

central subway

Digging down in Chinatown

Work to excavate the station headhouse and install temporary steel supports is underway.



Progress Report

October 2015



SFMTA
Municipal
Transportation
Agency

This page intentionally left blank

Table of Contents

Executive Summary	5
Key Milestones	6
Costs and Schedule	7
Costs	7
Earned Value Analysis	7
Schedule Highlights.....	8
Master Project Schedule	11
Contracts & Construction	12
Contract 1300 Stations, Surface, Track and Systems.....	13
Work Package1254 Chinatown Station	14
Work Package1253 Union Square / Market Street Station	16
Work Package1255 Yerba Buena / Moscone Station	19
Work Package1256 Systems, Trackwork & Stations	21
Program Components	23
Community Outreach	23
Quality Assurance	24
Risk Management	27
Program Safety & Security	27
Technical Capacity	30
Staffing	31
Third-Party Agreements and Licenses	32
LRV Procurement.....	32
Current Construction Activity and Progress Photos	33

See Table of Content page that follows for Cover Photo captions.

Table of Contents - continued

Appendices

Appendix A: Cost Report.....	A1
Appendix B: Schedule Report.....	B1
Appendix C: Program Overview of Scope and Funding.....	C1
Appendix D: Completed Contracts	D1
Appendix E: SBE Participation by Contract (Reported Quarterly).....	E1
Central Subway Project Contact Information.....	E5



Cover photo: At the intersection of Stockton and Washington, work to excavate the structure of the Chinatown Station headhouse is underway. Workers have excavated the first few levels of the headhouse interior, reinforcing the walls with temporary steel beams as they go. Drill crews inject grout into the soil outside the structure to help prevent against groundwater intrusion. **More photos of this construction can be found starting on page 33.**

Above photos: A long, steel tube called a “waler” rests at the entrance portal to the station box for Yerba Buena/Moscone Station. Walers are used to temporarily reinforce the walls during excavation.

See the Appendix E final page for CS websites hyperlinks and public outreach on line resources. The Project main web site is at: <http://www.centralsubwaysf.com/>



Welders attach steel reinforcing beams to the exterior slurry walls of the station box inside Yerba Buena/Moscone Station.

Executive Summary

Continue excavation at YBM, UMS and CTS Stations for the future stations.

Chinatown Station - Finish dewatering wells on Stockton Street. Finish compensation grout hole from inside headhouse and precondition under buildings. Finish drilling inclinometers on Stockton Street. Installing bracing level 2. Finished relocating ductbank on Stockton Street.

Union Square/Market Street Station - Deck and rebar installation at Ellis Annex and in the North Concourse. Discontinued grouting on the east and west sides of Stockton Street.

Yerba Buena/Moscone Station - Traffic flowing on east side of 4th Street with west side work area closed for construction. Backfill over portion of headhouse beneath Clementina Street is completed. Headhouse and station box temporary bracing strut level 2 (above Mezzanine Level) installation is completed with excavation in progress. Placement of mud slab and reinforcing steel for Mezzanine Level concrete pour 1 of 3 in station box is in progress. Archaeological monitoring by spot checking is in progress.

Surface, Track and Systems Continue Muni ductbank installation. Continue 78" sewer rehabilitation. Continue 36" sewer force main. Continue Auxiliary Water Supply System (AWSS) installation. Continue water line work. Continue AT&T crossing work. Continue tunnel prep work. Start 4th/King sewer structure modification.

Tunnel - Contract administrative closeout is ongoing.

Total project costs to date are \$862.56 million, an increase of \$15.44 million over last month. The total cost to date equals 54.65% of the total project budget of \$1.578 billion. The Master Project Schedule forecast the Revenue Service Date of May 2019.

The Stations Contractors' Safety Reports show no recordable accidents took place this month and the rates of work site accident incidents by the man hours worked continue to be below industry standards - see tables on page 29.

Key Milestones

Decking over at Ellis Street



1 Installation of deck panels over steel and reinforced concrete structural elements



2 Rebar is installed and concrete is poured

MILESTONE	DATE EXPECTED
General	
Revenue Service	December 2018
Contract 1252 Tunnels	
Substantial Completion	April 15, 2015 (A)
Contract 1300 Stations, Surface, Track, Systems	
Notice to Proceed (NTP 1)	June 17, 2013 (A)
Notice to Proceed (NTP 2)	January 12, 2014 (A)
Substantial Completion	November 7, 2018



3 Excavation of the concourse interior continues underground

Costs and Schedule

Costs (See Appendix A for Details)

The Current Cost Estimate (CCE) for the Central Subway Project is **\$1.578 billion** in year of expenditure dollars (\$YOE). This total project cost is shown at the top of Report 7.1, Program Project Budget. This capital cost projection incorporates allocated and un-allocated contingencies to cover the risks associated with the project completion.

Total net incurred costs for the project are \$862.56 million, a \$15.44 million increase over last month. The cost to date figure reflects expenditures through FAMIS 786 Report (\$818.44 million) plus the utilities joint trench Form B Reimbursement payment (\$10.52 million), invoices currently being processed (\$17.22 million) and estimates of outstanding pay requests (\$16.38 million). This incurred amount equals 54.65% of the total project budget of \$1.578 billion.

The current funding level to date is \$1,179.79 million includes FTA \$150 million FY2015 New Starts Grant appropriated in September 2015. This represents 75% of the total project budget.

Earned Value Analysis

In September 2015 Report, Central Subway Project Master Schedule has used Contract 1300 Station contractor baseline schedule and August monthly update schedule. Contractor continues address the cost issues in cost loaded schedule. Preliminary September Earned Value Analysis reports the Planned Value, Earned Value, Percent Complete and resulting indexes as follows:

Overall Budgeted Cost:	\$1,578,300,000
Planned Value:	\$1,129,829,298
Earned Value:	\$914,311,093
Actual Cost:	\$862,556,970
Schedule Performance Index (SPI):	0.81
Cost Performance Index (CPI):	1.06
Percent Complete:	57.9%

Earned Value Cost shows \$52M higher than Actual Cost for Contract 1300 is due to Tutor Perini Corporation Schedule has remaining cost in the completed activities.

Schedule Highlights - Continued

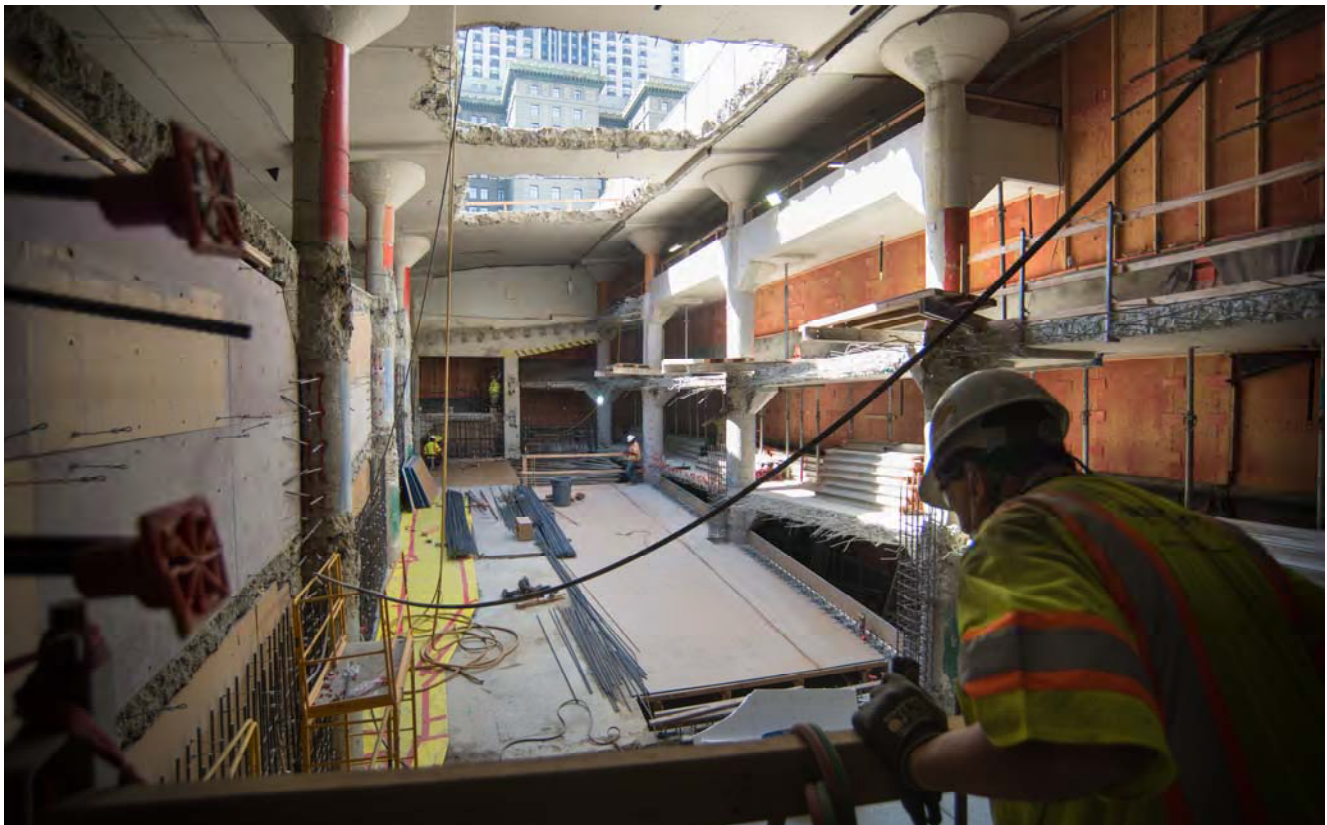
The Master Project Schedule (MPS) below includes progress through October 2015.

The MPS shows a forecast Revenue Service Date of May 13, 2019.

The controlling critical (longest) path of the MPS runs through CTS Excavation succeeded by STS Startup & Testing, Commissioning and Pre-Revenue Activities to the Baseline Finish and Revenue Service Date. See Appendix B – Longest Path. The latest schedule shows the longest path has changed from Union Square Station (UMS) to the Chinatown Station (CTS). The 1300 Contractor has re-sequenced the UMS Platform Station Jet Grout Operation and was able to recover schedule time within the UMS Station. This adjustment in the Critical Path will continue as the 1300 Contractor incorporates their schedule recovery options, resulting in revisions to their construction work plans.

Schedule Contingency is fully utilized on the critical path of the MPS, which is below the Minimum Schedule Contingency level of 6 months. A schedule re-evaluation will be performed, utilizing the updated Contract 1300 Schedule.

The 1300 Contractor submitted eleven (11) Schedule Updates from December 2014 to October 2015. The October 2015 Schedule Update is currently under review. Contractor has been directed to develop a Recovery Schedule as required by Contract and correct out-of-sequence and Retained Logic driving many of the forecast dates. The 1300 Contractor continues to address key critical activities to improve schedule performance. Review of schedule updates as well as identifying recovery options is ongoing.



A new vehicle ramp is being constructed inside the Union Square Garage, at the southeast corner. This ramp replaces a previous ramp further to the south removed for construction of the north station entrance.

Schedule Highlights - Continued

Contract 1300 Stations, Surface, Track and Systems

The Contractor, Tutor Perini Corporation's (TPC) baseline schedule is incorporated into the master program. The preliminary Tutor Perini Corporation's (TPC) October schedule is used in October Report. Anticipated to use Contractor, Tutor Perini Corporation's (TPC) approved schedule for October Report that incorporates all SFMTA schedule comments.

Contract 1300 has several interface points with Contract 1252. The timing of these interface points correlate to when the NTP was issued for Contract 1300. This table represents the current status of these interface points with respect to the Contract 1252 update. The 1300 Contractor is not ready to access the tunnel used by Contract 1252. The tunnel access from Contract 1252 is not expected to impact Contract 1300. The current completion date of the

Contract 1252 is shown in the table below.

Interface Points	CN1300 Requirement	CN 1252 Apr 2015 Update	Variance	
Tunnel Portal Completion	24-Sep-15	15-May-15	132	CD

Work Package P-1254R CTS performed the following work this month:

- Finish dewatering wells on Stockton St
- Finish compensation grout hole from inside headhouse and precondition under buildings
- Finish drilling inclinometers on Stockton Street
- Installing bracing level 2
- Finished relocating ductbank on Stockton Street

Work Package P-1253 UMS performed the following work this month:

- Deck and rebar installation at Ellis Annex and in the North Concourse
- Discontinued grouting on the east and west sides of Stockton Street

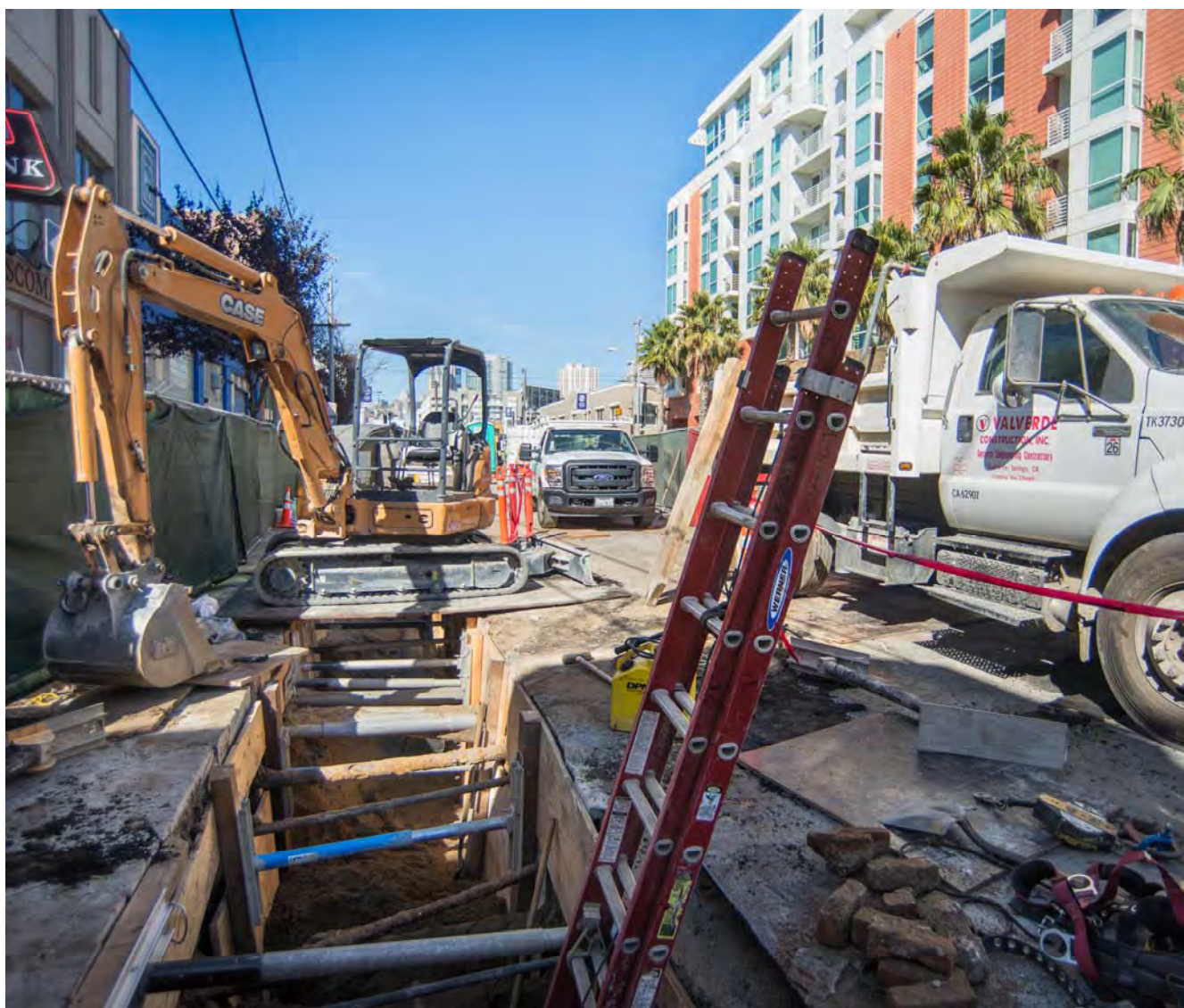
Work Package P-1255 YBM performed the following work this month:

- Traffic flowing on east side of 4th Street with west side work area closed for construction
- Backfill over portion of headhouse beneath Clementina Street is completed
- Headhouse and station box temporary bracing strut level 2 (above Mezzanine Level) installation is completed with excavation in progress
- Placement of mud slab and reinforcing steel for Mezzanine Level concrete pour 1 of 3 in station box is in progress
- Archaeological monitoring by spot checking is in progress

Schedule Highlights - Continued

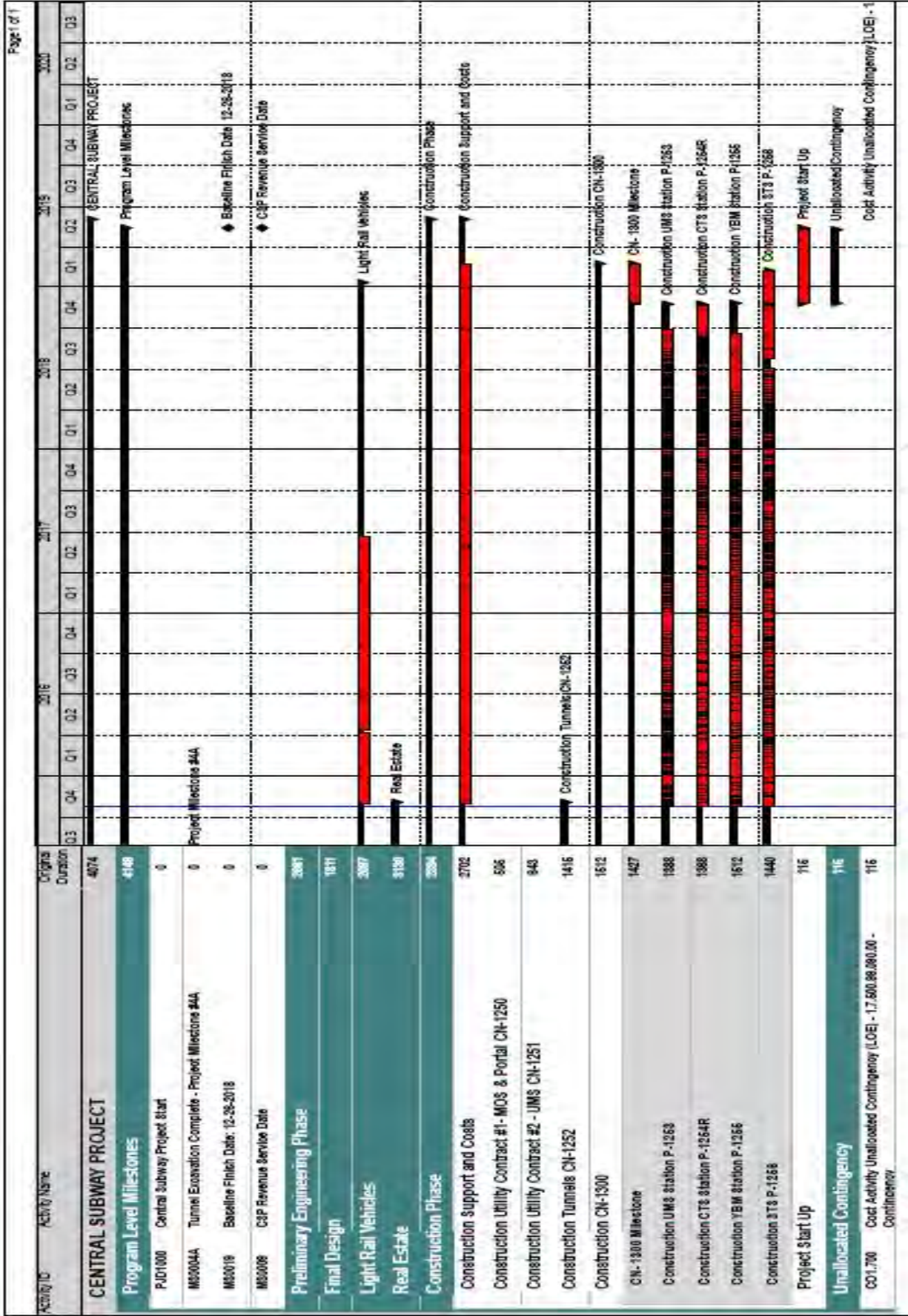
Work Package P-1256 STS performed the following work this month:

- Continue Muni ductbank installation
- Continue 78" sewer rehabilitation
- Continue 36" sewer force main
- Continue Auxiliary Water Supply System (AWSS) installation
- Continue water line work
- Continue AT&T crossing work
- Continue tunnel prep work



An open trench marks where a high-pressure water line is being installed on the west side of 4th Street near Freelon Alley.

Master Project Schedule





A newly-constructed section of roof slab is seen exposed between O'Farrell and Geary. After constructing each section of roof slab, workers backfill the section and move to the next.

Contracts & Construction

Construction Contracts In Progress

Contract 1300: Combined Work Packages 1253, 1254, 1255, 1256

- Contractor: Tutor - Perini Corporation
- Amount: \$838.65 million
- Contract Status: 43.22% complete construction

Contracts Completed

[See Appendix D](#)

Contract 1250: Moscone Station and Portal Utilities Relocation

Contract 1251: Union Square/Market Street Station Utility Relocation

Contract 1277: Pagoda Theater Site Demolition (Funded separately from the CS Project budget)

Contract 1252: Central Subway Tunneling

Contract SBE Participation (Updated Quarterly) [See Appendix E](#)

Stations, Surface, Track and Systems

Contract 1300 Contractor: Tutor-Perini Corporation

Description of Work

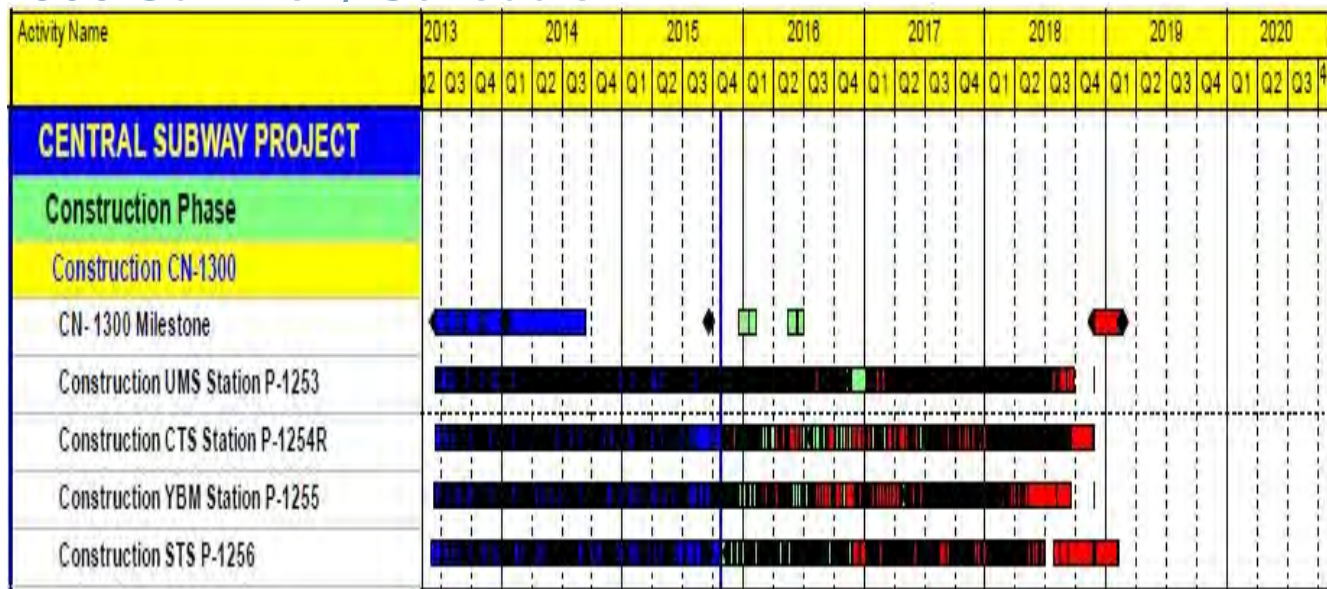
The Contract 1300 scope is to construct the Central Subway's three subway stations, one surface station, construct the 2,000 feet of surface track, and install track and operating systems throughout the new alignment. The separate station and systems work packages are presented in the following pages.

Work includes station finishes, AC and DC substations, elevators, escalators, lighting, emergency ventilation fans, HVAC fire alarm/ suppression/ protection, Cutter Soil Mixing, secant pile bottom up and Sequential Excavation Method construction, settlement monitoring, building protection, connecting to and modifying the BART Powell Street Station, PA, CCTV, signage, installation of fare collection equipment and station start-up and commissioning.

Contract Details	
Contract Awarded:	May 21, 2013
Notice to Proceed:	June 17, 2013
Substantial Completion:	November 7, 2018
Contract Award Value:	\$839,676,400
Modifications to Date:	(\$1,016,585)
Current Contract Value :	\$838,659,815

Budget/Expenditures▲	
Current Budget	\$859,676,400
Expenditures to Date	\$316,241,971

1300 Summary Schedule



Chinatown Station

Contract 1300 - Work Package 1254R



Description of Work

This Work Package is to construct one subway station. Includes station finishes, AC and DC Traction Power substations elevators, escalators, lighting, emergency ventilation fans, HVAC fire alarm/ suppression/ protection, slurry wall top-down construction, settlement monitoring, building protection, PA, CCTV, signage, installation of fare collection equipment and station start-up and commissioning.

Current Work Status

- Finish dewatering wells on Stockton St
- Finish compensation grout hole from inside headhouse and precondition under buildings
- Finish drilling inclinometers on Stockton Street
- Installing bracing level 2
- Finished relocating ductbank on Stockton Street

Work Expected Next Month

- Connecting dewatering wells on Stockton Street
- Excavate and install temp level 3 bracing beams
- Form and pour 1st pour of composite wall
- Finish installing level 2 bracing
- Install horizontal inclinometers and MBX's above crosscut crown
- Install weep hole through slurry walls
- Relocate OCS lines west, site set up for North Access Shaft
- Slip line brick sewer on Stockton Street



Three Month Look Ahead

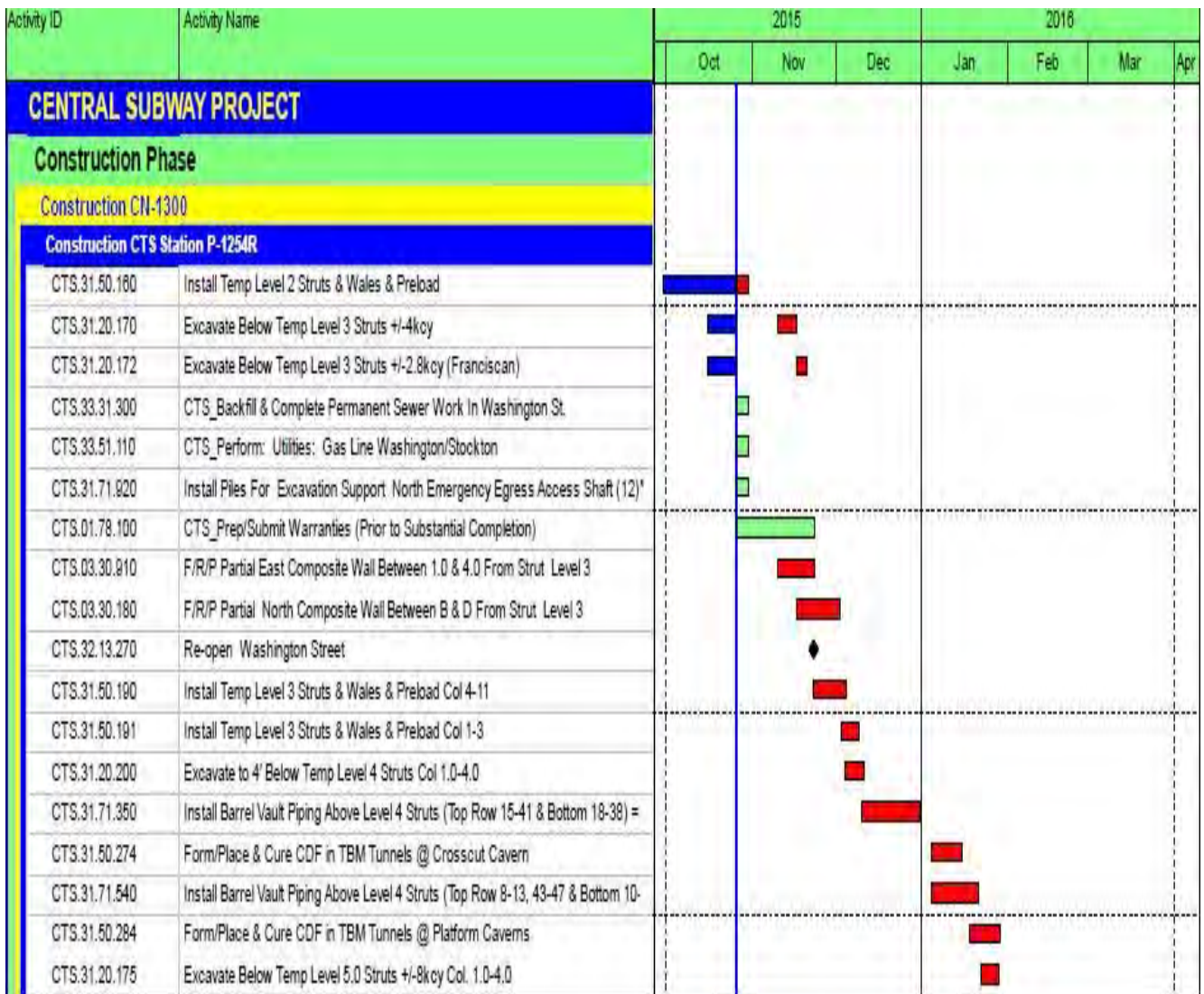
- Form and place CDF in TBM tunnels
- Build composite wall over crosscut cavern opening
- Slip line brick sewer on Stockton Street
- Drill and install barrel vaults pipe
- Finish installing temp level 3 bracing
- Excavate to level 4 bracing

Chinatown Station Construction Status - Continued

Contract Details	
Contract Awarded:	May 21, 2013
Notice to Proceed:	June 17, 2013
Substantial Completion:	November 7, 2018
Contract Award Value:	\$247,567,810
Modifications to Date:	\$230,956
Current Contract Value:	\$247,798,766

Budget/Expenditures	
Current Budget	\$252,567,810
Expenditures to Date	\$74,924,336

CTS Three Month Schedule



Union Square/Market Street Station

Contract 1300 Work Package 1253

Description of Work

This Work Package is to construct one subway station. Includes station finishes, AC and DC Traction Power substations elevators, escalators, lighting, emergency ventilation fans, HVAC fire alarm/ suppression/ protection, slurry wall top-down construction, settlement monitoring, building protection, PA, CCTV, signage, installation of fare collection equipment and station start-up and commissioning.

Current Status This Month

- Deck and rebar installation at Ellis Annex and in the North Concourse
- Discontinued grouting on the east and west sides of Stockton Street

Work Expected Next Month

- Street closures: Stockton Street between Ellis and Post and Ellis halfway, Stockton to Powell. Alternate lane configuration on O'Farrell at Stockton. Narrowing of Geary to 1 lane at Stockton
- Total closure expected for several days, at night hours, for Geary/Stockton Intersection
- Union Square Garage – demolition and abatement. Concrete installation
- North Concourse - install decking, concrete for roof deck and reroute of duct-banks and waterline
- Production and test jet grouting on the east and west sides of Stockton Street discontinued at month end until after moratorium
- Concrete installation at Ellis Street Annex
- Geary Street portion of North concourse sewer notch and roof installation should be complete
- Street restoration and barricade removal to begin at end of November, in preparation for Winter Wonderland holiday



Three Month Look Ahead

- Progress roof beam, roof deck and waterproofing installation for roof deck construction for main station box
- Continue construction operations at Ellis Street Annex and on connection to BART structure
- Completion of new roof on Ellis for BART Station entrance to UMS station
- Continue working on Union Square Garage demolition and construction
- Suspended jet-grouting operations on Stockton Street between Geary and O'Farrell Streets
- Install remainder of north concourse roof on Station between Maiden Lane and Geary Street
- Winter Wonderland holiday moratorium set-up to run until the New Year

Union Square Market Street Station Construction - Continued

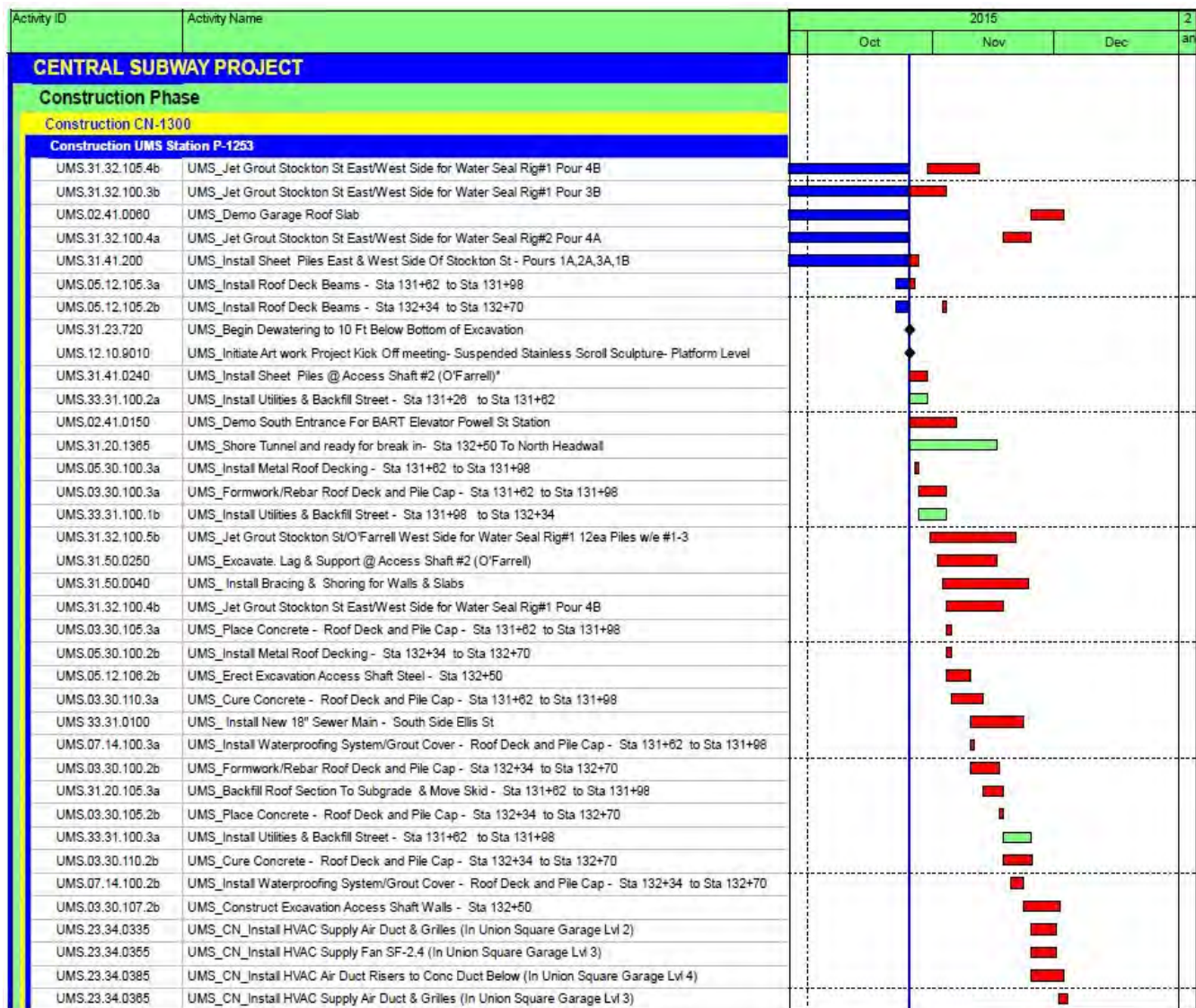
Contract Details

Contract Awarded:	May 21, 2013
Notice to Proceed:	June 17, 2013
Substantial Completion:	November 7, 2018
Contract Award Value:	\$294,030,590
Modifications to Date:	\$90,000
Current Contract Value:	\$294,120,590

Budget/Expenditures ▲

Current Budget	\$299,030,590
Expenditures to Date	\$132,591,502

UMS Three Month Schedule



Yerba Buena/Moscone Station

Contract 1300 - Work Package 1255



Description of Work

This Work Package is to construct one subway station. Includes station finishes, AC and DC Traction Power substations elevators, escalators, lighting, emergency ventilation fans, HVAC fire alarm/ suppression/ protection, slurry wall top-down construction, settlement monitoring, building protection, PA, CCTV, signage, installation of fare collection equipment and station start-up and commissioning.

Current Status

- Traffic flowing on east side of 4th Street with west side work area closed for construction
- Backfill over portion of headhouse beneath Clementina Street is completed
- Headhouse and station box temporary bracing strut level 2 (above Mezzanine Level) installation is completed with excavation in progress
- Placement of mud slab and reinforcing steel for Mezzanine Level concrete pour 1 of 3 in station box is in progress
- Archaeological monitoring by spot checking is in progress



Work Expected Next Month

- Continue utility installation on Folsom Street
- Continue excavation in headhouse and station box
- Continue placement of Mezzanine Level slab within station box, pours 2 and 3 of 3 (beneath 4th Street)

Three Month Look Ahead

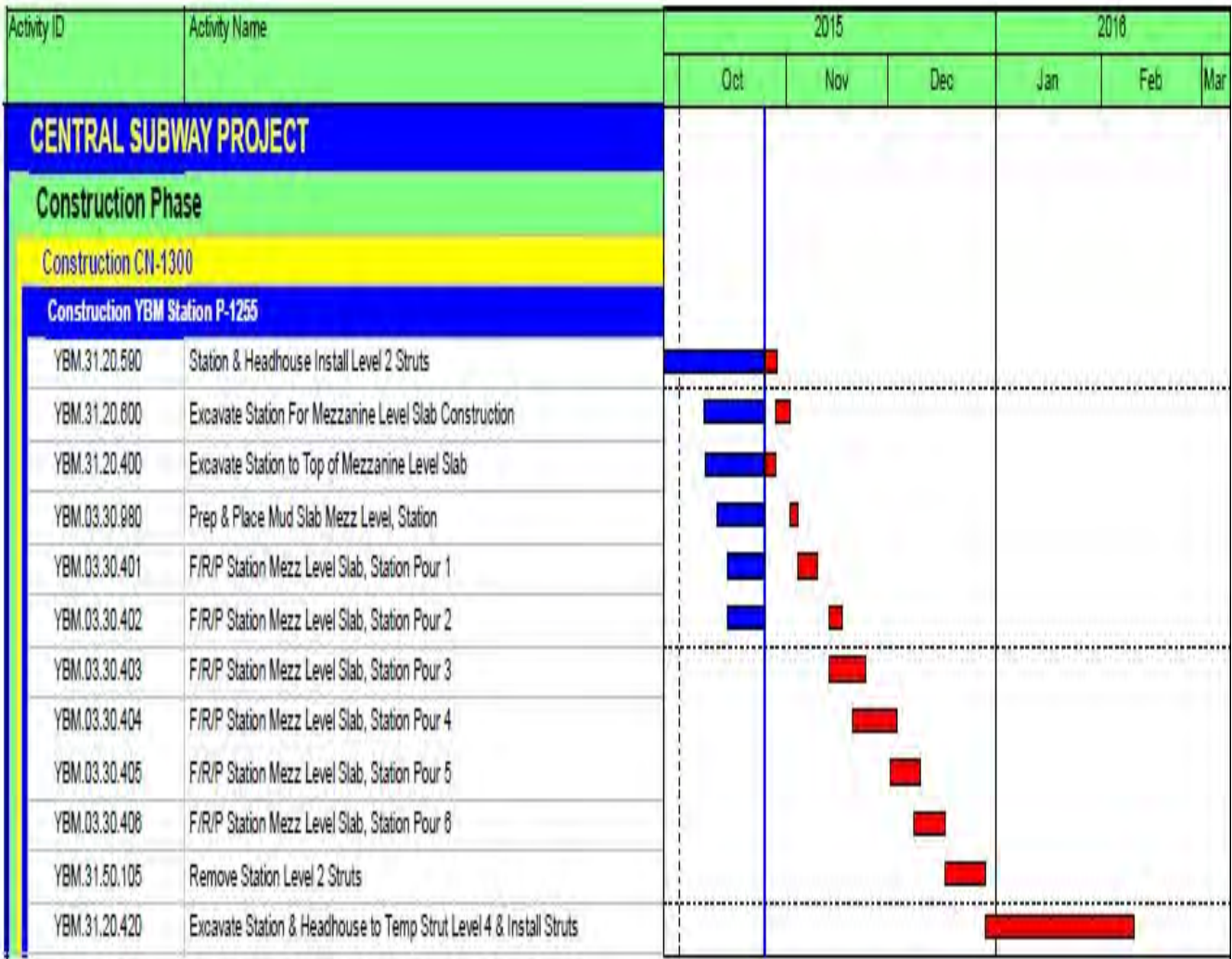
- Continuation of headhouse and station box excavation, install excavation temporary bracing
- Placement of Concourse Level slab within station box (beneath 4th Street)
- Archaeological Monitoring
- Utility installation at intersection of 4th Street with Howard Street

Yerba Buena Moscone Station Construction - Continued

Contract Details	
Contract Awarded:	May 21, 2013
Notice to Proceed:	June 17, 2013
Substantial Completion:	November 7, 2018
Contract Award Value:	\$158,089,000
Modifications to Date:	(\$1,642,919)
Current Contract Value:	\$156,446,081

Budget/Expenditures ▲	
Current Budget	\$163,089,000
Expenditures to Date	\$76,984,724

YBM Three Month Schedule



Systems, Trackwork, & Surface Station

Contract 1300 - Work Package 1256

Description of Work

This Work Package is to construct one Surface Station. Includes light rail track and systems, track invert, track safety walkways; light rail track and systems constructed on the 2,000 foot surface for the alignment from the tunnel portal, south to the tie-in to the existing Muni T-Line at Fourth and King Streets; and the surface Fourth and Brannan Street (FBS) Station.

Current Status

- Continue Muni ductbank installation
- Continue 78" sewer rehabilitation
- Continue 36" sewer force main
- Continue Auxiliary Water Supply System (AWSS) installation
- Continue water line work
- Continue AT&T crossing work
- Continue tunnel prep work
- Start 4th/King sewer structure modification



Work Expected Next Month

- Continue 78" sewer rehabilitation
- Continue MRY ductbank and vault installation
- Continue 36" sewer force main
- Continue AWSS installation
- Continue waterline installation
- Continue 4th/King sewer structure modification
- Continue 4th/King Special Trackwork (Shutdown #2)
- Continue tunnel prep work
- Start tunnel drainage system installation
- Start tunnel invert slab work

Three Month Look Ahead

- Waterline installation
- AWSS installation
- Muni ductbank installation and vault installation
- 36" sewer force main installation
- 78" sewer rehabilitation
- 4th/King sewer structure modification
- OCS pole foundation installation
- AT&T crossing installation
- 4th/King special Trackwork
- Tunnel drainage system installation
- Tunnel invert slab construction
- Tunnel walkway construction

Systems, Trackwork, & Surface Station Construction - Continued

Contract Details	
Contract Awarded:	May 21, 2013
Notice to Proceed:	June 17, 2013
Substantial Completion:	November 7, 2018
Contract Award Value:	\$139,989,000
Modifications to Date:	\$305,378
Current Contract Value:	\$140,294,378

Budget/Expenditures	
Current Budget	\$144,989,000
Expenditures to Date	\$31,741,408

Systems, Track and Surface Station Three Month Schedule

Activity ID	Activity Name	2015			2016		
		Oct	Nov	Dec	Jan	Feb	Mar
CENTRAL SUBWAY PROJECT							
Construction Phase							
Construction CN-1300							
Construction STS P-1256							
STS.33.11.100	STS_Install: Utilities: Auxiliary Water Supply 12" Main 4th St Through Bryant St Intersection						
STS.33.11.300	STS_Install: Utilities: Water Distribution 8" Main - 4th St Welsh St to Freelon St.						
STS.33.11.120	STS_Install: Utilities: Auxiliary Water Supply 12" Main 4th St (Bryant St To Welch St)						
STS.33.11.140	STS_Install: Utilities: Auxiliary Water Supply 12" Main 4th St (Welsh St To Freelon St))						
STS.33.11.360	STS_Install: Utilities: Track Drainage- 4th St (Townsend St To King St Tie-in)						
STS.33.11.150	STS_Install: Utilities: Auxiliary Water Supply 12" Main 4th St (Through Freelon St Intersection)						
STS.34.23.250	STS_Install: Utilities: Install Fiber Optic Ductbank From MH 1894 To Comm Cabinet @ 4th/Freelon						
STS.34.42.1080	STS_Install: Security - SB Portal Intrusion Devices						
STS.26.05.2720	STS_Install: OCS System - Install OCS Poles In Portal						
STS.26.05.120	STS_Install: Utilities: 230Kv Electrical Transmission Casing - Assist PG&E - 4th St @ Bryant Intersection						
STS.33.11.340	STS_Install: Utilities: Track Drainage- 4th St (Brannan St To Bluxome St)						
STS.26.05.0430	STS_Install: Tunnel Electrical -Lighting Fixtures - NB Portal To Moscone						
STS.33.51.150	STS_Pothole: Utilities: Pothole for Gas Distribution						
STS.01.78.100	STS_Prep/Submit Warranties (Prior to Substantial Completion)						
STS.33.11.310	STS_Install: Utilities: Water Distribution 8" Main - 4th St Freelon St. Brannan St						
STS.33.11.350	STS_Install: Utilities: Track Drainage- 4th St (Bluxome St To Townsend St)						
STS.26.05.0810	STS_Install: Tunnel Electrical -Lighting Fixtures - NB Moscone to Union Square						
STS.26.05.215	STS_Install: Utilities: MRY Ductbanks MH 1890 to Existing DB @ Sta 169+00 - 4th Street						
STS.33.11.270	STS_Install: Utilities: Sewer: Casing for 10" Force Main - 4th St Sta @ Brannan St Intersection						
STS.33.31.420	STS_Install New CB's Manhole, 10" & 15" Sewer Piping @ 4th St/Townsend - West Side						
STS.33.11.320	STS_Install: Utilities: Water Distribution 8" Main - 4th St Brannan St to Bluxome						
STS.33.31.280	STS_Install New 18" Sewer Lateral In Welsh St To Future 48" Manhole						
STS.33.31.330	STS_Install New Sewer Laterals & Sleeves West Side 4th Street - Brannan to Bluxome						
STS.26.05.1190	STS_Install: Tunnel Electrical -Lighting Fixtures - NB Union Square to Chinatown						
STS.26.05.3400	STS_Install: Utilities: MRY Ductbanks (E) MH 1847 to 1898						
STS.26.05.135	STS_Install: Utilities: MRY Ductbank From MH 1898 To MH 1899 - Townsend St						
STS.26.05.3750	STS_Install: Fireproofing - MRY Manholes						

Program Components

Community Outreach

Outreach public information, events and presentations for October 2015 include:

Outreach to stakeholders, merchants and residents for upcoming construction at 4th & King (Phase 2 Track Installation)

Completed APTA Tunnel Tours

Presented Chinatown Update at Chinatown Merchant meeting hosted by District 3

Attended Ball Park/Mission Bay Transportation Coordinating Committee

Hosted 4th & King Track Installation work Community Meeting

Presented construction update to Union Square BID

Outreach in Support of Mitigation and Monitoring

Team members participated in weekly progress and traffic meetings to address neighborhood concerns

Outreach and communication efforts continue in Chinatown, Union Square, and SOMA

Weekly photo documentation of project work and editing

Weekly construction update emails sent to list of approximately 700 residents and stakeholders

Distributed monthly construction update to STS and CTS neighborhood; CTS flier are bi-lingual

Media Coverage

Central Subway Media Coverage			
Date	Title (with link to story)	Source	Reporter/Writer
10/26/15	San Francisco railway project bringing new round of street closures	TI News Daily	
10/27/15	San Francisco's Central Subway Track Project Underway	Rail Resource	
10/28/15	SF lays blueprint for future subways	SF Bay	Jerold Chinn

Quality Assurance

Quality Assurance monthly activity of oversight, surveillance, audits, proactive feedback and QA records actively involves the Project construction management staff, the resident engineers, the prime construction contractor and their subcontractors.

Stations and Systems Contract CN1300 Quality Assurance Monitoring – On Going

- UMS roof beam to pile WF installation – Installation of steel continues
- UMS Water Proofing Subcontractor – Waterproofing Experts and substrate and application inspection/acceptance by RDH in lieu of Kingfield Construction Products (water proofing manufacturer)
- YBM Slurry Wall Panels to roof slab and interior slabs
- YBM Water Proofing Subcontractor – Waterproofing Experts and substrate and application inspection/acceptance by RDH in lieu of Kingfield Construction Products (water proofing manufacturer)
- TPC QC Daily Inspection Reports (which includes TPC's Specialty Subcontractor's QC checklists and associated documentation and Smith Emery Inspection Reports)
- Smith Emery - TPC's laboratory and Inspection Services – including Special Inspections that are also provided via email to the City of San Francisco's Department of Building Inspection (DBI) for all permitted Work)
- Preparatory, Initial and Additional Initial Phase Meetings as required by Specification Section 01 45 00, continue. Attendance/participation of these vital meetings (Contractor is not allowed to start work until such time that all prerequisite requirements have been met which continues to greatly facilitates all work being done to the strict requirements of the Contract Documents, the "first time"). Additional initial phase meetings are conducted, at the request of SFMTA for all concrete placements to ensure that all prerequisite requirements have been met
- Bi-Weekly and ad hoc (mostly at the request of the UMS SFMTA Resident Engineer with Additional Initial Phase Meetings, per 01 45 00 Section 1.09, for pre-concrete placement) bi-weekly Quality Task Force (QTF) Meetings are attended primarily to assist and support the Contractor's QC effort
- Instrumentation/Monitoring and Construction Management Task Force (CMTF) Meetings
- Progress Meetings for C1300, STS, YBM, UMS and CTS when conducted and as time constraints allow

Document comment and review:

- Contractor's submittals, e.g., review of welding, concrete and other Quality related submittals/comments as requested to support the RE's and CM, and RFIs related to quality
- QA Staff continues random/spot checks of the 1300 Contractor's Field Testing lab results; items requiring further action/investigation (missing or inconsistent data, compressive strength results that appear to have a very broad range of values and such) are brought to

Quality Assurance - Continued

the attention of the Contractor

- Review of updated CNCR logs and CNCRs that provide guidance to REs regarding content, accuracy of the disposition and confirmation close-out is correct – TPC QC has focused much attention to the accuracy and timeliness of the issuance and logging of CNCRs. The CNCR Log now has more accurate descriptions and status of each CNCR. The CNCR Log entries are now more accurate and timely – however, some improvement is still necessary. TPC QC is fulfilling the 24 hour initial generation of a CNCR once cognizant of a non-conforming condition, through the issuance of the “Initial Issue” of each CNCR. The Initial Issue of each CNCR provides the “what” and “where” but not the “why” or how to prevent repeating CNCRs. TPC QC is now posting CNCRs to CM13, using the Noncompliance module of CM13. Currently, existing (completed by “hand”) CNCRs are posted as attachments as are the SFMTA RE’s responses to Use-As-Is and Repair dispositioned CNCRs

QA Issues:

- Assurance that all RFIs, submittals and USE-AS-IS and REPAIR dispositioned CNCRs related to a particular concrete placement, have been approved by the SFMTA REs. Practically, SFMTA REs have imposed a concrete placement hold point for all concrete placements to collectively ensure that the Contractor has performed all work to the requirements of the Contract Documents, i.e., all RFIs, CNCRs and submittals have been approved and acceptably executed

QA Concerns:

- The annual moratorium schedule compression
- SFMTA’s provision of advance notification to TPC/TPC QC, of in-process work that appears to be deficient or of questionable nature, is not mitigated/reconciled in a timely manner, if at all.



Quality Assurance - Continued

- Necessity of using both Reinforcing Steel Design Drawings and approved Reinforcing Steel Shop Drawings to inspect/accept rebar placement
- Incomplete/confusing shop drawing submittals for UMS structural steel resulting in QC and or QA stopping TPC from making welded connections upon discovery that approved details are missing, continues
- Welding inspection and associated documentation of CWI acceptance of all welded joints, including tack welding, at UMS. Smith Emery continues to update and refine their spreadsheet “tool” that is used by TPC QC to account for the acceptability and associated documentation by CWIs for all welded joints that are to be embedded in concrete
- Use of CM13 as the document depository related to a user having the ability to delete submittal attachments after a submittal has been approved without any record or trail of the deletion (this was a finding included in QAA 025 Audit Report)
- The number of SFMTA CSP Field Notifications at UMS provided to TPC
- De-certification of waterproofing installer, Water Proofing Experts, by Kingfield, waterproofing system manufacturer

Other Program QA Practices Implemented

- Close-out of Corrective Action Requests: Close outs continued from Quality Assurance staff’s Audits, Surveillances and PMOC Quarterly Reviews. The status is tracked in the Corrective Action Log that is provided to the project team and the FTA PMOC
- On-going indoctrination to the CSP Quality Program continues on a one-to-one basis as new personnel are added to the PM/CM Staff
- The Contractor’s revised response to the 6 audit findings of Quality Assurance Audit QAS 025 of the implementation and effectiveness of some components of the C1300 Contractor’s approved Contractor Quality Program (CQP) has been provided. Final review and acceptance of TPC’s audit responses will be documented through the close-out of the Audit mid November 2015

Quality Assurance - Continued

Risk Management

Risk Mitigation Management Meeting No. 75 was held on October 8, 2015. The Risk Assessment Committee reviewed and discussed Risks that include Construction Risks with ratings above 6; Remaining Requirement and Design Risks; and, any New Risk Assessment and Mitigations identified to date.

Program Safety & Security

The San Francisco Municipal Transportation Agency is committed to the highest practical level of safety and security standards and practices in the public transit industry. The Safety and Security Management Plan (SSMP) components are reported on below as appropriate including, Safety and Security Committee, the Fire Life Safety and Security Committee the Construction Conformance Verification and Documentation and Contractor Safety and Security.

Project Management/Construction Management (PMCM) Team

The other half of the PMCM team attended a First Aid, CPR and AED class.

Safety bulletin boards have subjects covering the flu season and the Thanksgiving holiday safety. Both were a huge success and created lots of interest.

As an incentive for the PMCM team to continue to work safe and look for and report safety hazards to the Program Safety Manager, the PMCM team is working on creating safety incentives that will be handed out to those that demonstrate good safety awareness and good safety working records.

Safety Summary for the 1300 Stations Systems Track Construction Package

TPC is still waiting to hear the results of the Cal-OSHA investigation on the flagger incident. No word as to the final findings as of this writing.

During the month of September, safety work on the 1300 contract progressed as follows:

STS had no recordable or first aid incidents. In addition to moving utilities, they spent a considerable amount of time preparing for the next closure scheduled to start on November 6th and last through the 14th.

At YBM, TPC hired an independent industrial hygiene firm to test the air in the box. The initial verbal results were that no exposure exists. They are still waiting to receive the final written report.

UMS did not have any incidents during the last month. They are proceeding with pouring the last couple of roof decks and will start to repair the street in anticipation of the moratorium at the end of November.

CTS continue to excavate the head house area. The industrial hygiene firm conducted air sampling in the excavation areas during actual work since that is when the odors appear to be strongest.

Program Safety & Security - continued

Initial verbal results indicate that none of the substances found were above potential exposure limits.

TPC is removing ventilation ductwork in both tunnels in preparation of starting to work on the inverts in both tunnels. This work is anticipated to start during the middle of November.

Table 1300 Stations Construction Safety Record

Table 1300 below summarizes the Month to Date and Project to Date for the Stations, Systems and Track Construction contractor and subcontractors. Table 1300 shows that no recordable accidents took place in July for the work underway at the station sites and surface line.

Next Month Look Ahead

1300 Contract

1. AT CTS, a second level of excavation has started. TPC is planning on bringing in a new crane for lifting out the box of muck.
2. UMS has poured several sections of roof decking. However, they have not completed finished all of the decks. They are trying to get as many poured as possible prior to the moratorium. Then, preparation of Stockton will start.
3. AT YBM, the next level of excavation will be starting.
4. The critical work at STS is preparing for the closure during the beginning of November. Immediately after the closure is done, work will commence inside of the tunnel. Planned activities are pouring the inverts in both tunnels starting from the farthest north point (CTS) and moving south to the launch box.

Program Safety & Security - continued

Project Safety Record - Contract 1300

Through Month End - Oct 2015

SAFETY GOALS

OSHA Recordable Accidents, <3.4

Lost Time Cases, <1.6

JOB TO DATE	Tutor	Subs	Total Project	Rate*
OSHA Recordable Accidents	1	0	1	0.25
Job Transfer or Restricted Duty Cases	0	0	0	0.00
Lost Time Cases	0	0	0	0.00
Total Project Incidents	1	0	1	0.25
Man Hours Worked Through M/E Oct 2015	347,709	443,764	791,473	

YEAR TO DATE (Month ,Day, Year to Month, Day, Year)	Tutor	Subs	Total Project	Rate*
OSHA Recordable Accidents	1	0	1	0.50
Job Transfer or Restricted Duty Cases	0	0	0	0.00
Lost Time Cases	0	0	0	0.00
Total Project Incidents	1	0	1	0.50
Man Hours Worked Through M/E Oct 2015	211,938	187,547	399,485	

* Rate is calculated based on number of incidents divided by total number of man hours worked multiplied by 200,000 man hours.

OSHA Recordable Accidents - 2008 Construction Industry Rate for Highway, Street, and Bridge Construction = 3.9

Technical Capacity

No Project positions are currently open or unfilled.



A large crane is used at the Yerba Buena/Moscone Station site to lift steel reinforcing members and other heavy equipment into the headhouse and station box interior.

Staffing

The Central Subway Staffing Table shows Planned and Actual full-time equivalent staff (FTEs) working on the Program by organizational function and responsibility.

	Aug-2015		Sep-2015		Oct-2015	
	Planned	Actual	Planned	Actual	Planned	Actual
Project Management						
Program Management	7.10	6.00	7.10	6.00	7.10	6.00
Quality Assurance	1.80	2.30	1.80	2.30	1.80	2.30
Contract Administration	1.40	1.40	1.40	1.40	1.40	1.40
Community Outreach	5.50	5.10	5.50	5.10	5.50	5.50
Finance	2.00	2.00	2.00	2.00	2.00	2.00
Project Controls	5.80	5.80	5.80	5.80	5.80	5.80
Subtotal	23.60	22.60	23.60	22.60	23.60	23.00
Construction Management						
CM - CN 1252	1.00	1.00	1.00	1.00	1.00	1.00
CM - CN 1300	30.00	31.00	30.00	31.00	30.00	31.00
Design Support - CN 1252	0.00	0.10	0.00	0.10	0.00	0.00
Design Support - CN 1300	12.40	12.00	12.40	12.20	12.40	12.20
Subtotal	43.40	44.10	43.40	44.30	43.40	44.20
Start Up						
Start Up / Safety & Security	3.00	2.10	3.00	2.10	3.00	2.10
Subtotal	3.00	2.10	3.00	2.10	3.00	2.10
Total	70.00	68.80	70.00	69.00	70.00	69.30

Third-Party Agreements

In January 2015, the court issued a decision on the amount payable to the property for the single outstanding access license, for compensation grouting, at 19 Stockton Street, adjacent to the Union Square / Market Street Station. The City Attorney's Office is working with Project Staff to release payment of the Court ordered amount to the property owner.

Installation of instrumentation within the building is complete. Installation of the grout tubes commenced late June and continued into July. Project staff continue to keep the Property Owner informed of the status of the work.

LRV Procurement

The SFMTA light rail vehicle procurement continues to schedule the 24 LRVs that will be part of the Central Subway start up to be ready for service testing and training by mid 2018 in order to begin use at the start of Central Subway service in December 2018.

The SFMTA Board authorized award of the contract on July 15, 2014 to Siemens for up to 260 cars, which includes a base contract of 175 cars for a total of \$648 million, with an option to acquire up to 85 more, bringing the overall total to 260 cars. On September 19, 2014 Mayor Edwin Lee approved the contract.

LRV task date/month milestones will be further developed and integrated into CS Master Project Schedule, the Rail Activation Plan and the startup schedule in 2015.



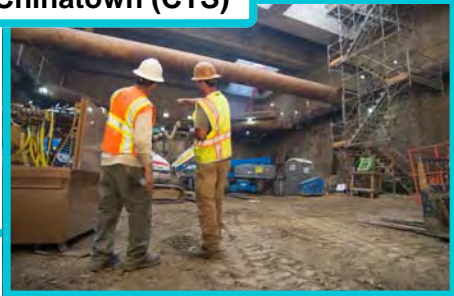
Corrugated steel decking covers the south concourse at Ellis and Stockton. Work to construct the roof of this portion of the station is ongoing.

central **T** subway

Current Construction Activity



Chinatown (CTS)



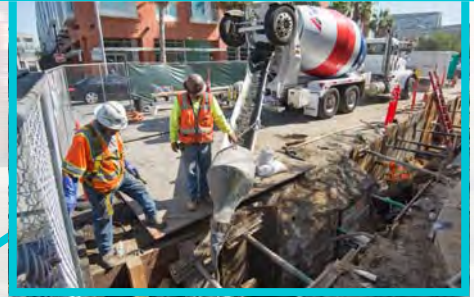
Union Square/Market Street (UMS)



Yerba Buena/Moscone (YBM)



4th St. Surface Track, Systems (STS)



— 8,500 feet of tunnel alignment completed June 2014.
— 2,000 feet of surface alignment to be completed.



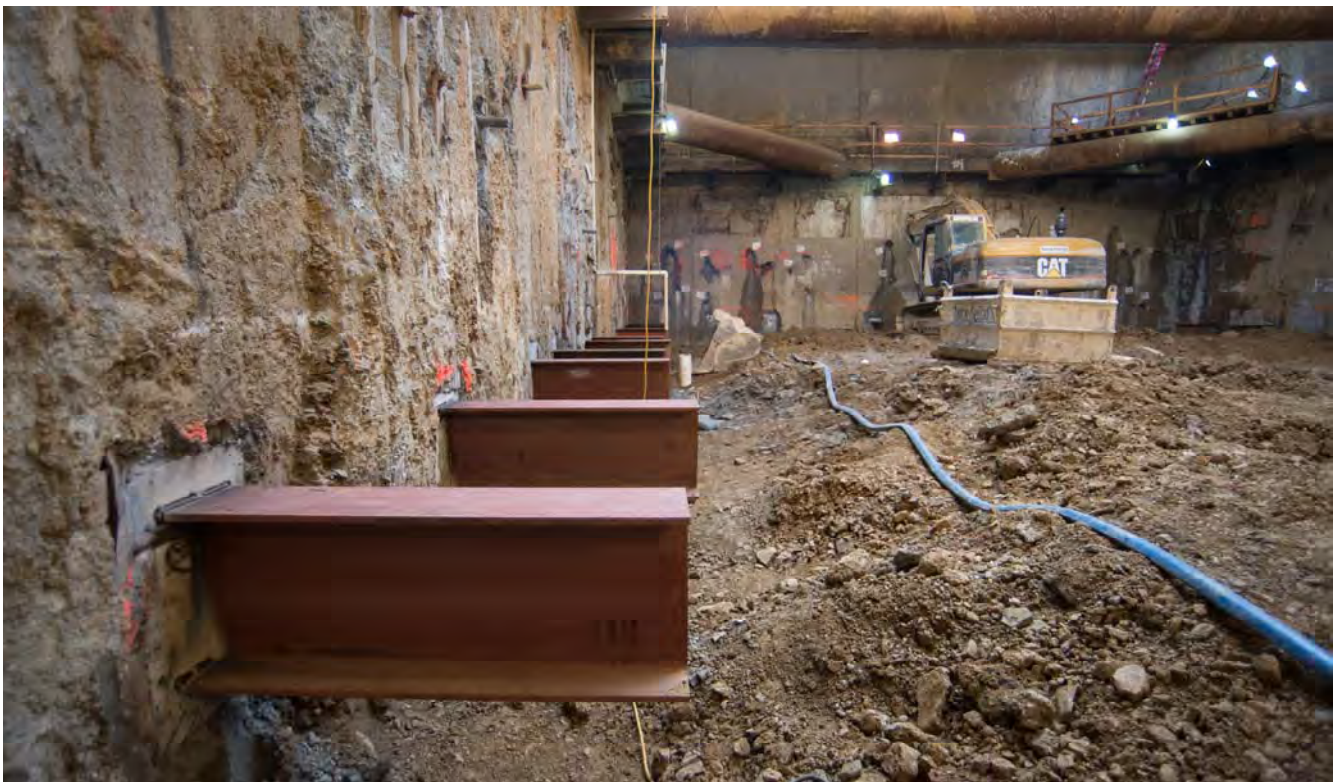
Inside the Chinatown Station headhouse structure, work to waterproof the exterior walls is conducted with each level of excavation completed.



A drill rig is used to inject a grout-like solution into the surrounding soil, improving its stability and waterproofing the station entrance structure.



A violet shadow is cast on the headhouse slurry wall from a welder fashioning brackets for temporary steel supports.



Segments of steel I-beams jut from the walls, attached to steel members inside the slurry walls. These segments will be attached to temporary steel bracing.



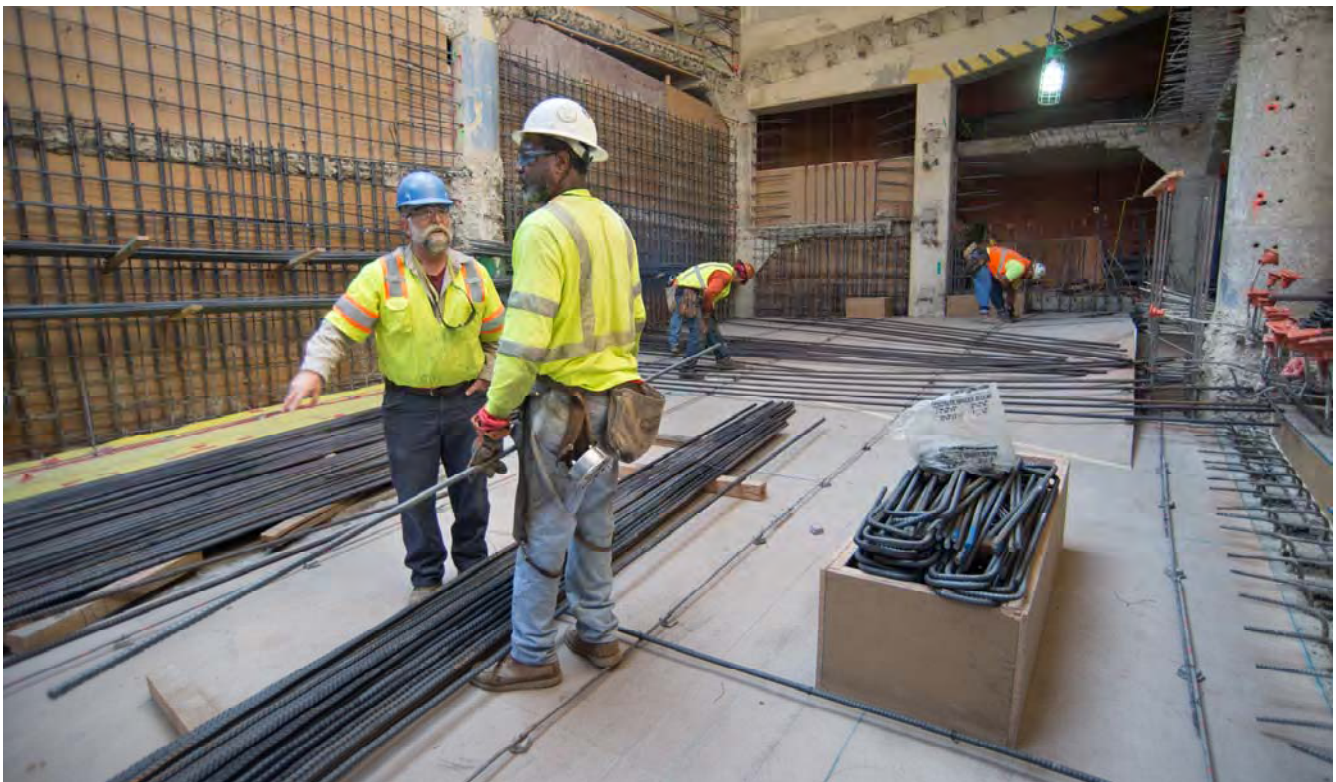
Inside the north concourse, work to build structural elements of the concourse roof is underway while live utilities are suspended overhead.



A metalworker cuts specific portions of a steel I-beam away to prepare it to be tied into additional structural elements.



Exposed structural steel of the north concourse roof deck greets the sun prior to being covered over in corrugated decking, and later rebar and concrete.



An ironworker and inspector discuss operations while rebar for the new Union Square Garage ramp is being installed.



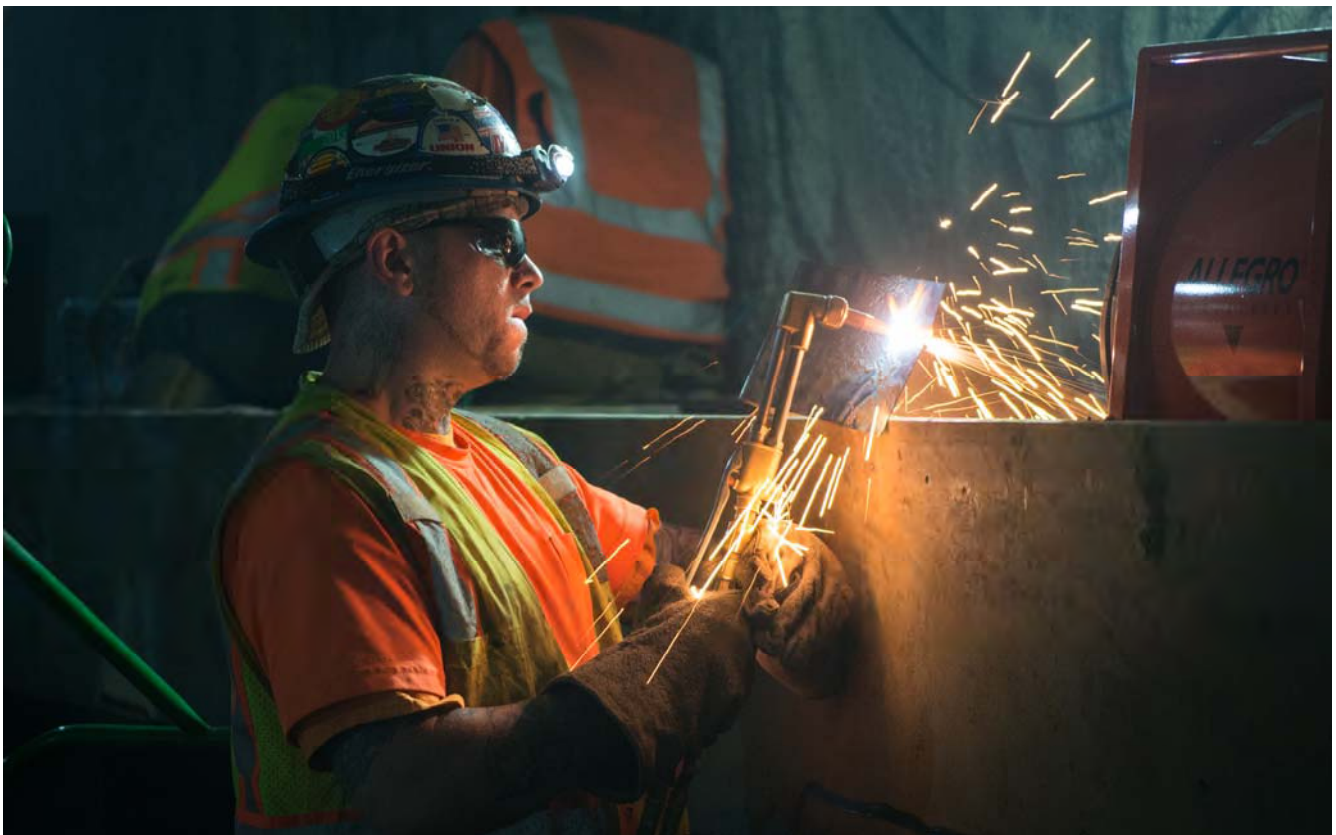
The site of the Yerba Buena/Moscone Station and headhouse structure as seen from the roof of a nearby building.



Inside the station box, a worker is seen using a high-pressure hose to remove loose material from the exterior slurry walls, and clean them ahead of reinforcing steel installation.



A temporary “rat slab” of concrete is installed as the base of a future concrete form.

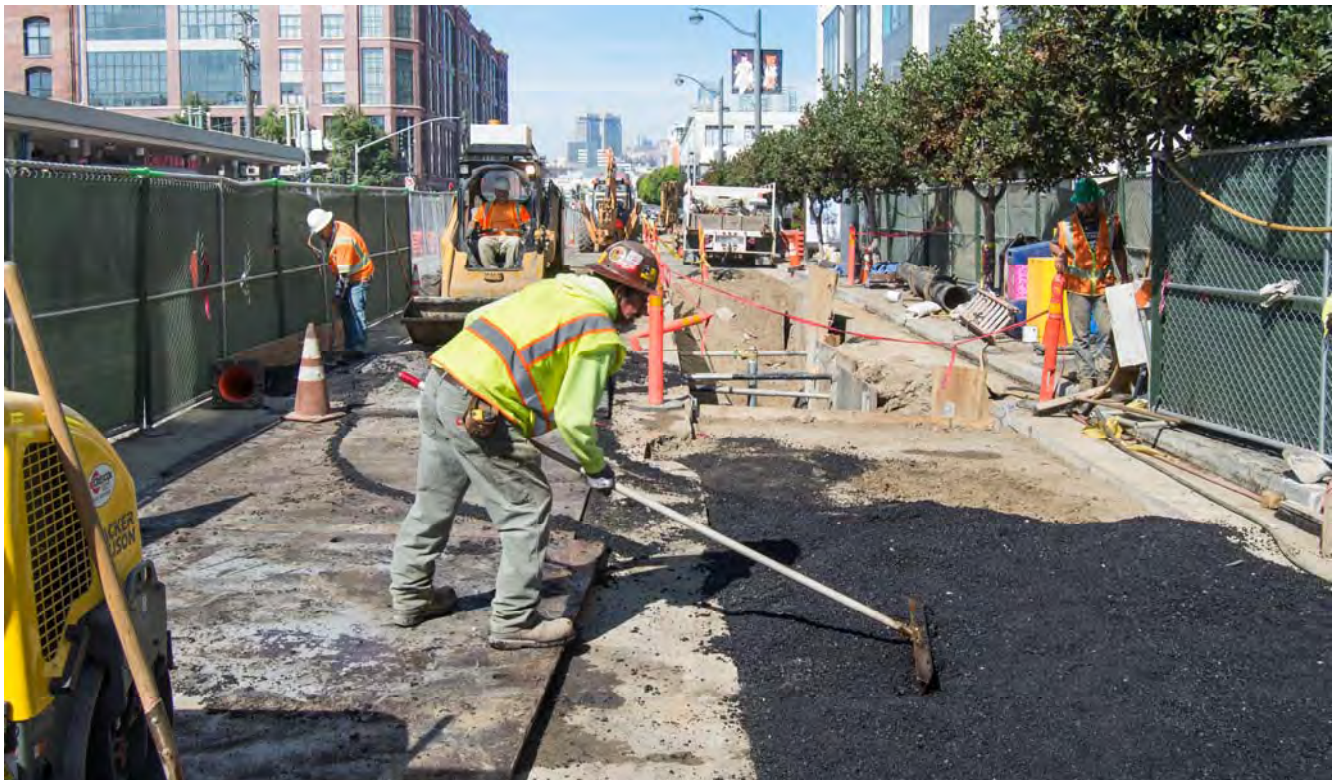


An ironworker cuts plate steel while fashioning brackets for the installation of temporary steel inside the station box.

STS



Workers carefully pour concrete in a trench to create a base slab for utilities to be installed here.

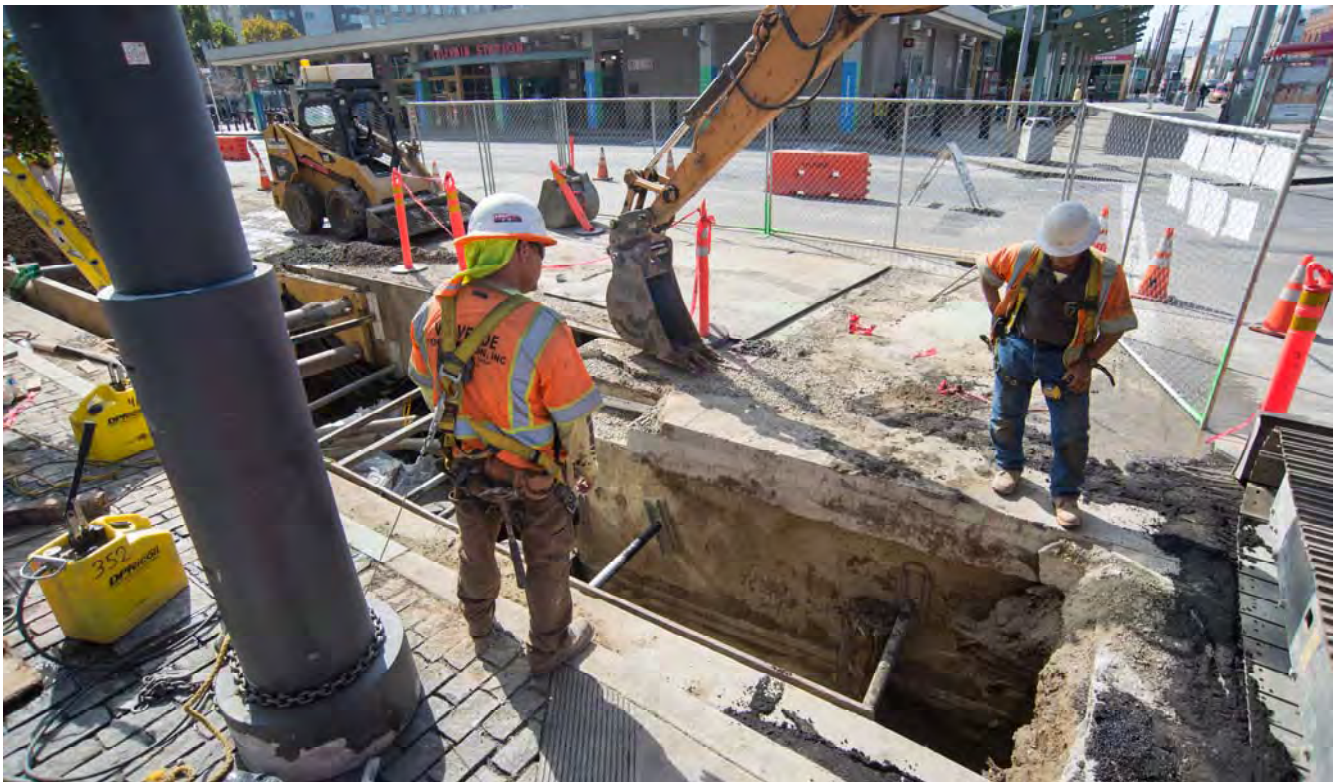


Temporary asphalt restoration on the east side of 4th just north of the King Street intersection follows the installation of sewer and water lines.

STS—Continued



Safety barriers contain the work zone on the north side of the 4th and King intersection. Work to install new track configuration here will commence the week of Veterans Day.



Workers inspect utility installation in a deep trench just south of Townsend on 4th.

Appendix A

DETAIL COST REPORTS

1. PROJECT COST

The Current Cost Estimate (CCE) for the Central Subway Project is **\$1.578 billion** in year of expenditure dollars (\$YOE). This total project cost is shown at the top of Report 7.1, Program Project Budget. This capital cost projection incorporates allocated and unallocated contingencies to cover the risks associated with the project completion.

Total net incurred costs for the project are \$862.56 million, a \$15.44 million increase over last month. The cost to date figure reflects expenditures through FAMIS 786 Report (\$818.44 million) plus the utilities joint trench Form B Reimbursement payment (\$10.52 million), invoices currently being processed (\$17.22 million) and estimates of outstanding pay requests (\$16.38 million). This incurred amount equals 54.65% of the total project budget of \$1.578 billion.

The current funding level to date is \$1,179.79 million includes FTA \$150 million FY2015 New Starts Grant appropriated in September 2015. This represents 75% of the total project budget.

CONTRACT	PP NO	PP PERIOD TO	PROG PYMT AMOUNT
CS155.1	52	3/31/2014	\$ 26,671.53
CS155.1	53	4/30/2014	\$ 19,500.26
CS155.1	54	5/31/2014	\$ 29,166.76
CS155.1	55	6/30/2014	\$ 22,666.39
CS155.1	56	7/31/2014	\$ 36,602.38
CS155.1	57	8/31/2014	\$ 40,273.52
CS155.1	58	9/30/2014	\$ 37,269.95
CS155.1	59	10/31/2014	\$ 30,018.32
CS155.2	60	11/30/2014	\$ 20,183.22
CS155.1	61	12/31/2014	\$ 18,828.97
CS155.1	62	1/31/2015	\$ 20,970.00
CS155.1	63	2/28/2015	\$ 19,921.99
CS155.1	64	3/31/2015	\$ 16,373.00
CS155.1	65	4/30/2015	\$ 16,369.00
CS155.1	66	5/31/2015	\$ 2,121.00
CS155.1*	67	6/30/2015	\$ 1,180.00
CS155.1*	68	7/31/2015	\$ 7,137.00
CS155.1*	69	8/31/2015	\$ 1,805.00
CS155.1*	70	9/30/2015	\$ 1,583.00
CS155.1*	71	10/31/2015	\$ 1,583.00
CS155.2	64	6/30/2015	\$ 186,844.91
CS155.2	65	7/31/2015	\$ 268,558.00
CS155.2*	66	8/31/2015	\$ 183,581.00

CONTRACT	PP NO	PP PERIOD TO	PROG PYMT AMOUNT
CS155.2*	67	9/30/2015	\$ 214,783.00
CS155.2*	68	10/31/2015	\$ 214,783.00
CS155.3	60	3/31/2015	\$ 71,356.82
CS155.3	61	4/30/2015	\$ 63,343.30
CS155.3	62	5/31/2015	\$ 65,121.96
CS155.3	63	6/30/2015	\$ 53,964.00
CS155.3*	64	7/31/2015	\$ 47,059.00
CS155.3*	65	8/31/2015	\$ 38,422.00
CS155.3*	66	9/30/2015	\$ 81,258.00
CS155.3*	67	10/31/2015	\$ 68,106.00
Contract 1252	39	4/30/2015	\$ 1,174,889.06
Contract 1252	40	5/31/2015	\$ 236,920.00
Contract 1300	21	9/30/2015	\$ 14,041,293.96
Contract 1300*	22	10/31/2015	\$ 13,928,083.00
CS149	79	8/31/2015	\$ 471,585.74
CS149*	80	9/30/2015	\$ 500,000.00
CS149*	81	10/31/2015	\$ 500,000.00
CS156.1	54	7/31/2015	\$ 72,750.06
CS156.1*	55	8/31/2015	\$ 72,775.13
CS156.1*	56	9/30/2015	\$ 74,324.20
CS156.1*	57	10/31/2015	\$ 72,592.96
other accruals*		10/31/2015	\$ 531,709.92

* Estimated Amount \$ 33,604,329.31

2. CONTINGENCY ALLOCATIONS AND USAGE

The current Total Project Contingency is **\$84.32 million**, which is a \$24.32 million favorable balance against the current Minimum Contingency level of \$60 million. The Contingency Drawdown Curve is shown in Report 7.3. Follows by Report 7.4 Contingency Management Trend Report with the Remaining Contingency after Approved Changes Deducted contingency items in column "i".

In this reporting period, Contract 1252 Tunnel and Contract 1300 Station had not processed any contract modifications. Refer to Report 7.5 for approved contract modifications and potential changes.

3. BUDGET TRANSFERS

No budget transfers in this reporting period.

4. FORM B

The Utilities Joint Trench Form B Details is listed in the Table A2 below. Total utilities joint trench Form B Reimbursement payment to three construction contracts is \$10.52 million.

TABLE A2: UTILITIES JOINT TRENCH FORM B DETAILS	[A] Mar 2015 BUDGET	[B] EXPENDED TO DATE	Associated Cost Account
1.3.491.07.040.02 - FORM B - CN1250 UTILITY REIMBURSEMENT	(2,275,419)	2,463,325	1.3.081.07.040.02 - 1UTL:SITWORK: UTILITIES & RELOC
1.3.491.08.040.02 - FORM B - CN1251 UTILITY REIMBURSEMENT	(7,618,412)	3,608,217	1.3.082.08.040.02 - 2UTL:SITWORK:UTILITIES&RELOCATE
1.3.491.02.040.02 - FORM B - CN1252 UTILITY REIMBURSEMENT	(254,050)	3,958,658	1.3.083.02.040.02 - TUNN:Sitework:Utilities & Relocate
1.3.491.04.040.02 - FORM B - CTS: CN1300 UTILITY REIMBURSEMENT	(451,703)		
1.3.491.09.040.02 - FORM B - STS: CN1300 UTILITY REIMBURSEMENT	(1,000,000)		
1.3.491.03.040.02 - FORM B - UMS: CN1300 UTILITY REIMBURSEMENT	(528,370)	428,712	1.3.084.03.040.02 - UMS.1253: SITE UTILITIES, UTILITY RELOCA
1.3.491.05.040.02 - FORM B - YBM: CN1300 UTILITY REIMBURSEMENT	(100,000)	56,451	1.3.086.05.040.02 - YBM.1255: SITE UTILITIES, UTILITY RELOCA
TOTAL	(12,227,954)	10,515,363	

Note: *Revised Form B Reimbursements SCC code from 900.01 to 040.02.

5. EARNED VALUE (EV) ANALYSIS

In September 2015 Report, Central Subway Project Master Schedule has used Contract 1300 Station contractor baseline schedule and September monthly update schedule. Contractor continues address the cost issues in cost loaded schedule. Preliminary September Earned Value Analysis reports the Planned Value, Earned Value, Percent Complete and resulting indexes as follows:

Preliminary September Earned Value

Overall Budgeted Cost:	\$1,578,300,000
Planned Value:	\$1,129,829,298
Earned Value:	\$914,311,093
Actual Cost:	\$862,556,970
Schedule Performance Index (SPI):	0.81
Cost Performance Index (CPI):	1.06
Percent Complete:	57.9%

Earned Value Analysis and Definitions

SPI is a measure of schedule efficiency on a project. It is the ratio of earned value (EV) to planned value (PV). A SPI equal to or greater than one indicates more work was completed than planned and a value of less than one indicates less work was completed than planned. A value of less than 0.9 is unfavorable.

CPI is a measure of cost efficiency on a project. It is the ratio of earned value (EV) to actual cost value (AC). A CPI equal to or greater than one indicates a cost under run and a value of less than one indicates a cost overrun. A value of less than 0.9 is unfavorable.

The following earning rules are established for each of the phase:

Cost Element Group	Planned Value (Primavera)	Earned Value (Primavera)	Actual Cost (SFMTA Cost Accounting (SAP))
Prelim. Engineering	Expenditure Plan Level of Effort (LOE)	Equals to Planned Value (LOE)	Time Keeping; Vendor Accruals and Invoices
Final Design	Expenditure Plan Level of Effort (LOE)	Equals to Planned Value (LOE)	Time Keeping; Vendor Accruals and Invoices
Procurement	Planned Delivery Date	Actual Delivery Date	Time Keeping; Vendor Accruals and Invoices
Real Estate	Expenditure Plan Level of Effort (LOE)	Equals to Planned Value (LOE)	Time Keeping; Vendor/ Material Accruals and Invoices
Construction	Schedule of Work	% Complete* x Budget at Completion (BAC)	Vendor Accruals and Invoices
Sub-Total	Performance Measurement Baseline (PMB)	Total Earned Value	Total Actual Cost
Below the Line	+ Contingency		
Total	Approved Budget		

6. FUNDING SUMMARY

The Funding Available Table below shows the total awarded funds to date vs. the total committed funds from the Project's seven funding sources.

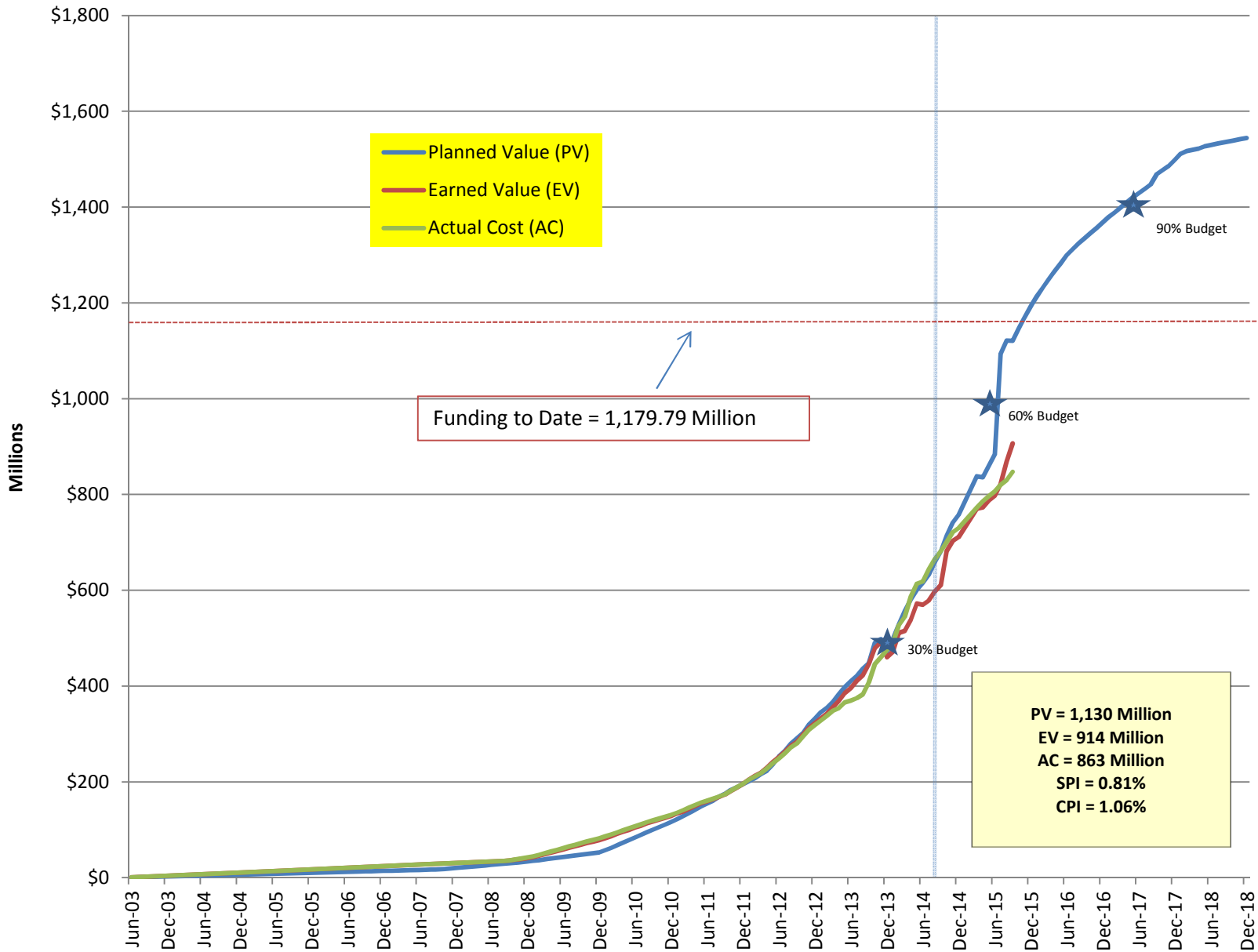
Funding Available Table		
	Funding	
	Committed Funding Sources	Total Awarded Funds to Date
Federal		
Sect. 5309-NS	\$942,200	\$619,196
CMAQ	\$41,025	\$41,025
Federal Subtotal	\$983,225	\$660,221
State		
TCRP	\$14,000	\$14,000
State RIP	\$88,000	\$12,498
Prop. 1B (I-Bond) PTIMSE	\$307,792	\$307,792
Prop. 1A (HSR-Bond)	\$61,308	\$61,308
State Subtotal	\$471,100	\$395,598
Local		
Prop. K	\$123,975	\$123,975
Local Subtotal	\$123,975	\$123,975
CPT 544 Total	\$1,578,300	\$1,179,794

7. LIST OF COST REPORTS

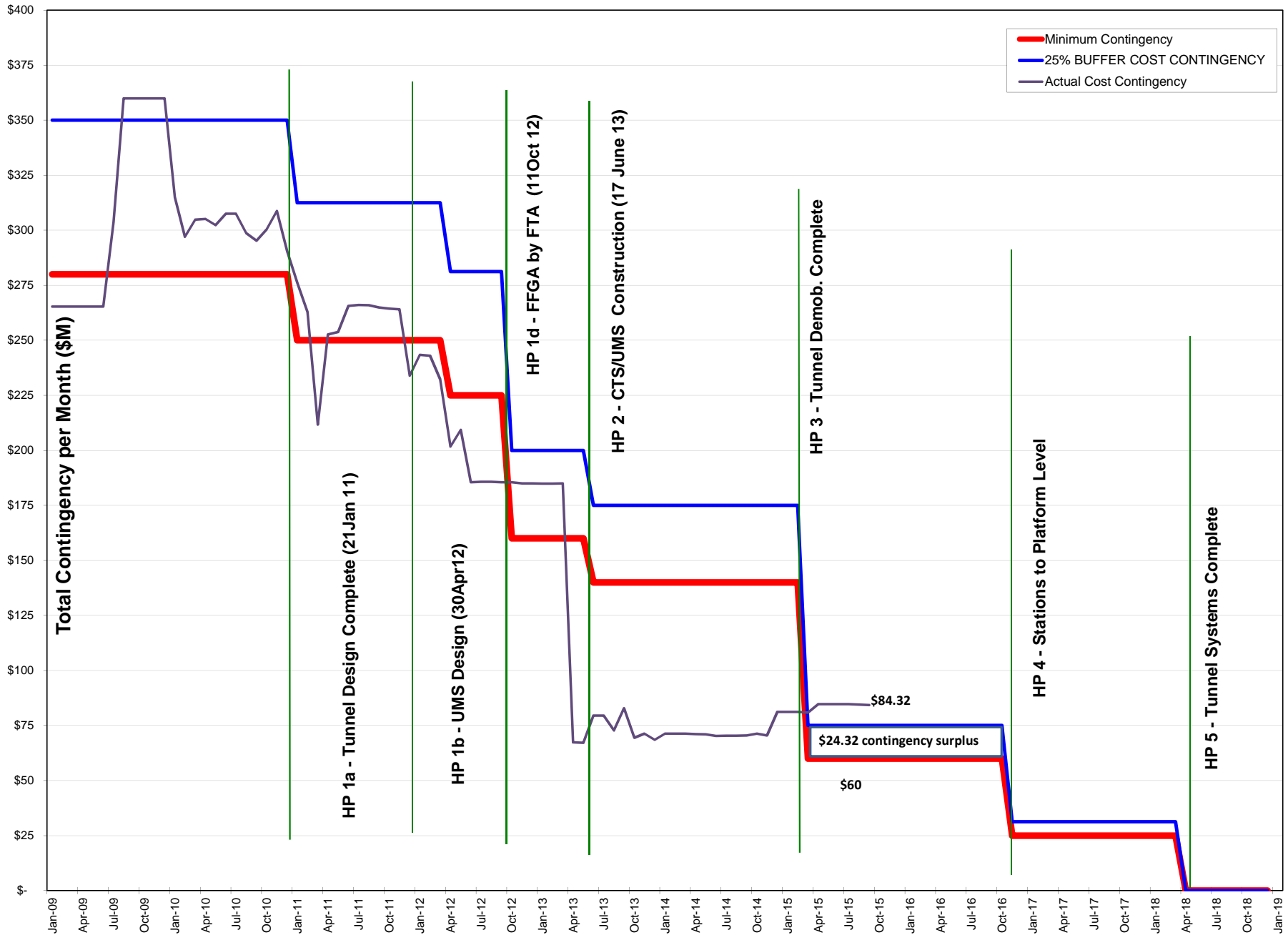
- 7.1 Program Project Budget
- 7.2 Earned Value Cash Flow
- 7.3 Contingency Drawdown Curve
- 7.4 Summary Contingency Management Trend Report
- 7.5 Detail Contingency Usage Report
- 7.6 Budget Revisions: Report sorted by Construction Packages & Soft Costs
- 7.7 Project Budget & Expenditure Report: Sorted by SCC Summary
- 7.8 Budget & Expenditure Report: Sorted by SCC Details
- 7.9 Detail Monthly Expenditure Report: grouped by Project Phase
- 7.10 Cost Report Notes

A. Central Subway Project						
Project	Name	Amount	PM	Funding Source	Reporting	Cost Report Notes
1	CPT544 Central Subway Project	\$1,578,300,000	J. Funghi	62% Fed, 30% State, 8% Local	yes	1
Total:		\$1,578,300,000				
B. Related SFMTA Capital Improvement Projects						
Project	Name	Amount	PM	Funding Source	Reporting	
2	CPT690 TBM Retrieval Shaft Relocation	\$9,700,000	Funghi/Magary	MTA Operating Funds	no	2
3	CPT718 Chinatown Metro Plaza	\$6,980,000	J. Funghi	Transbay Redevelopment	no	3
4	CPT665 Central Subway Project - Goodwill	\$2,367,750	K. Magary	I-Bond Interest	no	4
5	CPT705 MOH - Broadway/Sansome	\$8,000,000	K. Magary	MTA Operating Funds	no	5
Total:		\$27,047,750				
C. Central Subway Project - Project Offset Credits						
	From	Amount	Index	Notes	Reporting	
1	2009-2016 Utility Co. - Form B Reimbursement	\$12,227,954	--	Construction contracts	yes	6
2	2017-2019 PG&E - Power Feed Reimbursement	\$7,624,540	--	Not yet bill PG&E	yes	7
3	6/26/2013 BART Elevator	\$90,000	68CPT544135B	Not yet rec'd BART Funds	yes	8
4	11/6/2013 Tutor Perini - CAD Files	\$2,500	68CPT5441236	Deposit to Design Index	yes	9
5	1/27/2014 SFPUC - Sewer Main	\$2,925,296	68W251	Certified in Contract 1300	yes	10
6	9/27/2014 SFPUC - 24" Water Main	\$328,857	68CPT544135A	Contract 1252 CMod #41	yes	11
7	8/27/2014 SFMTA Traffic Effectiveness Project funded	\$694,651	--	Contract 1252 CMod #40	yes	12
8	3/27/2015 SFPUC - 24" Water Main Additional Work	\$112,102	68W409	Contract 1252 CMod #48	yes	13
Total:		\$24,005,900				

Earned Value Cash Flow Curve



Cost Contingency Drawdown



COST ELEMENT	CONTRACT COST					CONTINGENCY				BUDGET	VARIANCE	Cost Report Notes	
	ORIGINAL CONTRACT VALUE / September 2013 SUPPLEMENTAL BUDGET	APPROVED CHANGES	CURRENT CONTRACT VALUE	POTENTIAL CHANGES	ESTIMATE AT COMPLETION (EAC)	ORIGINAL CONTINGENCY / Sep 2013 SUPPLEMENTAL CONTINGENCY (Exclude CN 1250 & CN1251)	CONTINGENCY ADJUSTMENT TRANSFERS	REVISED AUTHORIZED CONTINGENCY (Exclude CN1250 & CN1251)	REMAINING CONTINGENCY AFTER APPROVED CHANGES DEDUCTED [h - b]	ORIGINAL CONTRACT VALUE + REVISED AUTHORIZED CONTINGENCY [a + h]	BUDGET - ESTIMATE AT COMPLETE [j - e]		
	a	b	c [a + b]	d	e [c + d]	f	g	h	i	j	k		
SCC 10-50 CONSTRUCTION CONTRACT PACKAGES													
1250	UTILITY RELOCATION PACKAGE #1	9,273,939	2,694,211	11,968,150		11,968,150	1,953,377	740,834	2,694,211		11,968,150	14	
	Contract 1250 Department of Technology	166,756		166,756		166,756					166,756		
1251	UTILITY RELOCATION PACKAGE #2	16,832,550	3,962,032	20,794,582		20,794,582	5,367,297	(1,405,265)	3,962,032		20,794,582	15	
	Contract 1251 Department of Technology	75,615		75,615		75,615					75,615		
1252	GUIDEWAY TUNNEL	233,584,015	1,421,807	235,005,822	(77,798)	234,928,024	23,658,464	(21,173,511)	2,484,953	1,063,145	236,068,968	1,140,943	16
1300	STATIONS	839,676,400	(1,016,585)	838,659,815	16,213,566	854,873,381	20,000,000		20,000,000	21,016,585	859,676,400	4,803,019	17
	1253 UNION SQUARE/MARKET ST STATION [UMS]	294,030,590	90,000	294,120,590	10,434,834	304,555,424	5,000,000		5,000,000	4,910,000	299,030,590	(5,524,834)	
	1254 CHINA TOWN STATION [CTS]	247,567,810	230,956	247,798,766	5,614,123	253,412,889	5,000,000		5,000,000	4,769,044	252,567,810	(845,079)	
	1255 YERBA BUENA/ MOSCONE STATION [YBM]	158,089,000	(1,642,919)	156,446,081	2,807,634	159,253,715	5,000,000		5,000,000	6,642,919	163,089,000	3,835,285	
	1256 SURFACE TRACKWORK & SYSTEMS [STS]	139,989,000	305,378	140,294,378	(2,643,026)	137,651,352	5,000,000		5,000,000	4,694,622	144,989,000	7,337,648	
OTHER		31,233,501		31,233,501	0	31,233,501	1,160,000		1,160,000	1,160,000	32,393,501	1,160,000	
	SCC 10 - 50 Construction Sub-total	1,130,842,776	7,061,465	1,137,904,241	16,135,768	1,154,040,008	44,818,464	(21,173,511)	23,644,953	23,239,730	1,161,143,971	7,103,963	
SCC 60 - 80 SOFT COSTS PACKAGES													
60	ROW, LAND, EXISTING IMPROVEMENTS	36,511,799	(4,265,478)	32,246,321		32,246,321	1,000,000	4,265,478	1,000,000	5,265,478	37,511,799	5,265,478	18
70	VEHICLES	24,108,712	(10,799,712)	13,309,000		13,309,000	2,276,941	10,799,712	13,076,653	13,076,653	26,385,653	13,076,653	19
80	PROFESSIONAL SERVICES	310,518,041		310,518,041		310,518,041	18,221,079		18,221,079	18,221,079	328,739,120	18,221,079	
	SCC 60 - 80 Construction Sub-total	371,138,552	(15,065,190)	356,073,362		356,073,362	21,498,020	15,065,190	32,297,732	36,563,210	392,636,572	36,563,210	
SCC 90	UNALLOCATED CONTINGENCY						3,845,945	20,673,511	24,519,456	24,519,456	24,519,456	24,519,456	20
TOTAL		1,501,981,328	(8,003,725)	1,493,977,603	16,135,768	1,510,113,371	70,162,429	14,565,190	80,462,141	84,322,396	1,578,299,999	68,186,628	
											Total Project Budget	1,578,300,000	21
											Estimate At Completion	1,510,113,371	22
											Variance	68,186,628	23
Note #15 - Adjusted Contract 1252 Guideway Tunnel contingency "column g" to reflect construction contract modifications #20, #40, #41 and #48 were funded by other funding sources.													

Contract Modification/Trend Log - Contract 1300 Stations

Awarded NTE Amount	\$839,676,400	\$838,659,815
Substantial Completion	2/10/2018	2/10/2018

	UMS	CTS	YBM	STS	COST REPORT NOTES
Potential Changes	\$10,434,834	\$5,614,123	\$2,807,634	(\$2,643,026)	24
Forecasted - Trends	\$5,045,374	\$3,025,602	\$861,077	\$1,015	
Negotiation					
Change of date range to receive art	(\$10,001)				
UMS -Remove AWSS Hydrant at OFarrel	\$8,421				
UMS BART Entry Hazmat Abatement	\$24,971				
CTS- AWSS Wk PGE relocation schedul		\$60,309			
STS Traffic Signal and SL Changes				\$298,307	
STS Comm and Elec Cabinets Relocati				\$67,221	
CTS-PCC#17 FH at Jade Galore		\$106,350			
STS - OCS Pole Changes				\$12,706	
STS Additional AT&T Ductbank at 4th				\$139,238	
CTS-FACO#39- Soil Testing cmplly OAB		\$5,000			
UMS FACO #31 NDSC Incomplete PGE DB	\$32,980				
UMS-FACO #25 OCS Pole@ Market/Ellis	\$3,349				
UMS-FACO #19 Street Light at Stock.	\$2,361				
STS-FACO #48 Work Related St Lght				\$5,000	
UMS FACO #34 Becho Obstruct J Piles	\$15,000				
UMS FACO #45 Blocked Stubs PG&E	\$7,500				
STS Traction Power Cable Downsizing				(\$117)	
UMS Transfer Instru BART Facilities	\$30,001				
CTS-PCC40 Plaza Surface Slab Pntrtn		\$5,525			
YBM PCC 41 Install #7 Box Clementin			\$52,667		
UMS Sewer Line Conflict	\$168,718				
UMS DSC Mass of Conc Stocktn 137+15	\$7,500				
UMS - PCC #030 (US Garage Underpin)	\$474,470				
UMS Install Floor Drain	\$6,579				
UMS - N. Entrance Art Glass and Gla	\$681,978				
YBM COR 44 Buried concrete wall			\$100,001		
YBM COR 45 Abandoned sewer not			\$6,001		
YBM COR 64 Buried sheet pile P-7			\$15,001		
STS COR #89 Extra Sewer Work				\$8,090	
STS COR #112 MRY MH 1890 TS Conduit				\$6,000	
STS COR 98 DSC MRY MH Conf w N CB				\$15,000	

Contract Modification/Trend Log - Contract 1300 Stations

Awarded NTE Amount	\$839,676,400	\$838,659,815
Substantial Completion	2/10/2018	2/10/2018

	UMS	CTS	YBM	STS	COST REPORT NOTES
YBM COR 116 Archaeological Discover			\$100,001		
USG COR 222 Shaft Wall Connections	\$33,272				
USG COR 223 Wall Footing Clarif.	\$75,001				
USG COR 224 (N) Concrete Wall Conn.	\$50,001				
USG COR 228 Tiebacks at Level X	\$75,001				
USG COR 227 Plaza Lvl. Sequence	\$200,001				
USG COR 225 SOG/Wall Connection	\$50,001				
USG COR 226 SOG/Column Connection	\$20,001				
USG COR 261 8" Wall at Grid 11B	\$15,001				
USG COR 263 12" Shear Walls	\$50,001				
USG COR 262 Maintenance Hatch Walls	\$15,001				
USG COR 264 Str. Steel Shop Dwgs.	\$5,001				
UMS COR 250 Macy's DSC	\$75,001				
USG COR 280 Strut Wall Grid 16 - 17	\$50,001				
USG COR 282 Survey Requirements	\$10,001				
USG COR 283 Slab Cantilver Detail	\$10,001				
USG COR 284 New Beam at Grid Line C	\$50,001				
USG COR 285 Elev.Edge of Slab Dtl.	\$50,001				
USG COR 286 Shear Wall GL 13/B-C	\$25,001				
USG COR 273 E.Slab/Shear Wall Conn.	\$100,001				
USG COR 274 Shear Wall at Grid 10	\$10,001				
USG COR 275 Conn. Plaza Grid B	\$50,001				
USG COR 276 Escalator #1	\$10,001				
STS - Deletion of ARS (Revision 1)				(\$4,689,000)	
YBM COR 63 Buried objects in P-86			\$24,648		
STS COR 100 DSC Zayo PVC Conf w SW				\$80,082	
STS COR 210 WD pit size increase				\$24,417	
CTS COR 085 Addl FACO 10 Costs		\$13,267			
CTS PCC 001 Delete DB on Stockton		(\$84,018)			
STS COR #92 PG&E Vault Conf 12 AWSS				\$79,173	
STS COR #118 78" Sewer Excess Debris				\$54,455	
YBM COR 054 FACO 028 Conc obst			\$56,654		
UMS - PCC #29 (Concrete Wale Suppt)	\$9,239				
CTS COR 041 FACO #4 JT Unid. Cond		\$28,026			

Contract Modification/Trend Log - Contract 1300 Stations

Awarded NTE Amount	\$839,676,400	\$838,659,815
Substantial Completion	2/10/2018	2/10/2018

	UMS	CTS	YBM	STS	COST REPORT NOTES
CTS COR 040 FACO 002 Connect (E) DB		\$16,936			
YBM COR 49 Buried timber piles			\$75,001		
STS 78 Sewer Existing Concrete Crow				\$7,200	
CTS-COR#201 Swr Line & Station Roof		\$55,000			
YBM COR 87 Transite ductbank removal			\$10,009		
YBM COR 50 Potential contam Soil			\$75,001		
YBM COR 115 C1250 unencased JT			\$9,843		
STS-COR #84 Sewer Cleaning and Dewa				\$3,598	
YBM COR 253 DSC Culvert Conflict			\$20,001		
STS COR #236 E WD Config at Freelon				\$20,000	
STS COR 272 Pier AWSS Conflict				\$2,000	
STS COR 258 WD/PVC Conflict				\$55,001	
STS COR 211 SW conf AWSS 4th/Freelo				\$4,561	
STS COR #229 Multi E Util Conf w N				\$4,000	
UMS COR 232 S.Walk Hatches RFI 240	\$10,141				
STS COR #233 18" VCP in 3'x5' Sewer				\$1,764	
STS COR 101 Cleaning for non-78" SW				\$58,906	
STS COR #187 DB Conf w N 12 Water				\$37,012	
CTS COR 231 Sidewalk Hatches		\$19,249			
YBM COR 240 Headhouse Contam. Mtl.			\$100,001		
STS COR #241 2-In WD Line w Offset				\$20,001	
STS COR #242 DSC Gas Confl w/ Pole				\$1,501	
YBM COR 243 Culvert Conflict			\$10,001		
YBM COR 248 Transite pipe			\$1,501		
YBM COR 249 Utility Conflicts			\$91,877		
YBM COR 295 DSC AT&T Vault Conflict			\$10,001		
STS COR #296 Mult E Util Confl AWSS				\$130,001	
YBM COR 298 Buried Conc Slab			\$20,001		
YBM PCC 062 Delete SW MH FOL 10+35			(\$21,453)		
STS COR 073 AWSS Incorrectly Shown				\$35,134	
STS COR 067 FACO #41 GW Lead Filter				\$22,695	
STS COR 037 FACO 037 30" FM Removal				\$63,972	
STS COR 090 Subsurface Obstrc				\$20,452	
YBM COR 310 12" WL Conf w/ 36" FM			\$15,001		

Contract Modification/Trend Log - Contract 1300 Stations

Awarded NTE Amount	\$839,676,400	\$838,659,815
Substantial Completion	2/10/2018	2/10/2018

	UMS	CTS	YBM	STS	COST REPORT NOTES
CTS COR 353 AWSS confl w HDPE in SW		\$3,203			
CTS COR 042 FACO 005 Unknown 3" Pip		\$319			
YBM COR 072 FACO 51 1252 N Headwall			\$90,027		
YBM COR 86 Side sewer conflict AT&T			\$75,001		
STS COR 091 PG&E Vault Conf 16" Wtr				\$20,001	
STS COR 093 12" Water Conf 12" Tee				\$5,001	
STS COR 094 Unknown DB Conf 12" Wtr				\$20,001	
USG COR 238 Grid 17 & Vent Conn.	\$50,001				
USG COR 237 Elevator Sump Pits	\$20,001				
STS COR 074 AWSS Offset/Sewer Demo				\$49,817	
USG COR 315 Beam Conflict at C Line	\$50,001				
USG COR 317 Subgrade Transition	\$5,001				
USG COR 281 8in Slab Conn.at B Line	\$25,001				
USG COR 345 8 inch Slab Support	\$15,001				
USG COR 343 Edge of Slab Detail	\$5,001				
USG COR 360 Column on Grid Line B	\$5,001				
USG COR 363 Grid Line B Footing	\$10,001				
USG COR 358 Elev. Machine Rooms	\$5,001				
USG COR 359 Escalator #1 Truss Span	\$2,001				
USG COR 361 Mud Slab Clarification	\$50,001				
USG COR 364 Barricades and Guardrai	\$70,001				
USG COR 374 Add Built Up Columns	\$25,001				
YBM COR 117 Additional Signage			\$12,001		
STS COR 367 DSC Conf w/ CP and FM				\$2,892	
STS PCC 066 Add CS ATCS Emer Stop B				\$315,001	
YBM COR 230 RFI 240 Response			\$31,474		
STS COR 387 Oil Line Confl SW MH				\$10,001	
YBM COR 385 PG&E Damage to Work			\$5,001		
USG COR 386 Built up Column	\$75,001				
USG COR 388 Grid Line C Beam Slab	\$10,001				
UMS COR 391 AT&T Duct Bank Conflict	\$25,001				
Proposed Contract Change (PCC)					
UMS - Geoprobe Credit	(\$30,320)				
UMS - Relocation of TS Cabinet	\$23,275				

Contract Modification/Trend Log - Contract 1300 Stations

Awarded NTE Amount	\$839,676,400	\$838,659,815
Substantial Completion	2/10/2018	2/10/2018

	UMS	CTS	YBM	STS	COST REPORT NOTES
STS - Tunnel Lowering				\$100,000	
CTS - Stairs 5, 6, 7 Mods		\$25,000			
UMS - Escalator Barricade	(\$122,511)				
UMS - USG HVAC Trench Modifications	\$75,001				
CTS - CMU Wall Bracing		\$10,200			
UMS MRY Duct Bank-East Conflict	\$25,000				
YBM PCC 32 Delete Post-Grout of TB-2			(\$10,001)		
YBM_CTS PCC 33 Platform Gate Revisio			\$100,001		
CTS - Removal of Bus Bulb		\$30,000			
CTS Delete PGE Work at Vault 732		(\$50,000)			
UMS Remove Fire Hose Valve	\$20,000				
UMS MRY Duct Bank-West	\$25,000				
YBM PCC 21 Dele Instrum & Monitoring			(\$1)		
STS PCC #51 Inventory Temp Crossover				\$20,001	
Deletion of ARS (Part II)				(\$600,000)	
YBM PCC 56 OCS Pole Foundations			\$30,001		
UMS - DCW and Hose Bibbs	\$30,000				
YBM PCC 059 Pavers Basis of Design			\$20,001		
STS Track Switch Mach Change (Rev 1)				\$50,000	
STS PCC 063 Del ATT/TSIC/PGE on 4th				(\$100,000)	
CTS PCC 061 (Rev1) Rev Escalator Pit		\$50,001			
UMS Roof Deck - Schedule Recovery	\$50,001				
CTS PCC 067 Wall Shift North Access		\$25,001			
USG Ramp Barriers and Handrail	\$75,001				
YBM Sidewalk Bulb-out at Stair#1			\$0		
Change Order Request (COR)					
STS - Additn manholes for 78" sewer				\$27,204	
STS COR 17 Changes to Sewer Structu				\$20,000	
STS-FACO #47 Sewer Related Work Pt2				\$33,898	
STS- AT&T Conduit Changes				\$22,290	
UMS COR 102 Cap on 12 inch Water	\$44,544				
STS - COR 181 DSC Poly Line Conf MR				\$10,000	
UMS COR 235 DSC Transite Pipe	\$10,000				
STS COR #88 Modify CBs and Culverts				\$4,395	

Contract Modification/Trend Log - Contract 1300 Stations

Awarded NTE Amount	\$839,676,400	\$838,659,815
Substantial Completion	2/10/2018	2/10/2018

	UMS	CTS	YBM	STS	COST REPORT NOTES
STS COR 113 DSC PVC Conduit/WL Conf				\$10,001	
STS COR 145 DSC PVC Conduits Conf				\$5,001	
STS COR 146 DSC 8" WL in Conf w SW				\$5,001	
STS COR 164 DSC 8" AWSS Lat Conf 78				\$10,000	
STS COR #170 Cores in 78" RC Crown				\$1,666	
STS COR 254 DSC 4" Confl w 36" FM				\$50,001	
CTS COR 255 Additional Instruments		\$429,777			
STS COR 270 SW AWSS Conflict				\$10,000	
STS COR 271 DB AWSS Conflict				\$20,001	
UMS COR 277 8" Steel line Asbestos	\$5,000				
STS COR 290 E CB PVC Confl WD AWSS				\$10,000	
STS COR 297 TC for Track Work at 4t				\$150,000	
UMS COR 301 Concr. Overpour and PVC	\$10,001				
UMS COR 110 DSC Obst. at JG Columns	\$965,550				
CTS COR 039 Time Adjust PGE Pole		\$0			
STS COR 198 NDSC Out of Spec Cover				\$8,123	
USG COR 246 Tieback Access RFI 1050	\$160,933				
UMS COR 300 14 HDPE at Geary	\$30,001				
CTS COR 305 Abandon OBW-CTS-03		\$1,790			
CTS COR 318 Delay Mobe of Monitor W		\$5,800			
STS COR 322 Existing Tunnel Cond.				\$345,001	
YBM COR 362 Temp Pavement Section			\$107,503		
STS COR 371 Conflicts w/ 12" AWSS				\$30,001	
CTS COR 372 DSC Potholing @ N Acces		\$20,001			
YBM COR 373 DSC NoD Contaminated Ma			\$30,783		
YBM COR 366 NDSC Mez. Soil Failure			\$45,001		
UMS COR 380 14 HDPE at O'Farrell	\$30,001				
UMS COR 381 UMS Roof Deck CJ Plan	\$10,001				
UMS COR 309 Hydro Waterstop	\$154,611				
UMS COR 11 NDSC Hydrant N. of Geary	\$24,016				
USG COR 365 Elevator Hoist Beam	\$0				
UMS COR 389 4" Steel line asbestos	\$5,001				
YBM COR 390 RFI 1654 Chip Headwall			\$10,001		
STS COR 392 Util at 4th-Town SW MH				\$25,001	

Contract Modification/Trend Log - Contract 1300 Stations

Awarded NTE Amount	\$839,676,400	\$838,659,815
Substantial Completion	2/10/2018	2/10/2018

	UMS	CTS	YBM	STS	COST REPORT NOTES
USG COR 394 Ramp Conflict with HVAC	\$0				
USG COR 395 Rebar Detail over HVAC	\$0				
USG COR 396 HVAC Wall Opening Rebar	\$0				
USG COR 398 Reinforcing at Columns	\$0				
STS COR 402 3x5 SW Confl w/ VCP SW				\$10,001	
UMS COR 403 Waterproofing at BART	\$20,001				
STS COR 404 Contam Soil in MRY DB				\$10,001	
UMS COR 407 8 inch SS Line Conflict	\$10,001				
Change Order - Pending					
CTS Sewer Line Demo at Washing		\$57,707			
CTS Utility Conflict at WD Tie-In		\$14,527			
CTS-Plaza Surface Level Struct Mods		(\$10,337)			
CTS-FACO #44 (E) Utl Cnflct w/ AWSS		\$25,211			
UMS-FACO #32 8" Waterline Conf NDSC	\$48,615				
CTS-FACO #27 AWSS vs PGE Vault #728		\$14,439			
YBM COR 78 Change POC by SFWD			\$50,493		
UMS DSC Tanks in Stockton N of Gear	\$97,817				
STS COR 052 Fire Hyd NE 4th&Bryant				\$5,346	
YBM COR 36 Culvert NW corn Folsom			\$8,033		
CTS COR 007 (E) Soil Characteristics		\$1,714,205			
YBM PCC 055 Add 24" water east side			\$241,607		
YBM Underground storage tank Folsom			\$156,733		
UMS Modification to Roof Composite	\$12,997				
YBM PCC 047 Add PGE conduits east			\$144,533		
YBM COR 182 Cap (E) 16" AWSS Tee			\$18,859		
YBM COR 196 Work on Hold			\$3,136		
STS COR #114 Conc Obs 4th-Bryant SE				\$1,262	
UMS COR 184 Slurry Fill Void	\$8,261				
STS COR #252 Thick Pavemnt 4th/Kin				\$905	
STS AL-3 Allowance Replenishment				\$32,302	
STS COR 048 Add'l Soil Testing Req				\$9,226	
YBM COR 046 FACO#18 Add analy tests			\$4,612		
Approved Contract Modification	\$90,000	\$230,956	(\$1,642,919)	\$305,378	

Contract Modification/Trend Log - Contract 1300 Stations

Awarded NTE Amount	\$839,676,400	\$838,659,815
Substantial Completion	2/10/2018	2/10/2018

	UMS	CTS	YBM	STS	COST REPORT NOTES
CMod #1 BART Elevator Option 1 @ Pow	\$90,000				
CMod #3 CTS Work Safely Ard Power Po		\$25,956			
CMod #5 YBM Deletion of Comp Groutin			(\$1,833,869)		
CMod #6 CTS Plaza Constr Supt Servi		\$75,000			
CMod#7 STS FACOs 016, 017 &COR 009				\$80,170	
CMod #4 CTS-Force Account Change Or		\$130,000			
Cmod #10 YBM PCC 042			\$64,287		
CMod#8 STS PCC 006 ATT MH, PB&Trench				\$225,208	
CMod #9 YBM COR 10,15,16,18,20,25			\$126,663		
Grand Total	10,524,834	5,845,079	1,164,715	(\$2,337,648)	

7.6 BUDGET REVISIONS: REPORT SORTED BY CONSTRUCTION PACKAGES & SOFT COSTS

Report Period: Oct 2015

Group by Contract & SCC	CATEGORY ITEM	Sept 2015			Oct 2015			BUDGET TRANSFERS [Oct 2015] vs. [Sept 2015]	Cost Report Notes
		Sept 2015 Base	Sept 2015 Allocated Contingency	Sept 2015 Base + Allocated Contingency (YOE)	Oct 2015 Base	Oct 2015 Allocated Contingency	Oct 2015 Base + Allocated Contingency (YOE)		
10-50	CONSTRUCTION CONTRACT PACKAGES	1,137,904,241	23,239,730	1,161,143,971	1,137,904,241	23,239,730	1,161,143,971	0	
1250	UTILITY RELOCATION PACKAGE #1	12,134,906		12,134,906	12,134,906		12,134,906	0	
	Contract 1250 Form B Credit	(2,275,419)		(2,275,419)	(2,275,419)		(2,275,419)	0	
1251	UTILITY RELOCATION PACKAGE #2	20,870,197		20,870,197	20,870,197		20,870,197	0	
	Contract 1251 Form B Credit	(7,618,412)		(7,618,412)	(7,618,412)		(7,618,412)	0	
1252	GUIDEWAY TUNNEL	235,005,822	1,063,145	236,068,968	235,005,822	1,063,145	236,068,968	0	25
	Contract 1252 Form B Credit	(254,050)		(254,050)	(254,050)		(254,050)	0	
1300	CN1300 STATIONS TOTAL	838,659,815	21,016,585	859,676,400	838,659,815	21,016,585	859,676,400	0	
1253: UMS	UNION SQUARE/MARKET STREET STATION [UMS]	294,120,590	4,910,000	299,030,590	294,120,590	4,910,000	299,030,590	0	
	UMS 1253 Form B Credit	(528,370)		(528,370)	(528,370)		(528,370)	0	
1254: CTS	CHINA TOWN STATION [CTS]	247,798,766	4,769,044	252,567,810	247,798,766	4,769,044	252,567,810	0	
	CTS 1254 Form B Credit	(451,703)		(451,703)	(451,703)		(451,703)	0	
1255: YBM	YERBA BUENA/ MOSCONE STATION [YBM]	156,446,081	6,642,920	163,089,001	156,446,081	6,642,920	163,089,001	0	
	YBM 1255 Form B Credit	(100,000)		(100,000)	(100,000)		(100,000)	0	
1256: STS	SURFACE TRACKWORK & SYSTEMS [STS]	140,294,378	4,694,621	144,988,999	140,294,378	4,694,621	144,988,999	0	
	STS 1256 SFPUC SEWER MAIN CREDIT	(2,925,296)		(2,925,296)	(2,925,296)		(2,925,296)	0	
	STS 1256 Form B Credit	(1,000,000)		(1,000,000)	(1,000,000)		(1,000,000)	0	
OTHER	OTHER CONSTRUCTION TOTAL	46,386,751	1,160,000	47,546,751	46,386,751	1,160,000	47,546,751	0	
40.06	PUBLIC ART PROGRAM	8,175,555	1,160,000	9,335,555	8,175,555	1,160,000	9,335,555	0	
40.02	MISC. CONSTR CONTRCT WK (TRACTION POWER FOR 1251)	258,202		258,202	258,202		258,202	0	
40.01	CONTRACT 1300 SOIL PROCESS	500,000		500,000	500,000		500,000	0	26
50.01	TEMPORARY LICENSE AGREEMENT (ATCS CENTRAL CONTROL)	487,972		487,972	487,972		487,972	0	
50.06	MTA FARE COLLECTION EQUIPMENT	5,400,000		5,400,000	5,400,000		5,400,000	0	
50.06	BART FARE COLLECTION EQUIPMENT	700,000		700,000	700,000		700,000	0	
40.02	JOB ORDER CONTRACTS (JOCS) - CONSTRUCTION	117,255		117,255	117,255		117,255	0	
40.08	AON RISK INSURANCE	18,088,750		18,088,750	18,088,750		18,088,750	0	

7.6 BUDGET REVISIONS: REPORT SORTED BY CONSTRUCTION PACKAGES & SOFT COSTS

Report Period: Oct 2015									
		Sept 2015			Oct 2015				
Group by Contract & SCC	CATEGORY ITEM	Sept 2015 Base	Sept 2015 Allocated Contingency	Sept 2015 Base + Allocated Contingency (YOE)	Oct 2015 Base	Oct 2015 Allocated Contingency	Oct 2015 Base + Allocated Contingency (YOE)	BUDGET TRANSFERS [Oct 2015] vs. [Sept 2015]	Cost Report Notes
40.02 40.08	PUBLIC AGENCIES UTILITY COORDINATION	3,713,215		3,713,215	3,713,215		3,713,215	0	
40.02	DEPARTMENT OF PARKING AND TRAFFIC (DPT)	1,200,000		1,200,000	1,200,000		1,200,000	0	
50.03	UNION SQUARE/ MARKET STREET STATION POWER FEED	2,959,826		2,959,826	2,959,826		2,959,826	0	
50.03	UNION SQUARE/ MARKET STREET STATIONS PERMANENT POWER	(2,350,000)		(2,350,000)	(2,350,000)		(2,350,000)	0	
50.03	CHINATOWN STATION POWER FEED	2,959,826		2,959,826	2,959,826		2,959,826	0	
50.03	CHINATOWN STATION PERMANENT POWER	(2,350,000)		(2,350,000)	(2,350,000)		(2,350,000)	0	
50.03	YERBA BUENA/ MOSCONE STATION [YBM] POWER FEED	3,125,222		3,125,222	3,125,222		3,125,222	0	
50.03	YERBA BUENA/ MOSCONE STATION [YBM] PERMANENT POWER	(2,368,540)		(2,368,540)	(2,368,540)		(2,368,540)	0	
50.03	SURFACE STATION POWER FEED	11,839		11,839	11,839		11,839	0	
50.04	COMMUNICATION CONNECTION COSTS	5,757,629		5,757,629	5,757,629		5,757,629	0	
60	ROW, LAND, EXISTING IMPROVEMENTS	32,246,321	5,265,478	37,511,799	32,246,321	5,265,478	37,511,799	0	
60.01	PURCHASE OR LEASE OF REAL ESTATE	30,065,810	5,265,478	35,331,288	30,065,810	5,265,478	35,331,288	0	27
60.02	RELOCATION OF EXISTING HOUSEHOLDS	2,180,511		2,180,511	2,180,511		2,180,511	0	
70	VEHICLES	13,309,000	13,076,653	26,385,653	13,309,000	13,076,653	26,385,653	0	
70.01	LIGHT RAIL	13,309,000	13,076,653	26,385,653	13,309,000	13,076,653	26,385,653	0	28
70.07	SPARE PARTS								
80	PROFESSIONAL SERVICES	310,518,041	18,221,079	328,739,120	310,518,041	18,221,079	328,739,120	0	
80.01	PRELIMINARY ENGINEERING	46,202,674		46,202,674	46,202,674		46,202,674	0	
80.02 80.06	FINAL DESIGN	61,322,751		61,322,751	61,322,751		61,322,751	0	
80.03	PROJECT MANAGEMENT FOR DESIGN & CONSTRUCTION	89,021,634	13,905,845	102,927,479	89,021,634	13,905,845	102,927,479	0	
80.04	CONSTRUCTION ADMINISTRATION & MANAGEMENT	91,037,791	2,956,812	93,994,603	91,037,791	2,956,812	93,994,603	0	

7.6 BUDGET REVISIONS: REPORT SORTED BY CONSTRUCTION PACKAGES & SOFT COSTS

Report Period: Oct 2015									
		Sept 2015			Oct 2015				
Group by Contract & SCC	CATEGORY ITEM	Sept 2015 Base	Sept 2015 Allocated Contingency	Sept 2015 Base + Allocated Contingency (YOE)	Oct 2015 Base	Oct 2015 Allocated Contingency	Oct 2015 Base + Allocated Contingency (YOE)	BUDGET TRANSFERS [Oct 2015] vs. [Sept 2015]	Cost Report Notes
80.05	INSURANCES	6,800,000		6,800,000	6,800,000		6,800,000	0	
80.06	LEGAL: PERMITS. REVIEW FEES BY OTHER AGENCIES	8,258,184		8,258,184	8,258,184		8,258,184	0	
80.07	SURVEYS, TESTING, INVESTIGATION. INSPECTION	883,100		883,100	883,100		883,100	0	
80.08	START-UP	6,991,907	1,358,422	8,350,329	6,991,907	1,358,422	8,350,329	0	
	ALL SCC CATEGORIES 10 TO 80	1,493,977,603	59,802,940	1,553,780,544	1,493,977,603	59,802,940	1,553,780,544	0	
90	UNALLOCATED CONTINGENCIES			24,519,459			24,519,459		29
	TOTAL PROJECT COST 10 TO 100			1,578,300,002			1,578,300,002		
	TOTAL CONTINGENCY			84,322,399			84,322,399		
	CONTINGENCY MINIMUM			60,000,000			60,000,000		
	BELOW OR ABOVE MINIMUM			24,322,399			24,322,399		

COST STATUS BY CATEGORY	SCC CODES	BUDGET Sept 2015	BUDGET TRANSFERS	BUDGET Oct 2015	Oct 2015 CTD	Oct 2015 EAC
CONSTRUCTION	SCC 010 - 050	1,161,143,972	0	1,161,143,972	609,836,339	1,154,040,008
REAL ESTATE	SCC 060	37,511,799	0	37,511,799	30,467,005	32,246,321
VEHICLES	SCC 070	26,385,653	0	26,385,653	2,147,204	13,309,000
PRELIM ENGINEERING	SCC 080.01	46,202,674	0	46,202,674	46,202,675	46,202,674
FINAL DESIGN	SCC 080.02	61,318,331	0	61,318,331	61,199,308	61,322,751
PM FOR DESIGN & CONSTRUCTION	SCC 080.03 - 080.04	196,922,082	0	196,922,082	101,462,516	180,059,425
OTHER PROF SRVCS	SCC 080.05 - 080.08	24,296,033	0	24,296,033	11,241,923	22,933,191
UNALLOC CONTINGENCY	SCC 090	24,519,456	0	24,519,456		
Grand Total		1,578,300,000	0	1,578,300,000	862,556,970	1,510,113,371

SCC DESCRIPTION	Oct 2015 BUDGET	Oct 2015 CTD
010 - GUIDEWAY & TRACK ELEMENTS	285,468,439	176,564,474
020 - STATIONS, STOPS, TERMINALS, INTERMODAL	577,989,115	246,188,043
040 - SITEWORK & SPECIAL CONDITIONS	202,315,319	172,280,737
050 - SYSTEMS	95,371,099	14,803,084
060 - ROW, LAND, EXISTING IMPROVEMENTS	37,511,799	30,467,005
070 - VEHICLES (number)	26,385,653	2,147,204
080 - PROFESSIONAL SERVICES (applies to Cats. 10-50)	328,739,120	220,106,422
090 - UNALLOCATED CONTINGENCY	24,519,456	
Grand Total	1,578,300,000	862,556,970

SCC DESCRIPTION	Oct 2015 BUDGET	Oct 2015 CTD
010.02-Guideway: At grade semi-exclusive (allows cross-traffic)	2,860,000	142,000
010.06-Guideway: Underground cut & cover	70,802,812	61,657,316
010.07-Guideway: Underground tunnel	200,594,901	110,379,671
010.09-Track: Direct fixation	6,761,089	2,416,871
010.12-Track: Special (switches, turnouts)	4,449,637	1,968,617
020.01-At-grade station, stop, shelter, mall, terminal, platform	6,673,138	1,248,534
020.02-Aerial station, stop, shelter, mall, terminal, platform	4,694,621	0
020.03-Underground station, stop, shelter, mall, terminal, platform	544,919,519	242,159,388
020.07-Elevators, escalators	21,701,837	2,780,121
040.01-Demolition, Clearing, Earthwork	11,144,242	8,598,999
040.02-Site Utilities, Utility Relocation	56,673,634	51,320,401
040.03-Haz. mat'l, contam'd soil removal/mitigation, ground water treatments	4,957,405	2,098,262
040.04-Environmental mitigation, e.g. wetlands, historic/archeologic, parks	1,020,165	465,707
040.05-Site structures including retaining walls, sound walls	2,706,431	2,706,431
040.06-Pedestrian / bike access and accommodation, landscaping	9,755,506	1,944,418
040.07-Automobile, bus, van accessways including roads, parking lots	6,967,874	2,107,265
040.08-Temporary Facilities and other indirect costs during construction	109,090,062	103,039,253
050.01-Train control and signals	28,031,423	4,845,040
050.02-Traffic signals and crossing protection	12,584,529	5,482,015
050.03-Traction power supply: substations	21,487,073	3,748,853
050.04-Traction power distribution: catenary and third rail	12,441,113	722,172
050.05-Communications	12,062,374	5,003
050.06-Fare collection system and equipment	6,100,000	0
050.07-Central Control	2,664,586	1
060.01-Purchase or lease of real estate	35,331,288	28,236,576
060.02-Relocation of existing households and businesses	2,180,511	2,230,430
070.01-Light Rail	26,385,653	2,147,204
080.01-Preliminary Engineering	46,202,674	46,202,675
080.02-Final Design	61,318,331	61,199,308
080.03-Project Management for Design and Construction	102,927,479	58,581,106

SCC DESCRIPTION	Oct 2015 BUDGET	Oct 2015 CTD
080.04-Construction Administration & Management	93,994,603	42,881,411
080.05-Professional Liability and other Non-Construction Insurance	6,800,000	6,340,196
080.06-Legal; Permits; Review Fees by other agencies, cities, etc.	8,262,604	4,569,839
080.07-Surveys, Testing, Investigation, Inspection	883,100	13,831
Grand Total	1,578,300,000	862,556,970

[A] Cost Account Description	BUDGET	ACTUAL COSTS				[G] VARIANCE (B - F)	COST REPORT NOTES
	[B] October 2015 Budget (YOE)	[C] PRIOR MONTH Total	[D] PRIOR MONTH Monthly	[E] CURRENT Monthly	[F] CURRENT Total		
TOTAL PRELIMINARY ENGINEERING	46,542,061	46,542,061	0	0	46,542,061	0	30
11 - SFMTA PROJECT MANAGEMENT	8,828,359	8,253,957	0	0	8,253,957	574,403	31
12 - SFMTA ENGINEERING SERVICES	11,425,594	11,425,594	0	0	11,425,594	0	32
16 - DEPARTMENT OF PARKING AND TRAFFIC (DPT)	935,451	802,883	0	0	802,883	132,568	
21 - ARTS COMMISSION	1,500,570	1,500,570	0	0	1,500,570	1	33
22 - FIRE DEPARTMENT	33,825	33,825	0	0	33,825	0	
23 - CITY ATTORNEY'S OFFICE	1,234,754	1,234,754	0	0	1,234,754	0	
24 - RISK MANAGEMENT	0	0	0	0	0	0	
26 - PLANNING	99,604	99,604	0	0	99,604	0	
27 - DEPARTMENT OF PUBLIC HEALTH (DPH)	4,420	4,420	0	0	4,420	0	
29 - CITY AUDITOR	308,540	315,616	0	0	315,616	(7,076)	
32 - DPW - IDC ENGINEERING (HYDRAULIC)	3,322,887	3,336,432	0	0	3,336,432	(13,545)	
34 - DPW - IDC CONSTRUCTION (CAPITAL)	17,462	17,462	0	0	17,462	0	
36 - DPW - BSM INFRASTRUCTURE (MAPPING)	76,549	76,549	0	0	76,549	0	
39 - DPW - PCS SITE ASSESSMENT & REMEDIATION (SAR)	13,993	13,993	0	0	13,993	0	
51 - 821 HOWARD STREET	1,005,653	1,005,653	0	0	1,005,653	0	
55 - 651 BRANNAN	2,294,910	2,294,910	0	0	2,294,910	0	34
63 - CENTRAL SUBWAY PARTNERSHIP - AECOM-EPC JV CONTRACT 149	26,793,234	26,793,234	0	0	26,793,234	0	35
66 - ANIL VERMA	395,204	395,204	0	0	395,204	0	36
67 - HILL INTERNATIONAL CONTRACT 156	6,716,294	6,716,294	0	0	6,716,294	0	
68 - ARTHUR GALLAGER & CO. CS 164	6,800,000	6,340,196	0	0	6,340,196	459,804	
71 - TUNNEL/UTILITIES - CONTRACT # CONTRACT 155-1	5,469,336	5,469,336	0	0	5,469,336	0	37
72 - STATIONS - CONTRACT # CONTRACT 155-2	26,220,609	26,220,609	0	0	26,220,609	0	38
73 - SYSTEMS/INTEGRATION - CONTRACT 155-3	11,432,312	11,432,312	0	0	11,432,312	(0)	39
331 - BAY AREA RAPID TRANSIT (BART)	146,427	146,427	0	0	146,427	0	
332 - SAN FRANCISCO COUNTY TRANSPORTATION AUTHORITY (SFCTA)	0	0	0	0	0	0	
TOTAL FINAL DESIGN	115,075,987	113,929,833	0	0	113,929,833	1,146,155	
11 - SFMTA PROJECT MANAGEMENT	15,589,933	5,233,240	213,953	313,826	5,547,066	0	
1.3.011.01.080.03 - CM:SFMTA LABOR-PROJECT MANAGEMENT	15,589,933	5,233,240	213,953	313,826	5,547,066		

[A] Cost Account Description	BUDGET	ACTUAL COSTS				[G] VARIANCE (B - F)	COST REPORT NOTES
	[B] October 2015 Budget (YOE)	[C] PRIOR MONTH Total	[D] PRIOR MONTH Monthly	[E] CURRENT Monthly	[F] CURRENT Total		
12 - SFMTA ENGINEERING SERVICES	905,264	1,773,044	55,789	136,669	1,909,713	13,869	
1.3.012.02.080.04 - CM: SFMTA LABOR-ENGINEERING CONTRACT 1252	123,582	57,648	0	0	57,648	65,934	
1.3.012.06.080.04 - CM: SFMTA LABOR-ENGINEERING CONTRACT 1300	1,800,000	1,715,396	55,789	136,669	1,852,065	(52,065)	
13 - SFMTA CONSTRUCTION MANAGEMENT	44,075,375	8,317,948	265,500	395,278	8,713,226	34,343,831	
1.3.013.01.080.04 - CM:SFMTA LABOR-CONSTR. MANAGEM	43,057,057	8,317,948	265,500	395,278	8,713,226	34,343,831	
16 - DEPARTMENT OF PARKING AND TRAFFIC (DPT)	3,588,074	1,393,901	34,803	37,533	1,431,434	2,166,399	
1.3.016.01.080.04 - DPT CONTRACT 1300 SUPPORT UMS	299,600	66,279	1,771	1,967	68,246	231,354	
1.3.016.01.080.04 - DPT CONTRACT 1300 SUPPORT CTS	274,900	49,298	5,865	9,150	58,448	216,452	
1.3.016.01.080.04 - DPT CONTRACT 1300 SUPPORT YBM	238,400	86,971	7,737	7,868	94,839	143,561	
1.3.016.01.080.04 - DPT CONTRACT 1300 SUPPORT STS	876,876	75,961	9,744	18,881	94,843	782,033	
1.3.016.02.040.08 - DPT: FIELD OPS TUNNEL [B84]	0	1,464	0	0	1,464	(1,464)	
1.3.016.02.040.08 - DPT: FIELD OPS TUNNEL [B86]	0	204,261	0	0	204,261	(204,261)	
1.3.016.06.040.02 - DPT:DPT TRAFFIC SHOP CONTRACT 1300	1,200,000	0	0	0	0	1,200,000	
1.3.016.07.080.04 - DPT:SSD DS/CN: 1UTL	38,450	0	0	0	0	38,450	
1.3.016.08.040.08 - DPT:PCOS:2UTL [68A]	400,728	400,728	0	0	400,728	0	
1.3.016.08.040.08 - DPT:SSD CN:2UTL	0	108,020	0	0	108,020	(108,020)	
1.3.016.08.080.04 - DPT:SSD [1326]	259,120	274,886	9,685	(334)	274,552	(15,432)	
1.3.016.08.080.04 - DPT:SSD [13BN]	0	23,302	0	0	23,302	(23,302)	
1.3.016.08.080.04 - DPT:SSD [13CN]	0	963	0	0	963	(963)	
1.3.016.08.080.04 - DPT:SSD [B85]	0	92,008	0	0	92,008	(92,008)	
1.3.016.09.040.08 - PCOS:1300/STS [68CPT544132Z.CPT544132Z]	0	9,759	0	0	9,759	(9,759)	
17 - MOTIVE POWER	2,195	0	0	0	0	2,195	
1.3.017.07.040.02 - PWR:SFMTA-MOTIVE POWER-UTL.REL	2,195	0	0	0	0	2,195	
18 - SFMTA OPERATIONS	400,000	39,586	215	0	39,586	286,883	
1.3.018.04.040.02 - OPS:SUPPORT TO CONTRACT 1300/CTS	100,000	26,469	0	0	26,469	73,531	
1.3.018.06.080.07 - OPS:SUPPORT TO CONTRACT 1300/UMS	300,000	13,117	215	0	13,117	286,883	
19 - OTHER SFMTA	700,000	170,354	10,605	274,567	444,921	255,079	
1.3.019.01.080.07 - OTH.MTA SURVEY AND TESTING	500,000	0	0	0	0	500,000	
1.3.019.07.080.07 - OTH.MTA SFMTA-SURVEY; TSTG [6840]	0	714	0	0	714	(714)	
1.3.019.08.040.08 - OTH.MTA 1251 MATERIALS	150,000	126,149	0	0	126,149	23,851	
1.3.019.08.080.08 - OTH.MTA OPERATION SUPPORT DUR1	50,000	43,491	10,605	274,567	318,058	(268,058)	
21 - ARTS COMMISSION	12,010,885	1,905,256	75,645	299,554	2,204,810	8,072,015	
1.3.021.01.040.06 - ARTS:CTYCO-ARTS COMMISSION CONSTRUCTION COSTS	4,772,555	0	0	0	0	4,772,555	

[A] Cost Account Description	BUDGET	ACTUAL COSTS				[G] VARIANCE (B - F)	COST REPORT NOTES
	[B] October 2015 Budget (YOE)	[C] PRIOR MONTH Total	[D] PRIOR MONTH Monthly	[E] CURRENT Monthly	[F] CURRENT Total		
1.3.021.01.080.03 - ARTS:CTYCO-ARTS COMMISSION [1227]	2,030,147	388,167	0	0	388,167	1,641,980	40
1.3.021.01.080.04 - ARTS:CTYCO-ARTS COMMISSION [PWE335MPFUNA.CPT544]	21,000	11,386	1,383	0	11,386	9,614	
1.3.021.06.080.03 - ARTS:CTYCO-ARTS COMMISSION PM [285MC.132J]	158,970	217,719	16,314	23,674	241,393	(82,423)	
1.3.021.01.080.03 - ARTS:CTYCO-ARTS COMMISSION [PWE335MPFUNA.CPT544]	0	1,265	461	0	1,265	(1,265)	
1.3.021.06.040.06 - ARTS:CTYCO-ARTS COMMISSION [68CPT5441327.CPT5441327]	1,500,000	1,129,180	53,400	264,480	1,393,660	106,340	
1.3.021.06.040.06 - ARTS:CTYCO-ARTS COMMISSION [285MCPFUNA.CPT5441327]	1,903,000	157,540	4,087	11,400	168,940		
1.3.021.01.080.03 - ARTS:CTYCO-ARTS COMMISSION [132J]	465,213	0	0	0	0	465,213	
1.3.021.97.040.06 - ARTS:ARTS COMMISSION ALLOC CO	1,160,000	0			0	1,160,000	
23 - CITY ATTORNEY'S OFFICE	2,171,781	1,555,745	0	0	1,555,745	616,036	
1.3.023.01.080.06 - ATTY:CN LEGAL-CITY ATTORNEY OF	2,171,781	1,555,745	0	0	1,555,745	616,036	
25 - PUBLIC UTILITIES COMMISSION SEWER	(2,925,296)	0	0	0	0	(2,925,296)	
1.3.025.09.040.02 - STS.1256: SITE UTILITIES SFPUC SEWER MAIN	(2,925,296)					(2,925,296)	
26 - PLANNING	137,062	19,493	0	0	19,493	117,569	
1.3.026.01.080.06 - CM:CTYCO-PLANNING DEPARTMENT	137,062	19,493	0	0	19,493	117,569	
28 - PUBLIC UTILITIES COMMISSION WATER	4,242,012	2,190,477	23,936	22,538	2,213,016	2,028,996	
1.3.028.02.040.02 - CM:CTYCO-PUBLIC UTIL COMM. (PUC)	0	4,745	0	0	4,745	(4,745)	
1.3.028.02.040.08 - PUC: FIELD OPERATIONS TUNNEL	398,400	537,814	0	51	537,865	(139,465)	
1.3.028.02.080.04 - PUC:MTA CSP CN1252 [470465]	105,000	91,587	0	0	91,587	13,413	
1.3.028.03.040.02 - PUC:CDD CONTRACT 1300/UMS SUPPORT	606,354	38,297	1,204	152	38,450	567,904	
1.3.028.03.080.04 - PUC:CMB CONTRACT 1300/UMS INSPECTION	230,000	30,363	0	0	30,363	199,637	
1.3.028.04.040.02 - PUC:CDD CONTRACT 1300/CTS SUPPORT	271,755	152,687	245	254	152,941	118,814	
1.3.028.04.080.04 - PUC:CMB CONTRACT 1300/CTS INSPECTION	115,000	6,956	0	0	6,956	108,044	
1.3.028.05.040.02 - PUC:CDD CONTRACT 1300/YBM SUPPORT	450,282	130,398	7,021	10,027	140,425	309,857	
1.3.028.05.080.04 - PUC:CMB CONTRACT 1300/YBM INSPECTION	184,000	4,008	0	0	4,008	179,992	
1.3.028.07.040.02 - PUC:PUC CDD WATER CONNECTION CONTRACT 1250	248,035	291,501	0	0	291,501	(43,466)	
1.3.028.07.080.04 - PUC:PUC CMB INSPECTION CONTRACT 1250	74,468	113,844	0	0	113,844	(39,376)	
1.3.028.08.040.02 - PUC:PUC CDD WATER CONNECTION CONTRACT 1251 [445]	565,389	318,130	0	0	318,130	247,259	
1.3.028.08.080.04 - PUC:PUC CMB INSPECTION CONTRACT 1251	266,252	289,424	0	0	289,424	(23,172)	
1.3.028.09.040.02 - PUC:CMB CONTRACT 1300/STS SUPPORT	520,077	115,987	15,466	12,054	128,041	392,036	
1.3.028.09.080.04 - PUC:CMB CONTRACT 1300/STS INSPECTION	207,000	64,737	0	0	64,737	142,263	
32 - DPW - IDC ENGINEERING (HYDRAULIC)	1,150,459	130,545	4,228	3,452	133,997	855,365	
1.3.032.01.080.04 - CM:DPW:1424J-BUREAU OF ENGINEERING (BOE) [AB12]	60,000	95,398	1,036	0	95,398	(35,398)	
1.3.032.03.080.04 - DPW IDC HYDRAULIC CN1300 UMS SUPPORT	297,938	1,011	0	0	1,011	296,927	

[A] Cost Account Description	BUDGET	ACTUAL COSTS				[G] VARIANCE (B - F)	COST REPORT NOTES
	[B] October 2015 Budget (YOE)	[C] PRIOR MONTH Total	[D] PRIOR MONTH Monthly	[E] CURRENT Monthly	[F] CURRENT Total		
1.3.032.04.080.04 - DPW IDC HYDRAULIC CN1300 CTS SUPPORT	295,639	6,758	1,569	0	6,758	288,882	
1.3.032.05.080.04 - DPW IDC HYDRAULIC CN1300 YBM SUPPORT	301,882	10,344	602	1,582	11,926	289,956	
1.3.032.06.080.04 - DPW: BOE: 1300 DSDC	6,000	0	0	0	0	6,000	
1.3.032.08.080.04 - DPW.HYRDDPW-BOE IDC ENG SVC DC	9,000	0	0	0	0	9,000	
1.3.032.09.080.04 - DPW IDC HYDRAULIC CN1300 STS SUPPOR	180,000	17,034	1,021	1,869	18,903	161,097	
34 - DPW - IDC CONSTRUCTION (CAPITAL)	6,695,348	4,205,246	67,664	100,986	4,306,232	2,389,116	
1.3.034.02.080.04 - DPW:CONSTR:1252 CM [CD12]	730,000	1,112,045	27,486	41,017	1,153,062	(423,062)	
1.3.034.02.080.04 - DPW:CONSTR:1252 CM [13AC12]	206,000	138,397	0	0	138,397	67,603	
1.3.034.01.080.04 - DPW:BCM LABOR [2113]	2,140,142	2,140,142	0	0	2,140,142	0	
1.3.034.06.080.04 - DPW:CONSTR:1300 CM [13CP12]	3,619,206	814,662	40,178	59,968	874,631	2,744,575	
36 - DPW - BSM INFRASTRUCTURE (MAPPING)	465,562	110,294	0	0	110,294	436,268	
1.3.036.01.080.04 - DPW:MPPG:DPW-BUREAU OF ST USE	417,129	32,680	0	0	32,680	384,449	
1.3.036.02.080.04 - DPW:MPPG:1300-DPW-BUREAU OF ST USE	0	33,084	0	0	33,084	(33,084)	
1.3.036.02.080.06 - DPW:MPPG:DPW-BUREAU OF ST USE	90,000	44,530	0	0	44,530	45,470	
1.3.036.06.080.06 - DPW:MPPG:BSM PERMIT REVIEW	39,433	0	0	0	0	39,433	
37 - DPW - PCS MATERIAL TESTING LABORATORY	83,100	0	0	0	0	83,100	
1.3.037.01.080.07 - DPW.MTL.LABDPW-MATERIAL TESTIN	83,100	0	0	0	0	83,100	
39 - DPW - PCS SITE ASSESSMENT & REMEDIATION (SAR)	622,474	935,904	0	0	935,904	(313,430)	
1.3.039.01.080.04 - DPW:SITE ASSESSMENT & REMEDIATION (SAR) [132112]	8,621	506,858	0	0	506,858	(498,237)	
1.3.039.01.080.04 - DPW:SITE ASSESSMENT & REMEDIATION (SAR) [2213]	92,459	92,459	0	0	92,459	0	
1.3.039.01.080.04 - DPW:SITE ASSESSMENT & REMEDIATION (SAR) [2250]	78,400	78,400	0	0	78,400	0	
1.3.039.01.080.04 -DPW:SITE ASSESSMENT & REMEDIATION (SAR) [2257]	151,515	151,515	0	0	151,515	0	
1.3.039.01.080.04 - DPW:SITE ASSESSMENT & REMEDIATION (SAR) [2313]	24,343	24,343	0	0	24,343	0	
1.3.039.01.080.04 - DPW:SITE ASSESSMENT & REMEDIATION	58,757	10,043	0	0	10,043	48,713	
1.3.039.01.080.04 - DPW:SITE ASSESSMENT & REMEDIATION (SAR) [CE13]	31,367	31,367	0	0	31,367	0	
1.3.039.01.080.04 - DPW:SITE ASSESSMENT & REMEDIATION (SAR) [CH13]	100,000	8,621	0	0	8,621	91,379	
1.3.039.01.080.04 - DPW:SITE ASSESSMENT & REMEDIATION (SAR)	17,000	0	0	0	0	17,000	
1.3.039.02.080.04 - DPW: SITE ASSESSMENT & REMEDIATION (SAR) – CN1252	18,632	16,880	0	0	16,880	1,753	
1.3.039.02.080.04 - DPW: SITE ASSESSMENT & REMEDIATION (SAR) – CN1300	41,379	15,417	0	0	15,417	25,962	
46 - MACY'S WEST - SFPUC SEWER WORK	258,202	258,202	0	0	258,202	0	
1.3.046.08.040.02 - MCY.SWRC. CONTRACT: MACY'S-SEW	258,202	258,202	0	0	258,202	0	
51 - 821 HOWARD STREET	4,690,481	460,946	13,650	15,291	476,237	4,214,244	
1.3.051.01.080.03 - ODC.HWRD:ODCs - 821 HOWARD STR	4,625,481	450,309	13,650	14,864	465,173	4,160,308	

[A] Cost Account Description	BUDGET	ACTUAL COSTS				[G] VARIANCE (B - F)	COST REPORT NOTES
	[B] October 2015 Budget (YOE)	[C] PRIOR MONTH Total	[D] PRIOR MONTH Monthly	[E] CURRENT Monthly	[F] CURRENT Total		
1.3.051.02.080.04 - ODC.HWRD:ODCs - TUNNEL CONTRACT 1252	10,000	1,056	0	0	1,056	8,944	
1.3.051.06.080.04 - ODC.HWRD:ODCs - STATION CONTRACT 1300	55,000	9,581	0	427	10,008	44,992	
55 - 651 BRANNAN	0	10,348	0	0	10,348	(10,348)	
1.3.055.01.080.03 - CM:ODCs - 651 BRANNAN STREET	0	10,348	0	0	10,348	(10,348)	41
63 - CENTRAL SUBWAY PARTNERSHIP - AECOM-EPC JV CONTRACT 149	42,373,401	24,212,952	321,585	(82,842)	24,130,110	18,243,291	
1.3.063.01.080.03 - CM:PM:AECOM.CS149 OM-EPC JV CS149-PM	9,507,939	6,062,803	128,634	(456,365)	5,606,438	3,901,501	42
1.3.063.01.080.04 - CM:AECOM.CS149OM-EPC JV CS-149 [3B]	5,218,630	3,536,712	192,951	(684,547)	2,852,165	2,366,465	
1.3.063.01.080.04 - CM:AECOM.CS149OM-EPC JV CS-149 [3E]	7,000,000	7,982,813	0	0	7,982,813	(982,813)	
1.3.063.01.080.04 - CM:AECOM.CS149OM-EPC JV CS-149 [3F]	0	0	0	989,084	989,084	(989,084)	
1.3.063.01.080.04 - CM:AECOM.CS149OM-EPC JV CS-149 [9B]	11,042	11,042	0	0	11,042	(0)	
1.3.063.01.080.04 - CM:AECOM.CS149OM-EPC JV CS-149 [9D]	550,000	515,694	0	0	515,694	34,306	
1.3.063.01.080.04 - CM:AECOM.CS149OM-EPC JV CS-149 [9E]	600,000	523,943	0	0	523,943	76,057	
1.3.063.01.080.04 - CM:AECOM.CS149OM-EPC JV CS-149 [9F]	0	0	0	68,987	68,987	(68,987)	
1.3.063.01.080.04 - FD:CM:EPC JV CS49-PM [123A]	5,579,945	5,579,945	0	0	5,579,945	0	
1.3.063.97.080.03 - AECOM.CS149 ALLOCAT CONTING	13,905,845					13,905,845	
67 - HILL INTERNATIONAL CONTRACT 156	11,000,000	1,334,694	74,324	1,825	1,336,519	9,663,481	
1.3.067.01.080.03 - HILL.CS156:HILL INTL. CS-156 [1336]	600,000	245,399	245,399	138,270	383,669	216,332	
1.3.067.01.080.03 - HILL.CS156:HILL INTL. CS-156 [1337]	9,400,000	962,035	(171,075)	(136,445)	825,590	8,574,410	
1.3.067.01.080.03 - HILL.CS156:HILL INTL. [1330]	1,000,000	127,261	0	0	127,261	872,739	
69 - BAYLAND SOIL PROCESS CONTRACT 175	500,000	95,001	0	0	95,001	404,999	43
1.3.069.06.040.01 - BAYLAND.CS175:BAYLAND SOIL PROCESS [133K]	500,000	95,001	0	0	95,001	404,999	
71 - TUNNEL/UTILITIES - CONTRACT # CONTRACT 155-1	1,358,950	1,886,246	7,539	1,583	1,887,829	(528,879)	
1.3.071.01.080.04 - CM: CS155.1 DESIGN SUPPORT DURING CM [1232]	0	(87,201)	0	0	(87,201)	87,201	44
1.3.071.02.080.04 - CM: CS155.1 DESIGN SUPPORT DURING CM [1332]	1,358,950	1,973,447	7,539	1,583	1,975,030	(616,080)	
72 - STATIONS - CONTRACT # CONTRACT 155-2	8,752,240	3,852,023	587,825	(70,218)	3,781,806	4,970,434	
1.3.072.01.080.04 - CM: CS155.2 DESIGN SUPPORT DURING CM [1233]	50,000	51,351	0	0	51,351	(1,351)	45
1.3.072.01.080.04 - CM: CS155.2 DESIGN SUPPORT DURING CM [1333]	8,702,240	3,800,673	587,825	(70,218)	3,730,455	4,971,785	
73 - SYSTEMS/INTEGRATION - CONTRACT 155-3	4,828,269	1,102,485	81,258	139,463	1,241,948	3,586,321	
1.3.073.01.080.04 - CM: CS155.3 DESIGN SUPPORT DURING CM [1236]	90,000	89,791	0	0	89,791	209	
1.3.073.01.080.04 - CM: CS155.3 DESIGN SUPPORT DURING CM [1334]	4,738,269	1,012,694	81,258	139,463	1,152,157	3,586,112	
81 - UTILITIES RELOCATION #1 (PORTAL & MOS) - CONTRACT 1250	11,968,150	11,968,150	0	0	11,968,150	0	
1.3.081.07.040.01 - UR1.CONTRACT 1250:SITWORK: DEMOLIT	167,458	167,458	0	0	167,458	0	
1.3.081.07.040.02 - UR1.CONTRACT 1250:SITWORK: UTILITI	10,099,341	10,099,341	0	0	10,099,341	0	

[A] Cost Account Description	BUDGET	ACTUAL COSTS				[G] VARIANCE (B - F)	COST REPORT NOTES
	[B] October 2015 Budget (YOE)	[C] PRIOR MONTH Total	[D] PRIOR MONTH Monthly	[E] CURRENT Monthly	[F] CURRENT Total		
1.3.081.07.040.03 - UR1.CONTRACT 1250:SITWORK:HAZMAT	453,321	453,321	0	0	453,321	0	
1.3.081.07.040.08 - UR1.CONTRACT 1250:SITWORK:TEMPORAR	1,248,030	1,248,030	0	0	1,248,030	0	
82 - UTILITIES RELOCATION #2 (UMS) - CONTRACT 1251	20,794,582	20,794,582	0	0	20,794,582	0	46
1.3.082.08.040.01 - UR2.CONTRACT 1251:SITWORK: DEMOLIT	752,240	752,240	0	0	752,240	0	
1.3.082.08.040.02 - UR2.CONTRACT 1251:SITWORK:UTILITI	10,328,044	10,328,044	0	0	10,328,044	0	
1.3.082.08.040.03 - UR2.CONTRACT 1251:SITWORK:HAZMAT	172,712	172,712	0	0	172,712	0	
1.3.082.08.040.05 - UR2.CONTRACT 1251:SITWORK: STRUCTU	2,706,431	2,706,431	0	0	2,706,431	0	
1.3.082.08.040.06 - UR2.CONTRACT 1251:SITWORK:PEDESTRA	319,317	319,317	0	0	319,317	0	
1.3.082.08.040.07 - UR2.CONTRACT 1251:SITWORK:AUTO/BUS	190,362	190,362	0	0	190,362	0	
1.3.082.08.040.08 - UR2.CONTRACT 1251:SITWORK:TEMP FAC	6,325,476	6,325,476	0	0	6,325,476	0	
GUIDEWAY TUNNELS TOTAL	236,068,968	234,616,308	0	0	234,616,308	1,452,660	
83 - GUIDEWAY TUNNELS - CONTRACT # 1252 BASE	233,584,015	232,689,302	0	0	232,689,302	894,713	
1.3.083.02.010.06 - CONTRACT 1252:GUIDEWAY:UNDERGRN'D CUT	60,446,425	61,377,878	0	0	61,377,878	(931,453)	
1.3.083.02.010.07 - CONTRACT 1252:GUIDEWAY:UNDERGROUND	105,423,090	105,097,157	0	0	105,097,157	325,933	
1.3.083.02.020.03 - CONTRACT 1252: STATIONS: UNDERGROUND	21,685,000	21,685,000	0	0	21,685,000	0	
1.3.083.02.040.01 - CONTRACT 1252:SITWORK:DEMO CLEARING	2,440,000	2,440,000	0	0	2,440,000	0	
1.3.083.02.040.02 - CONTRACT 1252:SITWORK:UTILITIES & RE	10,895,000	10,087,676	0	0	10,087,676	807,324	
1.3.083.02.040.03 - CONTRACT 1252:SITWORK:HAZMAT&MITIGAT	200,000	0	0	0	0	200,000	
1.3.083.02.040.04 - CONTRACT 1252:SITWORK:ENVIRON. MITIG	300,000	54,292	0	0	54,292	245,708	
1.3.083.02.040.06 - CONTRACT 1252:SITWORK:PED/BIKE ACCES	50,000	50,000	0	0	50,000	0	
1.3.083.02.040.07 - CONTRACT 1252:SITWORK:AUTO/BUS ACCES	1,345,000	1,345,000	0	0	1,345,000	0	
1.3.083.02.040.08 - CONTRACT 1252:SITWORK:TEMP FACILITIE	30,799,500	30,552,299	0	0	30,552,299	247,201	
83 - GUIDEWAY TUNNELS - CONTRACT # 1252 CMODs	1,421,807	1,927,006	0	0	1,927,006	(505,199)	
1.3.083.83.010.06 - CONTRACT 1252: CONTRACT MOD	81,937	81,937	0	0	81,937	0	
1.3.083.83.010.07 - CONTRACT 1252: CONTRACT MOD	1,180,322	1,280,322	0	0	1,280,322	(100,000)	
1.3.083.83.020.03 - CONTRACT 1252: CONTRACT MOD	965,121	1,077,223	0	0	1,077,223	(112,102)	
1.3.083.83.040.02 - CONTRACT 1252: CONTRACT MOD	1,665,207	1,665,208	0	0	1,665,208	(1)	
1.3.083.83.040.03 - CONTRACT 1252: CONTRACT MOD	401,933	401,933	0	0	401,933	0	
1.3.083.83.040.08 - CONTRACT 1252: CONTRACT MOD	(2,872,713)	(2,579,617)	0	0	(2,579,617)	(293,096)	
1.3.083.93.010.07 - CONTRACT 1252: TUNNEL ALLOC CONTING	1,063,146	0	0	0	0	1,063,146	47
CONTRACT 1300 - STATIONS, TRACKWORK AND SYSTEMS TOTAL	859,676,400	302,388,888	14,071,294	13,853,083	316,241,971	543,434,429	47a
84 - UNION SQUARE/MARKET STREET STATION (UMS) - WORK PACKAGE 12	294,030,590	127,378,542	3,977,798	5,122,960	132,501,502	161,529,088	17
1.3.084.03.020.03 - UMS.1253: UNDERGROUD STATION	253,822,452	110,082,309	3,037,800	4,299,045	114,381,354	139,441,098	

[A] Cost Account Description	BUDGET	ACTUAL COSTS				[G] VARIANCE (B - F)	COST REPORT NOTES
	[B] October 2015 Budget (YOE)	[C] PRIOR MONTH Total	[D] PRIOR MONTH Monthly	[E] CURRENT Monthly	[F] CURRENT Total		
1.3.084.03.020.07 - UMS.1253: ELEVATORS ESCALATOR	9,465,694	944,059	90,001	219,322	1,163,381	8,302,313	
1.3.084.03.040.01 - UMS.1253: DEMOLITION CLEARING	6,071,588	3,340,338	702,500	423,750	3,764,088	2,307,500	
1.3.084.03.040.02 - UMS.1253: SITE UTILITIES UTIL	3,971,620	1,247,601	15,000	18,750	1,266,351	2,705,269	
1.3.084.03.040.03 - UMS.1253: HAZARDOUS MATERIALS	550,000	22,490	22,490	0	22,490	527,510	
1.3.084.03.040.04 - UMS.1253: ENVIRONMENTAL MITIGA	244,500	97,250	0	48,625	145,875	98,625	
1.3.084.03.040.06 - UMS.1253: PEDESTRIAN/BIKE	18,969	12,501	1	0	12,501	6,468	
1.3.084.03.040.07 - UMS.1253: AUTOMOBILE BUS ACCE	1,547,185	22,001	1	5,000	27,001	1,520,184	
1.3.084.03.040.08 - UMS.1253: TEMPORARY FACILITIES	10,398,701	9,432,453	3	8,468	9,440,921	957,780	
1.3.084.03.050.02 - UMS.1253: TRAFFIC SIGNALS AND	4,773,076	2,100,000	110,000	100,000	2,200,000	2,573,076	
1.3.084.03.050.03 - UMS.1253: TRACTION POWER SUPPL	1,815,534	9,001	1	0	9,001	1,806,533	
1.3.084.03.050.04 - UMS.1253: TRACTION POWER DISTR	216,957	66,038	1	0	66,038	150,919	
1.3.084.03.050.05 - UMS.1253: COMMUNICATIONS	1,134,314	2,501	1	0	2,501	1,131,813	
84 - UNION SQUARE/MARKET STREET STATION (UMS) CMODs	90,000	90,000	0	0	90,000	0	
1.3.084.84.020.07 - CMOD:UMS.1253: ELEVATORS, ESCALATORS	90,000	90,000	0	0	90,000	0	
1.3.084.94.020.03 - UMS.1253: AC: ALLOC CONTING	4,910,000	0	0	0	0	4,910,000	48
85 - CHINATOWN STATION (CTS) - WORK PACKAGE 1254	247,567,810	72,378,747	4,766,927	2,342,889	74,721,636	172,846,174	
1.3.085.04.010.07 - CTS.1254: GUIDEWAY: UNDERGROUND TUNNEL	76,417,579	3,076,159	75,001	0	3,076,159	73,341,420	
1.3.085.04.020.03 - CTS.1254: UNDERGROUND STATION	133,001,053	48,662,859	523,728	426,770	49,089,630	83,911,424	
1.3.085.04.020.07 - CTS.1254: ELEVATORS ESCALATOR	6,812,856	955,063	67,563	(67,562)	887,501	5,925,355	
1.3.085.04.040.01 - CTS.1254: DEMOLITION CLEARING	400,000	355,131	55,131	219,869	575,000	(175,000)	
1.3.085.04.040.02 - CTS.1254: SITE UTILITIES UTIL	6,001,718	4,095,451	140,001	0	4,095,451	1,906,267	
1.3.085.04.040.03 - CTS.1254: HAZARDOUS MATERIALS	350,000	0	0	0	0	350,000	
1.3.085.04.040.04 - CTS.1254: ENVIRONMENTAL MITIGA	325,665	206,064	0	(25,665)	180,399	145,266	
1.3.085.04.040.06 - CTS.1254: PEDESTRIAN/BIKE	15,000	0	0	0	0	15,000	
1.3.085.04.040.07 - CTS.1254: AUTOMOBILE BUS ACCE	225,677	14,500	0	0	14,500	211,177	
1.3.085.04.040.08 - CTS.1254: TEMPORARY FACILITIES	16,571,322	14,048,585	3,872,503	1,756,177	15,804,762	766,560	
1.3.085.04.050.02 - CTS.1254: TRAFFIC SIGNALS AND	1,599,593	734,934	33,001	33,300	768,234	831,359	
1.3.085.04.050.03 - CTS.1254: TRACTION POWER SUPPL	4,063,927	227,500	0	0	227,500	3,836,427	
1.3.085.04.050.04 - CTS.1254: TRACTION POWER DISTRIBUTION	124,481	2,500	0	0	2,500	121,981	
1.3.085.04.050.05 - CTS.1254: COMMUNICATIONS	1,658,938	0	0	0	0	1,658,938	
85 - CHINATOWN STATION (CTS) CMODs	230,956	202,700	29,508	0	202,700	28,256	
1.3.085.85.020.03 - CMOD:CTS.1254: UNDERGROUND STATION	75,000	54,488	29,508	0	54,488	20,512	
1.3.085.85.040.01 - CMOD:CTS.1254: POWER POLE	155,956	148,212	0	0	148,212	7,744	

[A] Cost Account Description	BUDGET	ACTUAL COSTS				[G] VARIANCE (B - F)	COST REPORT NOTES
	[B] October 2015 Budget (YOE)	[C] PRIOR MONTH Total	[D] PRIOR MONTH Monthly	[E] CURRENT Monthly	[F] CURRENT Total		
1.3.085.95.020.03 - CTS.1254: AC: ALLOC CONTING	4,769,044	0	0	0	0	4,769,044	49
86 - YERBA BUENA MOSCONE STATION (YBM) - WORK PACKAGE 1255	158,089,000	74,007,915	1,936,664	2,785,859	76,793,774	81,295,226	
1.3.086.05.020.03 - YBM.1255: UNDERGROUND STATION	119,048,929	52,876,045	413,885	2,995,648	55,871,693	63,177,235	
1.3.086.05.020.07 - YBM.1255: ELEVATORS ESCALATOR	5,333,287	606,167	132,297	33,072	639,239	4,694,048	
1.3.086.05.040.01 - YBM.1255: DEMOLITION CLEARING	657,000	657,000	82,000	0	657,000	0	
1.3.086.05.040.02 - YBM.1255: SITE UTILITIES UTIL	6,520,189	5,665,219	891,839	(628,088)	5,037,130	1,483,059	
1.3.086.05.040.03 - YBM.1255: HAZARDOUS MATERIALS	2,629,439	760,460	140,000	287,346	1,047,806	1,581,633	
1.3.086.05.040.04 - YBM.1255: ENVIRONMENTAL MITIGA	100,000	59,476	0	25,665	85,141	14,859	
1.3.086.05.040.06 - YBM.1255: PEDESTRIAN/BIKE	16,665	1	1	0	1	16,664	
1.3.086.05.040.07 - YBM.1255: AUTOMOBILE BUS ACCE	1,542,725	491,401	243,201	0	491,401	1,051,324	
1.3.086.05.040.08 - YBM.1255: TEMPORARY FACILITIES	15,564,753	12,103,311	439	34,917	12,138,228	3,426,525	
1.3.086.05.050.02 - YBM.1255: TRAFFIC SIGNALS AND	1,726,492	730,534	33,001	33,300	763,834	962,658	
1.3.086.05.050.03 - YBM.1255: TRACTION POWER SUPPL	3,708,425	55,800	0	4,000	59,800	3,648,625	
1.3.086.05.050.05 - YBM.1255: COMMUNICATIONS	1,241,096	2,501	1	0	2,501	1,238,595	
86 - YERBA BUENA MOSCONE STATION (YBM) CMODs	(1,642,919)	190,950	190,950	0	190,950	(1,833,869)	
1.3.086.86.040.02 - CMOD:YBM.1255: SITE UTILITIES UTIL	190,950	190,950	190,950	0	190,950	0	
1.3.086.86.040.08 - CMOD:YBM.1255: TEMPORARY FACILITIES	(1,833,869)	0	0	0	0	(1,833,869)	
1.3.086.96.020.03 - YBM.1255: AC: ALLOC CONTING	6,642,920	0	0	0	0	6,642,920	50
87 - SURFACE TRACKWORK AND SYSTEMS -WORK PACKAGE 1256	139,989,000	27,834,656	3,151,506	3,601,374	31,436,030	108,552,970	
1.3.087.09.010.02 - STS.1256: GUIDEWAY: AT-GRADE SEMI-EXCLUSIVE (ALLO	2,860,000	142,000	17,500	0	142,000	2,718,000	
1.3.087.09.010.06 - STS.1256: GUIDEWAY: UNDERGROUND CUT & CVR	10,274,450	197,501	197,501	0	197,501	10,076,949	
1.3.087.09.010.07 - STS.1256: GUIDEWAY: UNDERGROUN	16,510,764	794,683	(154,300)	131,350	926,033	15,584,731	
1.3.087.09.010.09 - STS.1256: TRACK DIRECT FIXATION	6,761,089	1,182,347	104,000	1,234,524	2,416,871	4,344,219	
1.3.087.09.010.12 - STS.1256: TRACK: SPECIAL	4,449,637	847,000	767,000	1,121,617	1,968,617	2,481,020	
1.3.087.09.020.01 - STS.1256: AT-GRADE STATION	6,673,138	1,226,534	105,724	22,000	1,248,534	5,424,604	
1.3.087.09.040.02 - STS.1256: SITE UTILITIES, UTILITY RELOCA	17,509,893	6,391,247	368,689	494,723	6,885,970	10,623,924	
1.3.087.09.040.03 - STS.1256: HAZARDOUS MATERIALS	200,000	0	0	0	0	200,000	
1.3.087.09.040.04 - STS.1256: ENVIRONMENTAL MITIGA	50,000	0	0	0	0	50,000	
1.3.087.09.040.07 - STS.1256: AUTOMOBILE BUS ACCE	2,116,925	39,001	1	0	39,001	2,077,924	
1.3.087.09.040.08 - STS.1256: TEMPORARY FACILITIES	13,850,984	9,993,623	45,850	(45,848)	9,947,776	3,903,208	
1.3.087.09.050.01 - STS.1256: TRAIN CONTROL AND SIGNALS	27,543,451	4,268,932	850,664	526,108	4,795,040	22,748,411	
1.3.087.09.050.02 - STS.1256: TRAFFIC SIGNALS AND	4,485,368	1,633,047	455,075	116,900	1,749,947	2,735,421	
1.3.087.09.050.03 - STS.1256: TRACTION POWER SUPPL	9,911,014	702,640	143,000	0	702,640	9,208,374	
1.3.087.09.050.04 - STS.1256: TRACTION POWER DISTRIBUTION	6,099,675	416,100	250,800	0	416,100	5,683,575	

[A] Cost Account Description	BUDGET	ACTUAL COSTS				[G] VARIANCE (B - F)	COST REPORT NOTES
	[B] October 2015 Budget (YOE)	[C] PRIOR MONTH Total	[D] PRIOR MONTH Monthly	[E] CURRENT Monthly	[F] CURRENT Total		
1.3.087.09.050.05 - STS.1256: COMMUNICATIONS	8,028,025	0	0	0	0	8,028,025	
1.3.087.09.050.07 - STS.1256: CENTRAL CONTROL	2,664,586	1	1	0	1	2,664,585	
87 - SURFACE TRACKWORK AND SYSTEMS (STS) CMODs	305,378	305,378	17,941	0	305,378	0	
1.3.087.89.040.02 - CMOD:STS.1256: SITE UTILITIES, UTILITY RELOCA	305,378	305,378	17,941	0	305,378	0	
1.3.087.99.020.01 - STS.1256: AC: ALLOC CONTING	4,694,621	0	0	0	0	4,694,621	51
141 - CONSTRUCTION ADMINISTRATION	2,956,812	0	0	0	0	2,956,812	
1.3.141.97.080.04 - CONSTR.ADMIN:ALLOC CONTING	2,956,812					2,956,812	
142 - LEGAL/PERMITS	2,014,204	0	0	0	0	2,014,204	
1.3.142.01.080.06 - LGL.PRMTSF:LEGAL; PERMITS	2,014,204	0	0	0	0	2,014,204	
144 - STARTUP	8,300,329	0	0	0	0	8,300,329	
1.3.144.01.080.08 - STRT: STARTUP	6,941,907	0	0	0	0	6,941,907	
1.3.144.97.080.08 - STRTA: AC STARTUP ALLOC CONTIN	1,358,422					1,358,422	
151 - TEMPORARY LICENSE AGREEMENT	17,000	0	0	0	0	17,000	
1.3.151.01.080.06 - TEMP.LICPORARY LICENSE AGREEME	17,000	0	0	0	0	17,000	
170 - COMMUNICATIONS CONNECTIONS	5,757,629	0	0	0	0	5,757,629	
1.3.170.01.050.04 - COMM.CONNN:COMMUNICATION CONN	5,757,629	0	0	0	0	5,757,629	
181 - AON RISK INSURANCE CS 163	18,113,750	18,798,132	0	0	18,798,132	(684,382)	
1.3.181.01.040.08 - AON.CS163 AON RISK INS.	18,088,750	18,773,132	0	0	18,773,132	(684,382)	
1.3.181.01.080.03 - AON.CS171 AON RISK INS. STUDY	25,000	25,000	0	0	25,000	0	
191 - FARE COLLECTION CONTRACTOR	5,400,000	0	0	0	0	5,400,000	
1.3.191.01.050.06 - FARE.CONsul:FARE COLLECTION	5,400,000	0	0	0	0	5,400,000	
192 - THALES T&S CENTRAL CONTROL	487,972	50,000	0	0	50,000	437,972	
1.3.192.01.050.01 - THALES T&S ATCS	487,972	50,000	0	0	50,000	437,972	
202 - JOC2-022.0	63,938	0	0	0	0	63,938	
1.3.202.01.040.02 - JOC2-022:15&22 POTHOLING UTIL1 LGHT FNDS	63,938	0	0	0	0	63,938	
203 - JOC2-029.0	53,317	0	0	0	0	53,317	
1.3.203.07.040.02 - JOC0292-029: RELOCATE VAULTS-S	53,317	0	0	0	0	53,317	
302 - PG&E	1,988,173	2,749,912	0	0	2,749,912	(761,739)	
1.3.302.03.050.03 - PGE PERMANENT POWER UMS	(2,350,000)	0	0	0	0	(2,350,000)	
1.3.302.03.050.03 - PGE POWER FEED UMS	2,959,826	115,690	0	0	115,690	2,844,136	
1.3.302.04.050.03 - PGE PERMANENT POWER CTS	(2,350,000)	0	0	0	0	(2,350,000)	
1.3.302.04.050.03 - PGE POWER FEED CTS	2,959,826	0	0	0	0	2,959,826	
1.3.302.05.050.03 - PGE PERMANENT POWER YBM	(2,368,540)	0	0	0	0	(2,368,540)	

[A] Cost Account Description	BUDGET	ACTUAL COSTS				[G] VARIANCE (B - F)	COST REPORT NOTES
	[B] October 2015 Budget (YOE)	[C] PRIOR MONTH Total	[D] PRIOR MONTH Monthly	[E] CURRENT Monthly	[F] CURRENT Total		
1.3.302.05.050.03 - PGE POWER FEED YBM	3,125,222	2,634,222	0	0	2,634,222	491,000	
1.3.302.09.050.03 - PGE POWER FEED STS	11,839	0	0	0	0	11,839	
331 - BAY AREA RAPID TRANSIT (BART)	951,356	60,455	0	0	60,455	890,901	
1.3.331.01.080.04 - CM:SFMTA LABOR-ENG SVCS-IRP/BART/SF	0	33,152	0	0	33,152	(33,152)	
1.3.331.01.080.06 - CM: BAY AREA RAPID TRANSIT (BART) [122A]	951,356	27,304	0	0	27,304	924,052	
333 - AMERICAN PUBLIC TRANSP. ASSOCIATION (APTA) CS-APTA	146,500	54,612	0	0	54,612	91,888	
1.3.333.01.080.03 - APTA:APTA - IRP [2G]	46,500	27,304	0	0	27,304	19,196	
1.3.333.01.080.03 - APTA:APTA - IRP [2C]	100,000	27,308	0	0	27,308	72,692	
334 - BART FARE COLLECTION SYSTEM	700,000	0	0	0	0	700,000	
1.3.334.01.050.06 - BART:BART FARE COLLECTION EQP	700,000	0	0	0	0	700,000	
401 - ECONOMIC AND WORKFORCE DEVELOPMENT (EWD)	17,600	17,600	0	0	17,600	0	
1.3.401.01.080.04 - EWD: MAYORS OFFICE ECON DEV	17,600	17,600	0	0	17,600	0	
402 - DEPARTMENT OF TECHNOLOGY	242,371	237,534	0	0	237,534	4,837	
1.3.402.07.050.04 - DT:1UTL:COMM. CONNECTIONS	166,756	166,179	0	0	166,179	577	
1.3.402.08.050.04 - DT:2UTL:COMM.CONNECTIONS	75,615	71,354	0	0	71,354	4,261	
404 - DEPARTMENT OF BUILDING INSPECTION (DBI)	1,204,081	1,204,081	0	0	1,204,081	0	
1.3.404.01.080.06 - DPT OF BUILDING INSPECTION	1,204,081	1,204,081	0	0	1,204,081	0	
491 - FORM B - REIMBURSEMENT	(12,227,954)	0	0	0	0	(12,227,954)	
1.3.491.02.040.02 - FORMB - CONTRACT 1252 UTILITY REIMBUR	(254,050)	0			0	(254,050)	52
1.3.491.03.040.02 - FORMB - UMS:CONTRACT 1300 UTILITY REIMBURSEMENT	(528,370)	0			0	(528,370)	53
1.3.491.04.040.02 - FORMB - CTS:CONTRACT 1300 UTILITY REIMBURSEMENT	(451,703)	0			0	(451,703)	54
1.3.491.05.040.02 - FORMB - YBM:CONTRACT 1300 UTILITY REIMBURSEMENT	(100,000)	0			0	(100,000)	55
1.3.491.07.040.02 - FORMB - CONTRACT 1250 UTILITY REIMBUR	(2,275,419)	0			0	(2,275,419)	57
1.3.491.08.040.02 - FORMB - CONTRACT 1251 UTILITY REIMBUR	(7,618,412)	0			0	(7,618,412)	58
1.3.491.09.040.02 - FORMB - STS:CONTRACT 1300 UTILITY REIMBURSEMENT	(1,000,000)	0			0	(1,000,000)	59
TOTAL CONSTRUCTION PHASE	1,328,370,949	654,134,182	15,909,812	15,442,589	669,576,771	646,873,381	
1.4.091.01.070.01 - LRVS: LIGHT RAIL VEHICLES RFP [34B]	1,325,000	1,319,773	0	0	1,319,773	5,227	
1.4.091.01.070.01 - LRVS: LIGHT RAIL VEHICLES PROJECT MGT [68E]	827,132	827,132	0	299	827,431	(299)	
1.4.091.01.070.01 - LRVS: LRV PROCUREMENT ODC	25,000	0	0	0	0	25,000	
1.4.091.01.070.01 - LRVS: LRV PROCUREMENT	11,131,868	0	0	0	0	11,131,868	
1.4.091.97.070.01 - LRVA:AC: VEHICLES ALLOC CONTI	13,076,653					13,076,653	19
TOTAL VEHICLES	26,385,653	2,146,905	0	299	2,147,204	24,238,450	
1.5.015.01.060.01 - RE: EASEMENT ACQUISIT	400,000	322,939	0	0	322,939	77,061	

[A] Cost Account Description	BUDGET	ACTUAL COSTS				[G] VARIANCE (B - F)	COST REPORT NOTES
	[B] October 2015 Budget (YOE)	[C] PRIOR MONTH Total	[D] PRIOR MONTH Monthly	[E] CURRENT Monthly	[F] CURRENT Total		
1.5.015.01.060.01 - RE: REAL EST SITE ACQ	16,523,400	14,307,667	0	0	14,307,667	2,215,733	
1.5.015.01.060.01 - RE: REAL ESTATE	750,000	766,272	0	0	766,272	(16,272)	
1.5.015.01.060.01 - RE: REC & PARK MOU	6,987,624	6,987,624	0	0	6,987,624	0	
1.5.015.01.060.01 - RE:-DEPT OF TRANSPOR	2,686,000	2,686,000	0	0	2,686,000	0	
1.5.015.01.060.01 - RE:-LICENSES FEES	400,000	381,311	0	0	381,311	18,689	
1.5.015.97.060.01 - RE:A:AC: RE: REAL ESTATE ALLO	5,265,478					5,265,478	
1.5.023.01.060.01 - ATTY:REAL ES	2,212,882	2,678,858	0	0	2,678,858	(465,976)	
1.5.101.01.060.02 - RES.RELO: RELOCATION COST	1,275,200	1,289,701	0	0	1,289,701	(14,501)	
1.5.102.01.060.02 - COMM.RELO-RELOC COMMERCIAL	905,311	940,729	356,534	0	940,729	(35,418)	
TOTAL ROW, LAND, EXISTING IMPROVEMENTS	37,405,895	30,361,101	356,534	0	30,361,101	7,044,794	
90 - CONTINGENCY	84,322,397					84,322,397	
1.7.500.91.090.00 - UNALLOCATED CONTINGENCY	24,519,456					24,519,456	60
TOTAL ALLOCATED CONTINGENCY	59,802,941					59,802,941	
TOTAL PROJECT COST	1,578,300,001	847,114,082	16,266,346	15,442,888	862,556,970	703,822,235	

7.1 Program Project Budget	
1	The Central Subway Project (CSP) (SFMTA Capital Program CPT 544) is defined in the FTA-SFMTA October 2012 Full Funding Grant Agreement with a budget of \$1.578 billion.
2	The TBM Retrieval Shaft Relocation (SFMTA Capital Program CPT 690) is one of four capital projects that is related to CSP. These projects are reported for background information as needed outside of the main body of the Project Monthly Progress Report.
3	The Chinatown Plaza (CPT 718) is for Chinatown Station enhancement capital project. The project has funding outside of the Central Subway Project. The construction is carried out in Contract 1300 Contract Modifications.
4	CPT 665 is a Real Estate project to relocation in compliance with California regulations for business relocations but outside of the Central Subway Project as defined by the FTA FFGA.
5	CPT 705 is an SFMTA capital improvement between the Agency and community stakeholders outside of the Central Subway Project.
6	Utility company reimbursements (Form B) result in funds received for work carried out on behalf of utilities concurrent to CSP work to achieve efficiencies.
7	PG&E Power Feed reimbursement funds are the refunds from PG&E when completion of Stations construction and switch to permanent power.
8	BART Elevator funds are reimbursements for work carried out on behalf of BART to install BART Powell Street Station elevator.
9	The Tutor Perini - CAD Files funds are the result of payments by the contractor for project documentation not included in the contract.
10	SFPUC Sewer Main funds are reimbursements for work carried out on behalf of San Francisco PUC (includes 10% construction contingency).
11	SFPUC 24" Water Main funds are reimbursements for work carried out in Contract 1252 Contract Modification #41 on behalf of San Francisco PUC (includes construction management cost).
12	Traffic Effectiveness Project funded Contract Modification #40 for Culvert, Street & Sidewalk Restoration in North Beach are reimbursements for work carried out in Contract 1252 on behalf of SFMTA SSD.
13	SFPUC North Beach 24" Water Main Additional Work funds are reimbursements for work carried out in Contract 1252 Contract Modification #48 on behalf of San Francisco PUC (includes construction management cost).

7.4 Contingency Management Trend Report	
14	In Oct 2014 Report, updated Contract 1250 contract cost to segregate contract amount and contract modification amount. Note that September 2013 Supplemental Authorized Contingency "column f" did not include completed contract.
15	In Oct 2014 Report, updated Contract 1251 contract cost to segregate contract amount and contract modification amount. Note that September 2013 Supplemental Authorized Contingency "column f" did not include completed contract.

Connecting people. Connecting communities.

16	Contract 1252 Original Contract Value "column a" and Original Congency "column f" did not match September 2013 Supplemental due to Supplemental were used the revised value to reflet Contract Modifications #3-#18. Reduced Contract 1252 contingency to reflect CMod #20 for retrieval shaft relocation cost \$5.15M funded by CPT690, CMod #40 for Culvert, Street & Sidewalk Restoration cost \$694,651 funded by Traffic Effectiveness Project, and CMod #41 for install 24" Water Main in North Beach cost \$328,860 funded by SFPUC. In August 2015 report, release \$15M CN1252 Tunnel assigned contingency to program unallocated contingency.
17	BART Elevator scope and SFPUC Sewer Main scope is in Contract 1300; effort will be funded by BART. In January 2015 Report, corrected Station Contract value to match awarded amount.
18	In April 2015 report, real estate budget stated in RAMP Rev5 is \$36.7M, including \$1M contingency. The cost workbook ROW & contingency budget reflects this with \$36,511,799 and \$1,000,000 respectively. Revised costbook ROW budget & contingency to be \$37,511,799. The \$4,265,478 Caltrans lease savings is allocated to ROW allocated contingency.
19	In Dec 2014 Report, redistributed LRV budget to reflect recent firm bid cost per vehicle (\$3,327,250/unit) from vehicle procurement contract award. (SFMTA Board meeting 15JUL14, calendar item #11). Vehicle line item total budget remains unchanged, redistributed fund by reducing base amount to \$13,309,000, column "c" and increased allocated contingency column "h", by same amount.
20	In Oct 2014 Report, made two corrections: i) revised Professional Services, Original Contract Value "column a" from \$310,518,041 to \$310,618,041, ii) revised Original Congency. "column f" unallocated contingency from \$3,883,481 to \$3,845,945. In April 2015 report, used \$500K program contingency for CS-175 Bayland Soil Process contract. In August 2015 report, added \$15M from Contract 1252.
21	The total Central Subway Project budget of \$1.578 billion, based on the October 2012 FFGA with the FTA, is the primary MPR report reference.
22	Estimate at Completion is shown at Column "e".
23	Estimate at Completion vs. Budget variance is shown at Column "k".

7.5 Contract Modification/Trend Log - Contract 1300 Stations

24	Reported all trend cost for Contractor Change Order Requests and Proposed Contract Change and applied probability to forecasted trends.
----	---

7.6 Budget Revisions: Report Sorted by Construction Packages

25	In Dec 2014 Report, reduced CN1252 allocated contingency by \$28K to excute Contract Modification #46, see Report 7.5 Detail Contingency Usage Report. In August 2015 report, release \$15M CN1252 Tunnel assigned contingency to program unallocated contingency.
26	Program contingency increased by \$500,000. Refer to Report Notes #20.
27	Program contingency increased by \$5,265,478. Refer to Report Notes #18.

28	In Dec 2014 Report, redistributed LRV budget to reflect recent firm bid cost per vehicle (\$3,327,250/unit) from vehicle procurement contract award. (SFMTA Board meeting 15JUL14, calendar item #11). Vehicle line item total budget remains unchanged, redistributed fund by reducing base amount to \$13,309,000 and increased allocated contingency by same amount.
29	In April 2015 report, program contingency decreased by \$500,000. Refer to Reports #20. In August 2015 report, release \$15M CN1252 Tunnel assigned contingency to program unallocated contingency.

7.9 Detail Monthly Expenditure Report

Phase 1 Preliminary Engineering	
30	Famis cost for Preliminary Engineering (PE) is \$48,210,903.71. Cost Report for Preliminary Engineering (PE) is \$46,542,060. Some Design cost reported in Famis were moved to Design Phase, refer to Notes #19 and #20.

Phase 2 Design Phase	
31	Famis cost adjustment to transfer Project Management cost from July 2013 to Phase 3 Construction Phase.
32	Famis Phase 1 PE Index Code: 357906.CPT5441112 cost is \$10,222,939 \$8,949,300 is reported in Cost Report Phase 1 PE and the balance of \$1,273,639 is reported in Phase 2 Design.
33	1.2.021.01.080.03 - FD:CTYCO-ARTS COMMISSION [357909ART001.CPT5441227]: FAMIS: \$1,425,167 Cost Report: \$1,425,167 cost is reported in Phase 2 Design, 1.2.021.01.080.03 Cost Transfer: Remaining cost is reported in Phase 3 Construction, 1.3.021.01.080.03 - ARTS:CTYCO-ARTS COMMISSION [357909ART001.CPT5441227]
34	1.2.055.01.080.02 - FD:ODCs - 651 BRANNAN STREET [35CPT5441241.CPT5441241]: FAMIS: \$2,294,910 Cost Report: \$2,294,910 1.2.055.01.080.02 Cost Transfer: Future costs to be allocated to 1.3.055.01.080.02 - FD:ODCs - 651 BRANNAN STREET [35CPT5441241.CPT5441241]
35	1.2.063.01.080.03 - AECOM.CS149 OM-EPC JV CS149-PM [68CPT544133D.CPT544133D]: FAMIS: \$4,698,167 Cost Report: \$4,698,167 on 1.2.063.01.080.03 Cost Transfer: Future costs to 1.3.063.01.080.03 - AECOM.CS149 OM-EPC JV CS149-PM [68CPT544133D.CPT544133D]
36	AVA Cost \$395,204 is reported in Phase 2 Final Design 1.2.066.01.080.03
37	1.2.071.01.080.02 - FD:FINAL DESIGN-DP1 [35CPT5441232.CPT5441232]: FAMIS: \$5,608,147 Cost Report: \$5,469,336 Cost Transfer: \$138,811 to 1.3.071.01.080.04 - FD:FINAL DESIGN-DP1 [35CPT5441232.CPT5441232]

Connecting people. Connecting communities.

38	1.2.072.01.080.02 - FD:FINAL DESIGN-DP2 [35CPT5441233.CPT5441233]: FAMIS: \$26,268,511 COST REPORT: \$26,220,609 COST TRANSFER: \$47,902 to 1.3.072.01.080.04 - FD:FINAL DESIGN-DP2 [35CPT5441233.CPT5441233]
39	1.2.073.01.080.02 - FD:FINAL DESIGN-DP3 [35CPT5441236.CPT5441236]: FAMIS: \$11,502,372 COST REPORT: \$11,432,312 COST TRANSFER: \$70,060 to 1.3.073.01.080.04 - CM: DP3 [35CPT5441236.CPT5441236]

Phase 3 CONSTRUCTION PHASE	
40	1.3.021.01.080.03 - ARTS:CTYCO-ARTS COMMISSION [357909ART001.CPT5441227]: FAMIS: \$1,525,982 Cost Report: \$1,425,167 1.2.021.01.080.03 Cost Transfer: any future costs to 1.3.021.01.080.03
41	1.3.055.01.080.02 - FD:ODCs - 651 BRANNAN STREET [35CPT5441241.CPT5441241]: FAMIS: \$2,294,910 Cost Report: \$2,294,910 1.2.055.01.080.02 - FD:ODCs - 651 BRANNAN STREET [35CPT5441241.CPT5441241] Cost Transfer: Future costs to be allocated to 1.3.055.01.080.02
42	1.3.063.01.080.03 - AECOM.CS149 OM-EPC JV CS149-PM [68CPT544133D.CPT544133D]: FAMIS: \$4,698,167 Cost Report: \$4,698,167 on 1.2.063.01.080.03 Cost Transfer: Future costs to 1.3.063.01.080.03 - AECOM.CS149 OM-EPC JV CS149-PM [68CPT544133D.CPT544133D]
43	Used \$500K program contingency for CS-175 Bayland Soil Process contract. Refer to Report Notes #20.
44	1.3.071.01.080.04 - FD:FINAL DESIGN-DP1 [35CPT5441232.CPT5441232]: FAMIS: \$5,608,147 Cost Report: \$5,469,336 Cost Transfer: \$138,811 to 1.3.071.01.080.04 - FD:FINAL DESIGN-DP1 [35CPT5441232.CPT5441232]
45	1.3.072.01.080.04 - FD:FINAL DESIGN-DP2 [35CPT5441233.CPT5441233]: FAMIS: \$26,268,511 COST REPORT: \$26,220,609 COST TRANSFER: \$47,902 to 1.3.072.01.080.04 - FD:FINAL DESIGN-DP2 [35CPT5441233.CPT5441233]
46	Contract 1251 Final cost is \$20,794,582.
47a	In July 2015 Report, used Contract 1300 Contractor schedule to report budget and actual cost. The Standard Cost Categories (SCC) allocation changed from previous reports. In August 2015 Report, adjusted some of Contract 1300 Contractor SCC assignment to match most of previous SCC assignment.
47	Revised Contract 1252 allocated contingency SCC code from 040.08 to 010.07.

Connecting people. Connecting communities.

48	Revised Contract 1300/UMS allocated contingency SCC code from 040.08 to 020.03.
49	Revised Contract 1300/CTS allocated contingency SCC code from 040.08 to 020.03.
50	Revised Contract 1300/YBM allocated contingency SCC code from 040.08 to 020.03.
51	Revised Contract 1300/STS allocated contingency SCC code from 040.08 to 020.01.
52	Revised Form B Reimbursements SCC code from 900.01 to 040.02
53	Revised Form B Reimbursements SCC code from 900.01 to 040.02
54	Revised Form B Reimbursements SCC code from 900.01 to 040.02
55	Revised Form B Reimbursements SCC code from 900.01 to 040.02
56	Revised Form B Reimbursements SCC code from 900.01 to 040.02
57	Revised Form B Reimbursements SCC code from 900.01 to 040.02
58	Revised Form B Reimbursements SCC code from 900.01 to 040.02
59	Revised Form B Reimbursements SCC code from 900.01 to 040.02
60	Increase Program contingency \$1,023,508. Refer to Report Notes #11 and #12. In April 2015 report, program contingency decreased by \$500,000. Refer to Report Notes #20. In August 2015 report, release \$15M CN1252 Tunnel assigned contingency to program unallocated contingency.

Appendix B

DETAIL SCHEDULE REPORTS

SCHEDULE HIGHLIGHTS

The Master Project Schedule (MPS) below includes progress through October 2015.

The MPS shows a forecast Revenue Service Date of May 13, 2019.

The controlling critical (longest) path of the MPS runs through CTS Excavation succeeded by STS Startup & Testing, Commissioning and Pre-Revenue Activities to the Baseline Finish and Revenue Service Date. See Appendix B – Longest Path. The latest schedule shows the longest path has changed from Union Square Station (UMS) to the Chinatown Station (CTS). The 1300 Contractor has re-sequenced the UMS Platform Station Jet Grout Operation and was able to recover schedule time within the UMS Station. This adjustment in the Critical Path will continue as the 1300 Contractor incorporate their schedule recovery options, resulting in revisions to their construction work plans.

Schedule Contingency is fully utilized on the critical path of the MPS, which is below the Minimum Schedule Contingency level of 6 months. A schedule re-evaluation will be performed, utilizing the updated Contract 1300 Schedule. Recovery options are being implemented in key areas as work proceeds.

The 1300 Contractor submitted eleven (11) Schedule Updates from December 2014 to October 2015. The October 2015 Schedule Update is currently under review. Contractor has been directed to develop a Recovery Schedule as required by Contract and correct out-of-sequence and Retained Logic driving many of the forecast dates. The 1300 Contractor continues to address key critical activities to recover lost time and improve schedule performance. Review of schedule updates as well as identifying recovery options is ongoing.

Contract 1300 - WP1253 UMS / WP1254R CTS / WP1255 YBM / WP1256 STS:

The Contractor, Tutor Perini Corporation’s (TPC) baseline schedule is incorporated into the master program. The preliminary Tutor Perini Corporation’s (TPC) October schedule is used in October Report. Anticipated to use Contractor, Tutor Perini Corporation’s (TPC) approved schedule for November Report that incorporates all SFMTA schedule comments.

Contract 1300 has several interface points with Contract 1252. The timing of these interface points correlate to when the NTP was issued for Contract 1300. This table represents the current status of these interface points with respect to the Contract 1252 update. The 1300 Contractor is not ready to access the tunnel used by Contract 1252. The tunnel access from Contract 1252 is not expected to impact Contract 1300. The current completion date of the Contract 1252 is shown in the table below.

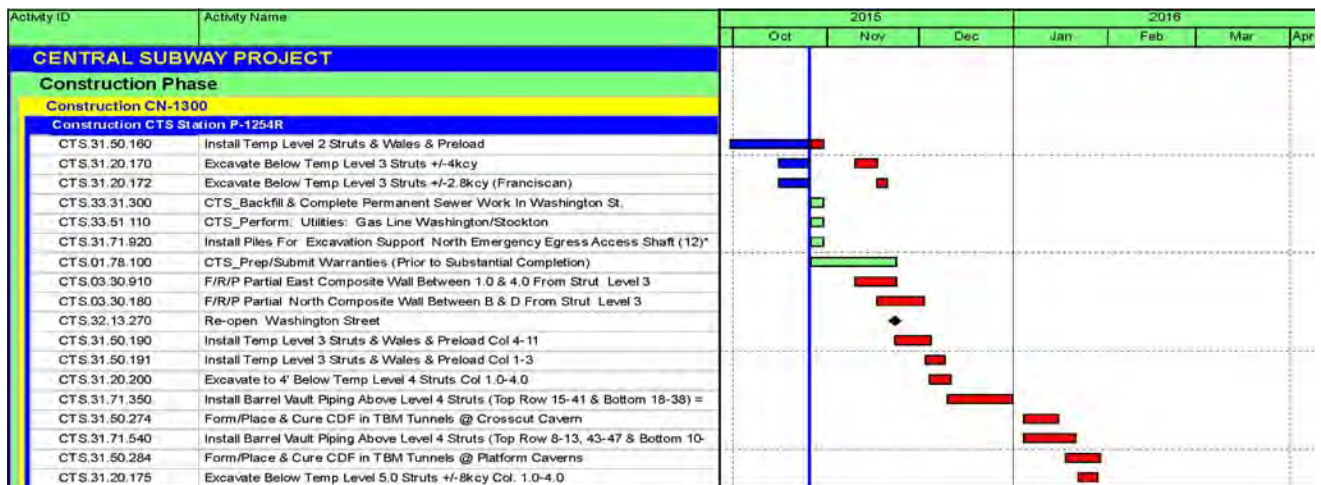
Interface Points	CN1300 Requirement	CN 1252 APR15 Update	Variance	
Tunnel Portal Completion 1252 Tunnel Final Completion	24-Sep-15	15-May-15	132	CD

Work Package P-1254R (CTS) has performed the following work this month:

- Finish dewatering wells on Stockton St
- Finish compensation grout hole from inside headhouse and precondition under buildings
- Finish drilling inclinometers on Stockton Street
- Installing bracing level 2
- Finished relocating ductbank on Stockton Street

Work Package P-1254R (CTS) will perform the following work next month

- Connecting dewatering wells on Stockton Street
- Excavate and install temp level 3 bracing beams
- Form and pour 1st pour of composite wall
- Finish installing level 2 bracing
- Install horizontal inclinometers and MBX's above crosscut crown
- Install weep hole through slurry walls
- Relocate OCS lines west, site set up for North Access Shaft
- Slip line brick sewer on Stockton Street

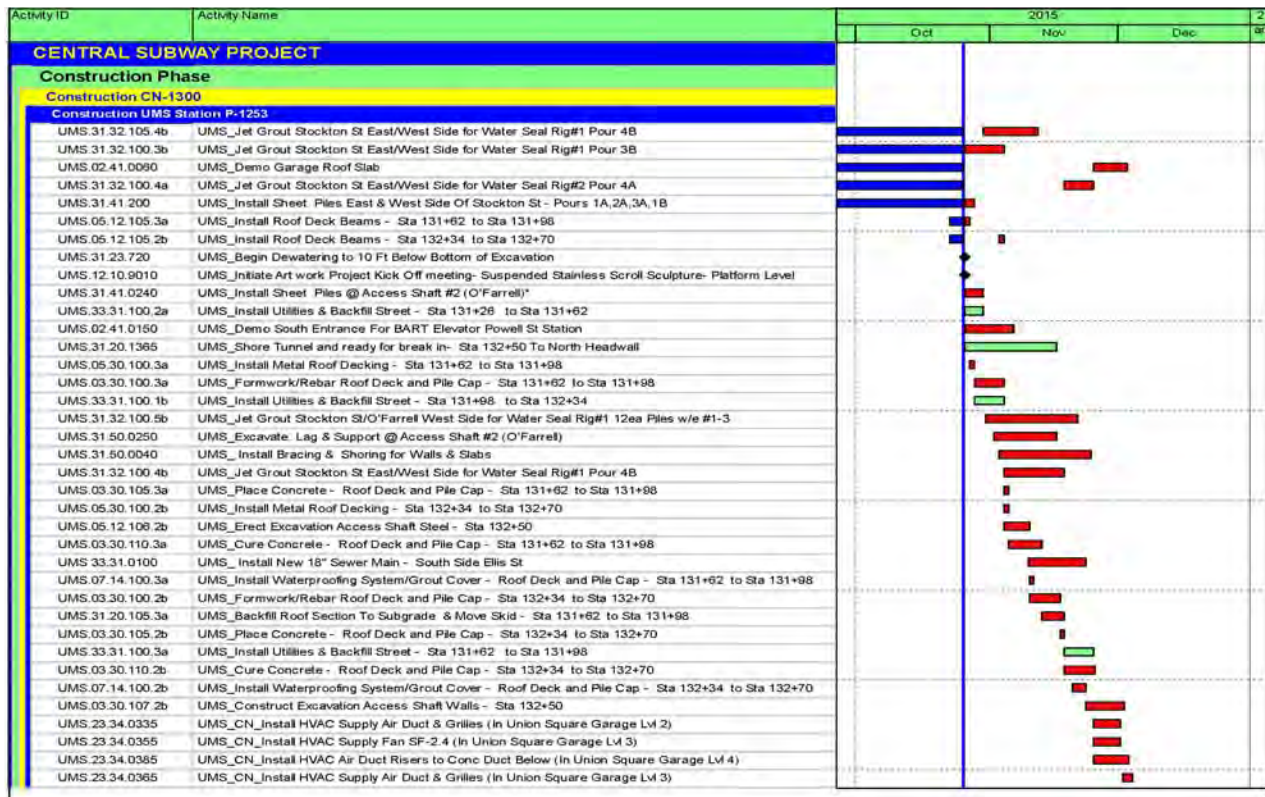


Work Package P-1253 (UMS) has performed the following work this month:

- Deck and rebar installation at Ellis Annex and in the North Concourse
- Discontinued grouting on the east and west sides of Stockton Street

Work Package P-1253 (UMS) will perform the following work next month:

- Street closures: Stockton Street between Ellis and Post and Ellis halfway, Stockton to Powell. Alternate lane configuration on O'Farrell at Stockton. Narrowing of Geary to 1 lane at Stockton.
- Total closure expected for several days, at night hours, for Geary/Stockton Intersection.
- Union Square Garage – demolition and abatement. Concrete installation.
- North Concourse - install decking, concrete for roof deck and reroute of ductbanks and waterline
- Production and test jet grouting on the east and west sides of Stockton Street discontinued at month end until after moratorium
- Concrete installation at Ellis Street Annex
- Geary Street portion of North concourse sewer notch and roof installation should be complete
- Street restoration and barricade removal to begin at end of November, in preparation for Winter Wonderland holiday moratorium

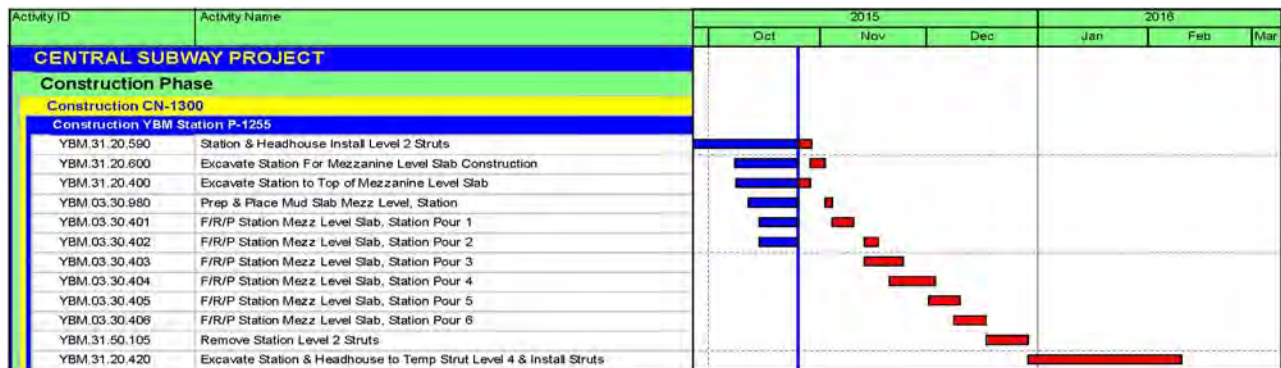


Work Package P-1255 (YBM) has performed the following work this month:

- Traffic flowing on east side of 4th Street with west side work area closed for construction
- Backfill over portion of headhouse beneath Clementina Street is completed
- Headhouse and station box temporary bracing strut level 2 (above Mezzanine Level) installation is completed with excavation in progress
- Placement of mud slab and reinforcing steel for Mezzanine Level concrete pour 1 of 3 in station box is in progress
- Archaeological monitoring by spot checking is in progress

Work Package P-1255 (YBM) will perform the following work next month:

- Continue utility installation on Folsom Street
- Continue excavation in headhouse and station box
- Continue placement of Mezzanine Level slab within station box, pours 2 and 3 of 3 (beneath 4th Street)



Work Package P-1256 (STS) has performed the following work this month:

- Continue Muni ductbank installation
- Continue 78" sewer rehabilitation
- Continue 36" sewer force main
- Continue Auxiliary Water Supply System (AWSS) installation
- Continue water line work
- Continue AT&T crossing work
- Continue tunnel prep work
- Start 4th/King sewer structure modification

Work Package P-1256 (STS) will perform the following work next month:

- Continue 78" sewer rehabilitation
- Continue MRY ductbank and vault installation
- Continue 36" sewer force main
- Continue AWSS installation
- Continue waterline installation
- Continue 4th/King sewer structure modification
- Continue 4th/King Special Trackwork (Shutdown #2)
- Continue tunnel prep work
- Start tunnel drainage system installation
- Start tunnel invert slab work



SCHEDULE REVISIONS

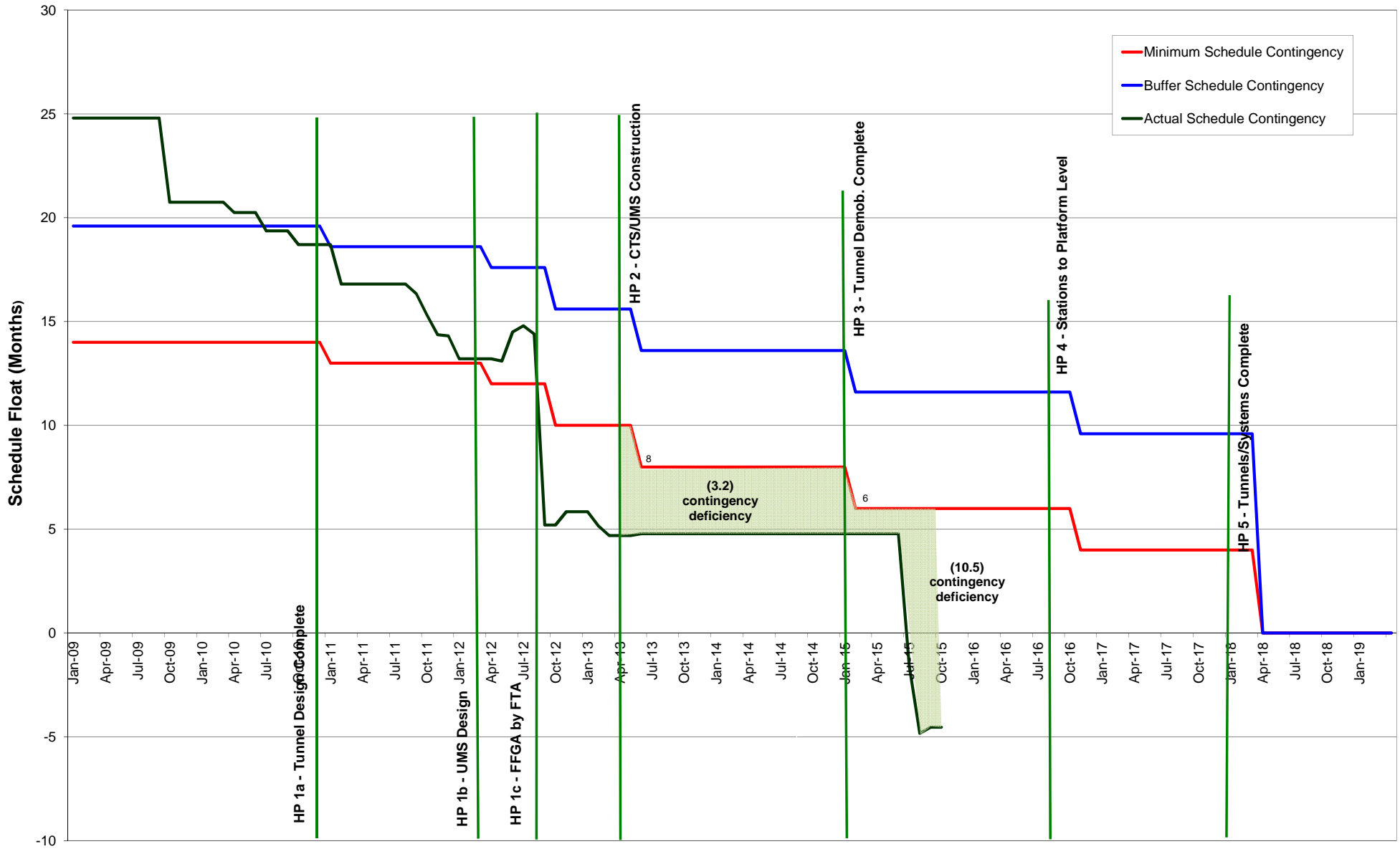
The 1300 Contract October 2015 Schedule Update was added to the Central Subway Project Master Schedule.

LIST OF SCHEDULE REPORTS

- 1.1. Schedule Contingency Drawdown
- 1.2. Master Summary Schedule
- 1.3. Program Critical Path Schedule
- 1.4. Construction Contract Summary Schedule
- 1.5. Detail Schedule for Remaining Work

CENTRAL SUBWAY PROJECT Schedule Contingency Drawdown

October 2015



Activity ID	Activity Name	Original Duration	Start	Finish	2016				2017				2018				2019				2020		
					Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3		
CENTRAL SUBWAY PROJECT					CENTRAL SUBWAY PROJECT																		
Program Level Milestones					Program Level Milestones																		
PJD1000	Central Subway Project Start	0	03-Jun-03 A																				
MS0004A	Tunnel Excavation Complete - Project Milestone #4A	0		05-Sep-14 A	Tunnel Excavation Complete - Project Milestone #4A																		
MS0019	Baseline Finish Date: 12-26-2018	0		13-May-19*	◆ Baseline Finish Date: 12-26-2018																		
MS0009	CSP Revenue Service Date	0		13-May-19*	◆ CSP Revenue Service Date																		
Preliminary Engineering Phase																							
Final Design																							
Light Rail Vehicles					Light Rail Vehicles																		
Real Estate					Real Estate																		
Construction Phase					Construction Phase																		
Construction Support and Costs					Construction Support and Costs																		
Construction Utility Contract #1- MOS & Portal CN-1250																							
Construction Utility Contract #2 - UMS CN-1251																							
Construction Tunnels CN-1252					Construction Tunnels CN-1252																		
Construction CN-1300					Construction CN-1300																		
CN- 1300 Milestone					CN- 1300 Milestone																		
Construction UMS Station P-1253					Construction UMS Station P-1253																		
Construction CTS Station P-1254R					Construction CTS Station P-1254R																		
Construction YBM Station P-1255					Construction YBM Station P-1255																		
Construction STS P-1256					Construction STS P-1256																		
Project Start Up					Project Start Up																		
Unallocated Contingency					Unallocated Contingency																		
CO1.700	Cost Activity Unallocated Contingency (LOE) - 1.7.500.99.090.00 - Contingency	115	26-Nov-18	10-May-19	Cost Activity Unallocated Contingency (LOE) - 1.7.500.99.090.00 - Contingency																		

Activity ID	Activity Name	Original Duration	Start	Finish	Total Float	2016		2017				2018				2019				2020				2021									
						Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2				
CENTRAL SUBWAY PROJECT						1319	05-Jun-14 A	13-May-19	-94																								
Program Level Milestones						115	26-Nov-18	13-May-19	-94																								
HidPt5	Systems Complete	0		26-Nov-18	-94	◆ Systems Complete																											
MS0019	Baseline Finish Date: 12-26-2018	0		13-May-19*	-94	◆ Baseline Finish Date: 12-26-2018																											
MS0009	CSP Revenue Service Date	0		13-May-19*	-94	◆ CSP Revenue Service Date																											
Construction Phase						1319	05-Jun-14 A	13-May-19	-94																								
Construction CN-1300						1263	05-Jun-14 A	21-Feb-19	-153																								
CN- 1300 Milestone						59	23-Nov-18	21-Feb-19	-153																								
STS1500	CN 1300 Substantial Completion	0		23-Nov-18	-135	◆ CN 1300 Substantial Completion																											
MS-10	Substantial Completion - 1,700 Calendar Days (SP-4.B)	0		23-Nov-18*	-286	◆ Substantial Completion - 1,700 Calendar Days (SP-4)																											
C.Punch	Closeout Punchlist/Remaining Work	90	24-Nov-18	21-Feb-19	-286	■ Closeout Punchlist/Remaining Work																											
BUF1017	STS Buffer Float- (0)	0	26-Nov-18	26-Nov-18	-94	STS Buffer Float- (0)																											
MS-20	Final Completion - 1,790 Calendar Days (SP-4C)	0		21-Feb-19*	-286	◆ Final Completion - 1,790 Calendar Days (SP-4)																											
Construction CTS Station P-1254R						1160	05-Jun-14 A	24-Jul-18	-209																								
CTS 03 05 15	CTS_Review\Approve: Concrete Mix Designs - Shotcrete	15	05-Jun-14 A	16-Nov-15	-205	■ CTS_Review\Approve: Concrete Mix Designs - Shotcrete																											
CTS 03 37 13	CTS_Review\Approve: Shotcrete Plans/Locations/Details	15	09-Jul-14 A	16-Nov-15	-205	■ CTS_Review\Approve: Shotcrete Plans/Locations/Details																											
CTS.03.30.180	F/R/P Partial North Composite Wall Between B & D From Strut Level 3	10	17-Nov-15	02-Dec-15	-205	■ F/R/P Partial North Composite Wall Between B & D From Strut Level 3																											
CTS.31.50.191	Install Temp Level 3 Struts & Wales & Preload Col 1-3	5	03-Dec-15	09-Dec-15	-205	■ Install Temp Level 3 Struts & Wales & Preload Col 1-3																											
CTS.31.71.350	Install Barrel Vault Piping Above Level 4 Struts (Top Row 15-41 & Bottom 18-38)	15	10-Dec-15	31-Dec-15	-205	■ Install Barrel Vault Piping Above Level 4 Struts (Top Row 15-41 & Bottom 18-38) = 25ea																											
CTS.31.71.540	Install Barrel Vault Piping Above Level 4 Struts (Top Row 8-13, 43-47 & Bottom	14	04-Jan-16	21-Jan-16	-205	■ Install Barrel Vault Piping Above Level 4 Struts (Top Row 8-13, 43-47 & Bottom 10-16, 40-48) = 18ea																											
CTS.31.20.175	Excavate Below Temp Level 5.0 Struts +/-8kcy Col. 1.0-4.0	5	22-Jan-16	28-Jan-16	-205	■ Excavate Below Temp Level 5.0 Struts +/-8kcy Col. 1.0-4.0																											
CTS.31.20.390	Excavate Below Temp Level 4.0 Struts +/-8kcy Col. 4.0-11.0	12	29-Jan-16	15-Feb-16	-205	■ Excavate Below Temp Level 4.0 Struts +/-8kcy Col. 4.0-11.0																											
CTS.31.50.900	CTS_CN Install Temp Level 4.0 Struts & Wales & Preload Col. 4.0-11.0	8	10-Feb-16	19-Feb-16	-205	■ CTS_CN Install Temp Level 4.0 Struts & Wales & Preload Col. 4.0-11.0																											
CTS.31.20.400	ExcavateTo Temp Level 5.0 Struts +/-8kcy Col. 4.0-11.0	11	22-Feb-16	07-Mar-16	-205	■ ExcavateTo Temp Level 5.0 Struts +/-8kcy Col. 4.0-11.0																											
CTS.31.71.020	Break-In & Construct Top Sidewall & Headwall Left - Crosscut Cavern	20	22-Feb-16	18-Mar-16	-205	■ Break-In & Construct Top Sidewall & Headwall Left - Crosscut Cavern																											
CTS.31.71.030	Break-In & Construct Top Sidewall & Headwall Right - Crosscut Cavern	20	24-Feb-16	22-Mar-16	-205	■ Break-In & Construct Top Sidewall & Headwall Right - Crosscut Cavern																											
CTS.31.71.040	Break-In & Construct Top Center Drift & Headwall - Crosscut Cavern	9	23-Mar-16	04-Apr-16	-205	■ Break-In & Construct Top Center Drift & Headwall - Crosscut Cavern																											
CTS.31.71.050	Excavate & Construct Sidewall Bench & Headwall Step 3 Left Heading - Crossc	10	05-Apr-16	18-Apr-16	-205	■ Excavate & Construct Sidewall Bench & Headwall Step 3 Left Heading - Crosscut Cavern																											
CTS.31.71.060	Excavate & Construct Sidewall Bench & Headwall Step 3 Right Heading - Cros	10	07-Apr-16	20-Apr-16	-205	■ Excavate & Construct Sidewall Bench & Headwall Step 3 Right Heading - Crosscut Cavern																											
CTS.31.71.070	Excavate & Construct Step 4 Left Heading Invert & Headwall - Crosscut Cavern	10	21-Apr-16	04-May-16	-205	■ Excavate & Construct Step 4 Left Heading Invert & Headwall - Crosscut Cavern																											
CTS.31.71.075	Excavate & Construct Step 4 Right Heading Invert & Headwall - Crosscut Cavern	10	25-Apr-16	06-May-16	-205	■ Excavate & Construct Step 4 Right Heading Invert & Headwall - Crosscut Cavern																											
CTS.31.71.080	Excavate Step 5 Bench 1 & Construct Headwall - Crosscut Cavern	5	09-May-16	13-May-16	-205	■ Excavate Step 5 Bench 1 & Construct Headwall - Crosscut Cavern																											
CTS.31.71.090	Excavate Step 6 Bench 2 & Construct Headwall - Crosscut Cavern	5	16-May-16	20-May-16	-205	■ Excavate Step 6 Bench 2 & Construct Headwall - Crosscut Cavern																											
CTS.31.71.100	Excavate & Support Step 7 Invert - Crosscut Cavern	10	23-May-16	06-Jun-16	-205	■ Excavate & Support Step 7 Invert - Crosscut Cavern																											
CTS.31.71.390	Breakout Remaining Cross-Cut Cavern Opening	5	07-Jun-16	13-Jun-16	-205	■ Breakout Remaining Cross-Cut Cavern Opening																											
CTS.31.71.400	Temporary Backfill Cross Cut Invert for Platform Cavern Excavation	5	14-Jun-16	20-Jun-16	-205	■ Temporary Backfill Cross Cut Invert for Platform Cavern Excavation																											
CTS.31.71.440	Barrel Vaults at South Platform Cavern Excavation	10	21-Jun-16	05-Jul-16	-205	■ Barrel Vaults at South Platform Cavern Excavation																											
CTS.31.71.450	Breakin Top Benches for South Platform Cavern Excavation	13	06-Jul-16	22-Jul-16	-205	■ Breakin Top Benches for South Platform Cavern Excavation																											
CTS.31.71.460	Excavate & Support Top Right Heading South Platform Cavern 176Lf	20	25-Jul-16	19-Aug-16	-205	■ Excavate & Support Top Right Heading South Platform Cavern 176Lf																											
CTS.31.71.465	Excavate & Support Top Left Heading South Platform Cavern 176Lf	20	27-Jul-16	23-Aug-16	-205	■ Excavate & Support Top Left Heading South Platform Cavern 176Lf																											
CTS.31.71.550	Excavate & Support Top Right Bench South Platform Cavern 176Lf	10	24-Aug-16	07-Sep-16	-205	■ Excavate & Support Top Right Bench South Platform Cavern 176Lf																											
CTS.31.71.560	Excavate & Support Top Left Bench South Platform Cavern 176Lf	10	26-Aug-16	09-Sep-16	-205	■ Excavate & Support Top Left Bench South Platform Cavern 176Lf																											
CTS.31.71.570	Excavate & Support Top Right Step 3 Invert South Platform Cavern 176Lf	14	12-Sep-16	29-Sep-16	-205	■ Excavate & Support Top Right Step 3 Invert South Platform Cavern 176Lf																											
CTS.31.71.580	Excavate & Support Top Left Step 3 Invert South Platform Cavern 176Lf	14	14-Sep-16	03-Oct-16	-205	■ Excavate & Support Top Left Step 3 Invert South Platform Cavern 176Lf																											
CTS.31.71.590	CTS- Install Temporary Bracing - Sidewalls (Platform Cavern)	6	04-Oct-16	11-Oct-16	-205	■ CTS- Install Temporary Bracing - Sidewalls (Platform Cavern)																											
CTS.31.71.600	Excavate & Support Top Center Drift Step 4 South Platform Cavern 176Lf	18	12-Oct-16	04-Nov-16	-205	■ Excavate & Support Top Center Drift Step 4 South Platform Cavern 176Lf																											
CTS.31.71.610	Excavate & Support Center Bench Step 5 South Platform Cavern 176Lf	11	07-Nov-16	22-Nov-16	-205	■ Excavate & Support Center Bench Step 5 South Platform Cavern 176Lf																											
CTS.31.71.620	Excavate & Construct Invert Step 6 South Platform Cavern 176Lf	10	23-Nov-16	08-Dec-16	-205	■ Excavate & Construct Invert Step 6 South Platform Cavern 176Lf																											

Activity Name	Original Duration	Start	Finish	2012				2013				2014				2015				2016				2017				2018				2019			
				Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4
CENTRAL SUBWAY PROJECT	2022	08-Jun-11 A	21-Feb-19																																
Construction Phase	2022	08-Jun-11 A	21-Feb-19																																
Construction Tunnels CN-1252	1799	08-Jun-11 A	15-May-15 A																																
1252 Tunnel Contract BIH	1799	08-Jun-11 A	15-May-15 A																																
Contract Milestones	1437	08-Jun-11 A	15-May-15 A																																
General Conditions	1752	01-Aug-11 A	15-May-15 A																																
4th & Bryant St TBM Launch Box Construction	686	30-Mar-12 A	02-Jun-14 A																																
Moscone Station Headwalls	430	14-May-12 A	20-Sep-13 A																																
UMS Station Headwalls	425	24-Jul-12 A	22-Nov-13 A																																
UMS - Remove Geary to Ellis OCS	5	24-Jul-12 A	26-Jul-12 A																																
UMS - Setup Traffic Control for Headwall Construction	1	30-Jul-12 A	30-Jul-12 A																																
North Headwall	237	27-Feb-13 A	22-Nov-13 A																																
South Headwall	404	31-Jul-12 A	22-Nov-13 A																																
4th St and Market Compensation Grouting	707	28-Jan-13 A	30-Apr-15 A																																
Southbound Tunneling	451	27-Apr-13 A	13-Oct-14 A																																
Ellis St Compensation Grouting	561	31-Jul-12 A	09-May-14 A																																
Green St Compensation Grouting	320	05-Aug-13 A	30-Jun-14 A																																
Retrieval Shaft	1070	31-Oct-11 A	20-Mar-15 A																																
Cross Passage 1-5	339	22-Mar-14 A	16-Apr-15 A																																
Cross Passage 1	79	14-Jun-14 A	13-Sep-14 A																																
Cross Passage 2	105	10-May-14 A	09-Sep-14 A																																
Cross Passage 3	127	31-Mar-14 A	28-Aug-14 A																																
Cross Passage 4	114	22-Mar-14 A	31-Jul-14 A																																
Cross Passage 5	277	31-May-14 A	16-Apr-15 A																																
Portal Structure	196	02-Sep-14 A	15-Apr-15 A																																
Contract Close Out	307	03-Mar-14 A	15-May-15 A																																
Construction CN-1300	1512	03-Jun-13 A	21-Feb-19																																
CN- 1300 Milestone	1427	17-Jun-13 A	21-Feb-19																																
Construction UMS Station P-1253	1388	17-Jun-13 A	24-Nov-18																																
Construction CTS Station P-1254R	1368	17-Jun-13 A	24-Nov-18																																
Construction YBM Station P-1255	1512	10-Jun-13 A	26-Nov-18																																
Construction STS P-1256	1440	03-Jun-13 A	06-Feb-19																																

Appendix C

PROJECT SCOPE AND FUNDING OVERVIEW

Project Overview

The Central Subway Project will construct a modern, efficient light-rail line that will improve public transit in San Francisco. This new 1.7-mile extension of Muni's T Third Line will provide direct connections to major retail, sporting and cultural venues while efficiently transporting people to jobs, educational opportunities and other amenities throughout the city.

The Central Subway Project is Phase 2 of the San Francisco Municipal Transportation Agency's (SFMTA) Third Street Light Rail Transit Project. Phase 1 of the project constructed a 5.1-mile light-rail line along the densely populated 3rd Street corridor. It began revenue service in April 2007, restoring light-rail service to a high transit-ridership area of San Francisco for the first time in 50 years.

The Central Subway Project will extend the T Third Line from the 4th Street Caltrain Station to Chinatown, providing a direct, rapid transit link from the Bayshore and Mission Bay areas to SoMa, Union Square and downtown.

Four new stations will be built along the 1.7-mile project alignment—an above-ground station at 4th and Brannan streets and three underground stations at Moscone Center, Union Square and Chinatown.

The Central Subway will run through the burgeoning technology and digital-media hub in SoMa, where dozens of companies have taken up residence along the 4th Street corridor. Increased



Project Overview - continued

transit options will attract new employers – the Central Subway makes travel more convenient throughout the corridor and improves connections to downtown, local and regional rail and the Muni bus system.

The Central Subway Project will contribute to San Francisco’s economic competitiveness and help secure the city’s status of a regional, national and global hub. It will provide a pollution-free transit option that will reduce the environmental impact of transportation in the city, save natural resources, reduce traffic congestion and improve public transit for thousands of San Franciscans.

Funding Overview

The Central Subway Project is funded by the federal government, the State of California, the Metropolitan Transportation Commission, the San Francisco County Transportation Authority (SFCTA) and the City and County of San Francisco.

The majority of funding for the Central Subway Project is expected to be provided by the Federal Transit Administration’s (FTA) New Starts program, with a total commitment over the life of the project of \$942.2 million. To date, \$41 million in Department of Transportation Congestion Mitigation and Air Quality Improvement Program funds have been committed and expended.

With the addition in the December 2013 MPR of work to relocate the retrieval site for two tunnel boring machines (TBMs), the SFMTA’s baseline budget for the Central Subway Project is \$1.588 billion. In total, about half of the Third Street Light Rail Transit Project’s funding is from federal sources, with the remaining half from state and local sources. This is in line with the expectations of the FTA for New Starts-financed programs.

The table below summarizes the local, state and federal fund sources for both phases of the T Third Line including with the addition of the retrieval shaft to the Phase 2 totals.

	T Third (Phase 1)	Central Subway (Phase 2 + Retrieval Shaft Relocation)	Total (Phase 1 + Phase 2 + Retrieval Shaft Relocation)	Percentage of Total
Federal	\$123.380	\$983.225	\$1,106.605	49.5%
State	\$160.700	\$471.100	\$631.800	28.2%
Local	\$364.380	\$133.675	\$498.055	22.3%
Total	\$648.460	\$1,588.000	\$2,236.460	100.0%

All amounts in millions of dollars

The six charts that follow summarize use of fund sources by phase and with the addition of the retrieval shaft relocation additional budget and funding:

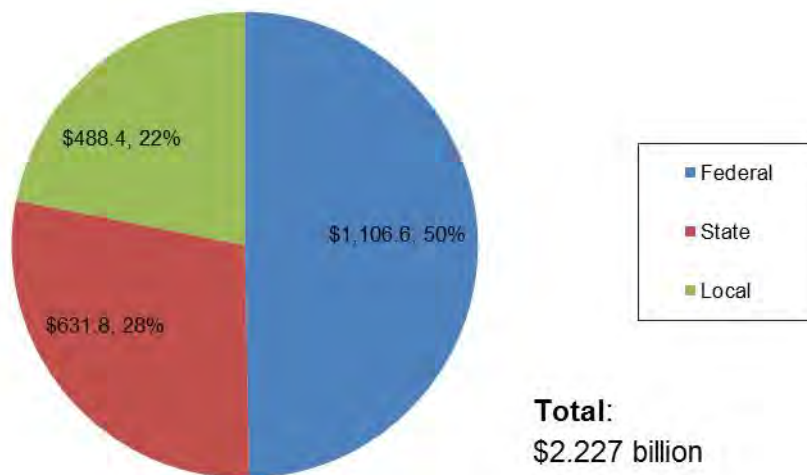
- Phase 1 + Phase 2 of the T Third Line federal, state and local funding percentages previous to the addition of the retrieval shaft relocation budget and funding in December 2013.

Funding Overview - continued

- Phase 2 Central Subway Project only total funding source percentages previous to the addition of the retrieval shaft relocation budget and funding.
- Phase 2 Central Subway Project only detail of the six State and Local funding sources previous to the addition of the retrieval shaft relocation.
- The next three charts that follow are the above three data sets above with the retrieval shaft relocation budget and funding added to the overall presentation.

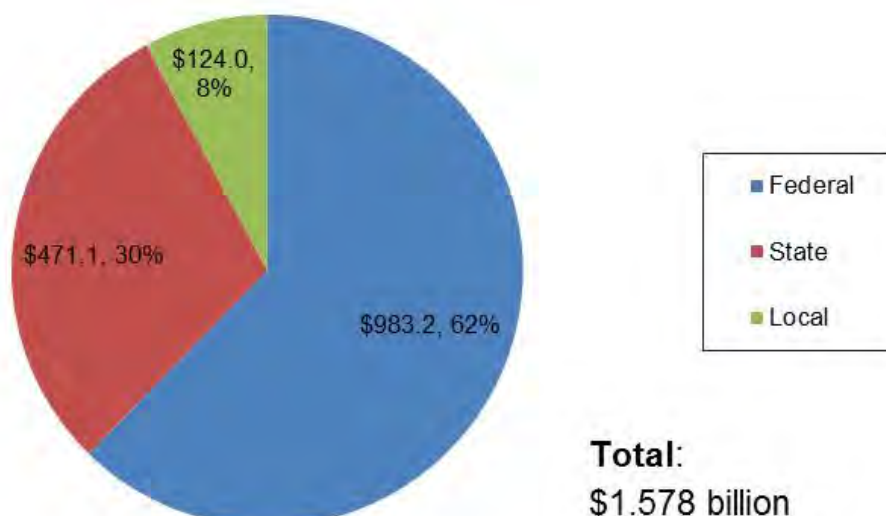
Third Street Light Rail Transit Project Funding

Phase 1 + Phase 2
(\$ in millions)



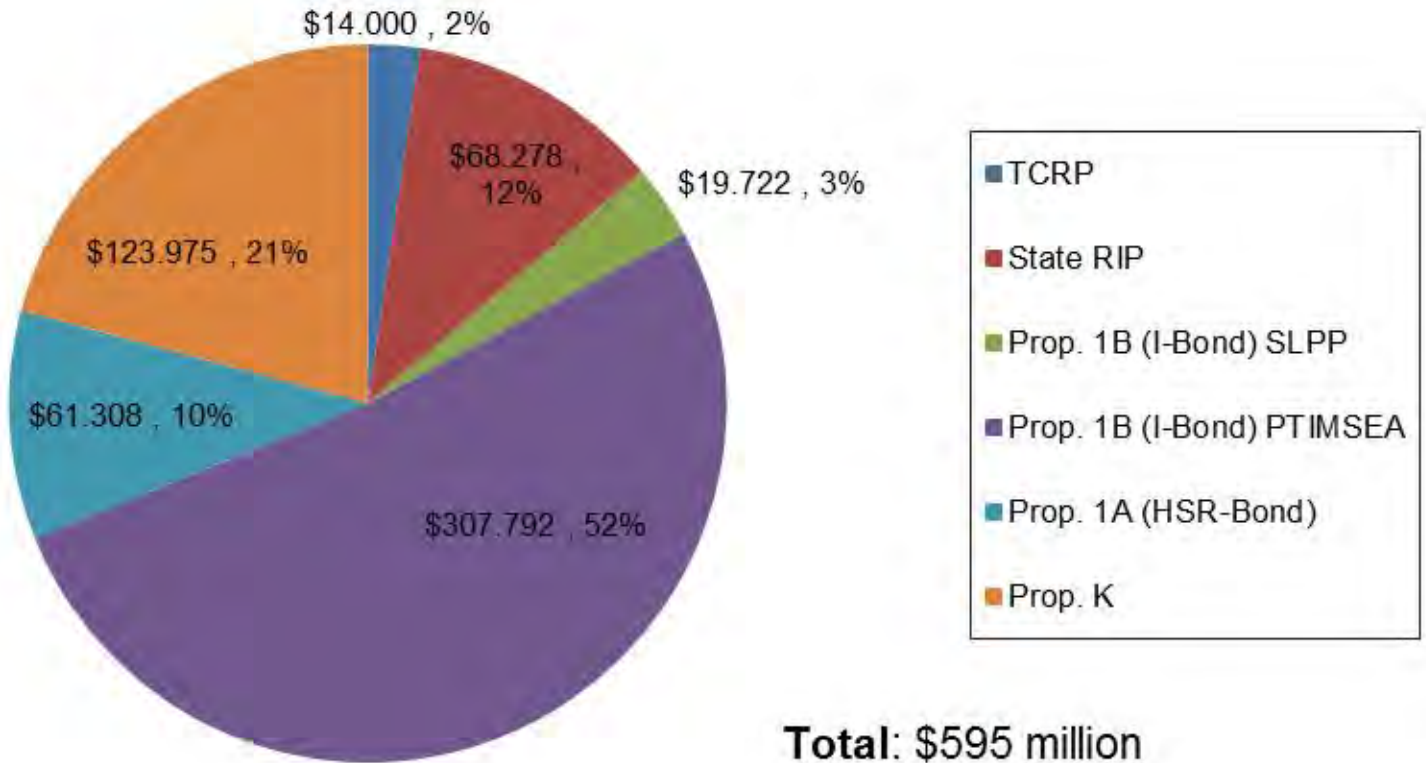
Central Subway Project Funding

Phase 2
(\$ in millions)



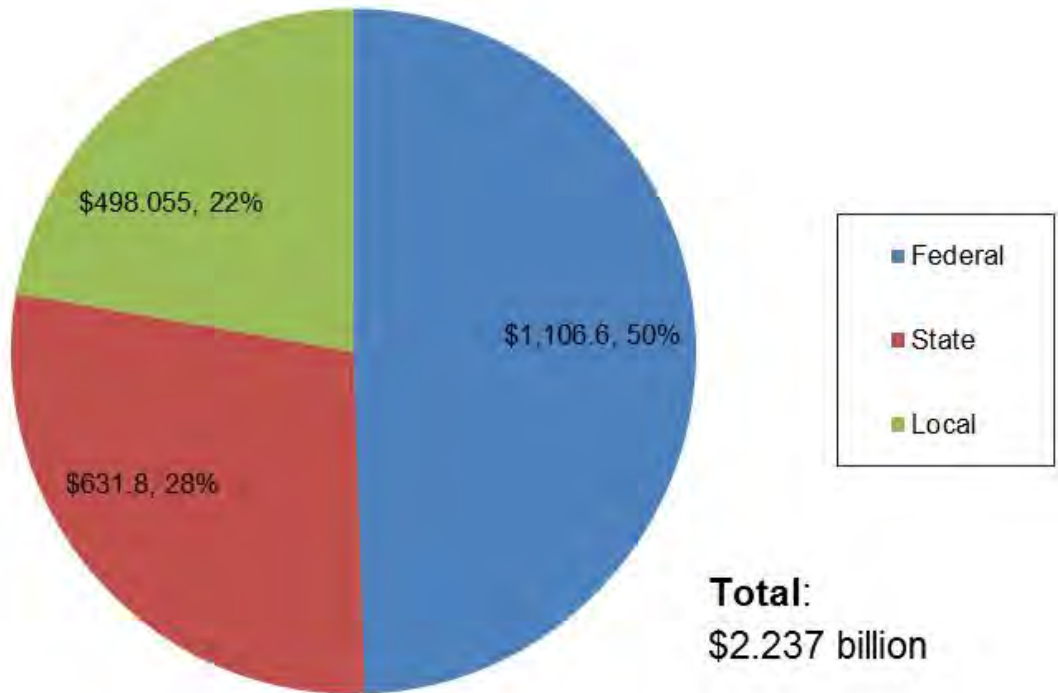
State and Local Funding

Phase 2
(\$ in millions)



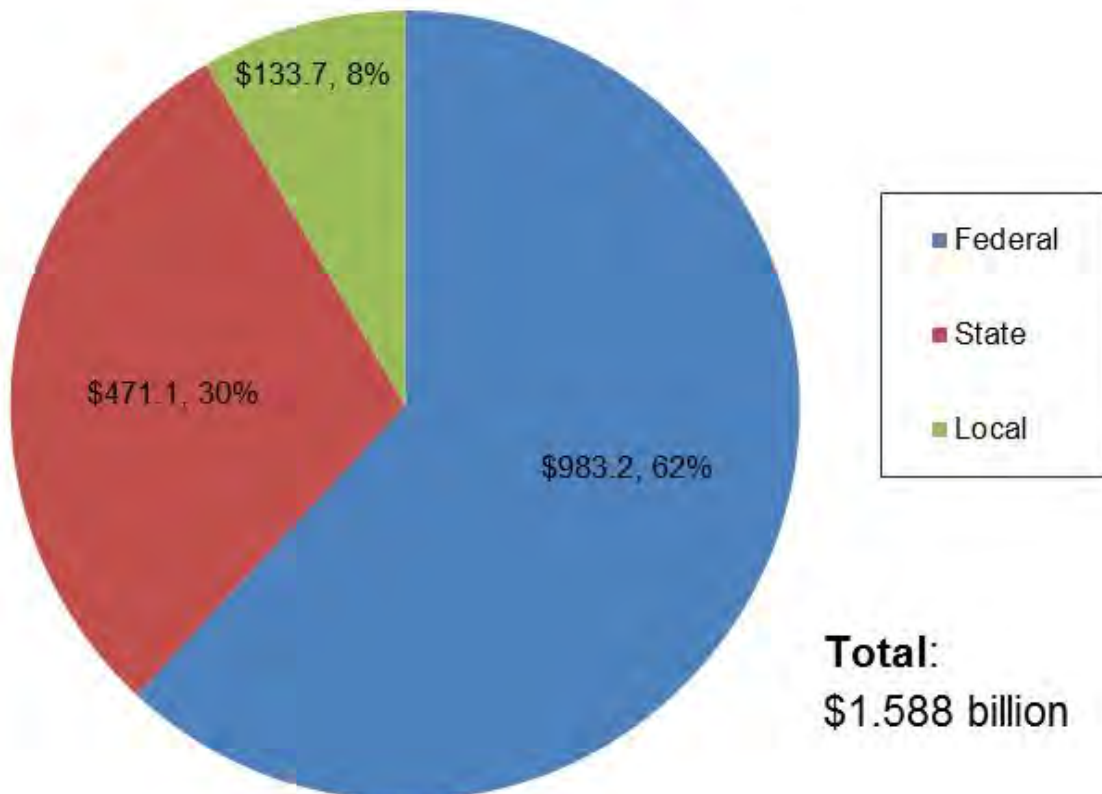
Third Street Light Rail Transit Project Funding

Phase 1 + Phase 2 + Retrieval Shaft Relocation
(\$ in millions)

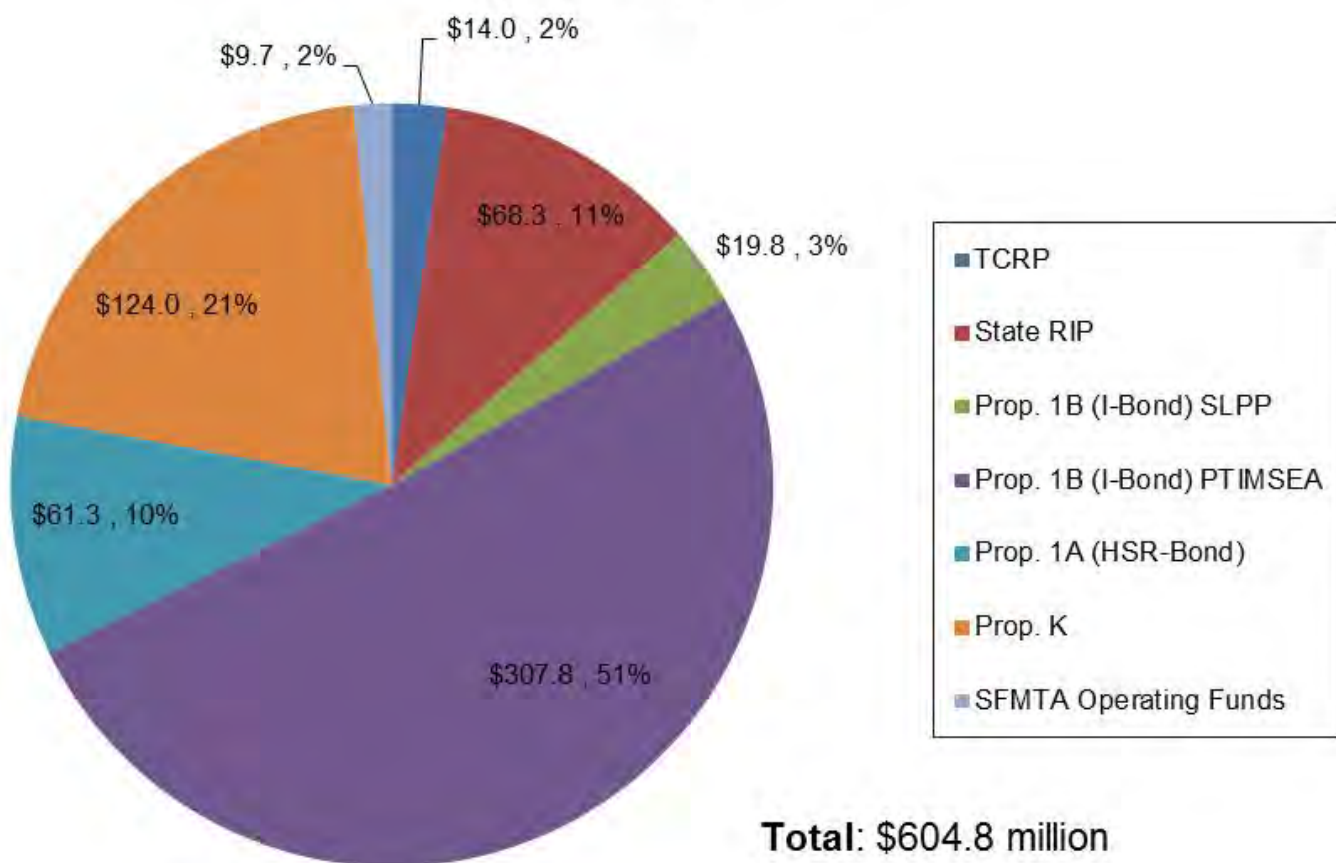


Central Subway Project Funding

Phase 2 + Retrieval Shaft Relocation
(\$ in millions)



State and Local Funding Phase 2 + Retrieval Shaft Relocation (\$ in millions)



Appendix D

COMPLETED CONTRACTS

Moscone Station and Portal Utility Relocation

Contract 1250

Contractor: Synergy Project Management, Inc.

Budget/Expenditures

Category	Amount
Original Budget	\$11,227,316
Expenditures Final	\$11,968,150
Utility Reimbursements	(\$2,275,419)
Final Program Cost	\$9,692,731
Budget Impact (Underrun)	(\$1,534,585)

Contract Details

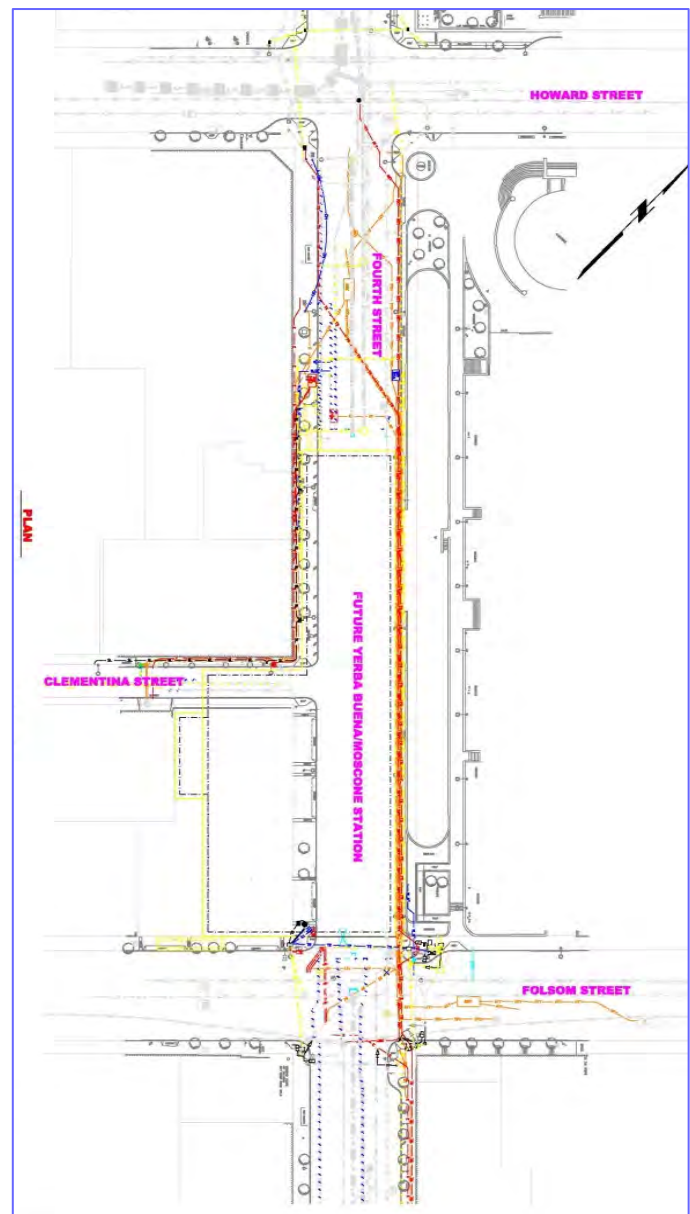
Contract Awarded:	November 17, 2009
Notice to Proceed:	January 4, 2010
Substantial Completion:	June 23, 2011
Contract Award Value:	\$ 9,273,939
Modifications Final :	\$ 2,694,211
Final Contract Value:	\$11,968,150

Status

- Work complete
- Project closeout administration and documentation
- Final Completion Date: June 23, 2011

Description

This project relocates utilities within the footprint of the proposed Yerba Buena/Moscone Station and the 4th Street Portal where the tunnel boring machines will descend underground. Also included is installation of building protections and monitoring of buildings adjacent to utility trenches.



Union Square/Market Street Station Utility Relocation

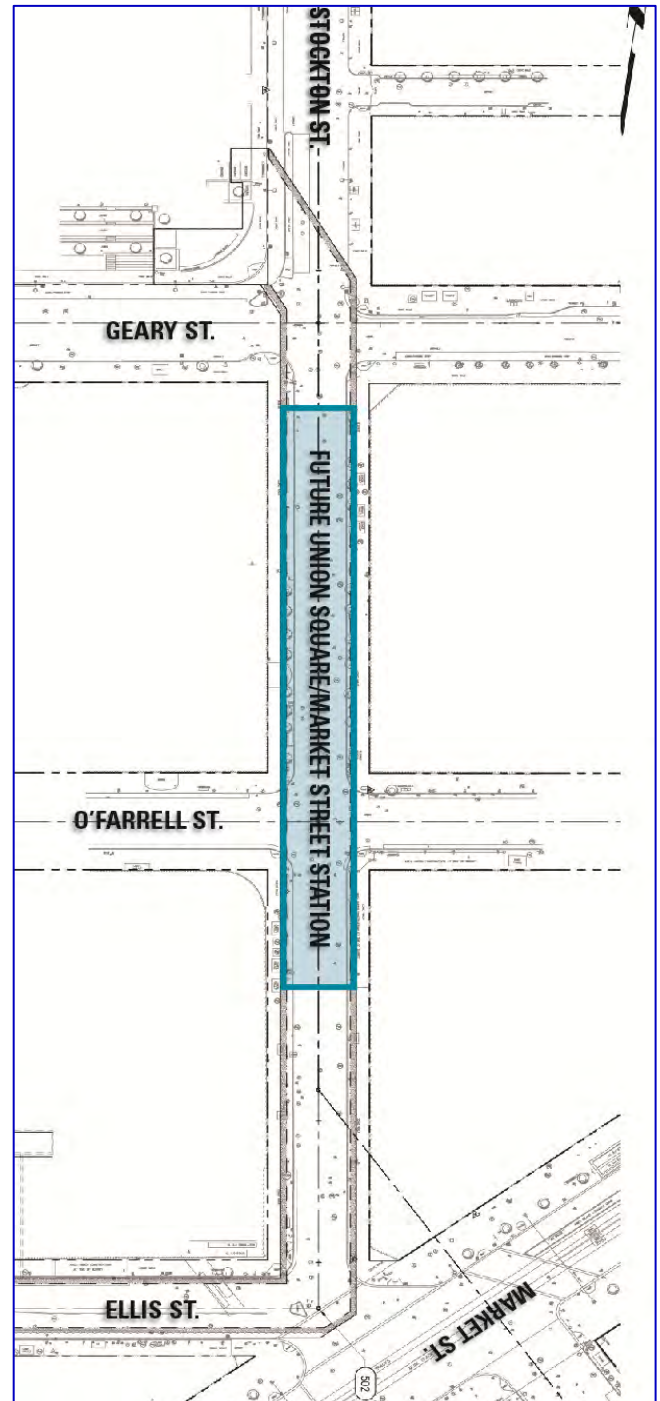
Contract 1251

Contractor: Synergy Project Management, Inc.

Budget/Expenditures	
Category	Amount
Original Budget	\$22,199,847
Expenditures Final	\$20,794,581
Utility Reimbursements	(7,413,510)
Final Program Costs	\$13,176,169
Budget Impact (Underrun)	(\$9,023,678)

Description

This project relocates utilities for the Union Square/Market Street Station and temporarily reroutes existing trolley coach lines around the construction.



Contract Details	
Contract Awarded:	December 7, 2010
Notice to Proceed:	January 12, 2011
Substantial Completion:	August 16, 2012
Contract Award Value:	\$16,832,550
Modifications Final:	\$3,962,031
Final Contract Value:	\$20,794,581

Status

- Final completion date October 15, 2012.
- Completed punch list work
- Project Final Acceptance by the SFMTA Board of Directors

Central Subway Pagoda Palace Demolition

Contract 1277 Contractor: MH Construction

Work Description

Demolish and clear the former Pagoda Theater for use the site to recover the tunnel boring machines when tunnels are completed in 2015. Locate and supply contractor facilities and installations. Obtain permits and approvals and coordinate work with City agencies and utility companies. Furnish and install signs and distribute notices to the local community prior to commencing with construction, cleanup and remove of debris from the site.

- Work was substantially completed Sept. 24, 2013.
- Administrative closeout in progress.

Budget/Expenditures	
Category	Amount
Current Budget	\$722,592
Expenditures to Date	\$638,278

Contract Details

Contract Awarded:	June 12, 2013
Notice to Proceed:	July 15, 2013
Substantial Completion:	Sept. 24, 2013
Contract Award Value:	\$498,995
Modifications to Date:	\$108,228
Current Contract Value:	\$678,134



Central Subway Tunneling

Contract 1252 Contractor: Barnard Impregilo Healy Joint Venture

Description of Work

1.5-mile twin bore tunnels from Hwy I-80 to North Beach using two tunnel boring machines (TBMs). Contractor procurement and installation of the TBMs; construction of the TBM launch box and retrieval shaft excavation support; Yerba Buena/Moscone Station and Union Square/Market Street Station end walls; tunnel excavation and installation of precast segmental lining, the 4th Street portal transition to the surface and cross passages. Throughout, settlement monitoring and protection of existing utilities, buildings and BART tunnels.

Status

- Final Completion Date: May 15, 2015
- Administrative closeout in progress

Budget/Expenditures	
Category	Amount
Current Budget	\$251,068,968
Other Project Budget	\$5,150,000
Other Offset Credits	\$1,135,610
Expenditures to Date	\$234,616,104

Contract Details	
Contract Awarded:	June 28, 2011
Notice to Proceed 1:	January 27, 2012
Notice to Proceed 2:	March 14, 2012
Partial NTP 3:	April 12, 2012
Notice to Proceed 3:	October 15, 2012
Substantial Completion:	April 15, 2015
Contract Award Value:	\$233,584,015
Modifications to Date:	\$7,707,417
Current Contract Value:	\$241,291,432



Appendix E

SBE PARTICIPATION

Quarterly Report

Current Report: July 2015 - September 2015

PROGRAM SUPPORT CONTRACTS – SBE PARTICIPATION

Appendix E presents the Central Subway Program Small Business Enterprise or SBE goals and the actual SBE participation achieved to date – as of September 30, 2015.¹

CS Program SBE Summary Table for Professional Services and Construction Contracts

The summary compares the dollar value of the Base Contracts, the SBE Contract Goals, the percent and dollar value expended to date and the SBE actual participation to date.

Contract No.	Contractor	Services/Segment	A	B	C	D	E	F	G	
			Contract Amount	SFMTA SBE Contract Goal	Contract Expenditure to Date (Est.)	SBE Actual to Date	SBE Contract \$ = A * B	SBE Amount to Date = C * D	Contractor's SBE Goal (in Bid)	
A Project Professional Services Contracts			<i>millions</i>		<i>millions</i>		<i>millions</i>		<i>millions</i>	
1	149	CS Partnership	Project Management	\$85.14	30%	\$51.96	36%	\$25.54	\$18.45	31.4%
2	156	Hill International	Project Controls Task 1	\$17.11	26%	\$8.58	29%	\$4.45	\$2.48	26.0%
3	155-1	PB Telemon	Tunnels Design	\$7.49	30%	\$7.73	30%	\$2.25	\$2.30	31.6%
4	155-2	CS Design Group	Stations Design	\$36.52	30%	\$30.10	43%	\$10.96	\$12.85	36.4%
5	155-3	HNTB, Inc.- B&C	Systems, Track & Surface Station Design	\$17.23	30%	\$12.55	28%	\$5.17	\$3.55	30.0%
Subtotal Professional Services				\$163.50		\$110.93		\$48.36	\$39.64	
B Project Construction Contracts			<i>millions</i>		<i>millions</i>		<i>millions</i>		<i>millions</i>	
1	1250	Synergy Inc	Utility Relocation 1	\$11.97	20%	\$11.97	97.2%	\$2.39	\$11.63	96.4%
2	1251	Synergy Inc	Utility Relocation 2	\$20.79	20%	\$20.79	87.4%	\$4.16	\$18.18	94.9%
3	1252	BIH	Tunnels and Portal - in Construction	\$241.29	6%	234.62	5.8%	\$14.48	\$13.71	6.1%
4	1277	MH Construction	Pagoda Demolition	\$0.68	100%	\$0.64	100.0%	\$0.68	\$0.64	100.0%
5	1300	Tutor-Perini	Stations/Track/Systems - in Construction	\$838.66	20%	\$302.39	15.1%	\$167.73	\$45.72	25.5%
Subtotal Construction Contracts				\$1,113.39		\$570.41		\$189.44	\$89.88	
Contract	Contractor	Services/Segment	Base Contract	SFMTA Goal	Expenditures	SBE Actual	= A * B	= C * D	Bid Goal	
			A	B	C	D	E	F	G	

SBE Summary Table Notes and Sources:

- a) Column A is the base contract amount awarded. Column B is the Agency SBE goal percent for each contract awarded.

The SFMTA SBE Contract Goals are also on the Central Subway web site under the listing of on-going contracts – see **“Closed and Awarded Contracts”** at this link: <http://centralsubwaysf.com/content/closed-and-awarded-contracts>

- b) Column C shows each contract’s current amount expended to date (estimated) including accruals. Column D is the actual SBE percent level of each contract based on payments to date. Column E is the expected SBE dollar amount when the contract amount is completed and the SFMTA SBE goal achieved using this calculation: Columns A * B = Column E, the SBE Expected \$ Amount.

Column F is the actual SBE dollar amount out of the total contract expenditure to date: Columns C * D = Column F, the SBE Expended \$ Amount.

The source of the SBE Actual percent to date and dollar amounts are Progress Payment Applications and Contractor’s monthly submittals that may include the current estimated accruals. The BIH SBE percent is from the contractor’s progress payment #38, Form 6.

¹ An SBE is a for-profit, small business concern with a three (3) year average gross revenue not exceeding \$14 million or \$12 million, depending on the scope of work to be performed, that is certified under any of the following programs: the State of California’s Small Business Program with the Department of General Services (“State Program”), the City and County of San Francisco’s LBE Program (“City Program”), or the California Unified Certification Program (“Federal DBE program”).

- c) Column G, the Contractor's SBE Goal in the submitted bid, is background information that is not calculated in the table. The table source of the Contractor's SBE Goals is from the SFMTA Contract Compliance Office. A Contractor's SBE goal in the bid is one source used by SFMTA Contract Compliance to assess and propose the Agency's SBE goal for a contract.
- d) The three constructions contracts shown in **bold type, 1250, 1251 and 1277**, with gray background, are completed contracts. Little to no changes will be shown in future reports.
- e) The SBE Hill International Actual to Date SBE participation is 28.9% for the overall SFMTA contract. The Hill International data is for the Central Subway Task 1 portion of the Hill International contract to provide SFMTA Project Controls services and systems.
- f) The SBE SFMTA goal for Contract 1300 Tutor-Perini is 20% SBE with a provision of 50% for trucking.
The 1300 Tutor-Perini SBE percent Actual is based on the SBE data provided in Progress Payment #21 September 2015, SFMTA SBE FORM No. 6.
- g) The SBE SFMTA goal for Contract 1277 MH Construction was based on an SBE set-aside.

SBE Participation Details

The two tables that follow present the Central Subway's professional services and construction contract amounts, expenditures and SBE levels with additional details.

Active Professional Services Contracts - SBE Participation Details

Contract:	Project Management and Construction management	
Contract No.	CS-149 Central Subway Partnership	
Status:	On-going	
	Base Contract Value	\$85,139,092
	Approved Change Orders	-0-
	Current Contract Value	\$85,139,092
	Expended to Date (est.)	\$51,964,942
	% Expended	61.0%
	SBE Participation	35.5%
Contract:	Project Controls Cost and Schedule Support	
Contract No.	CS 156 Hill International Task 1	
Status:	On-going	
	Base Contract Value	\$17,112,873
	Approved Change Orders	-0-
	Current Contract Value	\$17,112,873
	Expended to Date (est.)	\$8,575,771
	% Expended	50.1%
	SBE Participation	28.9%
Contract:	Design Package 1 for CNs 1250, 1251 and 1252 Tunnels	
Contract No.	CS-155-1 PB / Telemon	
Status:	Design is completed. Construction support ongoing	
	Base Contract Value	\$5,795,000
	Approved Change Orders (6)	\$1,697,245
	Current Contract Value	\$7,492,245
	Expended to Date (est.)	\$7,730,484
	% Expended	103.2%
	SBE Participation	29.7%
Contract:	Design Package 2 for 1253 UMS, 1254 CTS, 1255 YBM Stations.	
Contract No.	CS-155-2 Central Subway Design Group	
Status:	Design is completed. Construction support ongoing	
	Base Contract Value	\$35,059,252
	Approved Change Orders (1)	\$1,460,360
	Current Contract Value	\$36,519,612
	Expended to Date (est.)	\$30,104,946
	% Expended	82.4%
	SBE Participation	42.7%
Contract:	DP 3 Systems, Track work, Surface station.	
Contract No.	CS-155-3 HNTB-B&C	
Status:	Design is completed. Construction support ongoing	
	Base Contract Value	\$16,822,238
	Approved Change Orders (5)	\$312,814
	Current Contract Value	\$17,232,252
	Expended to Date (est.)	\$12,553,146
	% Expended	72.8%
	SBE Participation	28.3%

Active and Completed Construction Contracts - SBE Participation Details

Contract:	Synergy Inc Utility Relocation 1 YBM & Launch Box	
Contract No.	1250	
Status:	Contract is completed and closed out	
	Base Contract Value	\$9,273,939
	Approved Change Orders	\$2,694,211
	Final Contract Value	\$11,968,150
	% Expended	100%
	SBE SFMTA Goal	20%
	SBE Participation To Date	97.2%

Contract:	Synergy Inc Utility Relocation 2 UMS	
Contract No.	1251	
Status:	Contract is completed and closed out	
	Base Contract Value	\$16,832,550
	Approved Change Orders	3,962,031
	Final Contract Value	\$20,794,581
	% Expended	100%
	SBE SFMTA Goal	20.0%
	SBE Participation To Date	87.4%

Contract:	Pagoda Palace Demolition / MH Construction	
Contract No.	1277	
Status:	Construction is complete. Contract in Close Out	
	Base Contract Value	\$498,995
	Approved Change Orders	\$179,139
	Current Contract Value	\$678,134
	Expended to Date (est.)	\$638,278
	% Expended	94.12%
	SBE SFMTA Goal	100.0%
	SBE Participation To Date	100.0%

Contract:	Tunnels Barnard/Impregilo/Haley	
Contract No.	1252*	
Status:	Construction is underway and ongoing	
	Base Contract Value	\$233,584,015
	Approved Change Orders	\$7,707,417
	Current Contract Value	\$241,291,432
	Expended to Date (est.)	\$234,616,104
	% Expended	97.2%
	SBE SFMTA Goal	6.0%
	SBE Participation To Date	5.8%

Contract:	Stations and Systems / Tutor Perini	
Contract No.	1300*	
Status:	Construction is underway and ongoing	
	Base Contract Value	\$839,676,400
	Approved Change Orders	(\$1,016,585)
	Current Contract Value	\$838,659,815
	Expended to Date (est.)	\$302,388,888
	% Expended	36.1%
	SBE SFMTA Goal	20.0%
	SBE Participation To Date	15.1%

Notes:

* Contract 1252 and Contract 1300 March cost is accrual.

Photos on the next page:

The four photos recount the special street reconstruction in December that was carried out for the mandatory Holiday Moratorium on construction in the downtown core area: Food trucks from Off the Grid made regular appearances at the 2014 Winter Walk. Vintage-styled light poles and illuminated benches contributed to an enjoyable atmosphere for shoppers and tourists. A large, moving holiday display was projected onto the Macy's Men's building, setting the scene at Geary and O'Farrell Street. Shoppers, locals and tourists all enjoyed the extra seating and green turf unrolled for the 2014 Winter Walk.

central subway

Central Subway Project Office
821 Howard Street
San Francisco, CA 94103
Phone: 415.701.4371
Fax: 415.701.5222
Email: central.subway@sfmta.com



www.centralsubwaysf.com



www.twitter.com/central_subway



www.facebook.com/centralsubway



www.centralsubwayblog.com



www.youtube.com/municentralsubwaysf



www.flickr.com/municentralsubway



This document is published by the SFMTA and the City and County of San Francisco as a service to individuals and agencies interested in the Central Subway Project. Funding for the Central Subway is made possible through funds provided by the Federal Transit Administration, the State of California, the Metropolitan Transportation Commission and the San Francisco County Transportation Authority.



SFMTA
Municipal
Transportation
Agency