | |
| STS-FACO #37 30" Force Main Removal | | | | \$60,000 | |
| UMS FACO #31 NDSC Incomplete PGE DB | \$32,980 | | | | |
| STS-FACO #26 Fire Hyd NE 4th&Bryant | | | | \$20,000 | |
| UMS-FACO #25 OCS Pole@ Market/Ellis | \$2,000 | | | | |
| UMS-FACO #19 Street Light at Stock. | \$4,000 | | | | |
| STS-FACO #48 Work Related St Lght | | | | \$5,000 | |
| UMS FACO #34 Becho Obstruct J Piles | \$15,000 | | | | |
| UMS FACO #45 Blocked Stubs PG&E | \$7,500 | | | | |
| UMS FACO #38 Pile H3 & H4 Piles | \$15,000 | | | | |
| STS-FACO #41 Groundwater Lead Filtr | | | | \$5,000 | |
| UMS-FACO #30 NDSC Inadequate CDF | \$50,000 | | | | |
| STS Traction Power Cable Downsizing | | | | (\$117) | |
| UMS Transfer Instru BART Facilities | \$30,000 | | | | |
| CTS-PCC40 Plaza Surface Slab Pntrtn | | \$5,525 | | | |
| UMS Sewer Line Conflict | \$168,718 | | | | |
| UMS MRY Duct Bank-East Conflict | \$10,000 | | | | |
| STS - PCC#28 Portal Dowels | | | | (\$1,753) | |

Contract Modification/Trend Log - Contract 1300 Stations

Awarded NTE Amount **\$839,676,400**
Substantial Completion **2/10/2018**

\$838,243,657
2/10/2018

| | UMS | CTS | YBM | STS | COST REPORT NOTES |
|--------------------------------------|-----------|-----------|-----------|----------|-------------------|
| STS - 12" AWSS Incorrectly Shown | | | | \$35,134 | |
| UMS DSC Mass of Conc Stocktn 137+15 | \$7,500 | | | | |
| YBM COR 78 Change POC by SFWD | | | \$53,659 | | |
| STS Existing Fuel & Transite in MRY | | | | \$10,000 | |
| UMS - PCC #039 (12" WL Relocate) | \$225,468 | | | | |
| Pipe Protection for in-slab piping | \$103,475 | | | | |
| CTS-Delete Tree Planting | | (\$3,967) | | | |
| YBM COR 36 Culvert NW corn Folsom | | | \$7,799 | | |
| YBM COR 44 Buried concrete wall | | | \$60,000 | | |
| YBM COR 45 Abandoned sewer not | | | \$6,000 | | |
| YBM COR 46 Add analytical tests | | | \$5,000 | | |
| YBM COR 49 Buried timber piles | | | \$50,000 | | |
| YBM COR 54 Concrete Obstruc 20" cut | | | \$32,066 | | |
| YBM COR 64 Buried sheet pile P-7 | | | \$7,500 | | |
| YBM COR 71 Remove rigging straps | | | \$7,500 | | |
| YBM COR 72 CN1252 North Headwall | | | \$100,000 | | |
| YBM COR 86 Side sewer conflict AT&T | | | \$30,000 | | |
| STS 78 Sewer Existing Concrete Crow | | | | \$7,200 | |
| STS-COR #84 Sewer Cleaning and Dewa | | | | \$3,598 | |
| STS COR #89 Extra Sewer Work | | | | \$8,090 | |
| YBM COR 87 Transite ductbank removal | | | \$12,631 | | |
| YBM PCC 47 Add PG&E ductbank | | | \$129,255 | | |
| YBM PCC 55 Add 24-in water main | | | \$224,038 | | |
| STS COR #112 MRY MH 1890 TS Conduit | | | | \$6,000 | |
| STS COR #94 Unknown DB Conf 12" Wtr | | | | \$20,000 | |
| STS COR #93 12" Water Conf 12" Tee | | | | \$5,000 | |
| STS COR #92 PG&E Vault Conf 12 AWSS | | | | \$79,173 | |
| STS COR #91 PG&E Vault Conf 16" Wtr | | | | \$20,000 | |
| STS COR #118 78" Sewer Excess Debris | | | | \$54,455 | |
| STS COR 98 DSC MRY MH Conf w N CB | | | | \$15,000 | |
| STS COR 100 DSC Zayo PVC Conf w SW | | | | \$80,082 | |
| YBM COR 182 Cap (E) 16" AWSS Tee | | | \$14,868 | | |
| CTS-COR#201 Swr Line & Station Roof | | \$55,000 | | | |
| YBM COR 115 C1250 unencased JT | | | \$15,000 | | |
| YBM COR 116 Archaeological Discover | | | \$100,000 | | |
| YBM COR 196 Work on Hold | | | \$4,500 | | |

Contract Modification/Trend Log - Contract 1300 Stations

Awarded NTE Amount **\$839,676,400**
Substantial Completion **2/10/2018**

\$838,243,657
2/10/2018

| | UMS | CTS | YBM | STS | COST REPORT NOTES |
|---------------------------------------|-------------|------------|------------|---------------|-------------------|
| STS COR 210 WD pit size increase | | | | \$24,417 | |
| STS - Deletion of ARS (Revision 1) | | | | (\$4,689,000) | |
| CTS COR 085 Addl FACO 10 Costs | | \$13,267 | | | |
| CTS PCC 001 Delete DB on Stockton | | (\$84,018) | | | |
| YBM PCC 022 Addtl Signage on Clem | | | \$12,000 | | |
| YBM COR 050 Potential contam oil | | | \$50,000 | | |
| YBM COR 253 DSC Pipe Conflict | | | \$20,000 | | |
| Proposed Contract Change (PCC) | | | | | |
| UMS - Geoprobe Credit | (\$30,320) | | | | |
| UMS - Relocation of TS Cabinet | \$23,275 | | | | |
| STS - Tunnel Lowering | | | | \$100,000 | |
| CTS - Stairs 5, 6, 7 Mods | | \$25,000 | | | |
| STS - Additional AT&T DB at 4th | | | | \$73,418 | |
| UMS - Escalator Barricade | (\$122,511) | | | | |
| UMS - Union Sq. Garage Underpinning | \$100,000 | | | | |
| UMS - USG HVAC Trench Modifications | \$75,001 | | | | |
| CTS - CMU Wall Bracing | | \$25,000 | | | |
| YBM PCC 41 Install #7 Box Clementin | | | \$29,000 | | |
| UMS MRY Duct Bank-East Conflict | \$25,000 | | | | |
| UMS - N. Ent. Art Glass and Glazing | \$20,000 | | | | |
| CTS - Removal of Bus Bulb | | \$30,000 | | | |
| CTS Delete PGE Work at Vault 732 | | (\$50,000) | | | |
| UMS Install Floor Drain | \$20,000 | | | | |
| UMS Remove Fire Hose Valve | \$20,000 | | | | |
| YBM PCC 21 Dele Instrum & Monitoring | | | (\$1) | | |
| YBM PCC 32 Delete Post-Grout of TB-2 | | | (\$10,000) | | |
| UMS Install Deluge Main | \$30,000 | | | | |
| YBM_CTS PCC 33 Platform Gate Revisio | | | \$100,000 | | |
| STS PCC #51 Inventory Temp Crossover | | | | \$20,000 | |
| UMS - DCW and Hose Bibbs | \$30,000 | | | | |
| YBM PCC 059 Pavers Basis of Design | | | \$1 | | |
| STS Track Switch Mach Change (Rev 1) | | | | \$50,000 | |
| CTS PCC 061 Escalator Pit Provision | | \$50,000 | | | |
| Change Order Request (COR) | | | | | |
| UMS - Fire Hydrant at O'Farrell | \$92,978 | | | | |
| STS - Additn manholes for 78" sewer | | | | \$27,204 | |
| STS COR 17 Changes to Sewer Structu | | | | \$20,000 | |

Contract Modification/Trend Log - Contract 1300 Stations

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| | UMS | CTS | YBM | STS | COST REPORT NOTES |
|-------------------------------------|-----------|-----|-----------|----------|-------------------|
| STS-FACO #47 Sewer Related Work Pt2 | | | | \$33,898 | |
| UMS FACO #53 NDSC Transite Pipe | \$63,451 | | | | |
| UMS FACO #52 NDSC Transite Pipe | \$28,694 | | | | |
| UMS NDSC (e) 12 inch CI Pipe | \$1,124 | | | | |
| STS- AT&T Conduit Changes | | | | \$22,290 | |
| STS COR 74 AWSS Offset/Sewer Demo | | | | \$41,813 | |
| STS COR #88 Modify CBs and Culverts | | | | \$4,395 | |
| STS COR #90 Extra Work Related to S | | | | \$20,452 | |
| YBM COR 75 Concrete Overpour P-34 | | | \$169,775 | | |
| STS COR 113 DSC PVC Conduit/WL Conf | | | | \$15,000 | |
| UMS - COR 134 Asbestos at Ellis Ent | \$45,636 | | | | |
| STS COR 145 DSC PVC Conduits Conf | | | | \$5,000 | |
| STS COR 146 DSC 8" WL in Conf w SW | | | | \$5,000 | |
| STS COR 164 DSC 8" AWSS Lat Conf 78 | | | | \$10,000 | |
| STS - COR 181 DSC Poly Line Conf MR | | | | \$10,000 | |
| UMS COR 184 Slurry Fill Void | \$8,468 | | | | |
| UMS COR 191 DSC E PG&E Vault 582 | \$1,849 | | | | |
| UMS COR 202 DSC CG Grout Forever 21 | \$20,000 | | | | |
| UMS COR 203 Non-Resp.Review Process | \$20,001 | | | | |
| STS COR 211 SW conf AWSS 4th/Freelo | | | | \$4,561 | |
| UMS COR 212 Comp. Grout PSI Drop | \$150,000 | | | | |
| STS COR 220 DSC Relocate MRY DB&Vlt | | | | \$0 | |
| UMS COR 221 RFI 1295 | \$1 | | | | |
| UMS COR 222 RFI 1282 | \$10,001 | | | | |
| UMS COR 223 RFI 1317 | \$75,001 | | | | |
| UMS COR 224 RFI 1334 | \$50,001 | | | | |
| UMS COR 228 RFI 1141.4 | \$50,001 | | | | |
| UMS COR 232 RFI 240 Response | \$10,141 | | | | |
| STS COR #229 Multi E Util Conf w N | | | | \$3,500 | |
| STS COR #114 Conc Obs 4th-Bryant SE | | | | \$1,262 | |
| STS COR #170 Cores in 78" RC Crown | | | | \$1,666 | |
| STS COR #233 18" VCP in 3'x5' Sewer | | | | \$1,764 | |
| STS COR #187 DB Conf w N 12 Water | | | | \$37,012 | |
| UMS COR 35 DPW Citations | \$55,247 | | | | |
| UMS COR 102 Cap on 12 inch Water | \$44,544 | | | | |
| STS COR #168 Util Conf w N 18 Sewer | | | | \$31,016 | |
| UMS COR 189 NDSC Pipe Conflict | \$5,938 | | | | |

Contract Modification/Trend Log - Contract 1300 Stations

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Substantial Completion **2/10/2018**

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2/10/2018

| | UMS | CTS | YBM | STS | COST REPORT NOTES |
|--------------------------------------|----------|----------|-----------|----------|-------------------|
| CTS COR 217 Addl Slurry Wall Compen | | \$0 | | | |
| UMS COR 225 RFI 1335 | \$50,001 | | | | |
| UMS COR 226 RFI 1336 | \$20,001 | | | | |
| UMS COR 227 RFI 1287 | \$1 | | | | |
| YBM COR 230 Load Rating Siwalk Hatch | | | \$31,474 | | |
| CTS COR 231 Sidewalk Hatches | | \$19,249 | | | |
| UMS COR 235 DSC Transite Pipe | \$10,000 | | | | |
| STS COR #236 E WD Config at Freelon | | | | \$3,500 | |
| UMS COR 237 RFI 1289 | \$20,001 | | | | |
| UMS COR 238 RFI 1341 | \$50,001 | | | | |
| YBM COR 240 Headhouse Contam. Mtl. | | | \$47,549 | | |
| STS COR #241 2-In WD Line w Offset | | | | \$20,000 | |
| YBM COR 243 PCC 042 Culvert Conflict | | | \$10,001 | | |
| STS COR 244 DSC (E) CI Confl AWSS | | | | \$10,000 | |
| STS COR 245 DSC DB Conf AWSS/WL | | | | \$10,000 | |
| UMS COR 247 Shotcrete Plywood | \$45,000 | | | | |
| YBM COR 248 Transite pipe | | | \$1,500 | | |
| YBM COR 249 Utility Conflicts | | | \$60,001 | | |
| UMS COR 250 Macy's DSC | \$1 | | | | |
| CTS COR 260 DSC at Dewatering Wells | | \$15,000 | | | |
| UMS COR 261 RFI 1333 | \$1 | | | | |
| UMS COR 262 RFI 1340 | \$1 | | | | |
| UMS COR 263 RFI 1347 | \$50,001 | | | | |
| Change Order - Pending | | | | | |
| CTS Sewer Line Demo at Washing | | \$57,707 | | | |
| CTS Utility Conflict at WD Tie-In | | \$14,527 | | | |
| YBM COR 15 AWSS hydrant lateral | | | \$41,079 | | |
| YBM COR 16 Thick concrete street | | | \$4,785 | | |
| YBM COR 18 AWSS conflict with PG&E | | | \$14,093 | | |
| YBM COR 20 (E) conflicts with AWSS | | | \$12,506 | | |
| CTS-FACO #44 (E) Utl Cnflct w/ AWSS | | \$25,211 | | | |
| UMS-FACO #32 8" Waterline Conf NDSC | \$48,615 | | | | |
| CTS-FACO #27 AWSS vs PGE Vault #728 | | \$14,439 | | | |
| YBM COR 25 WM conflict with AWSS | | | \$13,850 | | |
| YBM PCC 42 10-in culvert on Folsom | | | \$64,287 | | |
| UMS DSC Tanks in Stockton N of Gear | \$97,817 | | | | |
| YBM Underground storage tank Folsom | | | \$156,733 | | |

Contract Modification/Trend Log - Contract 1300 Stations

Awarded NTE Amount
Substantial Completion

\$839,676,400
2/10/2018

\$838,243,657
2/10/2018

| | UMS | CTS | YBM | STS | COST REPORT NOTES |
|--------------------------------------|--------------------|--------------------|----------------------|----------------------|-------------------|
| YBM COR 10 PG&E conflict with AWSS | | | \$40,349 | | |
| Approved | \$90,000 | \$230,956 | (\$1,833,869) | \$80,170 | |
| Contract Modification | | | | | |
| CMod #1 BART Elevator Option 1 @ Pow | \$90,000 | | | | |
| CMod #3 CTS Work Safely Ard Power Po | | \$25,956 | | | |
| CMod #5 YBM Deletion of Comp Groutin | | | (\$1,833,869) | | |
| CMod #6 CTS Plaza Constr Supt Servi | | \$75,000 | | | |
| CMod#7 STS FACOs 016, 017 &COR 009 | | | | \$80,170 | |
| CMod #4 CTS-Force Account Change Or | | \$130,000 | | | |
| Grand Total | \$6,049,070 | \$6,516,819 | \$998,972 | (\$3,424,453) | |

7.6 BUDGET REVISIONS: REPORT SORTED BY CONSTRUCTION PACKAGES & SOFT COSTS

Report Period: July 2015

| Group by Contract & SCC | CATEGORY ITEM | June 2015 | | | July 2015 | | | BUDGET TRANSFERS [July 2015] vs. [June 2015] | Cost Report Notes |
|-------------------------|--|----------------------|---------------------------------|--|----------------------|---------------------------------|--|---|-------------------|
| | | June 2015 Base | June 2015 Allocated Contingency | June 2015 Base + Allocated Contingency (YOE) | July 2015 Base | July 2015 Allocated Contingency | July 2015 Base + Allocated Contingency (YOE) | | |
| 10-50 | CONSTRUCTION CONTRACT PACKAGES | 1,137,488,083 | 38,655,888 | 1,176,143,971 | 1,137,488,083 | 38,655,888 | 1,176,143,971 | 0 | |
| 1250 | UTILITY RELOCATION PACKAGE #1 | 12,134,906 | | 12,134,906 | 12,134,906 | | 12,134,906 | 0 | |
| | Contract 1250 Form B Credit | (2,275,419) | | (2,275,419) | (2,275,419) | | (2,275,419) | 0 | |
| 1251 | UTILITY RELOCATION PACKAGE #2 | 20,870,197 | | 20,870,197 | 20,870,197 | | 20,870,197 | 0 | |
| | Contract 1251 Form B Credit | (7,618,412) | | (7,618,412) | (7,618,412) | | (7,618,412) | 0 | |
| 1252 | GUIDEWAY TUNNEL | 235,005,822 | 16,063,145 | 251,068,967 | 235,005,822 | 16,063,145 | 251,068,967 | 0 | 25 |
| | Contract 1252 Form B Credit | (254,050) | | (254,050) | (254,050) | | (254,050) | 0 | |
| 1300 | CN1300 STATIONS TOTAL | 838,243,657 | 21,432,743 | 859,676,400 | 838,243,657 | 21,432,743 | 859,676,400 | 0 | |
| 1253: UMS | UNION SQUARE/MARKET STREET STATION [UMS] | 294,120,590 | 4,910,000 | 299,030,590 | 294,120,590 | 4,910,000 | 299,030,590 | 0 | |
| | UMS 1253 Form B Credit | (528,370) | | (528,370) | (528,370) | | (528,370) | 0 | |
| 1254: CTS | CHINA TOWN STATION [CTS] | 247,798,766 | 4,769,044 | 252,567,810 | 247,798,766 | 4,769,044 | 252,567,810 | 0 | |
| | CTS 1254 Form B Credit | (451,703) | | (451,703) | (451,703) | | (451,703) | 0 | |
| 1255: YBM | YERBA BUENA/ MOSCONE STATION [YBM] | 156,255,131 | 6,833,869 | 163,089,000 | 156,255,131 | 6,833,869 | 163,089,000 | 0 | |
| | YBM 1255 Form B Credit | (100,000) | | (100,000) | (100,000) | | (100,000) | 0 | |
| 1256: STS | SURFACE TRACKWORK & SYSTEMS [STS] | 140,069,170 | 4,919,830 | 144,989,000 | 140,069,170 | 4,919,830 | 144,989,000 | 0 | |
| | STS 1256 SFPUC SEWER MAIN CREDIT | (2,925,296) | | (2,925,296) | (2,925,296) | | (2,925,296) | 0 | |
| | STS 1256 Form B Credit | (1,000,000) | | (1,000,000) | (1,000,000) | | (1,000,000) | 0 | |
| OTHER | OTHER CONSTRUCTION TOTAL | 46,386,751 | 1,160,000 | 47,546,751 | 46,386,751 | 1,160,000 | 47,546,751 | 0 | |
| 40.06 | PUBLIC ART PROGRAM | 8,175,555 | 1,160,000 | 9,335,555 | 8,175,555 | 1,160,000 | 9,335,555 | 0 | |
| 40.02 | MISC. CONSTR CONTRCT WK (TRACTION POWER FOR 1251) | 258,202 | | 258,202 | 258,202 | | 258,202 | 0 | |
| 40.01 | CONTRACT 1300 SOIL PROCESS | 500,000 | 0 | 500,000 | 500,000 | 0 | 500,000 | 0 | 26 |
| 50.01 | TEMPORARY LICENSE AGREEMENT (ATCS CENTRAL CONTROL) | 487,972 | | 487,972 | 487,972 | | 487,972 | 0 | |
| 50.06 | MTA FARE COLLECTION EQUIPMENT | 5,400,000 | | 5,400,000 | 5,400,000 | | 5,400,000 | 0 | |
| 50.06 | BART FARE COLLECTION EQUIPMENT | 700,000 | | 700,000 | 700,000 | | 700,000 | 0 | |
| 40.02 | JOB ORDER CONTRACTS (JOCS) - CONSTRUCTION | 117,255 | | 117,255 | 117,255 | | 117,255 | 0 | |
| 40.08 | AON RISK INSURANCE | 18,088,750 | | 18,088,750 | 18,088,750 | | 18,088,750 | 0 | |

7.6 BUDGET REVISIONS: REPORT SORTED BY CONSTRUCTION PACKAGES & SOFT COSTS

Report Period: July 2015

| Group by Contract & SCC | CATEGORY ITEM | June 2015 | | | July 2015 | | | BUDGET TRANSFERS [July 2015] vs. [June 2015] | Cost Report Notes |
|-------------------------|--|--------------------|---------------------------------|--|--------------------|---------------------------------|--|---|-------------------|
| | | June 2015 Base | June 2015 Allocated Contingency | June 2015 Base + Allocated Contingency (YOE) | July 2015 Base | July 2015 Allocated Contingency | July 2015 Base + Allocated Contingency (YOE) | | |
| 40.02 40.08 | PUBLIC AGENCIES UTILITY COORDINATION | 3,713,215 | | 3,713,215 | 3,713,215 | | 3,713,215 | 0 | |
| 40.02 | DEPARTMENT OF PARKING AND TRAFFIC (DPT) | 1,200,000 | | 1,200,000 | 1,200,000 | | 1,200,000 | 0 | |
| 50.03 | UNION SQUARE/ MARKET STREET STATION POWER FEED | 2,959,826 | | 2,959,826 | 2,959,826 | | 2,959,826 | 0 | |
| 50.03 | UNION SQUARE/ MARKET STREET STATIONS PERMANENT POWER | (2,350,000) | | (2,350,000) | (2,350,000) | | (2,350,000) | 0 | |
| 50.03 | CHINATOWN STATION POWER FEED | 2,959,826 | | 2,959,826 | 2,959,826 | | 2,959,826 | 0 | |
| 50.03 | CHINATOWN STATION PERMANENT POWER | (2,350,000) | | (2,350,000) | (2,350,000) | | (2,350,000) | 0 | |
| 50.03 | YERBA BUENA/ MOSCONE STATION [YBM] POWER FEED | 3,125,222 | | 3,125,222 | 3,125,222 | | 3,125,222 | 0 | |
| 50.03 | YERBA BUENA/ MOSCONE STATION [YBM] PERMANENT POWER | (2,368,540) | | (2,368,540) | (2,368,540) | | (2,368,540) | 0 | |
| 50.03 | SURFACE STATION POWER FEED | 11,839 | | 11,839 | 11,839 | | 11,839 | 0 | |
| 50.04 | COMMUNICATION CONNECTION COSTS | 5,757,629 | | 5,757,629 | 5,757,629 | | 5,757,629 | 0 | |
| 60 | ROW, LAND, EXISTING IMPROVEMENTS | 32,246,321 | 5,265,478 | 37,511,799 | 32,246,321 | 5,265,478 | 37,511,799 | 0 | |
| 60.01 | PURCHASE OR LEASE OF REAL ESTATE | 30,065,810 | 5,265,478 | 35,331,288 | 30,065,810 | 5,265,478 | 35,331,288 | 0 | 27 |
| 60.02 | RELOCATION OF EXISTING HOUSEHOLDS | 2,180,511 | | 2,180,511 | 2,180,511 | | 2,180,511 | 0 | |
| 70 | VEHICLES | 13,309,000 | 13,076,653 | 26,385,653 | 13,309,000 | 13,076,653 | 26,385,653 | 0 | |
| 70.01 | LIGHT RAIL | 13,309,000 | 13,076,653 | 26,385,653 | 13,309,000 | 13,076,653 | 26,385,653 | 0 | 28 |
| 70.07 | SPARE PARTS | | | | | | | | |
| 80 | PROFESSIONAL SERVICES | 310,518,041 | 18,221,079 | 328,739,120 | 310,518,041 | 18,221,079 | 328,739,120 | 0 | |
| 80.01 | PRELIMINARY ENGINEERING | 46,202,674 | | 46,202,674 | 46,202,674 | | 46,202,674 | 0 | |
| 80.02 | FINAL DESIGN | 61,322,751 | | 61,322,751 | 61,322,751 | | 61,322,751 | 0 | |

7.6 BUDGET REVISIONS: REPORT SORTED BY CONSTRUCTION PACKAGES & SOFT COSTS

Report Period: July 2015

| Group by Contract & SCC | CATEGORY ITEM | June 2015 | | | July 2015 | | | BUDGET TRANSFERS [July 2015] vs. [June 2015] | Cost Report Notes |
|-------------------------|---|----------------------|---------------------------------|--|----------------------|---------------------------------|--|---|-------------------|
| | | June 2015 Base | June 2015 Allocated Contingency | June 2015 Base + Allocated Contingency (YOE) | July 2015 Base | July 2015 Allocated Contingency | July 2015 Base + Allocated Contingency (YOE) | | |
| 80.03 | PROJECT MANAGEMENT FOR DESIGN & CONSTRUCTION | 89,021,634 | 13,905,845 | 102,927,479 | 89,021,634 | 13,905,845 | 102,927,479 | 0 | |
| 80.04 | CONSTRUCTION ADMINISTRATION & MANAGEMENT | 91,037,791 | 2,956,812 | 93,994,603 | 91,037,791 | 2,956,812 | 93,994,603 | 0 | |
| 80.05 | INSURANCES | 6,800,000 | | 6,800,000 | 6,800,000 | | 6,800,000 | 0 | |
| 80.06 | LEGAL: PERMITS. REVIEW FEES BY OTHER AGENCIES | 8,258,184 | | 8,258,184 | 8,258,184 | | 8,258,184 | 0 | |
| 80.07 | SURVEYS, TESTING, INVESTIGATION. INSPECTION | 883,100 | | 883,100 | 883,100 | | 883,100 | 0 | |
| 80.08 | START-UP | 6,991,907 | 1,358,422 | 8,350,329 | 6,991,907 | 1,358,422 | 8,350,329 | 0 | |
| | ALL SCC CATEGORIES 10 TO 80 | 1,493,561,445 | 75,219,098 | 1,568,780,543 | 1,493,561,445 | 75,219,098 | 1,568,780,543 | 0 | |
| 90 | UNALLOCATED CONTINGENCIES | | | 9,519,459 | | | 9,519,459 | | 29 |
| | TOTAL PROJECT COST 10 TO 100 | | | 1,578,300,002 | | | 1,578,300,002 | | |
| | TOTAL CONTINGENCY | | | 84,738,557 | | | 84,738,557 | | |
| | CONTINGENCY MINIMUM | | | 60,000,000 | | | 60,000,000 | | |
| | BELOW OR ABOVE MINIMUM | | | 24,738,557 | | | 24,738,557 | | |

| COST STATUS BY CATEGORY | SCC CODES | BUDGET June 2015 | BUDGET TRANSFERS | BUDGET July 2015 | July 2015 CTD | July 2015 EAC |
|------------------------------|---------------------|----------------------|---------------------|----------------------|--------------------|----------------------|
| CONSTRUCTION | SCC 010 - 050 | 1,176,143,972 | 0 | 1,176,143,972 | 570,137,584 | 1,148,983,435 |
| REAL ESTATE | SCC 060 | 37,511,799 | 0 | 37,511,799 | 30,057,601 | 32,246,321 |
| VEHICLES | SCC 070 | 26,385,653 | 0 | 26,385,653 | 2,146,905 | 13,309,000 |
| PRELIM ENGINEERING | SCC 080.01 | 46,202,674 | 0 | 46,202,674 | 46,202,675 | 46,202,674 |
| FINAL DESIGN | SCC 080.02 | 61,318,331 | 0 | 61,318,331 | 61,199,308 | 61,322,751 |
| PM FOR DESIGN & CONSTRUCTION | SCC 080.03 - 080.04 | 196,922,082 | 0 | 196,922,082 | 98,223,159 | 180,059,425 |
| OTHER PROF SRVCS | SCC 080.05 - 080.08 | 24,296,033 | 0 | 24,296,033 | 10,805,751 | 22,933,191 |
| UNALLOC CONTINGENCY | SCC 090 | 9,519,456 | 0 | 9,519,456 | | |
| Grand Total | | 1,578,300,000 | 0 | 1,578,300,000 | 818,772,983 | 1,505,056,798 |

Note: In July 2015 Report, used Contract 1300 Contractor schedule to report budget and actual cost. The Standard Cost Categories (SCC) allocation changed from previous reports.

| SCC DESCRIPTION | July 2015 BUDGET | July 2015 CTD |
|--|----------------------|--------------------|
| 010 - GUIDEWAY & TRACK ELEMENTS | 225,635,467 | 169,714,277 |
| 020 - STATIONS, STOPS, TERMINALS, INTERMODAL | 589,456,547 | 176,238,216 |
| 040 - SITEWORK & SPECIAL CONDITIONS | 267,967,866 | 205,880,991 |
| 050 - SYSTEMS | 93,084,093 | 18,304,099 |
| 060 - ROW, LAND, EXISTING IMPROVEMENTS | 37,511,799 | 30,057,601 |
| 070 - VEHICLES (number) | 26,385,653 | 2,146,905 |
| 080 - PROFESSIONAL SERVICES (applies to Cats. 10-50) | 328,739,120 | 216,430,893 |
| 090 - UNALLOCATED CONTINGENCY | 9,519,456 | |
| Grand Total | 1,578,300,000 | 818,772,983 |

Note: In July 2015 Report, used Contract 1300 Contractor schedule to report budget and actual cost. The Standard Cost Categories (SCC) allocation changed from previous reports.



| SCC DESCRIPTION | July 2015 BUDGET | July 2015 CTD |
|--|------------------|---------------|
| 010.02-Guideway: At grade semi-exclusive (allows cross-traffic) | 9,150,000 | 870,000 |
| 010.06-Guideway: Underground cut & cover | 60,528,362 | 61,459,815 |
| 010.07-Guideway: Underground tunnel | 151,507,468 | 107,304,462 |
| 010.12-Track: Special (switches, turnouts) | 4,449,637 | 80,000 |
| 020.01-At-grade station, stop, shelter, mall, terminal, platform | 11,582,176 | 835,459 |
| 020.02-Aerial station, stop, shelter, mall, terminal, platform | 4,919,830 | 0 |
| 020.03-Underground station, stop, shelter, mall, terminal, platform | 551,485,663 | 173,478,583 |
| 020.07-Elevators, escalators | 21,468,878 | 1,924,175 |
| 040.01-Demolition, Clearing, Earthwork | 22,151,576 | 21,886,067 |
| 040.02-Site Utilities, Utility Relocation | 54,116,046 | 46,990,688 |
| 040.03-Haz. mat'l, contam'd soil removal/mitigation, ground water treatments | 5,157,405 | 1,742,230 |
| 040.04-Environmental mitigation, e.g. wetlands, historic/archeologic, parks | 820,165 | 239,969 |
| 040.05-Site structures including retaining walls, sound walls | 2,706,431 | 2,706,431 |
| 040.06-Pedestrian / bike access and accommodation, landscaping | 9,832,670 | 1,557,650 |
| 040.07-Automobile, bus, van accessways including roads, parking lots | 6,994,967 | 1,864,062 |
| 040.08-Temporary Facilities and other indirect costs during construction | 166,188,605 | 128,893,894 |
| 050.01-Train control and signals | 487,972 | 50,000 |
| 050.02-Traffic signals and crossing protection | 22,164,527 | 11,588,266 |
| 050.03-Traction power supply: substations | 21,532,073 | 3,553,372 |
| 050.04-Traction power distribution: catenary and third rail | 6,216,957 | 303,571 |
| 050.05-Communications | 5,183,040 | 1 |
| 050.06-Fare collection system and equipment | 6,100,000 | 0 |
| 050.07-Central Control | 31,399,523 | 2,808,890 |
| 060.01-Purchase or lease of real estate | 35,331,288 | 28,183,706 |
| 060.02-Relocation of existing households and businesses | 2,180,511 | 1,873,895 |
| 070.01-Light Rail | 26,385,653 | 2,146,905 |
| 080.01-Preliminary Engineering | 46,202,674 | 46,202,675 |
| 080.02-Final Design | 61,318,331 | 61,199,308 |
| 080.03-Project Management for Design and Construction | 102,927,479 | 58,055,360 |
| 080.04-Construction Administration & Management | 93,994,603 | 40,167,799 |
| 080.05-Professional Liability and other Non-Construction Insurance | 6,800,000 | 6,340,196 |
| 080.06-Legal; Permits; Review Fees by other agencies, cities, etc. | 8,262,604 | 4,420,572 |

| SCC DESCRIPTION | July 2015 BUDGET | July 2015 CTD |
|--|----------------------|--------------------|
| 080.07-Surveys, Testing, Investigation, Inspection | 883,100 | 12,097 |
| 080.08-Start up | 8,350,329 | 32,887 |
| 090.00-Unallocated Contingency | 9,519,456 | |
| Grand Total | 1,578,300,000 | 818,772,983 |

Note: In July 2015 Report, used Contract 1300 Contractor schedule to report budget and actual cost. The Standard Cost Categories (SCC) allocation changed from previous reports.

| [A] Cost Account Description | BUDGET | ACTUAL COSTS | | | | [G] VARIANCE (B - F) | COST REPORT NOTES |
|---|----------------------------|-----------------------|-------------------------|---------------------|--------------------|----------------------|-------------------|
| | [B] July 2015 Budget (YOE) | [C] PRIOR MONTH Total | [D] PRIOR MONTH Monthly | [E] CURRENT Monthly | [F] CURRENT Total | | |
| TOTAL PRELIMINARY ENGINEERING | 46,542,061 | 46,542,061 | 0 | 0 | 46,542,061 | 0 | 30 |
| 11 - SFMTA PROJECT MANAGEMENT | 8,828,359 | 8,253,957 | 0 | 0 | 8,253,957 | 574,403 | 31 |
| 12 - SFMTA ENGINEERING SERVICES | 11,425,594 | 11,425,594 | 0 | 0 | 11,425,594 | 0 | 32 |
| 16 - DEPARTMENT OF PARKING AND TRAFFIC (DPT) | 935,451 | 802,883 | 0 | 0 | 802,883 | 132,568 | |
| 21 - ARTS COMMISSION | 1,500,570 | 1,500,570 | 0 | 0 | 1,500,570 | 1 | 33 |
| 22 - FIRE DEPARTMENT | 33,825 | 33,825 | 0 | 0 | 33,825 | 0 | |
| 23 - CITY ATTORNEY'S OFFICE | 1,234,754 | 1,234,754 | 0 | 0 | 1,234,754 | 0 | |
| 24 - RISK MANAGEMENT | 0 | 0 | 0 | 0 | 0 | 0 | |
| 26 - PLANNING | 99,604 | 99,604 | 0 | 0 | 99,604 | 0 | |
| 27 - DEPARTMENT OF PUBLIC HEALTH (DPH) | 4,420 | 4,420 | 0 | 0 | 4,420 | 0 | |
| 29 - CITY AUDITOR | 308,540 | 315,616 | 0 | 0 | 315,616 | (7,076) | |
| 32 - DPW - IDC ENGINEERING (HYDRAULIC) | 3,322,887 | 3,336,432 | 0 | 0 | 3,336,432 | (13,545) | |
| 34 - DPW - IDC CONSTRUCTION (CAPTITAL) | 17,462 | 17,462 | 0 | 0 | 17,462 | 0 | |
| 36 - DPW - BSM INFRASTRUCTURE (MAPPING) | 76,549 | 76,549 | 0 | 0 | 76,549 | 0 | |
| 39 - DPW - PCS SITE ASSESSMENT & REMEDIATION (SAR) | 13,993 | 13,993 | 0 | 0 | 13,993 | 0 | |
| 51 - 821 HOWARD STREET | 1,005,653 | 1,005,653 | 0 | 0 | 1,005,653 | 0 | |
| 55 - 651 BRANNAN | 2,294,910 | 2,294,910 | 0 | 0 | 2,294,910 | 0 | 34 |
| 63 - CENTRAL SUBWAY PARTNERSHIP - AECOM-EPC JV CONTRACT 149 | 26,793,234 | 26,793,234 | 0 | 0 | 26,793,234 | 0 | 35 |
| 66 - ANIL VERMA | 395,204 | 395,204 | 0 | 0 | 395,204 | 0 | 36 |
| 67 - HILL INTERNATIONAL CONTRACT 156 | 6,716,294 | 6,716,294 | 0 | 0 | 6,716,294 | 0 | |
| 68 - ARTHUR GALLAGER & CO. CS 164 | 6,800,000 | 6,340,196 | 0 | 0 | 6,340,196 | 459,804 | |
| 71 - TUNNEL/UTILITIES - CONTRACT # CONTRACT 155-1 | 5,469,336 | 5,469,336 | 0 | 0 | 5,469,336 | 0 | 37 |
| 72 - STATIONS - CONTRACT # CONTRACT 155-2 | 26,220,609 | 26,220,609 | 0 | 0 | 26,220,609 | 0 | 38 |
| 73 - SYSTEMS/INTEGRATION - CONTRACT 155-3 | 11,432,312 | 11,432,312 | 0 | 0 | 11,432,312 | (0) | 39 |
| 331 - BAY AREA RAPID TRANSIT (BART) | 146,427 | 146,427 | 0 | 0 | 146,427 | 0 | |
| 332 - SAN FRANCISCO COUNTY TRANSPORTATION AUTHORITY (SFCTA) | 0 | 0 | 0 | 0 | 0 | 0 | |
| TOTAL FINAL DESIGN | 115,075,987 | 113,929,833 | 0 | 0 | 113,929,833 | 1,146,155 | |

| [A] Cost Account Description | BUDGET | ACTUAL COSTS | | | | [G] VARIANCE (B - F) | COST REPORT NOTES |
|---|----------------------------|-----------------------|-------------------------|---------------------|-------------------|----------------------|-------------------|
| | [B] July 2015 Budget (YOE) | [C] PRIOR MONTH Total | [D] PRIOR MONTH Monthly | [E] CURRENT Monthly | [F] CURRENT Total | | |
| 11 - SFMTA PROJECT MANAGEMENT | 15,589,933 | 4,614,011 | 216,764 | 302,667 | 4,916,678 | 0 | |
| 1.3.011.01.080.03 - CM:SFMTA LABOR-PROJECT MANAGEMENT | 15,589,933 | 4,614,011 | 216,764 | 302,667 | 4,916,678 | | |
| 12 - SFMTA ENGINEERING SERVICES | 905,264 | 1,569,606 | 91,343 | 109,389 | 1,678,995 | 244,587 | |
| 1.3.012.02.080.04 - CM: SFMTA LABOR-ENGINEERING CONTRACT 1252 | 123,582 | 57,648 | 0 | 0 | 57,648 | 65,934 | |
| 1.3.012.06.080.04 - CM: SFMTA LABOR-ENGINEERING CONTRACT 1300 | 1,800,000 | 1,511,958 | 91,343 | 109,389 | 1,621,347 | 178,653 | |
| 13 - SFMTA CONSTRUCTION MANAGEMENT | 44,075,375 | 7,542,595 | 299,407 | 386,728 | 7,929,323 | 35,127,734 | |
| 1.3.013.01.080.04 - CM:SFMTA LABOR-CONSTR. MANAGEM | 43,057,057 | 7,542,595 | 299,407 | 386,728 | 7,929,323 | 35,127,734 | |
| 16 - DEPARTMENT OF PARKING AND TRAFFIC (DPT) | 3,588,074 | 1,299,620 | 87,177 | 45,492 | 1,345,113 | 2,252,720 | |
| 1.3.016.01.080.04 - DPT CONTRACT 1300 SUPPORT UMS | 299,600 | 56,247 | 870 | 5,297 | 61,544 | 238,056 | |
| 1.3.016.01.080.04 - DPT CONTRACT 1300 SUPPORT CTS | 274,900 | 32,155 | 5,287 | 7,890 | 40,045 | 234,855 | |
| 1.3.016.01.080.04 - DPT CONTRACT 1300 SUPPORT YBM | 238,400 | 73,344 | 8,492 | 4,552 | 77,897 | 160,503 | |
| 1.3.016.01.080.04 - DPT CONTRACT 1300 SUPPORT STS | 876,876 | 61,594 | 21,118 | 4,433 | 66,027 | 810,849 | |
| 1.3.016.02.040.08 - DPT: FIELD OPS TUNNEL [B84] | 0 | 1,464 | 0 | 0 | 1,464 | (1,464) | |
| 1.3.016.02.040.08 - DPT: FIELD OPS TUNNEL [B86] | 0 | 187,910 | 50,926 | 16,351 | 204,261 | (204,261) | |
| 1.3.016.06.040.02 - DPT:DPT TRAFFIC SHOP CONTRACT 1300 | 1,200,000 | 0 | 0 | 0 | 0 | 1,200,000 | |
| 1.3.016.07.080.04 - DPT:SSD DS/CN: 1UTL | 38,450 | 0 | 0 | 0 | 0 | 38,450 | |
| 1.3.016.08.040.08 - DPT:PCOS:2UTL [68A] | 400,728 | 400,728 | 0 | 0 | 400,728 | 0 | |
| 1.3.016.08.040.08 - DPT:SSD CN:2UTL | 0 | 108,020 | 0 | 0 | 108,020 | (108,020) | |
| 1.3.016.08.080.04 - DPT:SSD [1326] | 259,120 | 253,184 | 0 | 5,911 | 259,095 | 25 | |
| 1.3.016.08.080.04 - DPT:SSD [13BN] | 0 | 23,302 | 0 | 0 | 23,302 | (23,302) | |
| 1.3.016.08.080.04 - DPT:SSD [13CN] | 0 | 963 | 0 | 0 | 963 | (963) | |
| 1.3.016.08.080.04 - DPT:SSD [B85] | 0 | 90,949 | 483 | 1,059 | 92,008 | (92,008) | |
| 1.3.016.09.040.08 - PCOS:1300/STS [68CPT544132Z.CPT544132Z] | 0 | 9,759 | 0 | 0 | 9,759 | (9,759) | |
| 17 - MOTIVE POWER | 2,195 | 0 | 0 | 0 | 0 | 2,195 | |
| 1.3.017.07.040.02 - PWR:SFMTA-MOTIVE POWER-UTL.REL | 2,195 | 0 | 0 | 0 | 0 | 2,195 | |
| 18 - SFMTA OPERATIONS | 400,000 | 37,852 | 0 | 0 | 37,852 | 288,616 | |
| 1.3.018.04.040.02 - OPS:SUPPORT TO CONTRACT 1300/CTS | 100,000 | 26,469 | 0 | 0 | 26,469 | 73,531 | |
| 1.3.018.06.080.07 - OPS:SUPPORT TO CONTRACT 1300/UMS | 300,000 | 11,384 | 0 | 0 | 11,384 | 288,616 | |
| 19 - OTHER SFMTA | 700,000 | 159,749 | 0 | 0 | 159,749 | 540,251 | |
| 1.3.019.01.080.07 - OTH.MTA SURVEY AND TESTING | 500,000 | 0 | 0 | 0 | 0 | 500,000 | |
| 1.3.019.07.080.07 - OTH.MTA SFMTA-SURVEY; TSTG [6840] | 0 | 714 | 0 | 0 | 714 | (714) | |
| 1.3.019.08.040.08 - OTH.MTA 1251 MATERIALS | 150,000 | 126,149 | 0 | 0 | 126,149 | 23,851 | |
| 1.3.019.08.080.08 - OTH.MTA OPERATION SUPPORT DUR1 | 50,000 | 32,887 | 0 | 0 | 32,887 | 17,113 | |

| [A] Cost Account Description | BUDGET | ACTUAL COSTS | | | | [G] VARIANCE (B - F) | COST REPORT NOTES |
|--|----------------------------|-----------------------|-------------------------|---------------------|-------------------|----------------------|-------------------|
| | [B] July 2015 Budget (YOE) | [C] PRIOR MONTH Total | [D] PRIOR MONTH Monthly | [E] CURRENT Monthly | [F] CURRENT Total | | |
| 21 - ARTS COMMISSION | 12,010,885 | 1,236,339 | 35,539 | 508,043 | 1,744,382 | 10,266,504 | |
| 1.3.021.01.040.06 - ARTS:CTYCO-ARTS COMMISSION CONSTRUCTION COSTS | 4,772,555 | 0 | 0 | 0 | 0 | 4,772,555 | |
| 1.3.021.01.080.03 - ARTS:CTYCO-ARTS COMMISSION [1227] | 2,030,147 | 388,167 | 0 | 0 | 388,167 | 1,641,980 | 40 |
| 1.3.021.01.080.04 - ARTS:CTYCO-ARTS COMMISSION [PWE335MPFUNA.CPT5441227] | 21,000 | 9,505 | 0 | 498 | 10,003 | 10,997 | |
| 1.3.021.06.080.03 - ARTS:CTYCO-ARTS COMMISSION PM [285MC.132J] | 158,970 | 93,176 | 10,597 | 76,399 | 169,575 | (10,605) | |
| 1.3.021.01.080.03 - ARTS:CTYCO-ARTS COMMISSION [PWE335MPFUNA.CPT5441327] | 0 | 0 | 0 | 804 | 804 | (804) | |
| 1.3.021.06.040.06 - ARTS:CTYCO-ARTS COMMISSION [68CPT5441327.CPT5441327] | 1,500,000 | 624,400 | 0 | 397,980 | 1,022,380 | 477,620 | |
| 1.3.021.06.040.06 - ARTS:CTYCO-ARTS COMMISSION [285MCPFUNA.CPT5441327] | 1,903,000 | 120,958 | 24,809 | 32,495 | 153,453 | 1,749,547 | |
| 1.3.021.01.080.03 - ARTS:CTYCO-ARTS COMMISSION [132J] | 465,213 | 0 | 0 | 0 | 0 | 465,213 | |
| 1.3.021.97.040.06 - ARTS:ARTS COMMISSION ALLOC CO | 1,160,000 | 0 | | | 0 | 1,160,000 | |
| 23 - CITY ATTORNEY'S OFFICE | 2,171,781 | 902,824 | 0 | 504,920 | 1,407,744 | 764,037 | |
| 1.3.023.01.080.06 - ATTY:CN LEGAL-CITY ATTORNEY OF | 2,171,781 | 902,824 | 0 | 504,920 | 1,407,744 | 764,037 | |
| 25 - PUBLIC UTILITIES COMMISSION SEWER | (2,925,296) | 0 | 0 | 0 | 0 | (2,925,296) | |
| 1.3.025.09.040.02 - STS.1256: SITE UTILITIES SFPUC SEWER MAIN | (2,925,296) | | | | | (2,925,296) | |
| 26 - PLANNING | 137,062 | 15,516 | 0 | 2,711 | 18,228 | 118,834 | |
| 1.3.026.01.080.06 - CM:CTYCO-PLANNING DEPARTMENT | 137,062 | 15,516 | 0 | 2,711 | 18,228 | 118,834 | |
| 28 - PUBLIC UTILITIES COMMISSION WATER | 4,242,012 | 2,039,995 | 19,402 | 58,898 | 2,098,894 | 2,143,118 | |
| 1.3.028.02.040.02 - CM:CTYCO-PUBLIC UTIL COMM. (PUC) | 0 | 4,745 | 0 | 0 | 4,745 | (4,745) | |
| 1.3.028.02.040.08 - PUC: FIELD OPERATIONS TUNNEL | 398,400 | 536,976 | 1,002 | 838 | 537,814 | (139,414) | |
| 1.3.028.02.080.04 - PUC:MTA CSP CN1252 [470465] | 105,000 | 91,587 | 0 | 0 | 91,587 | 13,413 | |
| 1.3.028.03.040.02 - PUC:CDD CONTRACT 1300/UMS SUPPORT | 606,354 | 35,313 | 25 | 1,197 | 36,510 | 569,844 | |
| 1.3.028.03.080.04 - PUC:CMB CONTRACT 1300/UMS INSPECTION | 230,000 | 30,363 | 0 | 0 | 30,363 | 199,637 | |
| 1.3.028.04.040.02 - PUC:CDD CONTRACT 1300/CTS SUPPORT | 271,755 | 85,670 | 3,824 | 19,809 | 105,479 | 166,276 | |
| 1.3.028.04.080.04 - PUC:CMB CONTRACT 1300/CTS INSPECTION | 115,000 | 6,956 | 0 | 0 | 6,956 | 108,044 | |
| 1.3.028.05.040.02 - PUC:CDD CONTRACT 1300/YBM SUPPORT | 450,282 | 84,536 | 10,410 | 27,309 | 111,846 | 338,436 | |
| 1.3.028.05.080.04 - PUC:CMB CONTRACT 1300/YBM INSPECTION | 184,000 | 4,008 | 0 | 0 | 4,008 | 179,992 | |
| 1.3.028.07.040.02 - PUC:PUC CDD WATER CONNECTION CONTRACT 1250 | 248,035 | 291,501 | 0 | 0 | 291,501 | (43,466) | |
| 1.3.028.07.080.04 - PUC:PUC CMB INSPECTION CONTRACT 1250 | 74,468 | 113,844 | 0 | 0 | 113,844 | (39,376) | |
| 1.3.028.08.040.02 - PUC:PUC CDD WATER CONNECTION CONTRACT 1251 [445] | 565,389 | 318,130 | 0 | 0 | 318,130 | 247,259 | |
| 1.3.028.08.080.04 - PUC:PUC CMB INSPECTION CONTRACT 1251 | 266,252 | 289,424 | 0 | 0 | 289,424 | (23,172) | |
| 1.3.028.09.040.02 - PUC:CMB CONTRACT 1300/STS SUPPORT | 520,077 | 82,207 | 4,141 | 9,745 | 91,952 | 428,125 | |
| 1.3.028.09.080.04 - PUC:CMB CONTRACT 1300/STS INSPECTION | 207,000 | 64,737 | 0 | 0 | 64,737 | 142,263 | |
| 32 - DPW - IDC ENGINEERING (HYDRAULIC) | 1,150,459 | 117,858 | 9,058 | 6,000 | 123,858 | 862,614 | |
| 1.3.032.01.080.04 - CM:DPW:1424J-BUREAU OF ENGINEERING (BOE) [AB12] | 60,000 | 92,818 | 1,937 | 585 | 93,403 | (33,403) | |
| 1.3.032.03.080.04 - DPW IDC HYDRAULIC CN1300 UMS SUPPORT | 297,938 | 1,011 | 0 | 0 | 1,011 | 296,927 | |
| 1.3.032.04.080.04 - DPW IDC HYDRAULIC CN1300 CTS SUPPORT | 295,639 | 5,188 | 1,555 | 0 | 5,188 | 290,451 | |
| 1.3.032.05.080.04 - DPW IDC HYDRAULIC CN1300 YBM SUPPORT | 301,882 | 4,193 | 2,883 | 4,049 | 8,242 | 293,640 | |
| 1.3.032.06.080.04 - DPW: BOE: 1300 DSDC | 6,000 | 0 | 0 | 0 | 0 | 6,000 | |
| 1.3.032.08.080.04 - DPW:HYRDDPW-BOE IDC ENG SVC DC | 9,000 | 0 | 0 | 0 | 0 | 9,000 | |

| [A] Cost Account Description | BUDGET | ACTUAL COSTS | | | | | |
|---|-------------------------------------|-----------------------------|----------------------------------|---------------------------|-------------------------|----------------------------|-------------------------|
| | [B] July 2015 Budget (YOE) | [C] PRIOR MONTH Total | [D] PRIOR MONTH Monthly | [E] CURRENT Monthly | [F] CURRENT Total | [G] VARIANCE (B - F) | COST REPORT NOTES |
| 1.3.032.09.080.04 - DPW IDC HYDRAULIC CN1300 STS SUPPOR | 180,000 | 14,647 | 2,683 | 1,366 | 16,013 | 163,987 | |

| [A] Cost Account Description | BUDGET | ACTUAL COSTS | | | | [G] VARIANCE (B - F) | COST REPORT NOTES |
|--|-------------------------------------|--------------------------------|----------------------------------|---------------------------|-------------------------|----------------------------|-------------------------|
| | [B] July 2015 Budget (YOE) | [C] PRIOR MONTH Total | [D] PRIOR MONTH Monthly | [E] CURRENT Monthly | [F] CURRENT Total | | |
| 34 - DPW - IDC CONSTRUCTION (CAPITAL) | 6,695,348 | 4,017,736 | 61,465 | 80,713 | 4,098,449 | 2,596,899 | |
| 1.3.034.02.080.04 - DPW:CONSTR:1252 CM [CD12] | 730,000 | 1,016,470 | 61,465 | 52,191 | 1,068,661 | (338,661) | |
| 1.3.034.02.080.04 - DPW:CONSTR:1252 CM [13AC12] | 206,000 | 138,397 | 0 | 0 | 138,397 | 67,603 | |
| 1.3.034.01.080.04 - DPW:BCM LABOR [2113] | 2,140,142 | 2,140,142 | 0 | 0 | 2,140,142 | 0 | |
| 1.3.034.06.080.04 - DPW:CONSTR:1300 CM [13CP12] | 3,619,206 | 722,726 | 0 | 28,522 | 751,249 | 2,867,957 | |
| 36 - DPW - BSM INFRASTRUCTURE (MAPPING) | 465,562 | 110,294 | 0 | 0 | 110,294 | 436,268 | |
| 1.3.036.01.080.04 - DPW:MPPG:DPW-BUREAU OF ST USE | 417,129 | 32,680 | 0 | 0 | 32,680 | 384,449 | |
| 1.3.036.02.080.04 - DPW:MPPG:1300-DPW-BUREAU OF ST USE | 0 | 33,084 | 0 | 0 | 33,084 | (33,084) | |
| 1.3.036.02.080.06 - DPW:MPPG:DPW-BUREAU OF ST USE | 90,000 | 44,530 | 0 | 0 | 44,530 | 45,470 | |
| 1.3.036.06.080.06 - DPW:MPPG:BSM PERMIT REVIEW | 39,433 | 0 | 0 | 0 | 0 | 39,433 | |
| 37 - DPW - PCS MATERIAL TESTING LABORATORY | 83,100 | 0 | 0 | 0 | 0 | 83,100 | |
| 1.3.037.01.080.07 - DPW.MTL.LABDPW-MATERIAL TESTIN | 83,100 | 0 | 0 | 0 | 0 | 83,100 | |
| 39 - DPW - PCS SITE ASSESSMENT & REMEDIATION (SAR) | 622,474 | 925,033 | 662 | 10,871 | 935,904 | (313,430) | |
| 1.3.039.01.080.04 - DPW:SITE ASSESSMENT & REMEDIATION (SAR) [132112] | 8,621 | 506,858 | 0 | 0 | 506,858 | (498,237) | |
| 1.3.039.01.080.04 - DPW:SITE ASSESSMENT & REMEDIATION (SAR) [2213] | 92,459 | 92,459 | 0 | 0 | 92,459 | 0 | |
| 1.3.039.01.080.04 - DPW:SITE ASSESSMENT & REMEDIATION (SAR) [2250] | 78,400 | 78,400 | 0 | 0 | 78,400 | 0 | |
| 1.3.039.01.080.04 -DPW:SITE ASSESSMENT & REMEDIATION (SAR) [2257] | 151,515 | 151,515 | 0 | 0 | 151,515 | 0 | |
| 1.3.039.01.080.04 - DPW:SITE ASSESSMENT & REMEDIATION (SAR) [2313] | 24,343 | 24,343 | 0 | 0 | 24,343 | 0 | |
| 1.3.039.01.080.04 - DPW:SITE ASSESSMENT & REMEDIATION | 58,757 | 0 | 0 | 10,043 | 10,043 | 48,713 | |
| 1.3.039.01.080.04 - DPW:SITE ASSESSMENT & REMEDIATION (SAR) [CE13] | 31,367 | 31,367 | 0 | 0 | 31,367 | 0 | |
| 1.3.039.01.080.04 - DPW:SITE ASSESSMENT & REMEDIATION (SAR) [CH13] | 100,000 | 8,621 | 0 | 0 | 8,621 | 91,379 | |
| 1.3.039.01.080.04 - DPW:SITE ASSESSMENT & REMEDIATION (SAR) | 17,000 | 0 | 0 | 0 | 0 | 17,000 | |
| 1.3.039.02.080.04 - DPW: SITE ASSESSMENT & REMEDIATION (SAR) – CN1252 [13CE11] | 18,632 | 16,880 | 662 | 0 | 16,880 | 1,753 | |
| 1.3.039.02.080.04 - DPW: SITE ASSESSMENT & REMEDIATION (SAR) – CN1300 [13CH11] | 41,379 | 14,590 | 0 | 828 | 15,417 | 25,962 | |
| 46 - MACY'S WEST - SFPUC SEWER WORK | 258,202 | 258,202 | 0 | 0 | 258,202 | 0 | |
| 1.3.046.08.040.02 - MCY.SWRC. CONTRACT: MACY'S-SEW | 258,202 | 258,202 | 0 | 0 | 258,202 | 0 | |
| 51 - 821 HOWARD STREET | 4,690,481 | 400,693 | 845 | 18,399 | 419,092 | 4,271,389 | |
| 1.3.051.01.080.03 - ODC.HWRD:ODCs - 821 HOWARD STR | 4,625,481 | 394,922 | 481 | 14,437 | 409,359 | 4,216,122 | |
| 1.3.051.02.080.04 - ODC.HWRD:ODCs - TUNNEL CONTRACT 1252 | 10,000 | 1,056 | 0 | 0 | 1,056 | 8,944 | |
| 1.3.051.06.080.04 - ODC.HWRD:ODCs - STATION CONTRACT 1300 | 55,000 | 4,715 | 363 | 3,961 | 8,677 | 46,323 | |
| 55 - 651 BRANNAN | 0 | 10,348 | 0 | 0 | 10,348 | (10,348) | |
| 1.3.055.01.080.03 - CM:ODCs - 651 BRANNAN STREET | 0 | 10,348 | 0 | 0 | 10,348 | (10,348) | 41 |

| [A] Cost Account Description | BUDGET | ACTUAL COSTS | | | | [G] VARIANCE (B - F) | COST REPORT NOTES |
|---|----------------------------|-----------------------|-------------------------|---------------------|--------------------|----------------------|-------------------|
| | [B] July 2015 Budget (YOE) | [C] PRIOR MONTH Total | [D] PRIOR MONTH Monthly | [E] CURRENT Monthly | [F] CURRENT Total | | |
| 63 - CENTRAL SUBWAY PARTNERSHIP - AECOM-EPC JV CONTRACT 149 | 42,373,401 | 21,963,885 | 190,838 | 2,076,206 | 24,040,091 | 18,333,310 | |
| 1.3.063.01.080.03 - CM:PM:AECOM.CS149 OM-EPC JV CS149-PM | 9,507,939 | 5,701,655 | (430,604) | 292,004 | 5,993,659 | 3,514,280 | 42 |
| 1.3.063.01.080.04 - CM:AECOM.CS149OM-EPC JV CS-149 [3B] | 5,218,630 | 2,994,990 | (645,906) | 438,006 | 3,432,996 | 1,785,634 | |
| 1.3.063.01.080.04 - CM:AECOM.CS149OM-EPC JV CS-149 [3E] | 7,000,000 | 6,777,144 | 1,067,124 | 1,205,669 | 7,982,813 | (982,813) | |
| 1.3.063.01.080.04 - CM:AECOM.CS149OM-EPC JV CS-149 [9B] | 11,042 | 11,042 | 0 | 0 | 11,042 | 0 | |
| 1.3.063.01.080.04 - CM:AECOM.CS149OM-EPC JV CS-149 [9D] | 550,000 | 515,694 | 0 | 0 | 515,694 | 34,306 | |
| 1.3.063.01.080.04 - CM:AECOM.CS149OM-EPC JV CS-149 [9E] | 600,000 | 383,415 | 200,223 | 140,528 | 523,943 | 76,057 | |
| 1.3.063.01.080.04 - FD:CM:EPC JV CS49-PM [123A] | 5,579,945 | 5,579,945 | 0 | 0 | 5,579,945 | 0 | |
| 1.3.063.97.080.03 - AECOM.CS149 ALLOCAT CONTING | 13,905,845 | | | | | 13,905,845 | |
| 67 - HILL INTERNATIONAL CONTRACT 156 | 11,000,000 | 1,148,899 | 69,614 | 33,134 | 1,182,034 | 8,945,227 | |
| 1.3.067.01.080.03 - HILL.CS156:HILL INTL. CS-156 [1337] | 10,000,000 | 1,021,639 | 69,614 | 33,134 | 1,054,773 | 8,945,227 | |
| 1.3.067.01.080.03 - HILL.CS156:HILL INTL. [133O] | 1,000,000 | 127,261 | 0 | 0 | 127,261 | 872,739 | |
| 69 - BAYLAND SOIL PROCESS CONTRACT 175 | 500,000 | 95,001 | 0 | 0 | 95,001 | 404,999 | 43 |
| 1.3.069.06.040.01 - BAYLAND.CS175:BAYLAND SOIL PROCESS [133K] | 500,000 | 95,001 | 0 | 0 | 95,001 | | |
| 71 - TUNNEL/UTILITIES - CONTRACT # CONTRACT 155-1 | 1,358,950 | 1,900,647 | 1,180 | 1,181 | 1,901,828 | (542,878) | |
| 1.3.071.01.080.04 - CM: CS155.1 DESIGN SUPPORT DURING CM [1232] | 0 | (87,201) | 0 | 0 | (87,201) | 87,201 | 44 |
| 1.3.071.02.080.04 - CM: CS155.1 DESIGN SUPPORT DURING CM [1332] | 1,358,950 | 1,987,849 | 1,180 | 1,181 | 1,989,030 | (630,080) | |
| 72 - STATIONS - CONTRACT # CONTRACT 155-2 | 8,752,240 | 2,915,074 | 188,805 | 196,468 | 3,111,542 | 5,640,698 | |
| 1.3.072.01.080.04 - CM: CS155.2 DESIGN SUPPORT DURING CM [1233] | 50,000 | 51,351 | 0 | 0 | 51,351 | (1,351) | 45 |
| 1.3.072.01.080.04 - CM: CS155.2 DESIGN SUPPORT DURING CM [1333] | 8,702,240 | 2,863,723 | 188,805 | 196,468 | 3,060,191 | 5,642,049 | |
| 73 - SYSTEMS/INTEGRATION - CONTRACT 155-3 | 4,828,269 | 936,359 | 55,026 | 47,059 | 983,418 | 3,844,851 | |
| 1.3.073.01.080.04 - CM: CS155.3 DESIGN SUPPORT DURING CM [1236] | 90,000 | 89,791 | 0 | 0 | 89,791 | 209 | |
| 1.3.073.01.080.04 - CM: CS155.3 DESIGN SUPPORT DURING CM [1334] | 4,738,269 | 846,568 | 55,026 | 47,059 | 893,627 | 3,844,642 | |
| 81 - UTILITIES RELOCATION #1 (PORTAL & MOS) - CONTRACT 1250 | 11,968,150 | 11,968,150 | 0 | 0 | 11,968,150 | 0 | |
| 1.3.081.07.040.01 - UR1.CONTRACT 1250:SITWORK: DEMOLIT | 167,458 | 167,458 | 0 | 0 | 167,458 | 0 | |
| 1.3.081.07.040.02 - UR1.CONTRACT 1250:SITWORK: UTILITI | 10,099,341 | 10,099,341 | 0 | 0 | 10,099,341 | 0 | |
| 1.3.081.07.040.03 - UR1.CONTRACT 1250:SITWORK:HAZMAT | 453,321 | 453,321 | 0 | 0 | 453,321 | 0 | |
| 1.3.081.07.040.08 - UR1.CONTRACT 1250:SITWORK:TEMPORAR | 1,248,030 | 1,248,030 | 0 | 0 | 1,248,030 | 0 | |
| 82 - UTILITIES RELOCATION #2 (UMS) - CONTRACT 1251 | 20,794,582 | 20,794,582 | 0 | 0 | 20,794,582 | 0 | 46 |
| 1.3.082.08.040.01 - UR2.CONTRACT 1251:SITWORK: DEMOLIT | 752,240 | 752,240 | 0 | 0 | 752,240 | 0 | |
| 1.3.082.08.040.02 - UR2.CONTRACT 1251:SITWORK:UTILITI | 10,328,044 | 10,328,044 | 0 | 0 | 10,328,044 | 0 | |
| 1.3.082.08.040.03 - UR2.CONTRACT 1251:SITWORK:HAZMAT | 172,712 | 172,712 | 0 | 0 | 172,712 | 0 | |
| 1.3.082.08.040.05 - UR2.CONTRACT 1251:SITWORK: STRUCTU | 2,706,431 | 2,706,431 | 0 | 0 | 2,706,431 | 0 | |
| 1.3.082.08.040.06 - UR2.CONTRACT 1251:SITWORK:PEDESTRA | 319,317 | 319,317 | 0 | 0 | 319,317 | 0 | |
| 1.3.082.08.040.07 - UR2.CONTRACT 1251:SITWORK:AUTO/BUS | 190,362 | 190,362 | 0 | 0 | 190,362 | 0 | |
| 1.3.082.08.040.08 - UR2.CONTRACT 1251:SITWORK:TEMP FAC | 6,325,476 | 6,325,476 | 0 | 0 | 6,325,476 | 0 | |
| GUIDEWAY TUNNELS TOTAL | 251,068,968 | 234,616,104 | 321,127 | 0 | 234,616,104 | 16,452,864 | |

| [A] Cost Account Description | BUDGET | ACTUAL COSTS | | | | [G] VARIANCE (B - F) | COST REPORT NOTES |
|---|----------------------------|-----------------------|-------------------------|---------------------|--------------------|----------------------|-------------------|
| | [B] July 2015 Budget (YOE) | [C] PRIOR MONTH Total | [D] PRIOR MONTH Monthly | [E] CURRENT Monthly | [F] CURRENT Total | | |
| 83 - GUIDEWAY TUNNELS - CONTRACT # 1252 BASE | 233,584,015 | 232,689,098 | 321,127 | 0 | 232,689,098 | 894,917 | |
| 1.3.083.02.010.06 - CONTRACT 1252:GUIDEWAY:UNDERGRN'D CUT | 60,446,425 | 61,377,878 | 20,000 | 0 | 61,377,878 | (931,453) | |
| 1.3.083.02.010.07 - CONTRACT 1252:GUIDEWAY:UNDERGROUND | 105,423,090 | 105,097,157 | 200,000 | 0 | 105,097,157 | 325,933 | |
| 1.3.083.02.020.03 - CONTRACT 1252: STATIONS: UNDERGROUND | 21,685,000 | 21,685,000 | 0 | 0 | 21,685,000 | 0 | |
| 1.3.083.02.040.01 - CONTRACT 1252:SITWORK:DEMO CLEARING | 2,440,000 | 2,440,000 | 0 | 0 | 2,440,000 | 0 | |
| 1.3.083.02.040.02 - CONTRACT 1252:SITWORK:UTILITIES & RE | 10,895,000 | 10,087,676 | 101,127 | 0 | 10,087,676 | 807,324 | |
| 1.3.083.02.040.03 - CONTRACT 1252:SITWORK:HAZMAT&MITIGAT | 200,000 | 0 | 0 | 0 | 0 | 200,000 | |
| 1.3.083.02.040.04 - CONTRACT 1252:SITWORK:ENVIRON. MITIG | 300,000 | 54,292 | 0 | 0 | 54,292 | 245,708 | |
| 1.3.083.02.040.06 - CONTRACT 1252:SITWORK:PED/BIKE ACCES | 50,000 | 50,000 | 0 | 0 | 50,000 | 0 | |
| 1.3.083.02.040.07 - CONTRACT 1252:SITWORK:AUTO/BUS ACCES | 1,345,000 | 1,345,000 | 0 | 0 | 1,345,000 | 0 | |
| 1.3.083.02.040.08 - CONTRACT 1252:SITWORK:TEMP FACILITIE | 30,799,500 | 30,552,095 | 0 | 0 | 30,552,095 | 247,405 | |
| 83 - GUIDEWAY TUNNELS - CONTRACT # 1252 CMODs | 1,326,807 | 1,927,006 | 0 | 0 | 1,927,006 | (505,199) | |
| 1.3.083.83.010.06 - CONTRACT 1252: CONTRACT MOD | 81,937 | 81,937 | 0 | 0 | 81,937 | 0 | |
| 1.3.083.83.010.07 - CONTRACT 1252: CONTRACT MOD | 1,180,322 | 1,280,322 | 0 | 0 | 1,280,322 | (100,000) | |
| 1.3.083.83.020.03 - CONTRACT 1252: CONTRACT MOD | 965,121 | 1,077,223 | 0 | 0 | 1,077,223 | (112,102) | |
| 1.3.083.83.040.02 - CONTRACT 1252: CONTRACT MOD | 1,665,207 | 1,665,208 | 0 | 0 | 1,665,208 | (1) | |
| 1.3.083.83.040.03 - CONTRACT 1252: CONTRACT MOD | 401,933 | 401,933 | 0 | 0 | 401,933 | 0 | |
| 1.3.083.83.040.08 - CONTRACT 1252: CONTRACT MOD | (2,872,713) | (2,579,617) | 0 | 0 | (2,579,617) | (293,096) | |
| 1.3.083.93.010.07 - CONTRACT 1252: TUNNEL ALLOC CONTING | 16,063,146 | 0 | 0 | 0 | 0 | 16,063,146 | 47 |
| CONTRACT 1300 - STATIONS, TRACKWORK AND SYSTEMS TOTAL | 859,676,400 | 269,197,952 | 8,237,998 | 7,846,357 | 277,044,309 | 584,838,853 | 47a |
| 84 - UNION SQUARE/MARKET STREET STATION (UMS) - WORK PACKAGE 1253 | 294,030,590 | 117,589,696 | 5,085,312 | 3,119,420 | 120,709,116 | 173,321,474 | 17 |
| 1.3.084.03.020.03 - UMS.1253: UNDERGROUD STATION | 213,090,872 | 83,899,659 | 2,910,000 | 700,000 | 84,599,659 | 128,491,213 | |
| 1.3.084.03.020.07 - UMS.1253: ELEVATORS ESCALATOR | 9,465,694 | 638,918 | 1,000,000 | 0 | 638,918 | 8,826,776 | |
| 1.3.084.03.040.01 - UMS.1253: DEMOLITION CLEARING | 13,882,350 | 2,272,326 | 75,312 | 300,000 | 2,572,326 | 11,310,024 | |
| 1.3.084.03.040.02 - UMS.1253: SITE UTILITIES UTIL | 3,971,620 | 932,601 | 100,000 | 300,000 | 1,232,601 | 2,739,019 | |
| 1.3.084.03.040.03 - UMS.1253: HAZARDOUS MATERIALS | 550,000 | 0 | 0 | 0 | 0 | 550,000 | |
| 1.3.084.03.040.04 - UMS.1253: ENVIRONMENTAL MITIGA | 244,500 | 90,000 | 0 | 7,250 | 97,250 | 147,250 | |
| 1.3.084.03.040.06 - UMS.1253: PEDESTRIAN/BIKE | 18,969 | 12,500 | 0 | 0 | 12,500 | 6,469 | |
| 1.3.084.03.040.07 - UMS.1253: AUTOMOBILE BUS ACCE | 1,547,185 | 22,000 | 0 | 0 | 22,000 | 1,525,185 | |
| 1.3.084.03.040.08 - UMS.1253: TEMPORARY FACILITIES | 40,667,193 | 25,500,000 | 1,000,000 | 1,312,170 | 26,812,170 | 13,855,022 | |
| 1.3.084.03.050.02 - UMS.1253: TRAFFIC SIGNALS AND | 8,200,230 | 4,146,654 | 0 | 500,000 | 4,646,654 | 3,553,577 | |
| 1.3.084.03.050.03 - UMS.1253: TRACTION POWER SUPPL | 1,865,534 | 9,000 | 0 | 0 | 9,000 | 1,856,534 | |
| 1.3.084.03.050.04 - UMS.1253: TRACTION POWER DISTR | 216,957 | 66,037 | 0 | 0 | 66,037 | 150,920 | |
| 84 - UNION SQUARE/MARKET STREET STATION (UMS) CMODs | 90,000 | 152,229 | 0 | 0 | 152,229 | 17,941 | |
| 1.3.084.84.020.07 - CMOD:UMS.1253: ELEVATORS, ESCALATORS | 170,170 | 152,229 | 0 | 0 | 152,229 | 17,941 | |
| 1.3.084.94.020.03 - UMS.1253: AC: ALLOC CONTING | 4,910,000 | 0 | 0 | 0 | 0 | 4,910,000 | 48 |

| [A] Cost Account Description | BUDGET | ACTUAL COSTS | | | | [G] VARIANCE (B - F) | COST REPORT NOTES |
|--|----------------------------|-----------------------|-------------------------|---------------------|-------------------|----------------------|-------------------|
| | [B] July 2015 Budget (YOE) | [C] PRIOR MONTH Total | [D] PRIOR MONTH Monthly | [E] CURRENT Monthly | [F] CURRENT Total | | |
| 85 - CHINATOWN STATION (CTS) - WORK PACKAGE 1254 | 247,567,810 | 61,086,205 | 290,166 | 1,296,158 | 62,382,363 | 187,388,209 | |
| 1.3.085.04.020.03 - CTS.1254: UNDERGROUND STATION | 190,695,749 | 21,500,000 | 290,166 | 490,067 | 21,990,067 | 168,705,682 | |
| 1.3.085.04.020.07 - CTS.1254: ELEVATORS ESCALATOR | 6,579,897 | 747,355 | 0 | 0 | 747,355 | 5,832,542 | |
| 1.3.085.04.040.01 - CTS.1254: DEMOLITION CLEARING | 1,000,000 | 12,500,000 | 0 | 125,324 | 12,625,324 | (11,625,324) | |
| 1.3.085.04.040.02 - CTS.1254: SITE UTILITIES UTIL | 3,951,718 | 2,462,450 | 0 | 0 | 2,462,450 | 1,489,268 | |
| 1.3.085.04.040.03 - CTS.1254: HAZARDOUS MATERIALS | 350,000 | 177,113 | 0 | 0 | 177,113 | 172,887 | |
| 1.3.085.04.040.04 - CTS.1254: ENVIRONMENTAL MITIGA | 100,000 | 3,286 | 0 | 0 | 3,286 | 96,714 | |
| 1.3.085.04.040.06 - CTS.1254: PEDESTRIAN/BIKE | 92,164 | 0 | 0 | 0 | 0 | 92,164 | |
| 1.3.085.04.040.07 - CTS.1254: AUTOMOBILE BUS ACCE | 252,770 | 18,500 | 0 | 1,000 | 19,500 | 233,270 | |
| 1.3.085.04.040.08 - CTS.1254: TEMPORARY FACILITIES | 36,734,725 | 21,500,000 | 0 | 576,915 | 22,076,915 | 14,657,811 | |
| 1.3.085.04.050.02 - CTS.1254: TRAFFIC SIGNALS AND | 3,385,621 | 1,950,000 | 0 | 102,852 | 2,052,852 | 1,332,769 | |
| 1.3.085.04.050.03 - CTS.1254: TRACTION POWER SUPPL | 4,063,927 | 227,500 | 0 | 0 | 227,500 | 3,836,427 | |
| 1.3.085.04.050.05 - CTS.1254: COMMUNICATIONS | 361,238 | 0 | 0 | 0 | 0 | 2,564,000 | |
| 85 - CHINATOWN STATION (CTS) CMODs | 230,956 | 173,192 | 0 | 0 | 173,192 | 57,764 | |
| 1.3.085.85.020.03 - CMOD:CTS.1254: UNDERGROUND STATION | 75,000 | 24,980 | 0 | 0 | 24,980 | 50,020 | |
| 1.3.085.85.040.01 - CMOD:CTS.1254: POWER POLE | 155,956 | 148,212 | 0 | 0 | 148,212 | 7,744 | |
| 1.3.085.95.020.03 - CTS.1254: AC: ALLOC CONTING | 4,769,044 | 0 | 0 | 0 | 0 | 4,769,044 | 49 |
| 86 - YERBA BUENA MOSCONE STATION (YBM) - WORK PACKAGE 1255 | 158,089,000 | 67,951,191 | 2,249,380 | 2,310,779 | 70,261,970 | 87,827,030 | |
| 1.3.086.05.020.03 - YBM.1255: UNDERGROUND STATION | 108,386,008 | 43,070,875 | 2,000,000 | 1,030,779 | 44,101,654 | 64,284,354 | |
| 1.3.086.05.020.07 - YBM.1255: ELEVATORS ESCALATOR | 5,333,287 | 385,672 | 149,380 | 0 | 385,672 | 4,947,615 | |
| 1.3.086.05.040.01 - YBM.1255: DEMOLITION CLEARING | 3,227,506 | 3,005,506 | 0 | 80,000 | 3,085,506 | 142,000 | |
| 1.3.086.05.040.02 - YBM.1255: SITE UTILITIES UTIL | 6,520,189 | 4,248,501 | 100,000 | 100,000 | 4,348,501 | 2,171,689 | |
| 1.3.086.05.040.03 - YBM.1255: HAZARDOUS MATERIALS | 2,829,439 | 537,151 | 0 | 0 | 537,151 | 2,292,288 | |
| 1.3.086.05.040.04 - YBM.1255: ENVIRONMENTAL MITIGA | 125,665 | 85,141 | 0 | 0 | 85,141 | 40,524 | |
| 1.3.086.05.040.06 - YBM.1255: PEDESTRIAN/BIKE | 16,665 | 0 | 0 | 0 | 0 | 16,665 | |
| 1.3.086.05.040.07 - YBM.1255: AUTOMOBILE BUS ACCE | 1,542,725 | 248,200 | 0 | 0 | 248,200 | 1,294,525 | |
| 1.3.086.05.040.08 - YBM.1255: TEMPORARY FACILITIES | 20,459,591 | 12,965,605 | 0 | 1,000,000 | 13,965,605 | 6,493,986 | |
| 1.3.086.05.050.02 - YBM.1255: TRAFFIC SIGNALS AND | 5,622,218 | 3,348,741 | 0 | 100,000 | 3,448,741 | 2,173,477 | |
| 1.3.086.05.050.03 - YBM.1255: TRACTION POWER SUPPL | 3,708,425 | 55,800 | 0 | 0 | 55,800 | 3,652,625 | |
| 1.3.086.05.050.05 - YBM.1255: COMMUNICATIONS | 317,283 | 0 | 0 | 0 | 0 | 317,283 | |

| [A] Cost Account Description | BUDGET | ACTUAL COSTS | | | | [G] VARIANCE (B - F) | COST REPORT NOTES |
|--|----------------------------|-----------------------|-------------------------|---------------------|-------------------|----------------------|-------------------|
| | [B] July 2015 Budget (YOE) | [C] PRIOR MONTH Total | [D] PRIOR MONTH Monthly | [E] CURRENT Monthly | [F] CURRENT Total | | |
| 86 - YERBA BUENA MOSCONE STATION (YBM) CMODs | (1,833,869) | 0 | 0 | 0 | 0 | (1,833,869) | |
| 1.3.086.86.040.08 - CMOD:YBM.1255: TEMPORARY FACILITIES | (1,833,869) | 0 | 0 | 0 | 0 | (1,833,869) | |
| 1.3.086.96.020.03 - YBM.1255: AC: ALLOC CONTING | 6,833,869 | 0 | 0 | 0 | 0 | 6,833,869 | 50 |
| 87 - SURFACE TRACKWORK AND SYSTEMS -WORK PACKAGE 1256 | 139,989,000 | 22,245,439 | 613,140 | 1,120,000 | 23,365,439 | 116,623,561 | |
| 1.3.087.09.010.02 - STS.1256: GUIDEWAY: AT-GRADE SEMI-EXCLUSIVE (ALLOWS CROSS TRAFFIC) | 9,150,000 | 870,000 | 0 | 0 | 870,000 | 8,280,000 | |
| 1.3.087.09.010.07 - STS.1256: GUIDEWAY: UNDERGROUND | 28,745,910 | 926,983 | 0 | 0 | 926,983 | 27,818,926 | |
| 1.3.087.09.010.12 - STS.1256: TRACK: SPECIAL | 4,449,637 | 80,000 | 0 | 0 | 80,000 | 4,369,637 | |
| 1.3.087.09.020.01 - STS.1256: AT-GRADE STATION | 11,582,176 | 835,459 | 0 | 0 | 835,459 | 10,746,717 | |
| 1.3.087.09.020.03 - STS.1256: UNDERGROUND STATION | 75,000 | 0 | 0 | 0 | 0 | 75,000 | |
| 1.3.087.09.040.01 - STS.1256: DEMOLITION, CLEARING, EARTHWORK | 26,066 | 0 | 0 | 0 | 0 | 26,066 | |
| 1.3.087.09.040.02 - STS.1256: SITE UTILITIES, UTILITY RELOCA | 17,513,463 | 5,022,036 | 200,000 | 500,000 | 5,522,036 | 11,991,427 | |
| 1.3.087.09.040.03 - STS.1256: HAZARDOUS MATERIALS | 200,000 | 0 | 0 | 0 | 0 | 200,000 | |
| 1.3.087.09.040.04 - STS.1256: ENVIRONMENTAL MITIGA | 50,000 | 0 | 0 | 0 | 0 | 50,000 | |
| 1.3.087.09.040.07 - STS.1256: AUTOMOBILE BUS ACCE | 2,116,925 | 39,000 | 0 | 0 | 39,000 | 2,077,925 | |
| 1.3.087.09.040.08 - STS.1256: TEMPORARY FACILITIES | 15,622,795 | 9,831,892 | 300,000 | 500,000 | 10,331,892 | 5,290,903 | |
| 1.3.087.09.050.02 - STS.1256: TRAFFIC SIGNALS AND | 4,956,458 | 1,420,019 | 0 | 20,000 | 1,440,019 | 3,516,439 | |
| 1.3.087.09.050.03 - STS.1256: TRACTION POWER SUPPL | 9,906,014 | 511,160 | 0 | 0 | 511,160 | 9,394,854 | |
| 1.3.087.09.050.05 - STS.1256: COMMUNICATIONS | 4,195,033 | 0 | 0 | 0 | 0 | 4,195,033 | |
| 1.3.087.09.050.07 - STS.1256: CENTRAL CONTROL | 31,399,523 | 2,708,890 | 0 | 100,000 | 2,808,890 | 28,590,633 | |
| 87 - SURFACE TRACKWORK AND SYSTEMS (STS) CMODs | 80,170 | 0 | 0 | 0 | 0 | 80,170 | |
| 1.3.087.89.040.02 - CMOD:STS.1256: SITE UTILITIES, UTILITY RELOCA | 80,170 | 0 | 0 | 0 | 0 | 80,170 | |
| 1.3.087.99.020.01 - STS.1256: AC: ALLOC CONTING | 4,919,830 | 0 | 0 | 0 | 0 | 4,919,830 | 51 |
| 141 - CONSTRUCTION ADMINISTRATION | 2,956,812 | 0 | 0 | 0 | 0 | 2,956,812 | |
| 1.3.141.97.080.04 - CONSTR.ADMIN:ALLOC CONTING | 2,956,812 | | | | | 2,956,812 | |
| 142 - LEGAL/PERMITS | 2,014,204 | 0 | 0 | 0 | 0 | 2,014,204 | |
| 1.3.142.01.080.06 - LGL.PRMTSF:LEGAL; PERMITS | 2,014,204 | 0 | 0 | 0 | 0 | 2,014,204 | |
| 144 - STARTUP | 8,300,329 | 0 | 0 | 0 | 0 | 8,300,329 | |
| 1.3.144.01.080.08 - STRT: STARTUP | 6,941,907 | 0 | 0 | 0 | 0 | 6,941,907 | |
| 1.3.144.97.080.08 - STRTA: AC STARTUP ALLOC CONTIN | 1,358,422 | | | | | 1,358,422 | |
| 151 - TEMPORARY LICENSE AGREEMENT | 17,000 | 0 | 0 | 0 | 0 | 17,000 | |
| 1.3.151.01.080.06 - TEMP.LICPORARY LICENSE AGREEME | 17,000 | 0 | 0 | 0 | 0 | 17,000 | |
| 170 - COMMUNICATIONS CONNECTIONS | 5,757,629 | 0 | 0 | 0 | 0 | 5,757,629 | |
| 1.3.170.01.050.04 - COMM.CONNN:COMMUNICATION CONN | 5,757,629 | 0 | 0 | 0 | 0 | 5,757,629 | |
| 181 - AON RISK INSURANCE CS 163 | 18,113,750 | 18,798,132 | 0 | 0 | 18,798,132 | (684,382) | |
| 1.3.181.01.040.08 - AON.CS163 AON RISK INS. | 18,088,750 | 18,773,132 | 0 | 0 | 18,773,132 | (684,382) | |
| 1.3.181.01.080.03 - AON.CS171 AON RISK INS. STUDY | 25,000 | 25,000 | 0 | 0 | 25,000 | 0 | |

| [A] Cost Account Description | BUDGET | ACTUAL COSTS | | | | [G] VARIANCE (B - F) | COST REPORT NOTES |
|--|----------------------------|-----------------------|-------------------------|---------------------|-------------------|----------------------|-------------------|
| | [B] July 2015 Budget (YOE) | [C] PRIOR MONTH Total | [D] PRIOR MONTH Monthly | [E] CURRENT Monthly | [F] CURRENT Total | | |
| 191 - FARE COLLECTION CONTRACTOR | 5,400,000 | 0 | 0 | 0 | 0 | 5,400,000 | |
| 1.3.191.01.050.06 - FARE.CONSUM:FARE COLLECTION | 5,400,000 | 0 | 0 | 0 | 0 | 5,400,000 | |
| 192 - THALES T&S CENTRAL CONTROL | 487,972 | 50,000 | 0 | 0 | 50,000 | 437,972 | |
| 1.3.192.01.050.01 - THALES T&S ATCS | 487,972 | 50,000 | 0 | 0 | 50,000 | 437,972 | |
| 202 - JOC2-022.0 | 63,938 | 0 | 0 | 0 | 0 | 63,938 | |
| 1.3.202.01.040.02 - JOC2-022:15&22 POTHOLING UTILI LGHT FNDS | 63,938 | 0 | 0 | 0 | 0 | 63,938 | |
| 203 - JOC2-029.0 | 53,317 | 0 | 0 | 0 | 0 | 53,317 | |
| 1.3.203.07.040.02 - JOC0292-029: RELOCATE VAULTS-S | 53,317 | 0 | 0 | 0 | 0 | 53,317 | |
| 302 - PG&E | 1,988,173 | 2,749,912 | 0 | 0 | 2,749,912 | (761,739) | |
| 1.3.302.03.050.03 - PGE PERMANENT POWER UMS | (2,350,000) | 0 | 0 | 0 | 0 | (2,350,000) | |
| 1.3.302.03.050.03 - PGE POWER FEED UMS | 2,959,826 | 115,690 | 0 | 0 | 115,690 | 2,844,136 | |
| 1.3.302.04.050.03 - PGE PERMANENT POWER CTS | (2,350,000) | 0 | 0 | 0 | 0 | (2,350,000) | |
| 1.3.302.04.050.03 - PGE POWER FEED CTS | 2,959,826 | 0 | 0 | 0 | 0 | 2,959,826 | |
| 1.3.302.05.050.03 - PGE PERMANENT POWER YBM | (2,368,540) | 0 | 0 | 0 | 0 | (2,368,540) | |
| 1.3.302.05.050.03 - PGE POWER FEED YBM | 3,125,222 | 2,634,222 | 0 | 0 | 2,634,222 | 491,000 | |
| 1.3.302.09.050.03 - PGE POWER FEED STS | 11,839 | 0 | 0 | 0 | 0 | 11,839 | |
| 331 - BAY AREA RAPID TRANSIT (BART) | 951,356 | 60,455 | 0 | 0 | 60,455 | 890,901 | |
| 1.3.331.01.080.04 - CM:SFMTA LABOR-ENG SVCS-IRP/BART/SF | 0 | 33,152 | 0 | 0 | 33,152 | (33,152) | |
| 1.3.331.01.080.06 - CM: BAY AREA RAPID TRANSIT (BART) [122A] | 951,356 | 27,304 | 0 | 0 | 27,304 | 924,052 | |
| 333 - AMERICAN PUBLIC TRANSP. ASSOCIATION (APTA) CS-APTA | 146,500 | 54,612 | 0 | 0 | 54,612 | 91,888 | |
| 1.3.333.01.080.03 - APTA:APTA - IRP [2G] | 46,500 | 27,304 | 0 | 0 | 27,304 | 19,196 | |
| 1.3.333.01.080.03 - APTA:APTA - IRP [2C] | 100,000 | 27,308 | 0 | 0 | 27,308 | 72,692 | |
| 334 - BART FARE COLLECTION SYSTEM | 700,000 | 0 | 0 | 0 | 0 | 700,000 | |
| 1.3.334.01.050.06 - BART:BART FARE COLLECTION EQP | 700,000 | 0 | 0 | 0 | 0 | 700,000 | |
| 401 - ECONOMIC AND WORKFORCE DEVELOPMENT (EWD) | 17,600 | 17,600 | 0 | 0 | 17,600 | 0 | |
| 1.3.401.01.080.04 - EWD: MAYORS OFFICE ECON DEV | 17,600 | 17,600 | 0 | 0 | 17,600 | 0 | |
| 402 - DEPARTMENT OF TECHNOLOGY | 242,371 | 237,534 | 0 | 0 | 237,534 | 4,837 | |
| 1.3.402.07.050.04 - DT:1UTL:COMM. CONNECTIONS | 166,756 | 166,179 | 0 | 0 | 166,179 | 577 | |
| 1.3.402.08.050.04 - DT:2UTL:COMM.CONNECTIONS | 75,615 | 71,354 | 0 | 0 | 71,354 | 4,261 | |
| 404 - DEPARTMENT OF BUILDING INSPECTION (DBI) | 1,204,081 | 1,204,081 | 0 | 0 | 1,204,081 | 0 | |
| 1.3.404.01.080.06 - DPT OF BUILDING INSPECTION | 1,204,081 | 1,204,081 | 0 | 0 | 1,204,081 | 0 | |

| [A] Cost Account Description | BUDGET | ACTUAL COSTS | | | | [G] VARIANCE (B - F) | COST REPORT NOTES |
|---|-------------------------------------|--------------------------------|----------------------------------|---------------------------|-------------------------|----------------------------|-------------------------|
| | [B] July 2015 Budget (YOE) | [C] PRIOR MONTH Total | [D] PRIOR MONTH Monthly | [E] CURRENT Monthly | [F] CURRENT Total | | |
| 491 - FORM B - REIMBURSEMENT | (12,227,954) | 0 | 0 | 0 | 0 | (12,227,954) | |
| 1.3.491.02.040.02 - FORMB - CONTRACT 1252 UTILITY REIMBUR | (254,050) | 0 | | | 0 | (254,050) | 52 |
| 1.3.491.03.040.02 - FORMB - UMS:CONTRACT 1300 UTILITY REIMBURSEMENT | (528,370) | 0 | | | 0 | (528,370) | 53 |
| 1.3.491.04.040.02 - FORMB - CTS:CONTRACT 1300 UTILITY REIMBURSEMENT | (451,703) | 0 | | | 0 | (451,703) | 54 |
| 1.3.491.05.040.02 - FORMB - YBM:CONTRACT 1300 UTILITY REIMBURSEMENT | (100,000) | 0 | | | 0 | (100,000) | 55 |
| 1.3.491.07.040.02 - FORMB - CONTRACT 1250 UTILITY REIMBUR | (2,275,419) | 0 | | | 0 | (2,275,419) | 57 |
| 1.3.491.08.040.02 - FORMB - CONTRACT 1251 UTILITY REIMBUR | (7,618,412) | 0 | | | 0 | (7,618,412) | 58 |
| 1.3.491.09.040.02 - FORMB - STS:CONTRACT 1300 UTILITY REIMBURSEMENT | (1,000,000) | 0 | | | 0 | (1,000,000) | 59 |
| TOTAL CONSTRUCTION PHASE | 1,343,370,949 | 613,967,250 | 9,886,251 | 12,235,237 | 626,202,487 | 707,277,471 | |
| 1.4.091.01.070.01 - LRVS: LIGHT RAIL VEHICLES PROJECT MGT [68E] | 827,132 | 827,132 | 0 | 0 | 827,132 | 0 | |
| 1.4.091.01.070.01 - LRVS: LIGHT RAIL VEHICLES RFP [34B] | 1,325,000 | 1,319,773 | 0 | 0 | 1,319,773 | 5,227 | |
| 1.4.091.01.070.01 - LRVS: LRV PROCUREMENT ODC | 25,000 | 0 | 0 | 0 | 0 | 25,000 | |
| 1.4.091.01.070.01 - LRVS: LRV PROCUREMENT | 11,131,868 | 0 | 0 | 0 | 0 | 11,131,868 | |
| 1.4.091.97.070.01 - LRVA:AC: VEHICLES ALLOC CONTI | 13,076,653 | | | | | 13,076,653 | 19 |
| TOTAL VEHICLES | 26,385,653 | 2,146,905 | 0 | 0 | 2,146,905 | 24,238,748 | |
| 1.5.015.01.060.01 - RE: EASEMENT ACQUISIT | 400,000 | 322,939 | 0 | 0 | 322,939 | 77,061 | |
| 1.5.015.01.060.01 - RE: REAL EST SITE ACQ | 18,474,878 | 14,307,667 | 0 | 0 | 14,307,667 | 4,167,211 | |
| 1.5.015.01.060.01 - RE: REAL ESTATE | 750,000 | 765,960 | 6,247 | 312 | 766,272 | (16,272) | |
| 1.5.015.01.060.01 - RE: REC & PARK MOU | 6,987,624 | 6,987,624 | 0 | 0 | 6,987,624 | 0 | |
| 1.5.015.01.060.01 - RE:-DEPT OF TRANSPOR | 5,000,000 | 2,686,000 | 0 | 0 | 2,686,000 | 2,314,000 | |
| 1.5.015.01.060.01 - RE:-LICENSES FEES | 400,000 | 381,311 | 0 | 0 | 381,311 | 18,689 | |
| 1.5.015.97.060.01 - RE:A:AC: RE: REAL ESTATE ALLO | 1,000,000 | | | | | 1,000,000 | |
| 1.5.023.01.060.01 - ATTY:REAL ES | 2,212,882 | 2,469,421 | 0 | 156,567 | 2,625,988 | (413,106) | |
| 1.5.101.01.060.02 - RES.RELO: RELOCATION COST | 1,275,200 | 1,289,701 | (2,300) | 0 | 1,289,701 | (14,501) | |
| 1.5.102.01.060.02 - COMM.RELO-RELOC COMMERCIAL | 905,311 | 584,194 | 0 | 0 | 584,194 | 321,117 | |
| TOTAL ROW, LAND, EXISTING IMPROVEMENTS | 37,405,895 | 29,794,818 | 3,947 | 156,879 | 29,951,697 | 7,454,198 | |
| 90 - CONTINGENCY | 80,473,077 | | | | | 80,473,077 | |
| 1.7.500.91.090.00 - UNALLOCATED CONTINGENCY | 9,519,456 | | | | | 9,519,456 | 60 |
| TOTAL ALLOCATED CONTINGENCY | 70,953,621 | | | | | 70,953,621 | |
| TOTAL PROJECT COST | 1,578,300,001 | 806,380,867 | 9,890,199 | 12,392,117 | 818,772,983 | 749,636,028 | |

| 7.1 Program Project Budget | |
|----------------------------|--|
| 1 | The Central Subway Project (CSP) (SFMTA Capital Program CPT 544) is defined in the FTA-SFMTA October 2012 Full Funding Grant Agreement with a budget of \$1.578 billion. |
| 2 | The TBM Retrieval Shaft Relocation (SFMTA Capital Program CPT 690) is one of four capital projects that is related to CSP. These projects are reported for background information as needed outside of the main body of the Project Monthly Progress Report. |
| 3 | The Chinatown Plaza (CPT 718) is for Chinatown Station enhancement capital project. The project has funding outside of the Central Subway Project. The construction is carried out in Contract 1300 Contract Modifications. |
| 4 | CPT 665 is a Real Estate project to relocation in compliance with California regulations for business relocations but outside of the Central Subway Project as defined by the FTA FFGA. |
| 5 | CPT 705 is an SFMTA capital improvement between the Agency and community stakeholders outside of the Central Subway Project. |
| 6 | Utility company reimbursements (Form B) result in funds received for work carried out on behalf of utilities concurrent to CSP work to achieve efficiencies. |
| 7 | PG&E Power Feed reimbursement funds are the refunds from PG&E when completion of Stations construction and switch to permanent power. |
| 8 | BART Elevator funds are reimbursements for work carried out on behalf of BART to install BART Powell Street Station elevator. |
| 9 | The Tutor Perini - CAD Files funds are the result of payments by the contractor for project documentation not included in the contract. |
| 10 | SFPUC Sewer Main funds are reimbursements for work carried out on behalf of San Francisco PUC (includes 10% construction contingency). |
| 11 | SFPUC 24" Water Main funds are reimbursements for work carried out in Contract 1252 Contract Modification #41 on behalf of San Francisco PUC (includes construction management cost). |
| 12 | Traffic Effectiveness Project funded Contract Modification #40 for Culvert, Street & Sidewalk Restoration in North Beach are reimbursements for work carried out in Contract 1252 on behalf of SFMTA SSD. |
| 13 | SFPUC North Beach 24" Water Main Additional Work funds are reimbursements for work carried out in Contract 1252 Contract Modification #48 on behalf of San Francisco PUC (includes construction management cost). |

| 7.4 Contingency Management Trend Report | |
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| 14 | In Oct 2014 Report, updated Contract 1250 contract cost to segregate contract amount and contract modification amount. Note that September 2013 Supplemental Authorized Contingency "column f" did not include completed contract. |
| 15 | In Oct 2014 Report, updated Contract 1251 contract cost to segregate contract amount and contract modification amount. Note that September 2013 Supplemental Authorized Contingency "column f" did not include completed contract. |

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| 16 | Contract 1252 Original Contract Value "column a" and Original Congency "column f" did not match September 2013 Supplemental due to Supplemental were used the revised value to reflet Contract Modifications #3-#18. Reduced Contract 1252 contingency to reflect CMod #20 for retrieval shaft relocation cost \$5.15M funded by CPT690, CMod #40 for Culvert, Street & Sidewalk Restoration cost \$694,651 funded by Traffic Effectiveness Project, and CMod #41 for install 24" Water Main in North Beach cost \$328,860 funded by SFPUC. |
| 17 | BART Elevator scope and SFPUC Sewer Main scope is in Contract 1300; effort will be funded by BART. In January 2015 Report, corrected Station Contract value to match awarded amount. |
| 18 | In April 2015 report, real estate budget stated in RAMP Rev5 is \$36.7M, including \$1M contingency. The cost workbook ROW & contingency budget reflects this with \$36,511,799 and \$1,000,000 respectively. Revised costbook ROW budget & contingency to be \$37,511,799. The \$4,265,478 Caltrans lease savings is used for ROW allocated contingency. |
| 19 | In Dec 2014 Report, redistributed LRV budget to reflect recent firm bid cost per vehicle (\$3,327,250/unit) from vehicle procurement contract award. (SFMTA Board meeting 15JUL14, calendar item #11). Vehicle line item total budget remains unchanged, redistributed fund by reducing base amount to \$13,309,000, column "c" and increased allocated contingency column "h", by same amount. |
| 20 | In Oct 2014 Report, made two corrections: i) revised Professional Services, Original Contract Value "column a" from \$310,518,041 to \$310,618,041, ii) revised Original Congency. "column f" unallocated contingency from \$3,883,481 to \$3,845,945. In April 2015 report, used \$500K program contingency for CS-175 Bayland Soil Process contract. |
| 21 | The total Central Subway Project budget of \$1.578 billion, based on the October 2012 FFGA with the FTA, is the primary MPR report reference. |
| 22 | Estimate at Completion is shown at Column "e". |
| 23 | Estimate at Completion vs. Budget variance is shown at Column "k". |

7.5 Contract Modification/Trend Log - Contract 1300 Stations

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| 24 | Reported all trend cost for Contractor Change Order Requests and Proposed Contract Change and applied probability to forecasted trends. |
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7.6 Budget Revisions: Report Sorted by Construction Packages

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| 25 | In Dec 2014 Report, reduced CN1252 allocated contingency by \$28K to excute Contract Modification #46, see Report 7.5 Detail Contingency Usage Report. |
| 26 | Program contingency increased by \$500,000. Refer to Report Notes #20. |
| 27 | Program contingency increased by \$5,265,478. Refer to Report Notes #18. |
| 28 | In Dec 2014 Report, redistributed LRV budget to reflect recent firm bid cost per vehicle (\$3,327,250/unit) from vehicle procurement contract award. (SFMTA Board meeting 15JUL14, calendar item #11). Vehicle line item total budget remains unchanged, redistributed fund by reducing base amount to \$13,309,000 and increased allocated contingency by same amount. |
| 29 | Program contingency decreased by \$500,000. Refer to Reports #20. |

7.9 Detail Monthly Expenditure Report

Phase 1 Preliminary Engineering

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| 30 | Famis cost for Preliminary Engineering (PE) is \$48,210,903.71. Cost Report for Preliminary Engineering (PE) is \$46,542,060. Some Design cost reported in Famis were moved to Design Phase, refer to Notes #19 and #20. |
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Phase 2 Design Phase

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| 31 | Famis cost adjustment to transfer Project Management cost from July 2013 to Phase 3 Construction Phase. |
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| 32 | Famis Phase 1 PE Index Code: 357906.CPT5441112 cost is \$10,222,939 \$8,949,300 is reported in Cost Report Phase 1 PE and the balance of \$1,273,639 is reported in Phase 2 Design. |
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| 33 | 1.2.021.01.080.03 - FD:CTYCO-ARTS COMMISSION [357909ART001.CPT5441227]: FAMIS: \$1,425,167 Cost Report: \$1,425,167 cost is reported in Phase 2 Design, 1.2.021.01.080.03 Cost Transfer: Remaining cost is reported in Phase 3 Construction, 1.3.021.01.080.03 - ARTS:CTYCO-ARTS COMMISSION [357909ART001.CPT5441227] |
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| 34 | 1.2.055.01.080.02 - FD:ODCs - 651 BRANNAN STREET [35CPT5441241.CPT5441241]: FAMIS: \$2,294,910 Cost Report: \$2,294,910 1.2.055.01.080.02 Cost Transfer: Future costs to be allocated to 1.3.055.01.080.02 - FD:ODCs - 651 BRANNAN STREET [35CPT5441241.CPT5441241] |
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| 35 | 1.2.063.01.080.03 - AECOM.CS149 OM-EPC JV CS149-PM [68CPT544133D.CPT544133D]: FAMIS: \$4,698,167 Cost Report: \$4,698,167 on 1.2.063.01.080.03 Cost Transfer: Future costs to 1.3.063.01.080.03 - AECOM.CS149 OM-EPC JV CS149-PM [68CPT544133D.CPT544133D] |
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| 36 | AVA Cost \$395,204 is reported in Phase 2 Final Design 1.2.066.01.080.03 |
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| 37 | 1.2.071.01.080.02 - FD:FINAL DESIGN-DP1 [35CPT5441232.CPT5441232]: FAMIS: \$5,608,147 Cost Report: \$5,469,336 Cost Transfer: \$138,811 to 1.3.071.01.080.04 - FD:FINAL DESIGN-DP1 [35CPT5441232.CPT5441232] |
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| 38 | 1.2.072.01.080.02 - FD:FINAL DESIGN-DP2 [35CPT5441233.CPT5441233]: FAMIS: \$26,268,511 COST REPORT: \$26,220,609 COST TRANSFER: \$47,902 to 1.3.072.01.080.04 - FD:FINAL DESIGN-DP2 [35CPT5441233.CPT5441233] |
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Connecting people. Connecting communities.

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| 39 | 1.2.073.01.080.02 - FD:FINAL DESIGN-DP3 [35CPT5441236.CPT5441236]: FAMIS: \$11,502,372 COST REPORT: \$11,432,312 COST TRANSFER: \$70,060 to 1.3.073.01.080.04 - CM: DP3 [35CPT5441236.CPT5441236] |
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| Phase 3 CONSTRUCTION PHASE | |
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| 40 | 1.3.021.01.080.03 - ARTS:CTYCO-ARTS COMMISSION [357909ART001.CPT5441227]: FAMIS: \$1,525,982 Cost Report: \$1,425,167 1.2.021.01.080.03 Cost Transfer: any future costs to 1.3.021.01.080.03 |
| 41 | 1.3.055.01.080.02 - FD:ODCs - 651 BRANNAN STREET [35CPT5441241.CPT5441241]: FAMIS: \$2,294,910 Cost Report: \$2,294,910 1.2.055.01.080.02 - FD:ODCs - 651 BRANNAN STREET [35CPT5441241.CPT5441241] Cost Transfer: Future costs to be allocated to 1.3.055.01.080.02 |
| 42 | 1.3.063.01.080.03 - AECOM.CS149 OM-EPC JV CS149-PM [68CPT544133D.CPT544133D]: FAMIS: \$4,698,167 Cost Report: \$4,698,167 on 1.2.063.01.080.03 Cost Transfer: Future costs to 1.3.063.01.080.03 - AECOM.CS149 OM-EPC JV CS149-PM [68CPT544133D.CPT544133D] |
| 43 | Used \$500K program contingency for CS-175 Bayland Soil Process contract. Refer to Report Notes #20. |
| 44 | 1.3.071.01.080.04 - FD:FINAL DESIGN-DP1 [35CPT5441232.CPT5441232]: FAMIS: \$5,608,147 Cost Report: \$5,469,336 Cost Transfer: \$138,811 to 1.3.071.01.080.04 - FD:FINAL DESIGN-DP1 [35CPT5441232.CPT5441232] |
| 45 | 1.3.072.01.080.04 - FD:FINAL DESIGN-DP2 [35CPT5441233.CPT5441233]: FAMIS: \$26,268,511 COST REPORT: \$26,220,609 COST TRANSFER: \$47,902 to 1.3.072.01.080.04 - FD:FINAL DESIGN-DP2 [35CPT5441233.CPT5441233] |
| 46 | Contract 1251 Final cost is \$20,794,582. |

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| 47a | In July 2015 Report, used Contract 1300 Contractor schedule to report budget and actual cost. The Standard Cost Categories (SCC) allocation changed from previous reports. |
| 47 | Revised Contract 1252 allocated contingency SCC code from 040.08 to 010.07. |
| 48 | Revised Contract 1300/UMS allocated contingency SCC code from 040.08 to 020.03. |
| 49 | Revised Contract 1300/CTS allocated contingency SCC code from 040.08 to 020.03. |
| 50 | Revised Contract 1300/YBM allocated contingency SCC code from 040.08 to 020.03. |
| 51 | Revised Contract 1300/STS allocated contingency SCC code from 040.08 to 020.01. |
| 52 | Revised Form B Reimbursements SCC code from 900.01 to 040.02 |
| 53 | Revised Form B Reimbursements SCC code from 900.01 to 040.02 |
| 54 | Revised Form B Reimbursements SCC code from 900.01 to 040.02 |
| 55 | Revised Form B Reimbursements SCC code from 900.01 to 040.02 |
| 56 | Revised Form B Reimbursements SCC code from 900.01 to 040.02 |
| 57 | Revised Form B Reimbursements SCC code from 900.01 to 040.02 |
| 58 | Revised Form B Reimbursements SCC code from 900.01 to 040.02 |
| 59 | Revised Form B Reimbursements SCC code from 900.01 to 040.02 |
| 60 | Increase Program contingency \$1,023,508. Refer to Report Notes #11 and #12. Program contingency decreased by \$500,000. Refer to Report Notes #20. |