

central subway

Tunnel Breakthrough at YBM

Workers cut into the top of the twin tunnels in order to facilitate construction of the mezzanine floor slab.



Progress Report

February 2016 Rev 1

2016
YEAR OF THE MONKEY



SFMTA
Municipal
Transportation
Agency

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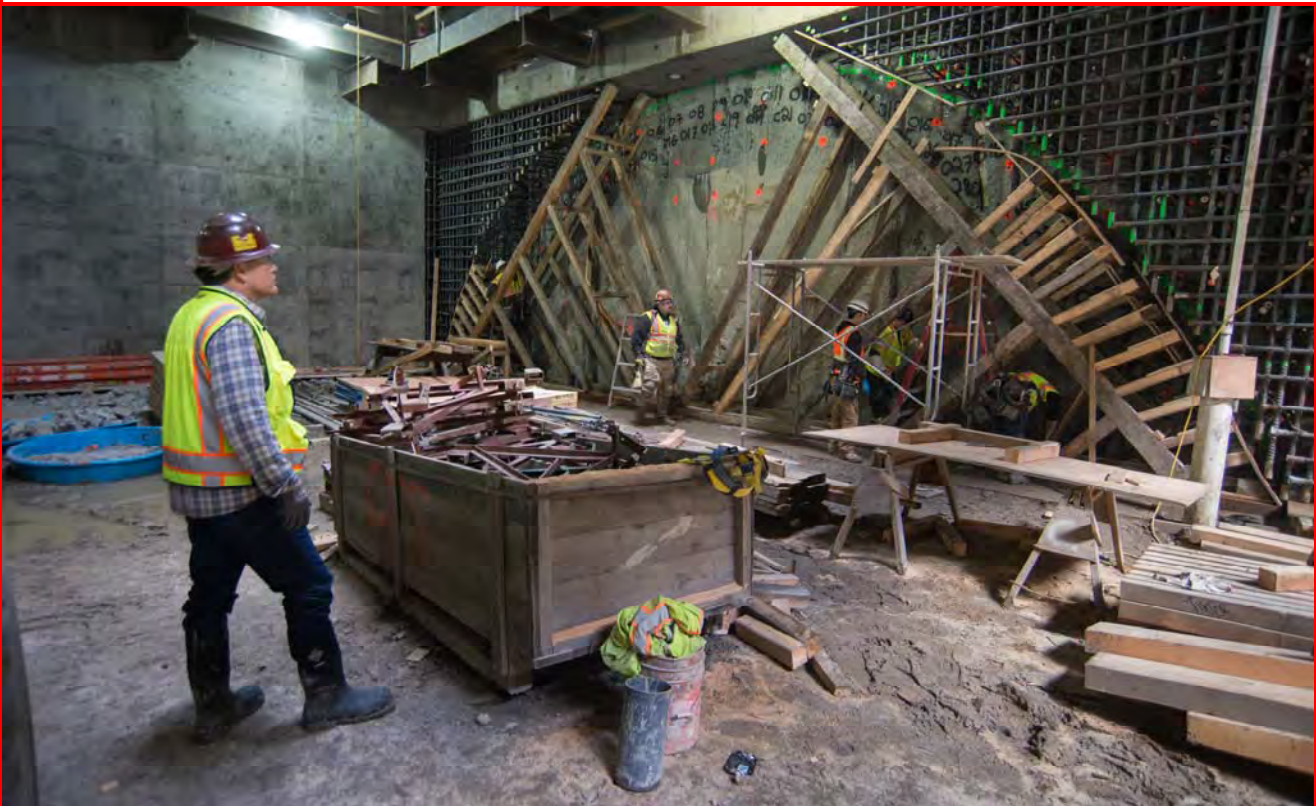
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Cover photo: In order to facilitate construction of the mezzanine-level floor slab inside the Yerba Buena/Moscone station box, workers cut notches across the tops of the twin tunnels. This marked the first station-to-tunnel breakthrough. Workers then began constructing concrete forms to install reinforced concrete beams across these openings. **More photos can be found starting on page 34.**

Above photos: At the northeast corner of the Chinatown Station headhouse, work to construct the archway which will lead passengers from the headhouse to the future station cavern is underway. Rebar has been installed, and workers can be seen constructing concrete forms.

See the Appendix E final page for CS websites hyperlinks and public outreach on line resources. The Project main web site is at: <http://www.centalsubwaysf.com/>



Lights from a bobcat illuminate the underside of Stockton at Geary, where excavation of the north station concourse is underway for the Union Square/Market Street Station.

Executive Summary

Continue excavation at YBM, UMS and CTS Stations for the future stations.

Chinatown Station - Crosscut Cavern: continue coring barrel vaults, started drilling and installing barrel vaults. Excavation to elevation—50'-60' level. Connections for the brick sewer. Completed CDF backfill from Headhouse, SB Tunnel platform south. Headhouse: Install rebar and form and pour west wall. Chinatown Moratorium in February, before Chinese New Year to accommodate street fair.

Union Square/Market Street Station - Traffic Handling -Stockton Street closure between Geary and Ellis, no southband lane on Stockton between Post and Geary. South Concourse and Platform Station: continued mobilizing jet grout operations. Platform Station: Resumed jet grouting installation. Excavated and chipped piles for roof deck sections 1A and 4A-9, installed metal decking, studs and rebar for roof deck pour 9-4A. North Concourse: set up compensation grout plant and drill and install compensation grout pipes; perform utility work on Geary Street and Stockton Streets; chip piles in preparation for wall construction. UMS Garage: Continued demolition on the plaza level. UMS Garage: Completed installation of tiebacks. UMS Garage: Continue work on shear walls between new ramp and Stockton Street.

Yerba Buena/Moscone Station - Traffic flowing on east side of 4th Street with west side work area closed for construction. Station box level 4 temporary bracing installation is in progress beneath Mezzanine level; excavation for Concourse level slab is in progress below level 4 bracing. Temporary bracing is being installed within tunnel to provide support during installation of Concourse level beams and slab. Headhouse level 4 temporary bracing installation has begun. Archaeological monitoring by spot checking is in progress.

Surface, Track and Systems— Continue Muni ductbank installation. Continue 78" sewer rehabilitation. Continue 36" sewer force main. Continue Auxiliary Water Supply System (AWSS) installation. Continue water line work. Continue AT&T cutover. Continue tunnel prep work. Continue OCS pole foundation installation.

Tunnel - Contract administrative closeout is ongoing.

Total project costs to date are \$907.95 million, an increase of \$14.25 million over last month. The total cost to date equals 57.53% of the total project budget of \$1.578 billion. The Master Project Schedule forecast the Revenue Service Date of May 2019.

Key Milestones

Uncovering and breaking through the tunnels at YBM

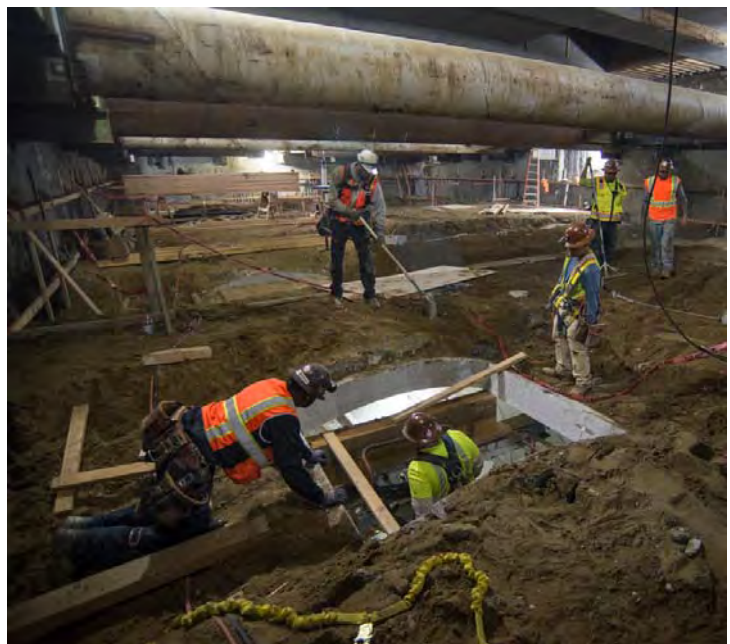


1 Workers carefully uncover the tunnels using small equipment



2 Steel bracing is installed in the tunnels to reinforce their structure

MILESTONE	DATE EXPECTED
General	
Revenue Service	May 2019
Contract 1252 Tunnels	
Substantial Completion	April 15, 2015 (A)
Contract 1300 Stations, Surface, Track, Systems	
Notice to Proceed (NTP 1)	June 17, 2013 (A)
Notice to Proceed (NTP 2)	January 12, 2014 (A)
Substantial Completion	February 10, 2018



3 Workers saw openings in the concrete tunnel lining and remove material

Costs and Schedule

Costs (See Appendix A for Details)

The Current Cost Estimate (CCE) for the Central Subway Project is **\$1.578 billion** in year of expenditure dollars (\$YOE). This total project cost is shown at the top of Report 7.1, Program Project Budget. This capital cost projection incorporates allocated and unallocated contingencies to cover the risks associated with the project completion.

Total net incurred costs for the project are \$907.95 million, a \$14.25 million increase over last month. The cost to date figure reflects expenditures through FAMIS 786 Report (\$867.68 million) plus the utilities joint trench Form B Reimbursement payment (\$10.52 million), invoices currently being processed (\$27.21 million) and estimates of outstanding pay requests (\$2.54 million). This incurred amount equals 57.53% of the total project budget of \$1.578 billion.

The current funding level to date is \$1,179.79 million. This represents 75% of the total project budget.

Earned Value Analysis

In February 2016 Report, Central Subway Project Master Schedule has used Contract 1300 Station contractor baseline schedule and rejected February monthly update schedule. Contractor has not addressed the cost issues in cost loaded schedule. Preliminary February Earned Value Analysis reports the Planned Value, Earned Value, Percent Complete and resulting indexes as follows:

Preliminary February Earned Value

Overall Budgeted Cost:	\$1,578,300,000
Planned Value:	\$1,174,792,723
Earned Value:	\$936,198,133
Actual Cost:	\$907,951,763
Schedule Performance Index (SPI):	0.8
Cost Performance Index (CPI):	1.03
Percent Complete:	59%

Schedule Highlights - Continued

The Master Project Schedule (MPS) below includes progress through December 2015. The February 2016 Schedule Update submittal from Contract 1300 Contractor is rejected based on erroneous logic changes. The Contractor's February 2016 Schedule Update shows a 4-day improvement on the Critical Path. The Contract 1300 schedule represented in this report is based on the December 2015 Update. The Program is continuing to working on a properly updated Contract 1300 schedule.

The MPS shows a forecast Revenue Service Date of May 24, 2019.

The controlling critical (longest) path of the MPS runs through CTS Excavation succeeded by STS Startup & Testing, Commissioning and Pre-Revenue Activities to the Baseline Finish and Revenue Service Date. See Appendix B – Longest Path. The latest schedule shows the longest path running through the Chinatown Station (CTS). The CN-1300 Contractor continues to fall behind schedule by not following their current plan and by not working on critical activities to meet schedule goals and Contract Milestones. Contractor is required to implement a Recovery Schedule to put the Project back on schedule.

Schedule Contingency is fully utilized on the critical path of the MPS, which is below the Minimum Schedule Contingency level of 6 months. A schedule re-evaluation will be performed, utilizing the updated Contract 1300 Schedule. Recovery options are being implemented in key areas as work proceeds. SFMTA continues to meet with Contractor to discuss all schedule concerns and comments. The Contractor, Tutor Perini Corporation's (TPC) schedule continues to show schedule slippage due to lack of schedule progress on Critical Path activities.

The 1300 Contractor submitted fifteen (15) Schedule Updates from December 2014 to February 2016. SFMTA rejected six (6) Schedule Updates from September 2015 to February 2016. Contractor has been directed to develop a Recovery Schedule as required by Contract and correct out-of-sequence and Retained Logic driving many of the forecast dates. Review of schedule updates as well as identifying recovery options is ongoing.



Rebar sticks out of the side of a finish wall being constructed against the exterior slurry walls at the bottom of the Chinatown Station headhouse.

Schedule Highlights - Continued

Contract 1300 Stations, Surface, Track and Systems

The Contractor, Tutor Perini Corporation's (TPC) baseline schedule is incorporated into the master program. The preliminary Tutor Perini Corporation's (TPC) December schedule is used in February Report.

Work Package P-1254R CTS performed the following work this month:

- Crosscut Cavern: continue coring barrel vaults, started drilling and installing Barrel Vaults.
- Excavation to elevation – 50'-60' level.
- Connections for the brick sewer
- Completed CDF backfill from Headhouse, SB Tunnel platform south
- Headhouse: Install rebar and form and pour west wall
- Chinatown Moratorium in February, before Chinese New Year to accommodate street fair

Work Package P-1253 UMS performed the following work this month:

- Traffic Handling – Stockton Street closure between Geary and Ellis, no southbound lane on Stockton between Post and Geary.
- South Concourse and Platform Station: continued mobilizing jet grout operations
- Platform Station: Resumed jet grouting installation. Excavated and chipped piles for roof deck sections 1A and 4A-9, installed temporary bridge at 1A, continue rebar installation
- North Concourse: Continue compensation grout plant; perform utility work on Geary Street and Stockton Streets
- UMS Garage: Continued demolition on the plaza level
- UMS Garage: Continued installation of tiebacks
- UMS Garage: Continue work on shear walls between new ramp and Stockton Street

Work Package P-1255 YBM performed the following work this month:

- Traffic flowing on east side of 4th Street with west side work area closed for construction
- Station box level 4 temporary bracing installation is in progress beneath Mezzanine level; excavation for Concourse level slab is in progress below level 4 bracing
- Temporary bracing is being installed within tunnel to provide support during installation of Concourse level beams and slab.
- Headhouse level 4 temporary bracing installation has begun
- Archaeological monitoring by spot checking is in progress

Work Package P-1256 STS performed the following work this month:

- Continue Muni ductbank installation
- Continue 78" sewer rehabilitation
- Continue 36" sewer force main
- Continue Auxiliary Water Supply System (AWSS) installation
- Continue water line work
- Continue AT&T cutover
- Continue tunnel prep work
- Continue OCS pole foundation installation



Concrete forms are in place both inside and outside the work area at the southeast corner of the Union Square Garage, where work to refit the garage with the north station entrance is underway.

Contracts & Construction

Construction Contracts In Progress

Contract 1300: Combined Work Packages 1253, 1254, 1255, 1256

- Contractor: Tutor - Perini Corporation
- Amount: \$838.65 million
- Contract Status: 45.19% complete construction

Contracts Completed

[See Appendix D](#)

Contract 1250: Moscone Station and Portal Utilities Relocation

Contract 1251: Union Square/Market Street Station Utility Relocation

Contract 1277: Pagoda Theater Site Demolition (Funded separately from the CS Project budget)

Contract 1252: Central Subway Tunneling

Contract SBE Participation (Updated Quarterly) [See Appendix E](#)

Stations, Surface, Track and Systems

Contract 1300 Contractor: Tutor-Perini Corporation

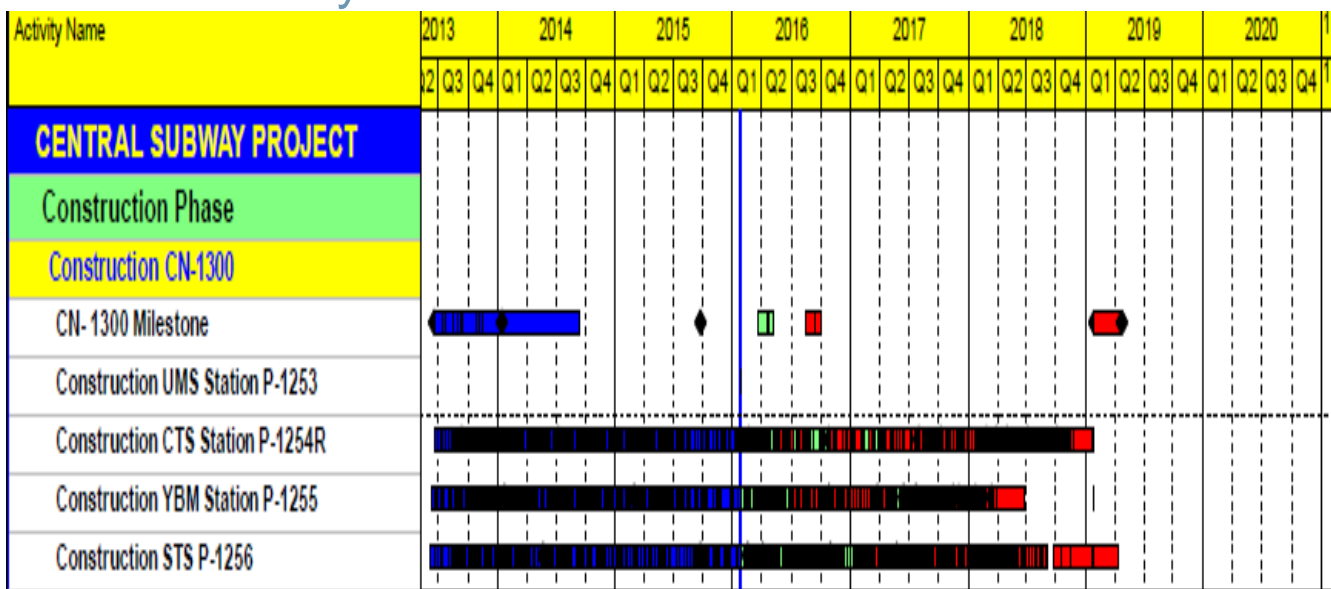
Description of Work

The Contract 1300 scope is to construct the Central Subway's three subway stations, one surface station, construct the 2,000 feet of surface track, and install track and operating systems throughout the new alignment. The separate station and systems work packages are presented in the following pages.

Work includes station finishes, AC and DC substations, elevators, escalators, lighting, emergency ventilation fans, HVAC fire alarm/ suppression/ protection, Cutter Soil Mixing, secant pile bottom up and Sequential Excavation Method construction, settlement monitoring, building protection, connecting to and modifying the BART Powell Street Station, PA, CCTV, signage, installation of fare collection equipment and station start-up and commissioning.

Contract Details		Budget/Expenditures▲	
Contract Awarded:	May 21, 2013	Current Budget	\$859,676,400
Notice to Proceed:	June 17, 2013	Expenditures to Date	\$353,422,196
Substantial Completion:	February 10, 2018		
Contract Award Value:	\$839,676,400		
Modifications to Date:	\$270,956		
Current Contract Value :	\$839,947,356		

1300 Summary Schedule



Chinatown Station

Contract 1300 - Work Package 1254R



Description of Work

This Work Package is to construct one subway station. Includes station finishes, AC and DC Traction Power substations elevators, escalators, lighting, emergency ventilation fans, HVAC fire alarm/ suppression/ protection, slurry wall top-down construction, settlement monitoring, building protection, PA, CCTV, signage, installation of fare collection equipment and station start-up and commissioning.

Current Work Status

- Crosscut Cavern: continue coring barrel vaults, started drilling and installing Barrel Vaults.
- Excavation to elevation – 50'-60' level.
- Connections for the brick sewer
- Completed CDF backfill from Headhouse, SB Tunnel platform south
- Headhouse: Install rebar and form and pour west wall



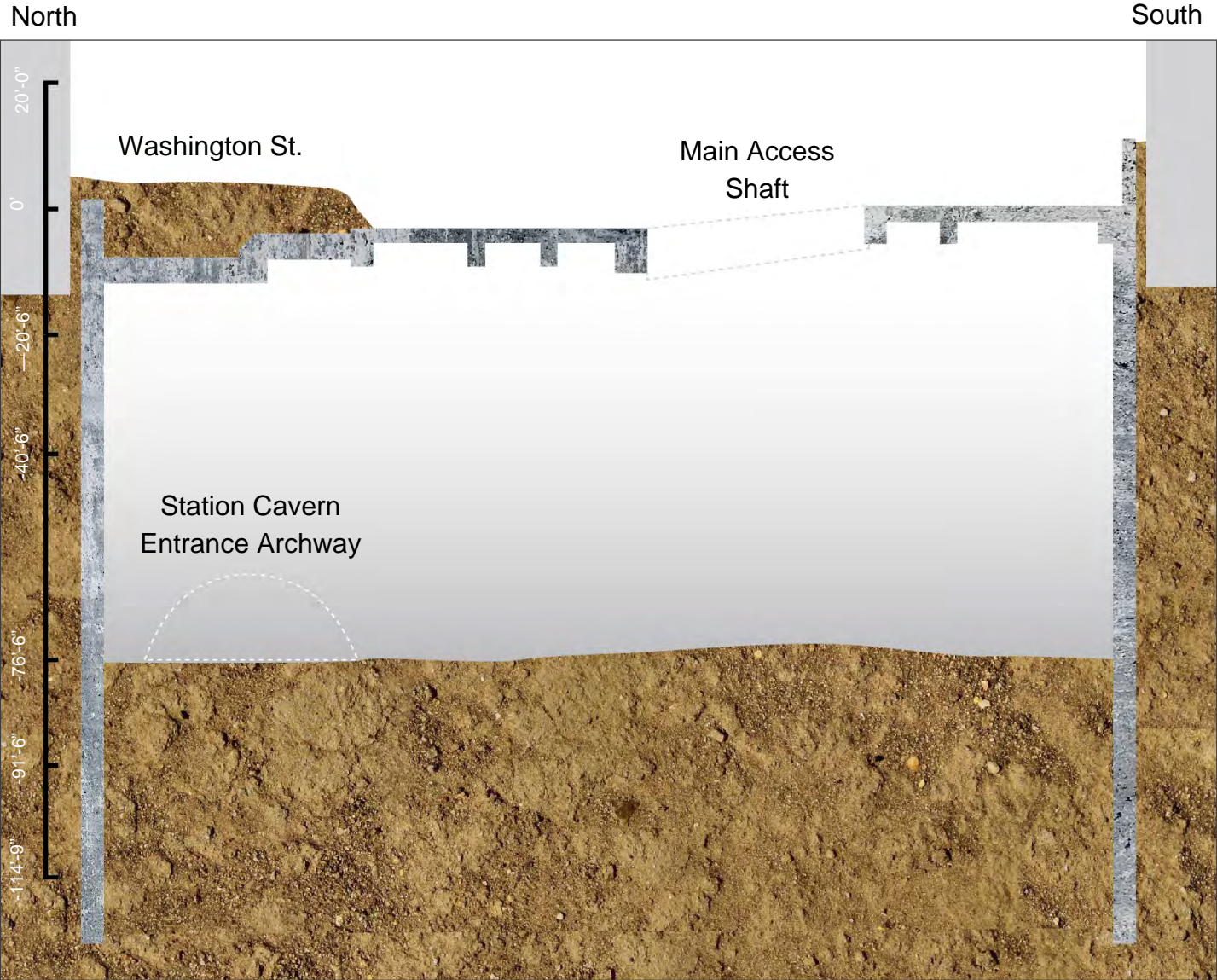
Work Expected Next Month

- Utility demolition
- Headhouse: Install Temp level 4.0 Struts and excavate to Temp level 5.0 Struts
- Crosscut Cavern: complete barrel vault piping
- Install spilling over South Egress Exit
- Complete water distribution

Three Month Look Ahead

- Headhouse: Construct Level 3 composite wall
- Excavate below Temp level 5.0 Struts; compensation grouting
- Crosscut Cavern: Break in and construct top sidewall and headwall left
- Crosscut Cavern: Break in and construct top sidewall and headwall right
- Excavate & Construct Sidewall Bench & Headwall

Station Excavation and Construction Progress Section

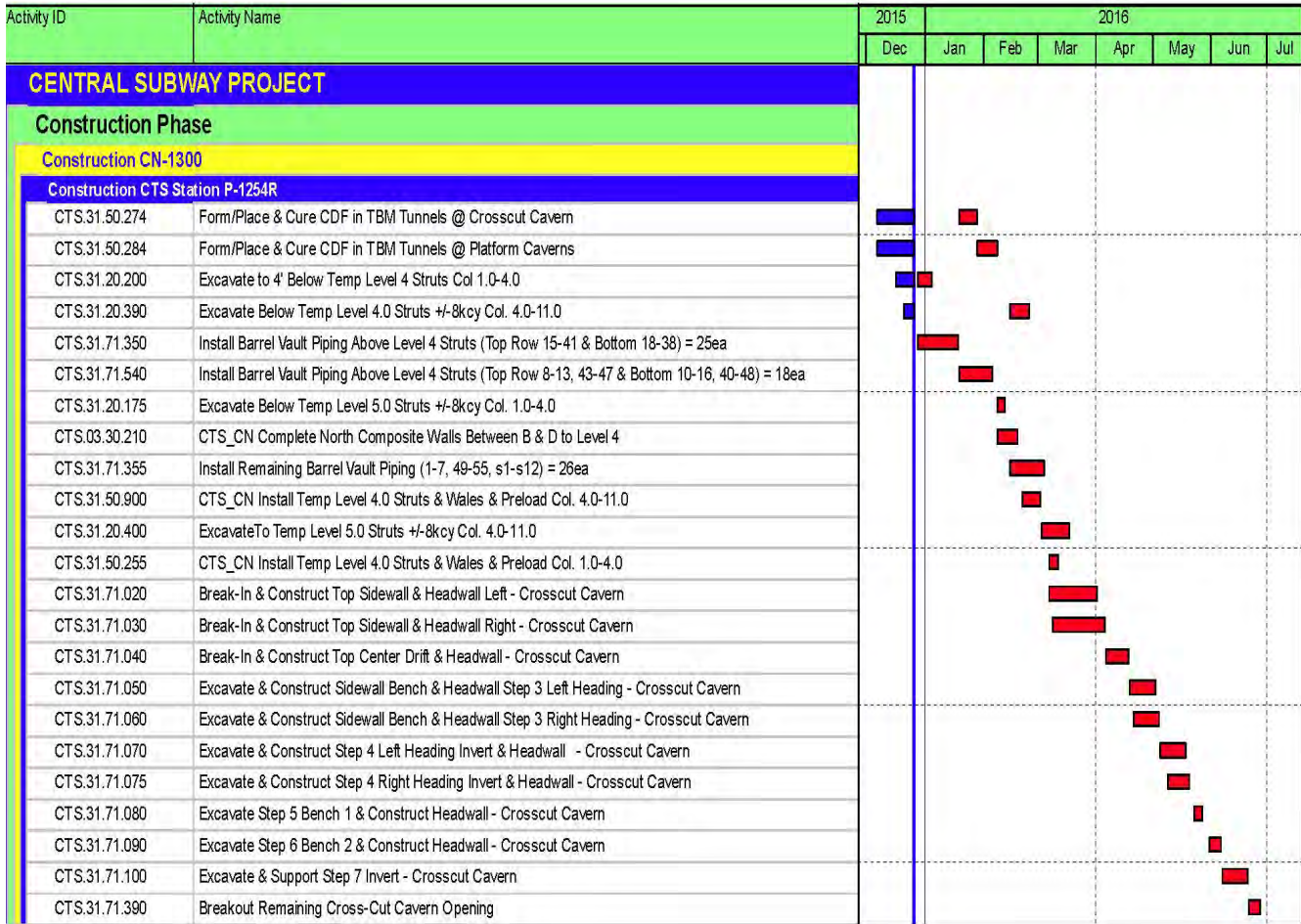


Chinatown Station Construction Status - Continued

Contract Details	
Contract Awarded:	May 21, 2013
Notice to Proceed:	June 17, 2013
Substantial Completion:	February 10, 2018
Contract Award Value:	\$247,567,810
Modifications to Date:	\$403,233
Current Contract Value:	\$247,971,043

Budget/Expenditures	
Current Budget	\$252,567,810
Expenditures to Date	\$91,423,369

CTS Three Month Schedule



Schedule: Contract 1300 December 2015 Update

Union Square/Market Street Station

Contract 1300 Work Package 1253

Description of Work

This Work Package is to construct one subway station. Includes station finishes, AC and DC Traction Power substations elevators, escalators, lighting, emergency ventilation fans, HVAC fire alarm/ suppression/ protection, slurry wall top-down construction, settlement monitoring, building protection, PA, CCTV, signage, installation of fare collection equipment and station start-up and commissioning.

Current Status This Month

- Traffic Handling – Stockton Street closure between Geary and Ellis, no southbound lane on Stockton between Post and Geary.
- South Concourse and Platform Station: continued mobilizing jet grout operations
- Platform Station: Resumed jet grouting installation. Excavated and chipped piles for roof deck sections 1A and 4A-9, installed temporary bridge at 1A, installed metal decking, studs, and rebar for roof deck pour 9-4A
- North Concourse: Set up compensation grout plant and drill and install compensation grout pipes; perform utility work on Geary Street and Stockton Streets; chip piles in preparation for wall construction
- UMS Garage: Continued demolition on the plaza level
- UMS Garage: Continued installation of tiebacks
- UMS Garage: Continue work on shear walls between new ramp and Stockton Street



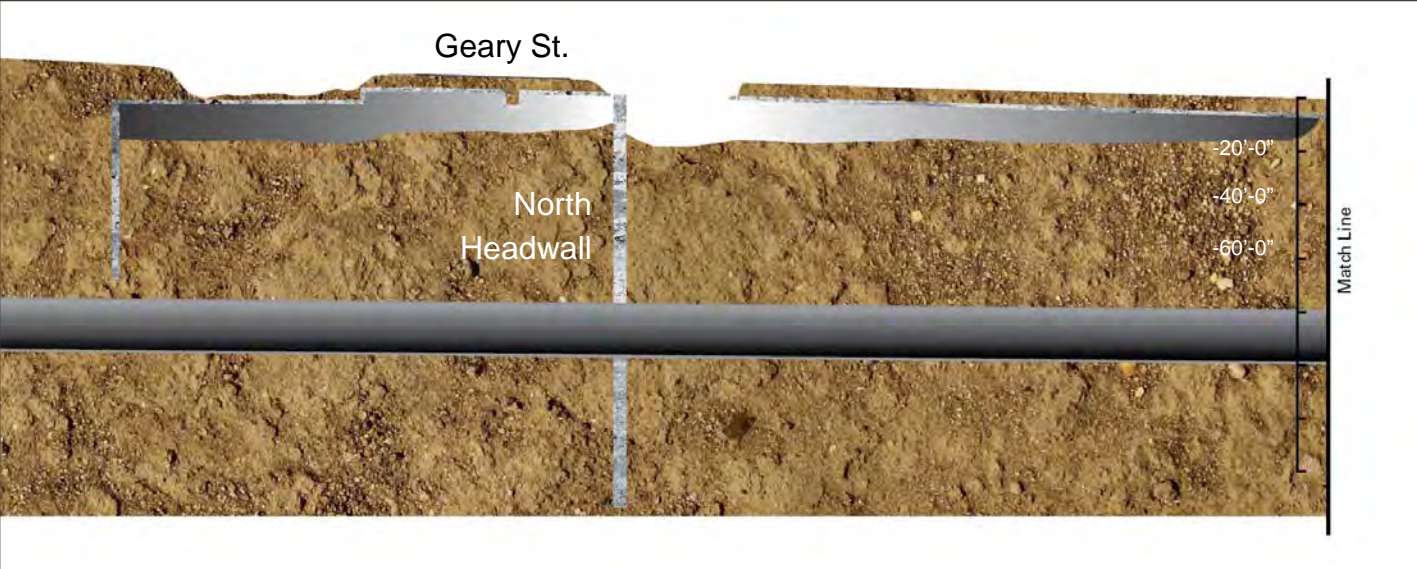
Three Month Look Ahead

- Platform Station: Continue jet grout activities; backfill, install utilities and restore street; dewatering; install permanent wales; excavate bench; install studs, mesh, drain pipe & shotcrete pile walls
 - Access Shaft: break through battered piles & frame construction
 - North Concourse: excavate to intermediate strut level, duct chase invert; install W-section piles reinforcement
 - Ellis Street: Pave Stockton/Market/Ellis Street & intersection
 - UMS Garage: structural demolition existing USG soldier pile
 - UMS Garage: install and erect structural steel columns; install and preload entrance roof frame and bracing
- ## Work Expected Next Month
- North Concourse: Complete compensation grouting; continue chip, pile, install, rebar mesh & shotcrete leveling
 - Platform Station: Continue jet grout installation; deck, form, rebar, pour station box;
 - Start backfill Roof Deck 4A at Platform Station
 - Access Shaft: Excavate, lag and support; install temporary lid
 - Ellis Street: Install water main on south side
 - UMS Garage: continue demolition; installing bracing & shoring walls & slabs

Station Excavation and Construction Progress Section

North

South



Union Square Market Street Station Construction - Continued

Contract Details

Contract Awarded:	May 21, 2013
Notice to Proceed:	June 17, 2013
Substantial Completion:	February 10, 2018
Contract Award Value:	\$294,030,590
Modifications to Date:	\$111,258
Current Contract Value:	\$294,141,848

Budget/Expenditures ▲

Current Budget	\$299,030,590
Expenditures to Date	\$143,367,650

UMS Three Month Schedule

Activity ID	Activity Name	2015			
		Dec	Jan	Feb	Mar
CENTRAL SUBWAY PROJECT					
Construction Phase					
Construction CN-1300					
Construction UMS Station P-1253					
UMS.02.41.0060	UMS_Demo Garage Roof Slab				
UMS.31.41.200	UMS_Install Sheet Piles East & West Side Of Stockton St - Pours 1A,2A,3A,1B				
UMS.31.41.0240	UMS_Install Sheet Piles @ Access Shaft #2 (O'Farrell)*				
UMS.02.41.430	UMS_Complete Demo South Headwall				
UMS.00.SP39.115	UMS_Post Holiday Moratorium - Remove 2015 Moratorium Furnishings				
UMS.02.41.0150	UMS_Demo South Entrance For BART Elevator Powell St Station				
UMS.05.12.200.4a	UMS_Set Bridging Support Skid - South Headwall to Sta 134+26				
UMS.31.50.0250	UMS_Excavate - Lag & Support @ Access Shaft #2 (O'Farrell)				
UMS.31.20.200.4a	UMS_Excavate For Roof Section & Support Utilities (Where Required) South Headwall to Sta 134+26				
UMS.31.50.0040	UMS_Install Bracing & Shoring for Walls & Slabs				
UMS.31.20.101.4a	UMS_As-Build Beam Connections / Fabricate / Deliver Roof Beams - South Headwall to Sta 134+26				
UMS.31.32.100.4b	UMS_Jet Grout Stockton St East/West Side for Water Seal Rig#1 Pour 4B				
UMS.05.12.106.2b	UMS_Erect Excavation Access Shaft Steel - Sta 132+50				
UMS.05.12.100.1a	UMS_Set Bridging Support Skid - North Headwall to Sta 131+26				
UMS.31.20.100.1a	UMS_Excavate For Roof Section & Support Utilities (Where Required) North Headwall to Sta 131+26				
UMS.03.30.107.2b	UMS_Construct Excavation Access Shaft Walls - Sta 132+50				
UMS.31.20.101.1a	UMS_As-Build Beam Connections / Fabricate / Deliver Roof Beams - North Headwall to Sta 131+26				
UMS.05.12.205.4a	UMS_Install Roof Deck Beams - South Headwall to Sta 134+26				
UMS.02.41.420	UMS_Demo North Headwall For Roof Deck Installation				
UMS.31.20.0960	UMS_Complete Excavate South Concourse to Invert				
UMS.31.50.0260	UMS_Install Temporary Lid @ Access Shaft #2 (O'Farrell)				
UMS.05.30.200.4a	UMS_Install Metal Roof Decking - South Headwall to Sta 134+26				
UMS.31.20.102.2b	UMS_Excavate To Compensation Grout Level - Sta 132+34 To Sta 132+70				
UMS.05.12.600	UMS_Install Station Box Roof Beams - Sta 134+50 to 134+00				
UMS.03.30.200.4a	UMS_UMS_Formwork/Rebar Roof Deck and Pile Cap - South Headwall to Sta 134+26				
UMS.05.12.105.1a	UMS_Install Roof Deck Beams - North Headwall to Sta 131+26				
UMS.31.43.290.2b	UMS_Install Compensation Grouting Tubes North of O'Farrell St - Macy's Mens Sta 132+34 to 132+70				
UMS.31.43.300.2b	UMS_Install Compensation Grouting Tubes North of O'Farrell St - Macy's Womens Sta 132+34 to 132+70				
UMS.31.32.100.5b	UMS_Jet Grout Stockton St/O'Farrell West Side for Water Seal Rig#1 12ea Piles w/e #1-3				
UMS.05.30.100.1a	UMS_Install Metal Roof Decking - North Headwall to Sta 131+26				
UMS.03.30.205.4a	UMS_Place Concrete - Roof Deck and Pile Cap - South Headwall to Sta 134+26				
UMS.03.30.640	UMS_Deck / Form / Rebar / Pour Station Box Roof - Sta 134+50 to 134+00				
UMS.03.30.790	UMS_Form / Rebar / Pour Pile Cap Beam Sta 134+50 to 134+00				
UMS.03.30.100.1a	UMS_Formwork/Rebar Roof Deck and Pile Cap - North Headwall to Sta 131+26				
UMS.03.30.210.4a	UMS_Cure Concrete - Roof Deck and Pile Cap - South Headwall to Sta 134+26				
UMS.07.14.200.4a	UMS_Install Waterproofing System/Grout Cover - Roof Deck and Pile Cap - South Headwall to Sta 134+26				
UMS.31.50.0560	UMS_Install Spiling @ Access Shaft #2 & Excavate to Pile Face				
UMS.31.32.0070	UMS_Jet Grout Under South Wall Footings - USSG				
UMS.31.20.205.4a	UMS_Backfill Roof Section To Subgrade & Move Skid - South Headwall to Sta 134+26				
UMS.03.30.105.1a	UMS_Place Concrete - Roof Deck and Pile Cap - North Headwall to Sta 131+26				
UMS.03.30.110.1a	UMS_Cure Concrete - Roof Deck and Pile Cap - North Headwall to Sta 131+26				
UMS.31.20.102.3b	UMS_Excavate To Compensation Grout Level - Sta 132+70 To Sta 133+06				
UMS.31.20.102.4a	UMS_Excavate To Compensation Grout Level - South Headwall To Sta 134+26				

Schedule: Contract 1300 December 2015 Update

Yerba Buena/Moscone Station

Contract 1300 - Work Package 1255



Description of Work

This Work Package is to construct one subway station. Includes station finishes, AC and DC Traction Power substations elevators, escalators, lighting, emergency ventilation fans, HVAC fire alarm/ suppression/ protection, slurry wall top-down construction, settlement monitoring, building protection, PA, CCTV, signage, installation of fare collection equipment and station start-up and commissioning.

Current Status

- Traffic flowing on east side of 4th Street with west side work area closed for construction
- Station box level 4 temporary bracing installation is in progress beneath Mezzanine level
- Temporary bracing is being installed within tunnel to provide support during installation of Concourse level beams and slab.
- Headhouse level 4 temporary bracing installation has begun
- Archaeological monitoring by spot checking is in progress



Work Expected Next Month

- Continue excavation and temporary bracing installation in headhouse and station box, including beginning excavation to allow Concourse level slab installation
- Station box interior walls: rebar for type C wall on Mezzanine level GL 05-08
- Install in slab drains Concourse sector 1
- Continue utility installation on 4th Street north of the north headwall and on Folsom Street

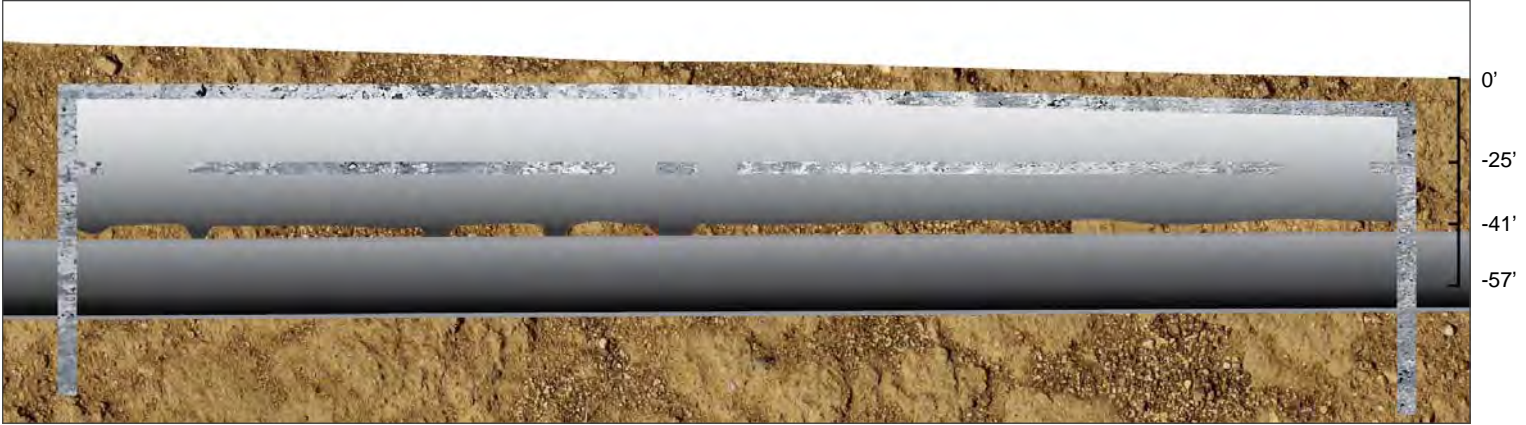
Three Month Look Ahead

- Complete Station Concourse Level Slab Pours;
- Remove Station Level 4 Struts;
- Excavate station and headhouse to Temp Strut Level 6
- Install in slab drains Concourse sector 2
- Utility installation at intersection of 4th Street with Howard Street and Folsom Streets

Station Excavation and Construction Progress Section

North

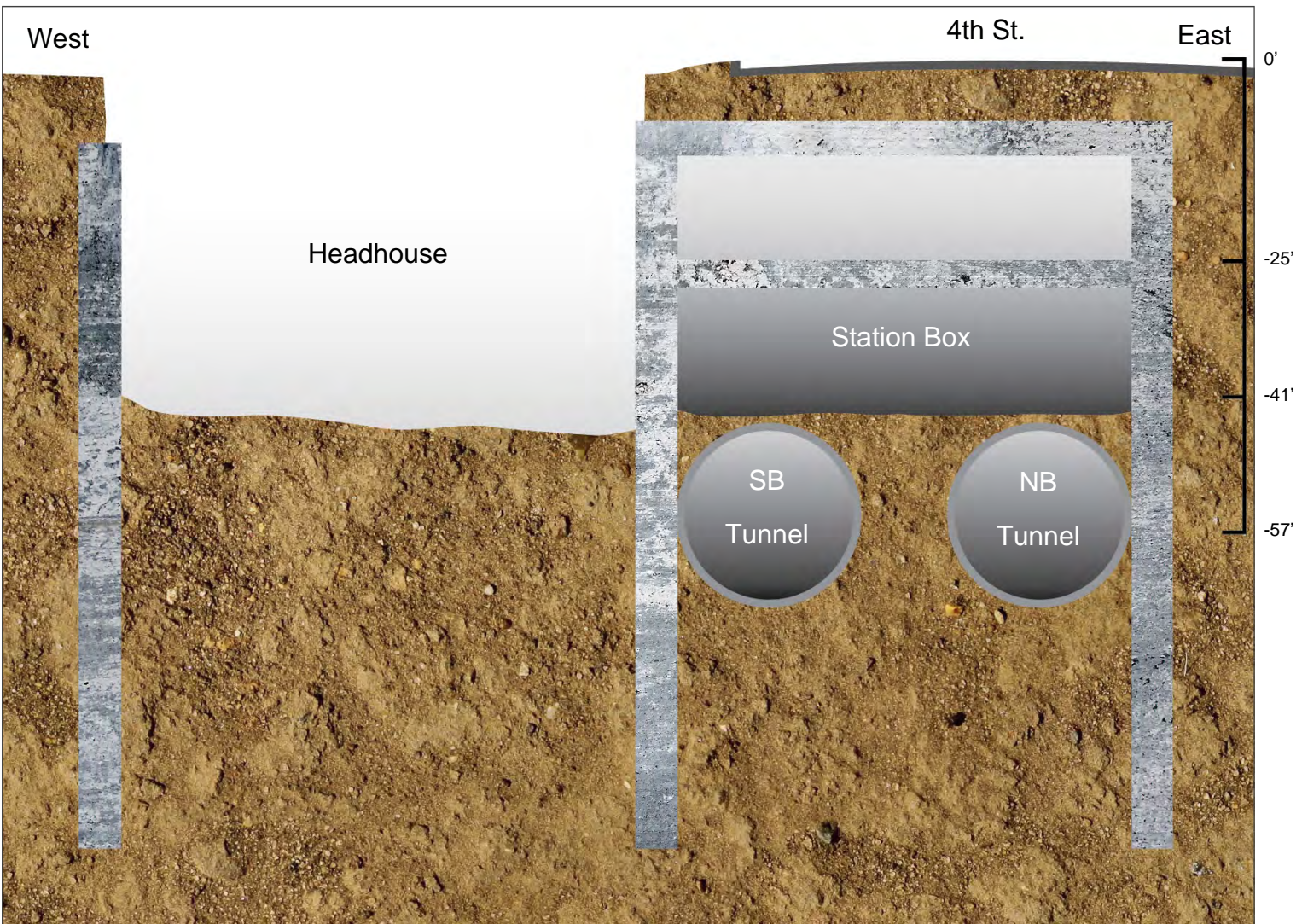
South



West

4th St.

East

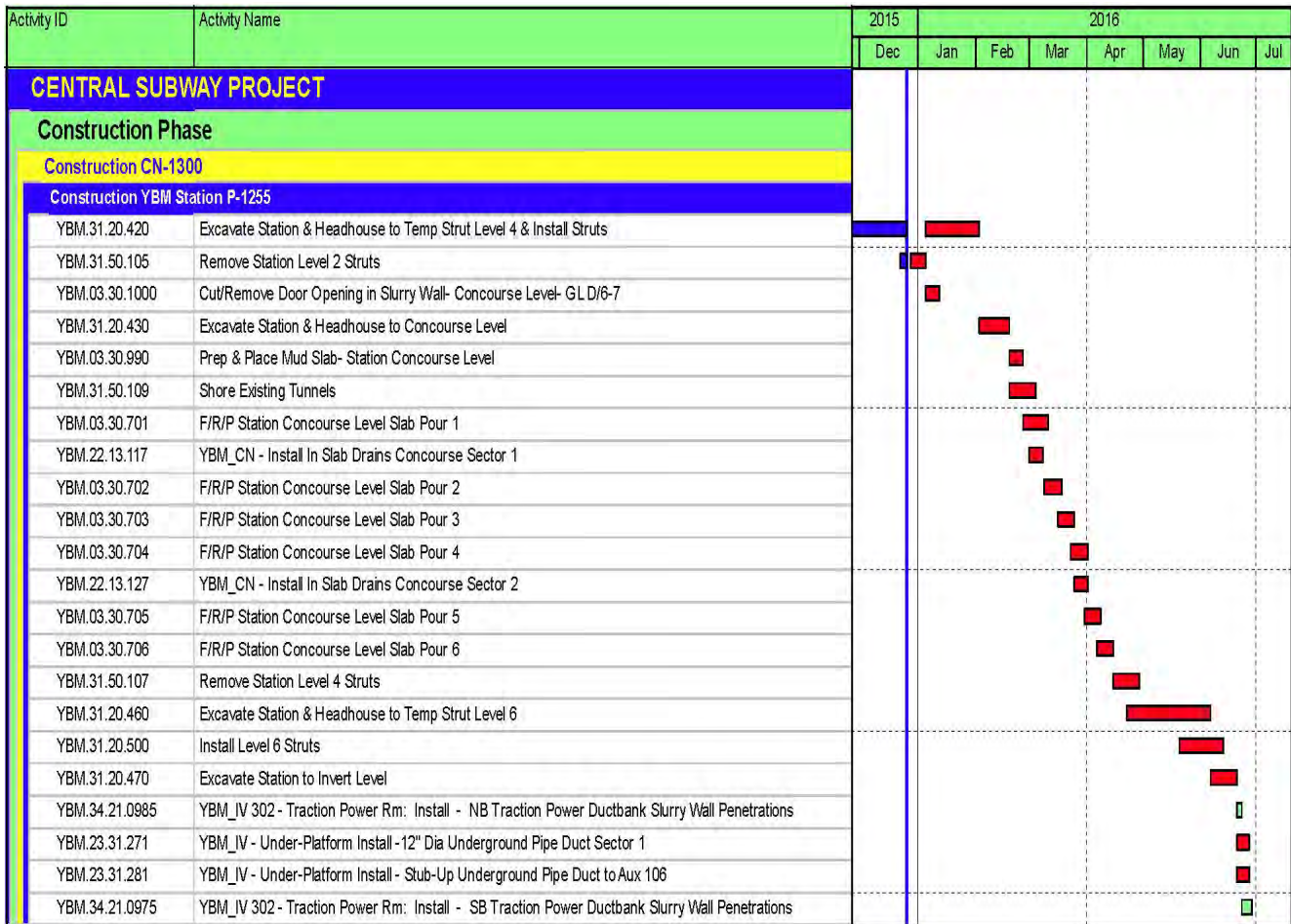


Yerba Buena Moscone Station Construction - Continued

Contract Details	
Contract Awarded:	May 21, 2013
Notice to Proceed:	June 17, 2013
Substantial Completion:	February 10, 2018
Contract Award Value:	\$158,089,000
Modifications to Date:	(\$1,581,215)
Current Contract Value:	\$156,507,785

Budget/Expenditures ▲	
Current Budget	\$163,089,000
Expenditures to Date	\$81,903,977

YBM Three Month Schedule



Schedule: Contract 1300 December 2015 Update

Systems, Trackwork, & Surface Station

Contract 1300 - Work Package 1256

Description of Work

This Work Package is to construct one Surface Station. Includes light rail track and systems, track invert, track safety walkways; light rail track and systems constructed on the 2,000 foot surface for the alignment from the tunnel portal, south to the tie-in to the existing Muni T-Line at Fourth and King Streets; and the surface Fourth and Brannan Street (FBS) Station.

Current Status

- Continue Muni ductbank installation
- Continue 78" sewer rehabilitation
- Continue 36" sewer force main
- Continue Auxiliary Water Supply System (AWSS) installation
- Continue water line work
- Started AT&T cutover
- Continue tunnel prep work
- Continue 4th/King sewer structure modification
- Continue OCS pole foundation installation



Work Expected Next Month

- Continue 78" sewer rehabilitation on west side of 4th St.
- Continue MRY ductbank and vault installation at 4th/Freelon
- Completed 36" force main at intersection 4th and Townsend and continue work north of Townsend
- Continue 4th/King sewer structure modification
- Continue tunnel prep work
- Continue OCS pole foundation installation
- AT&T on site working on cutover

Three Month Look Ahead

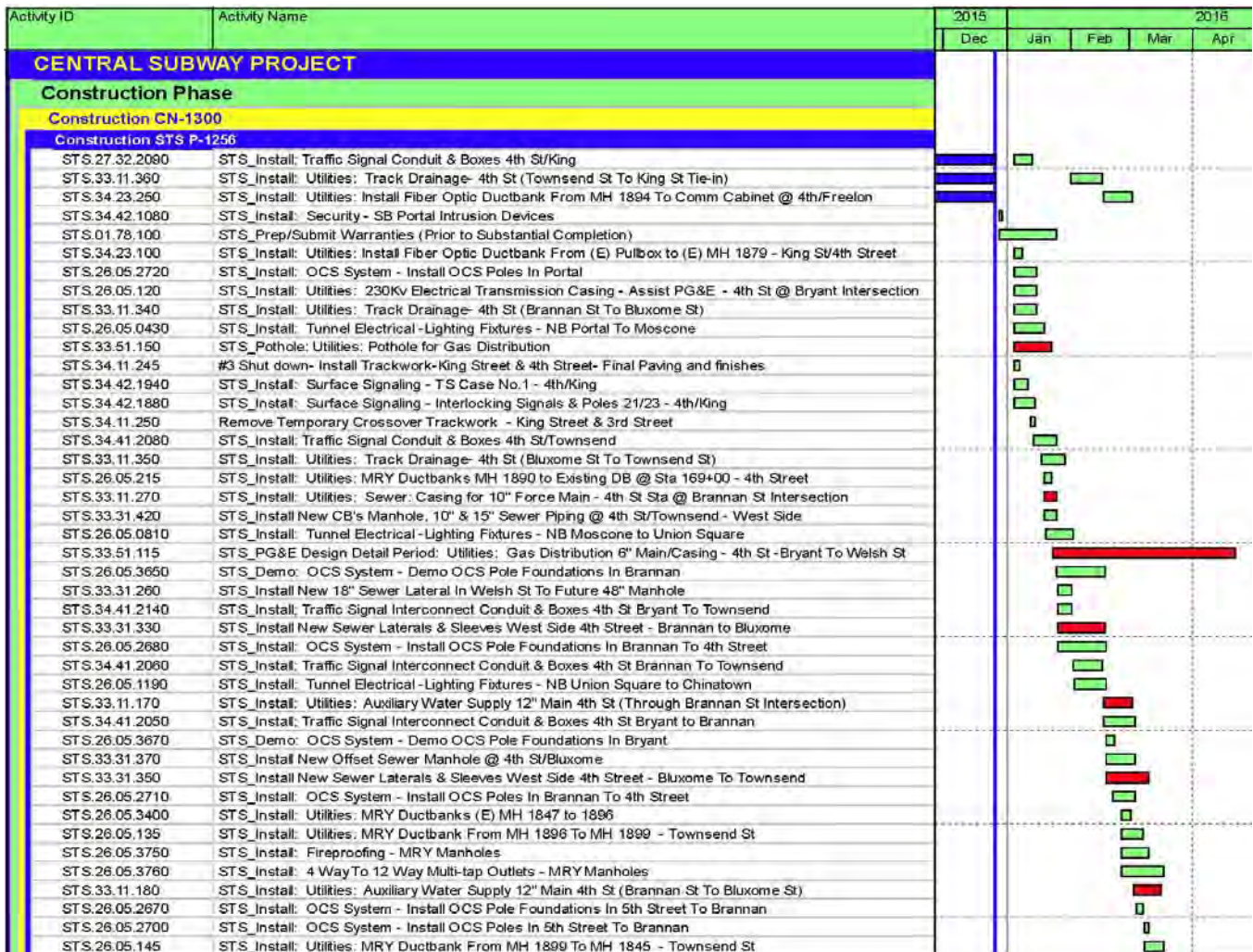
- Waterline installation
- AWSS installation
- Muni ductbank installation and vault installation
- 36" sewer force main installation
- 78" sewer rehabilitation
- 4th/King sewer structure modification
- OCS pole foundation installation
- Tunnel drainage system installation
- Tunnel invert slab construction
- Tunnel walkway construction
- AT&T cutover

Systems, Trackwork, & Surface Station Construction - Continued

Contract Details	
Contract Awarded:	May 21, 2013
Notice to Proceed:	June 17, 2013
Substantial Completion:	February 10, 2018
Contract Award Value:	\$139,989,000
Modifications to Date:	\$1,337,680
Current Contract Value:	\$141,326,680

Budget/Expenditures	
Current Budget	\$144,989,000
Expenditures to Date	\$36,727,200

Systems, Track and Surface Station Three Month Schedule



Schedule: Contract 1300 December 2015 Update

Program Components

Community Outreach

Outreach public information, events and presentations for November 2015 include:

Community Advisory Group Meeting

Prepare and disseminate monthly Chinatown Station Construction Update

Distributed night work construction notices to residents and merchants regarding work at 4th and Townsend

Provided update to BOS report

Finalize Chinese newsletter

Responded to merchant complain with dirt/mud on visual windows

Outreach in Support of Mitigation and Monitoring

Team members participated in weekly progress and traffic meetings to address neighborhood concerns

Outreach and communication efforts continue in Chinatown, Union Square, and SOMA

Weekly photo documentation of project work and editing

Weekly construction update emails sent to list of approximately 700 residents and stakeholders

Distributed monthly construction update to STS and CTS neighborhood; CTS flier are bi-lingual

Media Coverage

Central Subway Media Coverage			
Date	Title (with link to story)	Source	Reporter/Writer
2/11/16	T-Third Mission Bay Loop Moves Forward	SF Bay	Jerrold Chinn

Quality Assurance - Continued

Quality Assurance

Quality Assurance monthly activity of oversight, surveillance, audits, proactive feedback and QA records actively involves the Project construction management staff, the resident engineers, the prime construction contractor and their subcontractors.

Stations and Systems Contract CN1300 Quality Assurance Monitoring – On Going

- UMS roof beam to pile WF installation – Installation of steel continues
- UMS and YBM Water Proofing Subcontractor – Waterproofing Experts and substrate and application inspection/acceptance by RDH in lieu of Kingfield Construction Products (water proofing manufacturer) continue
- TPC QC Daily Inspection Reports (which includes TPC’s Specialty Subcontractor’s QC checklists and associated documentation and Smith Emery Inspection Reports)
- Smith Emery - TPC’s laboratory and Inspection Services – including Special Inspections that are also provided via email to the City of San Francisco’s Department of Building Inspection (DBI) for all permitted Work
- Preparatory, Initial and Additional Initial Phase Meetings as required by Specification Section 01 45 00, continue. Attendance/participation of these vital meetings (Contractor is not allowed to start work until such time that all prerequisite requirements have been met which continues to greatly facilitates all work being done to the strict requirements of the Contract Documents, the “first time”). Additional initial phase meetings are conducted, at the request of SFMTA for all concrete placements to ensure that all prerequisite requirements have been met
- Bi-Weekly and ad hoc Additional Initial Phase Meetings, per 01 45 00 Section 1.09, continue for pre-concrete placement (including CDF), bi-weekly Quality Task Force (QTF) Meetings are attended primarily to assist and support the Contractor’s QC effort and to discuss issues related to the identification of in-process items which left un-mitigated manifest into SFMTA not releasing SFMTA Hold Points
- Instrumentation/Monitoring and Construction Management Task Force (CMTF) Meetings continue which now include compaction and jet grouting, monitoring of dewatering at CTS and the ongoing CDF filling for removal of the precast liners at CTS and removal of portions, at the crown tunnel precast liners at YBM
- Bi-Weekly Quality Task Force Meetings – ongoing dialog regarding identification and mitigation of in-process potentially unsatisfactory work, generation of CNCR and other items related to TPC’s QC efforts in implementing TPC’s approved Quality Control Program (QCP)
- Weekly Work Package Progress Meetings for STS, YBM, UMS and CTS and RE/PE Progress Meetings for STS, YBM, UMS and CTS when conducted and as time constraints allow

Document comment and review:

- Contractor’s submittals, e.g., review of welding, concrete and other Quality related submittals/

Quality Assurance - Continued

comments as requested to support the RE's and CM, and RFIs related to quality and welding

- QA Staff continues random/spot checks of the 1300 Contractor's Field Testing lab results; items requiring further action/investigation (missing or inconsistent data, compressive strength results that appear to have a very broad range of values and such) are brought to the attention of the Contractor

QA Issues:

- Assurance that all RFIs, submittals and USE-AS-IS and REPAIR dispositioned CNCRs related to a particular concrete placement, have been approved by the SFMTA REs. Practically, SFMTA REs have imposed a concrete placement hold point for all concrete placements to collectively ensure that the Contractor has performed all work to the requirements of the Contract Documents, i.e., all RFIs, CNCRs and submittals have been approved and acceptably executed.

QA Concerns:

- SFMTA's provision of advance notification to TPC/TPC QC, of in-process work that appears to be deficient or of questionable nature.
- Necessity of using both Reinforcing Steel Design Drawings and approved Reinforcing Steel Shop Drawings to inspect/accept rebar placement.
- Approved submittals for UMS structural steel are awkward, at best, for TPC Production, TPC QC and RE's QA Inspectors to ensure that all work is performed as required by AWS D1.1 and our Contract Documents. TPC QC Engineers review TPC QC CWI's (Smith Emery provides the TPC QC Inspections) welding inspection documentation to verify that all welds are accounted for and accepted prior to concrete placement (see next item). One, complete and comprehensive submittal continues to be off concern and, as identified by the UMS RE at the Contract Package weekly Progress Meeting, continues to be overdue.
- Welding inspection and associated documentation of CWI acceptance of all welded joints, including tack welding, at UMS.
- SFMTA CSP Field Notifications at UMS continues as a topic of discussion at the bi-weekly Quality Task Force Meetings.
- De-certification of waterproofing installer, Water Proofing Experts, by Kingfield, waterproofing system manufacturer and documentation (potential lack of) issues associated with the required warranty from the waterproofing manufacturer(s)
- Systems Special Trackwork manufacturing and circuit grounding issues

Other Program QA Practices Implemented

- Close-out of Corrective Action Requests: Close outs continued from Quality Assurance staff's Audits, Surveillances and PMOC Quarterly Reviews. The status is tracked in the Corrective Action Log that is provided to the project team and the FTA PMOC
- Quality Assurance Surveillance QAS 071, follow-up to QAS 063, verification that certain TPC Production supervisory personnel were indoctrinated to TPC's QCP was conducted and

Quality Assurance - Continued

posted to CM13 (results were satisfactory)

- As report last month, Quality Assurance Surveillance QAS 072, Project Record Documentation (AS-Built) was conducted, posted to CM13 and findings provided via letter to TPC for their action regarding discrepancies with the Contract Document requirements. The follow-up Surveillance was conducted and the associated report will be completed and posted to CM 13 mid-March 2016



Risk Management

Risk Mitigation Management Meeting No. 79 was held on February 4, 2016. The Risk Assessment Committee reviewed and discussed Risks that include Construction Risks with ratings above 6; Remaining Requirement and Design Risks; and, any New Risk Assessment and Mitigations identified to date.

Program Safety & Security

Program Safety & Security

The San Francisco Municipal Transportation Agency is committed to the highest practical level of safety and security standards and practices in the public transit industry. The Safety and Security Management Plan (SSMP) components are reported on below as appropriate including, Safety and Security Committee, the Fire Life Safety and Security Committee the Construction Conformance Verification and Documentation and Contractor Safety and Security.

Project Management/Construction Management (PMCM) Team

Safety bulletin boards have subjects covering the complacency and daily job briefings .

Cal-OSHA Mining and Tunneling conducted their bi-monthly inspection. They found several items that included:

1. At the Layne jet grouting plant on the north concourse, an electrical panel was found to be not fully secured from a Lock Out Tag Out (LOTO) procedure.
2. At Layne's silo at UMS south, the motors under the silos were improperly guarded and thus had an opening that a worker could insert a finger into.
3. At CTS, one of the fire extinguishers was tied to its' stand by means of tie wire
4. At CTS, a worker was observed wearing a harness and using a lanyard, however there was a knot in the lanyard.

All of the above items were corrected.

Safety Summary for the 1300 Stations Systems Track Construction Package

During the month of February, safety work on the 1300 contract progressed as follows:

STS had no recordable or first aid incidents. Valverde continued with work on 4th street. They had good use of trenching procedures and followed all regulations.

At YBM, no injuries or incidents occurred. The contractor cut openings in the top of the tunnels so the platform could be formed and poured. Good communication and procedures were used to ensure that no one was inside the tunnel under the areas being cut.

UMS did not have any incidents during the last month. The contractor was mucking and hauling spoils out of the hole at Geary and Stockton Streets. Traffic control issues were addressed and flaggers and/or PCOs were placed there to help control and direct the public out of the traffic patterns being used.

CTS had one first aid injury. A worker got her hand caught in between two items and pinched her finger. Originally, it was thought that it was broken however at the clinic, it was determined not be broken, only pinched. She returned the work the next day with no restrictions. Frontier-Kemper worked on the barrel vault during the month. No other incidents occurred.

Program Safety & Security - continued

CTS continue to excavate the head house area. Frontier Kemper installed bulkheads in the tunnels and has filled them. They are starting to install the ventilation system in preparation for excavation to commence.

Next Month Look Ahead

1300 Contract

1. AT CTS, Frontier-Kemper (FK) is getting close to starting the SEM excavation. We will closely watching this process. We will be looking for sampling to be conducted if large amounts of dust are created.
2. At UMS, we are watching the contractor work in placing roofs on the station and both jet and compensation grouting. Due to the rain coming in, we will be watching how the contractor controls the rain runoff.
3. AT YBM, we are watching air flow to ensure that the work in the lower levels of the station is properly ventilated while work is being performed.
4. At STS, work is supposed to commence on the invert starting at the north end and working south in both tunnels.

Program Safety & Security - continued

Project Safety Record - Contract 1300

SAFETY GOALS

Through Month End - Feb 2016

OSHA Recordable Accidents, <3.4 Lost Time Cases, <1.6
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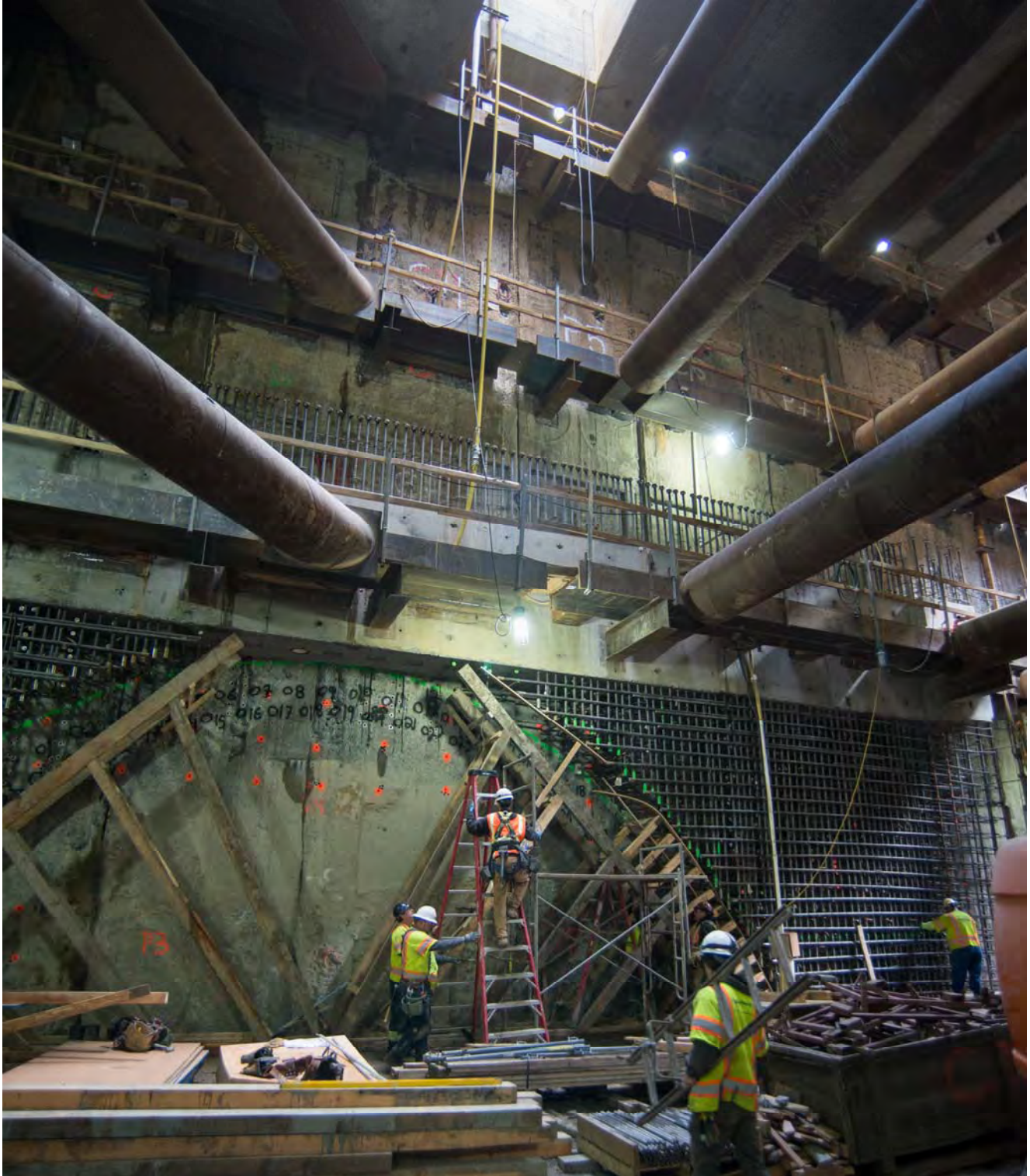
JOB TO DATE	Tutor	Subs	Total Project	Rate*
OSHA Recordable Accidents	3	0	3	0.62
Job Transfer or Restricted Duty Cases	0	0	0	0.00
Lost Time Cases	0	0	0	0.00
Total Project Incidents	3	0	3	0.62
Man Hours Worked Through M/E Feb 2016	453,066	512,558	965,624	

YEAR TO DATE (Month ,Day, Year to Month, Day, Year)	Tutor	Subs	Total Project	Rate*
OSHA Recordable Accidents	0	0	0	0.00
Job Transfer or Restricted Duty Cases	0	0	0	0.00
Lost Time Cases	0	0	0	0.00
Total Project Incidents	0	0	0	0.00
Man Hours Worked Through M/E Feb 2016	51,351	35,713	87,064	

* Rate is calculated based on number of incidents divided by total number of man hours worked multiplied by 200,000 man hours.
 OSHA Recordable Accidents - 2008 Construction Industry Rate for Highway, Street, and Bridge Construction = 3.9

Technical Capacity

No Project positions are currently open or unfilled.



Light shines in an access shaft sixty feet above an archway structure being created inside the Chinatown Station headhouse.

Staffing

The Central Subway Staffing Table shows Planned and Actual full-time equivalent staff (FTEs) working on the Program by organizational function and responsibility.

	Dec-2015		Jan-2016		Feb-2016	
	Planned	Actual	Planned	Actual	Planned	Actual
Project Management						
Program Management	7.10	6.00	6.60	6.00	6.60	6.00
Quality Assurance	1.80	2.30	1.80	2.30	1.80	2.30
Contract Administration	1.40	1.40	1.40	1.40	1.40	1.40
Community Outreach	5.50	5.50	5.50	5.50	5.50	5.50
Finance	2.00	2.00	2.00	2.00	2.00	2.00
Project Controls	5.80	5.80	4.80	5.80	4.80	5.80
Subtotal	23.60	23.00	22.10	23.00	22.10	23.00
Construction Management						
CM - CN 1252	1.00	1.00	1.00	1.00	1.00	1.00
CM - CN 1300	30.00	31.00	30.00	31.00	30.00	31.00
Design Support - CN 1252	0.00	0.00	0.00	0.00	0.00	0.00
Design Support - CN 1300	12.40	12.40	12.40	12.40	12.40	12.40
Subtotal	43.40	44.40	43.40	44.40	43.40	44.40
Start Up						
Start Up / Safety & Security	3.00	2.10	3.00	2.10	3.00	2.10
Subtotal	3.00	2.10	3.00	2.10	3.00	2.10
Total	70.00	69.50	68.50	69.50	68.50	69.50

Third-Party Agreements

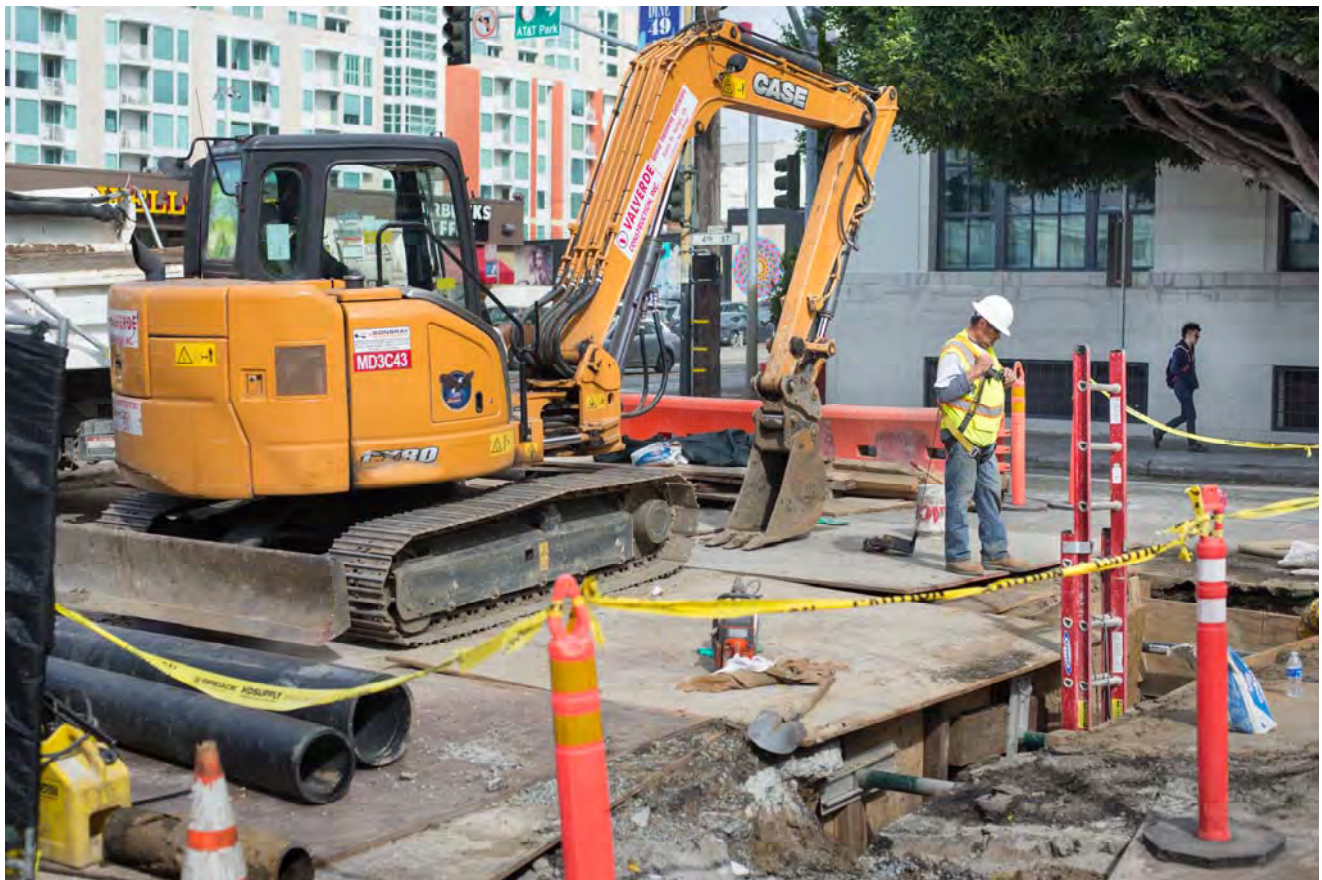
No activity in this reporting month.

LRV Procurement

SFMTA has initiated a new light rail vehicle procurement to acquire up to 260 vehicles over the next 15 years. The scope includes the design, manufacture, delivery and testing of up to 260 light rail vehicles together with associated services, spare parts, special tools, training and documentation. This includes an initial delivery of 24 cars, scheduled for delivery in 2018 to supplement the fleet when the SFMTA's Third Street Phase 2 - Central Subway Project extension opens.

Four out of five Final Design Reviews completed and Prototype testing for selected systems underway.

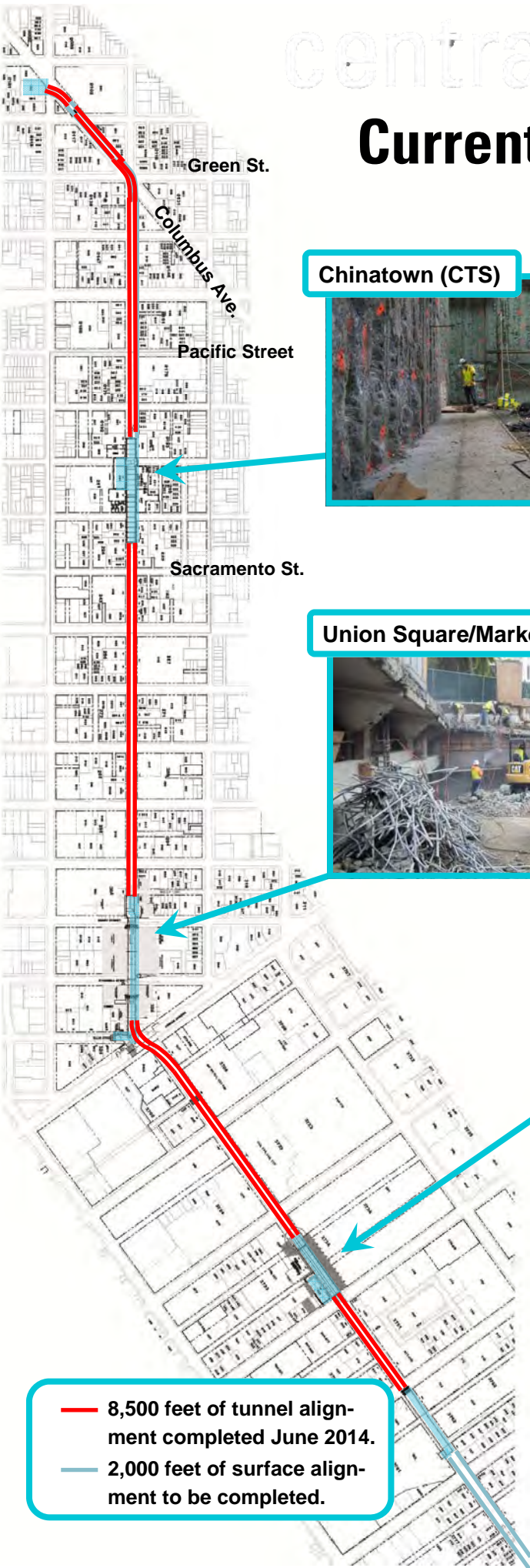
LRV Procurement contract working through integration challenges with parallel projects such as radio replacement project and fare box replacement project. Production of the first carbody structure is underway and the frame of the first half carshell is approaching completion.



Work to reinforce and install new utilities is underway just south of Brannan on the west side of 4th.

central **T** subway

Current Construction Activity



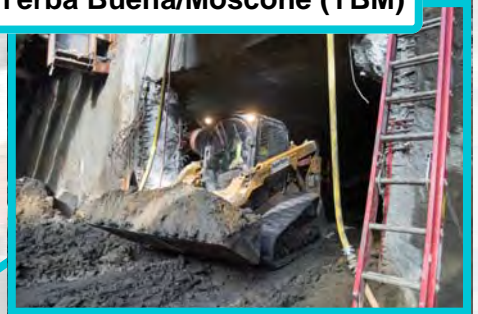
Chinatown (CTS)



Union Square/Market Street (UMS)



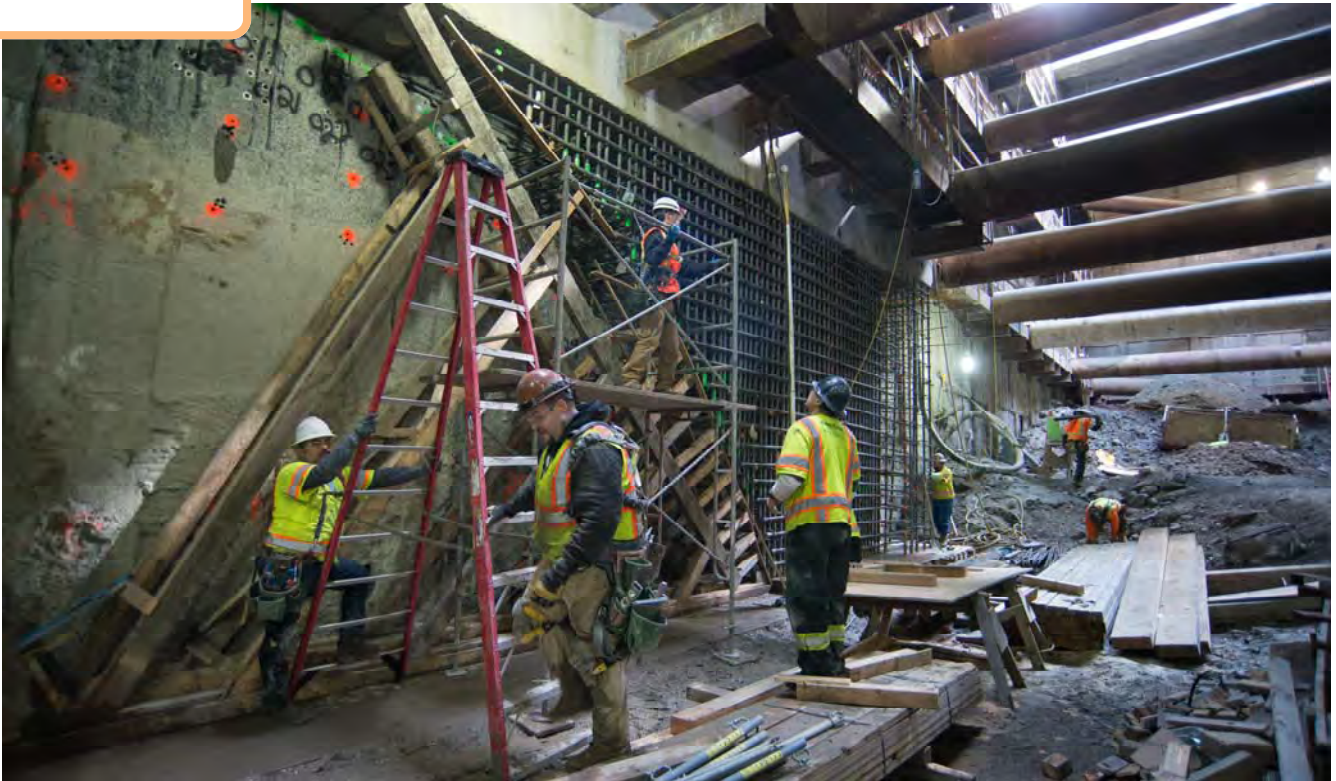
Yerba Buena/Moscone (YBM)



4th St. Surface Track, Systems (STS)



- 8,500 feet of tunnel alignment completed June 2014.
- 2,000 feet of surface alignment to be completed.



Workers erect scaffolding to aid construction of the entrance archway to the future station cavern area.



The entry archway to the station cavern is taking shape. Crews have constructed a reinforced concrete wall which will become part of the permanent station structure.



Heavy equipment waits for the lunch break to end at the bottom of the Chinatown head-house.



Work at the north access shaft between Washington and Jackson is currently on pause for the Chinese New Year.

UMS



Steel pegs are installed intervals on top of the corrugated steel sheeting of the station box roof deck, prior to installing rebar and pouring concrete.



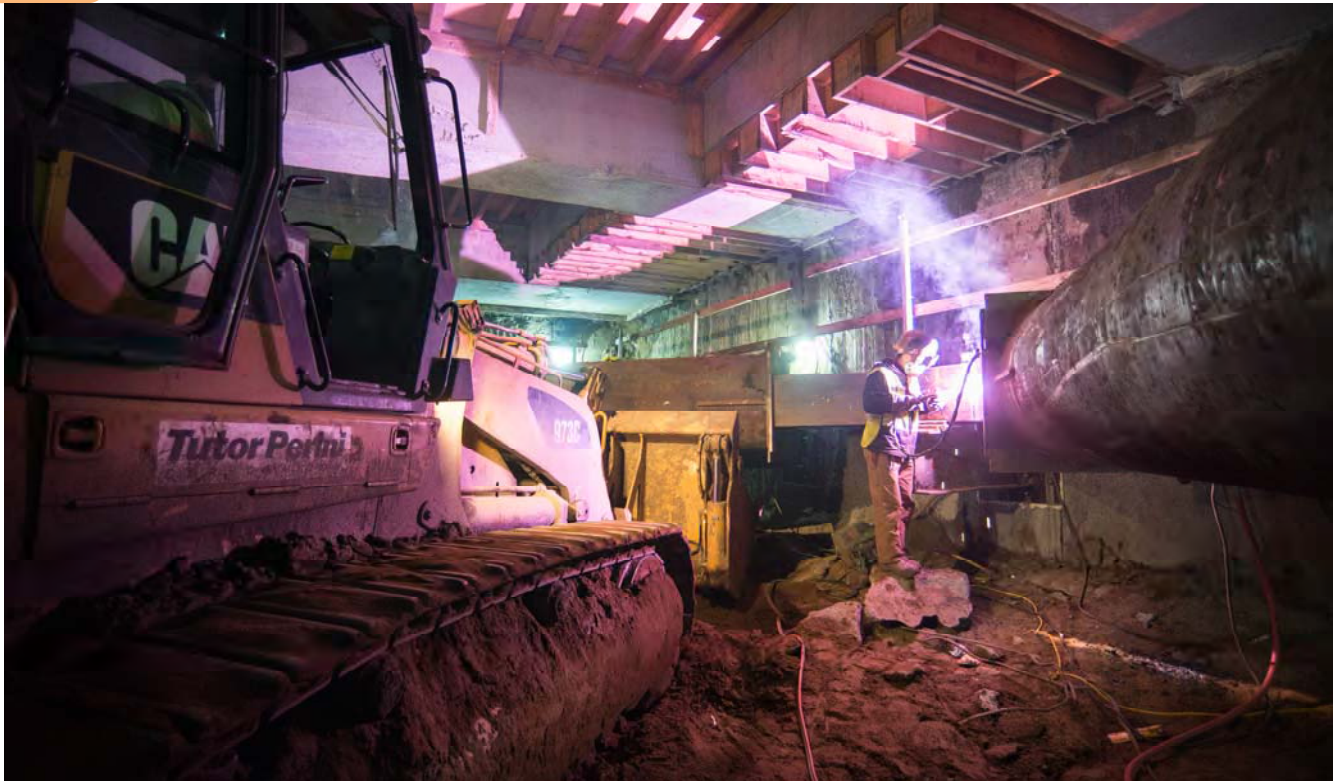
Just south of Geary on Stockton, a small access shaft has been created to a large excavator to lift excavated material to the surface, where it is loaded onto waiting trucks.



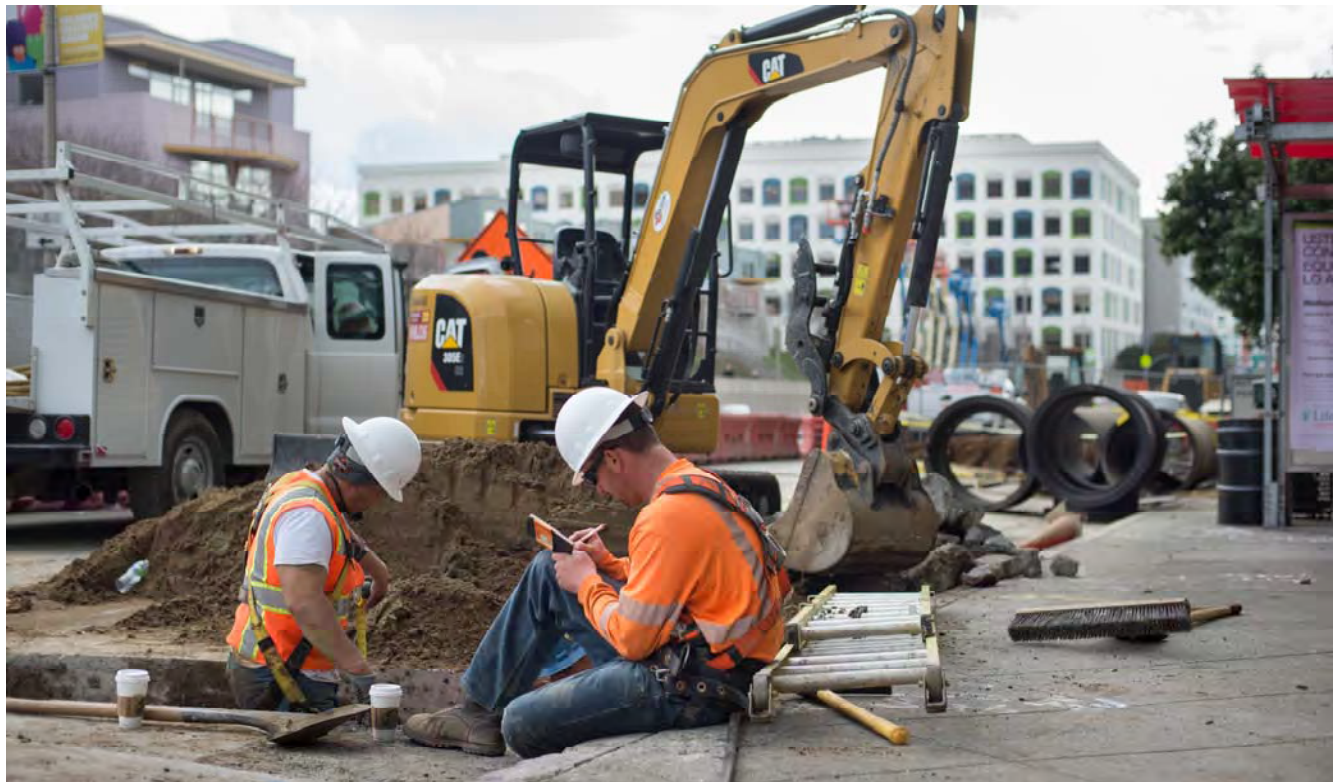
A worker aligns scaffolding for concrete forms on a newly-rebuilt ramp of the Union Square Garage alongside demolition work to prepare for construction of the north station entrance.



A bobcat operator uses a small jackhammer to remove loose material along the east wall at the northern end of the north concourse, while an associate keeps the dust down.



A welder works to attach a large steel strut to temporary supports spanning the north end of the station box under 4th Street.



A worker makes notes at a trench created to locate existing utilities south of Howard on the west side of 4th.

YBM - continued

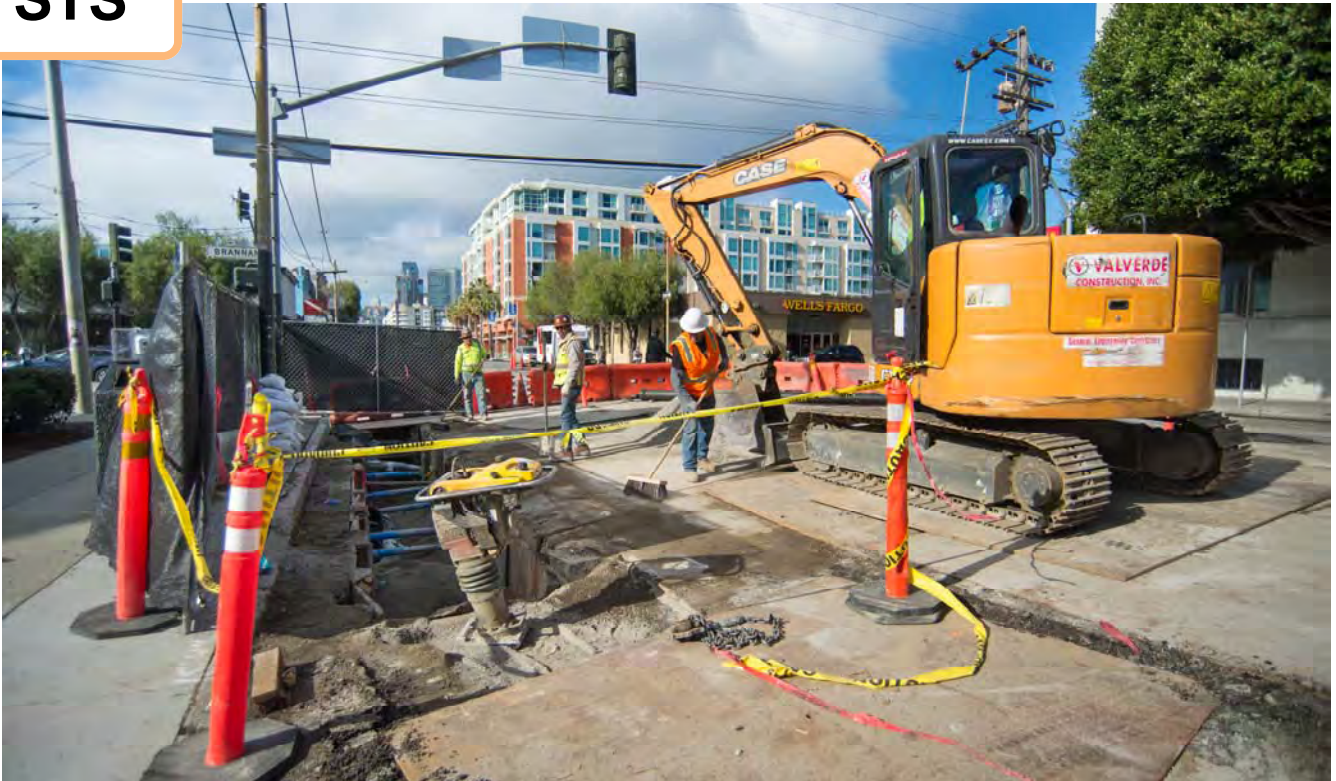


Inside the upper level of the station box, workers use high-pressure water jets to clean surfaces and remove any loose debris before installing rebar and building a finished wall.



A bobcat dumps material at a transfer point inside the station box, where workers are grading the north end to begin installing concrete forms for the mezzanine floor slab.

STS



Workers clean up the work site while conducting backfilling operations just south of Brannan on 4th.



Equipment storage area and sawcutting machine for trenching operations between Brannan and Bluxome.

STS—Continued



A crew works to uncover a utility pipe near Bluxome on 4th Street. Crews are working to relocate and reinforce existing utilities.



An excavator operator observes backfilling work on 4th Street just north of Bluxome.

Appendix A

DETAIL COST REPORTS

1. PROJECT COST

The Current Cost Estimate (CCE) for the Central Subway Project is **\$1.578 billion** in year of expenditure dollars (\$YOE). This total project cost is shown at the top of Report 7.1, Program Project Budget. This capital cost projection incorporates allocated and unallocated contingencies to cover the risks associated with the project completion.

Total net incurred costs for the project are \$907.95 million, a \$14.25 million increase over last month. The cost to date figure reflects expenditures through FAMIS 786 Report (\$867.68 million) plus the utilities joint trench Form B Reimbursement payment (\$10.52 million), invoices currently being processed (\$27.21 million) and estimates of outstanding pay requests (\$2.54 million). This incurred amount equals 57.53% of the total project budget of \$1.578 billion.

The current funding level to date is \$1,179.79 million. This represents 75% of the total project budget.

CONTRACT	PP NO	PP PERIOD TO	PROG PYMT AMOUNT
CS155.1	52	3/31/2014	\$ 26,671.53
CS155.1	53	4/30/2014	\$ 19,500.26
CS155.1	54	5/31/2014	\$ 29,166.76
CS155.1	55	6/30/2014	\$ 22,666.39
CS155.1	56	7/31/2014	\$ 36,602.38
CS155.1	57	8/31/2014	\$ 40,273.52
CS155.1	58	9/30/2014	\$ 37,269.95
CS155.1	59	10/31/2014	\$ 30,018.32
CS155.1	60	11/30/2014	\$ 20,183.22
CS155.1	61	12/31/2014	\$ 18,828.97
CS155.1	62	1/31/2015	\$ 19,921.99
CS155.1	63	2/28/2015	\$ 19,921.99
CS155.1	64	3/31/2015	\$ 15,554.73
CS155.1	65	4/30/2015	\$ 15,550.60
CS155.1*	66	5/31/2015	\$ 2,121.00
CS155.1*	67	6/30/2015	\$ 1,180.00
CS155.1*	68	7/31/2015	\$ 7,137.00
CS155.1*	69	8/31/2015	\$ 1,805.00
CS155.1*	70	9/30/2015	\$ 1,583.00
CS155.1*	71	10/31/2015	\$ 3,186.00
CS155.1*	72	11/30/2015	\$ 203.00
CS155.1*	73	12/31/2015	\$ 7,695.00
CS155.1*	74	1/31/2016	\$ 12,080.00
CS155.1*	75	2/29/2016	\$ 12,080.00
CS155.2	67	9/30/2015	\$ 212,164.50
CS155.2	68	10/31/2015	\$ 307,824.44
CS155.2*	69	11/30/2015	\$ 205,020.00

CONTRACT	PP NO	PP PERIOD TO	PROG PYMT AMOUNT
CS155.2*	70	12/31/2015	\$ 240,756.00
CS155.2*	71	1/31/2016	\$ 175,376.00
CS155.2*	72	2/29/2016	\$ 175,376.00
CS155.3	63	6/30/2015	\$ 102,156.72
CS155.3	64	7/31/2015	\$ 46,704.85
CS155.3	65	8/31/2015	\$ 38,423.13
CS155.3*	66	9/30/2015	\$ 81,258.00
CS155.3*	67	10/31/2015	\$ 68,106.00
CS155.3*	68	11/30/2015	\$ 190,717.00
CS155.3*	69	12/31/2015	\$ 87,013.00
CS155.3*	70	1/31/2016	\$ 63,057.43
CS155.3*	71	2/29/2016	\$ 63,057.43
Contract 1252	39	4/30/2015	\$ 1,174,889.06
Contract 1252	40	1/0/1900	\$ 236,920.00
Contract 1300	25	1/31/2016	\$ 8,789,129.00
Contract 1300*	26	2/29/2016	\$ 13,022,920.00
CS149	82	11/30/2015	\$ 437,084.46
CS149*	83	12/31/2015	\$ 500,000.00
CS149*	84	1/31/2016	\$ 500,000.00
CS149*	85	2/29/2016	\$ 500,000.00
CS156.1	56	9/30/2015	\$ 83,624.15
CS156.1	57	10/31/2015	\$ 74,006.83
CS156.1	58	11/30/2015	\$ 64,665.02
CS156.1	59	12/31/2015	\$ 75,523.93
CS156.1*	60	1/31/2016	\$ 69,532.98
CS156.1*	61	2/29/2016	\$ 71,225.68
other accruals*		2/29/2016	\$ 1,698,117.41

* Estimated Amount \$ 29,754,043.72

2. CONTINGENCY ALLOCATIONS AND USAGE

The current Total Project Contingency is **\$83.03 million**, which is a \$23.03 million favorable balance against the current Minimum Contingency level of \$60 million. The Contingency Drawdown Curve is shown in Report 7.3. Follows by Report 7.4 Contingency Management

Trend Report with the Remaining Contingency after Approved Changes Deducted contingency items in column "I".

In this reporting period, Contract 1252 Tunnel and Contract 1300 Station have not processed any contract modifications. Refer to Report 7.5 for approved contract modifications and potential changes.

3. **BUDGET TRANSFERS**

No budget transfers in this reporting period.

4. **FORM B**

The Utilities Joint Trench Form B Details is listed in the Table A2 below. Total utilities joint trench Form B Reimbursement payment to three construction contracts is \$10.52 million.

TABLE A2: UTILITIES JOINT TRENCH FORM B DETAILS	[A] Mar 2015 BUDGET	[B] EXPENDED TO DATE	Associated Cost Account
1.3.491.07.040.02 - FORM B - CN1250 UTILITY REIMBURSEMENT	(2,275,419)	2,463,325	1.3.081.07.040.02 - 1UTL:SITWORK: UTILITIES & RELOC
1.3.491.08.040.02 - FORM B - CN1251 UTILITY REIMBURSEMENT	(7,618,412)	3,608,217	1.3.082.08.040.02 - 2UTL:SITWORK:UTILITIES&RELOCATE
1.3.491.02.040.02 - FORM B - CN1252 UTILITY REIMBURSEMENT	(254,050)	3,958,658	1.3.083.02.040.02 - TUNN:Sitework:Utilities & Relocate
1.3.491.04.040.02 - FORM B - CTS: CN1300 UTILITY REIMBURSEMENT	(451,703)		
1.3.491.09.040.02 - FORM B - STS: CN1300 UTILITY REIMBURSEMENT	(1,000,000)		
1.3.491.03.040.02 - FORM B - UMS: CN1300 UTILITY REIMBURSEMENT	(528,370)	428,712	1.3.084.03.040.02 - UMS.1253: SITE UTILITIES, UTILITY RELOCA
1.3.491.05.040.02 - FORM B - YBM: CN1300 UTILITY REIMBURSEMENT	(100,000)	56,451	1.3.086.05.040.02 - YBM.1255: SITE UTILITIES, UTILITY RELOCA
TOTAL	(12,227,954)	10,515,363	

5. **EARNED VALUE (EV) ANALYSIS**

In February 2016 Report, Central Subway Project Master Schedule has used Contract 1300 Station contractor baseline schedule and rejected February monthly update schedule. Contractor has not addressed the cost issues in cost loaded schedule. Preliminary February Earned Value Analysis reports the Planned Value, Earned Value, Percent Complete and resulting indexes as follows:

Preliminary February Earned Value

Overall Budgeted Cost:	\$1,578,300,000
Planned Value:	\$1,174,792,723
Earned Value:	\$936,198,133
Actual Cost:	\$907,951,763
Schedule Performance Index (SPI):	0.80
Cost Performance Index (CPI):	1.03
Percent Complete:	59%

SFMTA, EV Chart											
FEBRUARY 29, 2016 Update											
ID:CSP-CMPS-YY-20..., Proj Name:3-SFMTA Central Subway Project - January 2016 Update CN 1252...											
Layout:AA 01.MPR.EV.CHART											
02/26/16, 03/11/16,15:43											
Activity ID	Activity Name	Start	Finish	Performance % Complete	Budgeted Total Cost	Planned Value Cost (PV)	Earned Value Cost (EV)	Actual Total Cost (AC)	CPI	SPI	Revision
	CENTRAL SUBWAY PROJECT	06/03/03 A	07/08/21	59.28%	1,578,300,000.86	1,174,792,723.39	936,198,133.04	907,951,763.00	1.03	0.80	
	Preliminary Engineering Phase	06/03/03 A	01/07/10 A	100%	46,542,061.02	46,542,061.02	46,542,061.02	46,542,061.02	1.00	1.00	
	Final Design	01/08/10 A	06/17/13 A	100%	115,075,987.06	115,075,987.06	115,075,987.06	114,307,464.66	1.01	1.00	
	Light Rail Vehicles	04/15/13 A	10/10/16	8.25%	26,385,653.00	2,177,131.58	2,177,131.58	2,147,204.00	1.01	1.00	
	Real Estate	08/01/08 A	02/29/16	80.31%	37,405,895.00	32,191,169.33	30,041,759.16	30,540,100.64	0.98	0.93	
	Construction Phase	01/03/10 A	08/31/19	55.22%	1,343,370,948.29	978,806,374.40	742,361,194.22	714,414,932.68	1.04	0.76	
	Construction Support and Costs	01/03/10 A	08/31/19	43.57%	189,862,849.06	87,291,287.91	87,086,391.71	93,613,901.68	0.93	1.00	
	Construction Utility Contract #1- MOS & Portal CN-12	01/04/10 A	05/23/11 A	100%	11,968,150.00	11,968,150.00	11,968,150.00	11,968,150.00	1.00	1.00	
	Construction Utility Contract #2- UMS CN-1251	01/12/11 A	10/15/12 A	100%	20,794,562.00	20,794,562.00	20,794,562.00	20,794,562.00	1.00	1.00	
	Construction Tunnels CN-1252	06/08/11 A	03/01/16	93.05%	251,068,967.23	251,067,366.78	233,608,884.28	234,616,103.00	1.00	0.93	
	Construction CN-1300	06/03/13 A	04/21/19	45.19%	859,676,400.00	607,684,987.71	388,903,176.22	353,422,196.00	1.10	0.64	
	Unallocated Contingency	01/21/19	07/03/19	0%	9,519,456.49	0.00	0.00	0.00	0.00	0.00	
	Project Management	07/03/19	07/08/21	0%	0.00	0.00	0.00	0.00	0.00	0.00	

Contract 1300 shows Earned Value Cost \$36M higher than Actual Cost is due to Tutor Perini Corporation Schedule has remaining cost in completed activities.

Earned Value Analysis and Definitions

SPI is a measure of schedule efficiency on a project. It is the ratio of earned value (EV) to planned value (PV). A SPI equal to or greater than one indicates more work was completed than planned and a value of less than one indicates less work was completed than planned. A value of less than 0.9 is unfavorable.

CPI is a measure of cost efficiency on a project. It is the ratio of earned value (EV) to actual cost value (AC). A CPI equal to or greater than one indicates a cost under run and a value of less than one indicates a cost overrun. A value of less than 0.9 is unfavorable.

The following earning rules are established for each of the phase:

Cost Element Group	Planned Value (Primavera)	Earned Value (Primavera)	Actual Cost (SFMTA Cost Accounting (SAP))
Prelim. Engineering	Expenditure Plan Level of Effort (LOE)	Equals to Planned Value (LOE)	Time Keeping; Vendor Accruals and Invoices
Final Design	Expenditure Plan Level of Effort (LOE)	Equals to Planned Value (LOE)	Time Keeping; Vendor Accruals and Invoices
Procurement	Planned Delivery Date	Actual Delivery Date	Time Keeping; Vendor Accruals and Invoices
Real Estate	Expenditure Plan Level of Effort (LOE)	Equals to Planned Value (LOE)	Time Keeping; Vendor/ Material Accruals and Invoices
Construction	Schedule of Work	% Complete* x Budget at Completion (BAC)	Vendor Accruals and Invoices
Sub-Total	Performance Measurement Baseline (PMB)	Total Earned Value	Total Actual Cost
Below the Line	+ Contingency		
Total	Approved Budget		

6. FUNDING SUMMARY

The Funding Available Table below shows the total awarded funds to date vs. the total committed funds from the Project's seven funding sources.

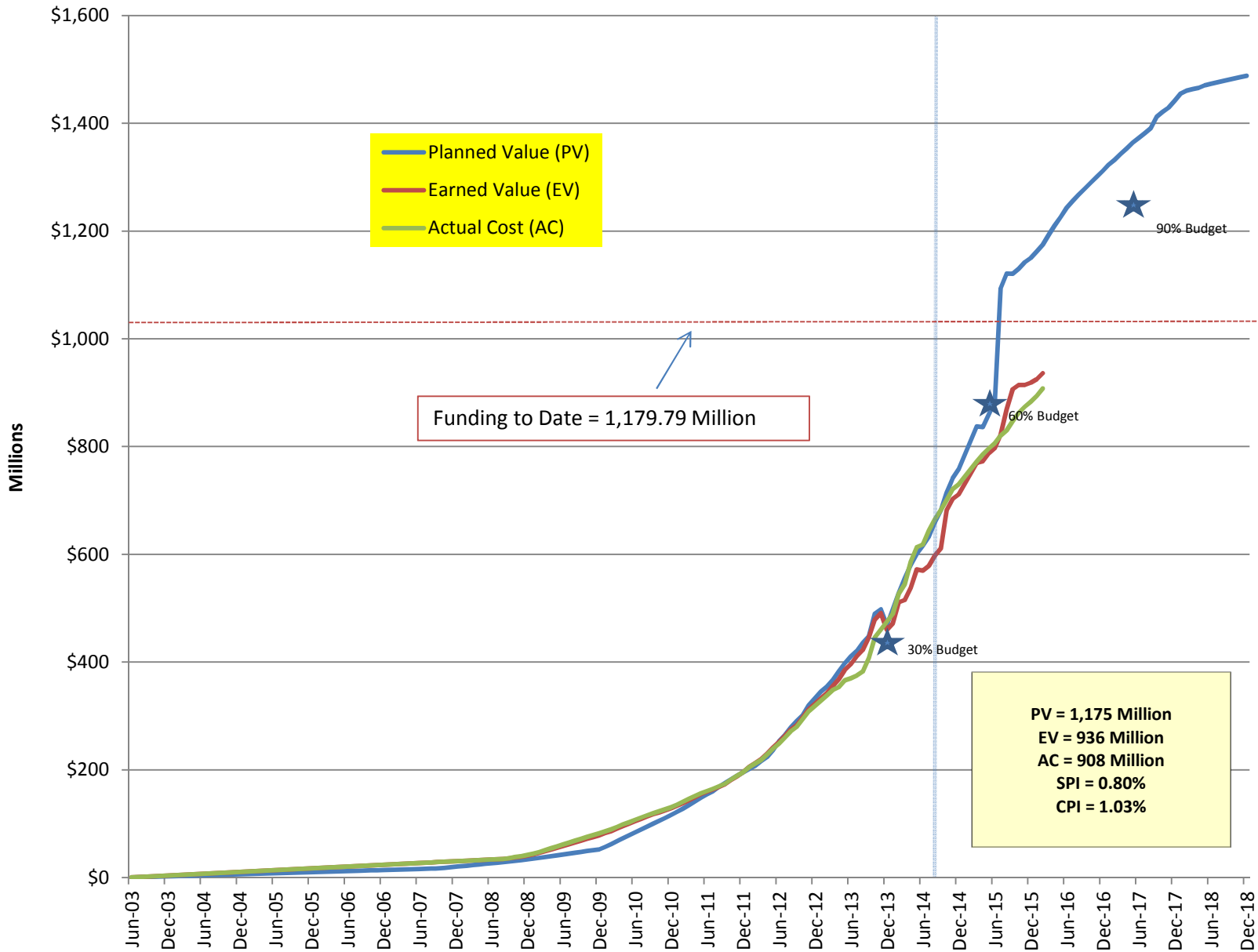
Funding Available Table		
	Funding	
	Committed Funding Sources	Total Awarded Funds to Date
Federal		
Sect. 5309-NS	\$942,200	\$619,196
CMAQ	\$41,025	\$41,025
Federal Subtotal	\$983,225	\$660,221
State		
TCRP	\$14,000	\$14,000
State RIP	\$88,000	\$12,498
Prop. 1B (I-Bond) PTIMSE	\$307,792	\$307,792
Prop. 1A (HSR-Bond)	\$61,308	\$61,308
State Subtotal	\$471,100	\$395,598
Local		
Prop. K	\$123,975	\$123,975
Local Subtotal	\$123,975	\$123,975
CPT 544 Total	\$1,578,300	\$1,179,794

7. LIST OF COST REPORTS

- 7.1 Program Project Budget
- 7.2 Earned Value Cash Flow
- 7.3 Contingency Drawdown Curve
- 7.4 Summary Contingency Management Trend Report
- 7.5 Detail Contingency Usage Report
- 7.6 Budget Revisions: Report sorted by Construction Packages & Soft Costs
- 7.7 Project Budget & Expenditure Report: Sorted by SCC Summary
- 7.8 Budget & Expenditure Report: Sorted by SCC Details
- 7.9 Detail Monthly Expenditure Report: grouped by Project Phase
- 7.10 Cost Report Notes

A. Central Subway Project						
Project	Name	Amount	PM	Funding Source	Reporting	Cost Report Notes
1	CPT544 Central Subway Project	\$1,578,300,000	J. Funghi	62% Fed, 30% State, 8% Local	yes	1
Total:		\$1,578,300,000				
B. Related SFMTA Capital Improvement Projects						
Project	Name	Amount	PM	Funding Source	Reporting	
2	CPT690 TBM Retrieval Shaft Relocation	\$9,700,000	Funghi/Magary	MTA Operating Funds	no	2
3	CPT718 Chinatown Metro Plaza	\$6,980,000	J. Funghi	Transbay Redevelopment	no	3
4	CPT665 Central Subway Project - Goodwill	\$2,367,750	K. Magary	I-Bond Interest	no	4
5	CPT705 MOH - Broadway/Sansome	\$8,000,000	K. Magary	MTA Operating Funds	no	5
Total:		\$27,047,750				
C. Central Subway Project - Project Offset Credits						
	From	Amount	Index	Notes	Reporting	
1	2009-2016 Utility Co. - Form B Reimbursement	\$12,227,954	--	Construction contracts	yes	6
2	2017-2019 PG&E - Power Feed Reimbursement	\$7,624,540	--	Not yet bill PG&E	yes	7
3	6/26/2013 BART Elevator	\$90,000	68CPT544135B	Not yet rec'd BART Funds	yes	8
4	11/6/2013 Tutor Perini - CAD Files	\$2,500	68CPT5441236	Deposit to Design Index	yes	9
5	1/27/2014 SFPUC - Sewer Main	\$2,925,296	68W251	Certified in Contract 1300	yes	10
6	9/27/2014 SFPUC - 24" Water Main	\$328,857	68CPT544135A	Contract 1252 CMod #41	yes	11
7	8/27/2014 SFMTA Traffic Effectiveness Project funded	\$694,651	--	Contract 1252 CMod #40	yes	12
8	3/27/2015 SFPUC - 24" Water Main Additional Work	\$112,102	68W409	Contract 1252 CMod #48	yes	13
Total:		\$24,005,900				

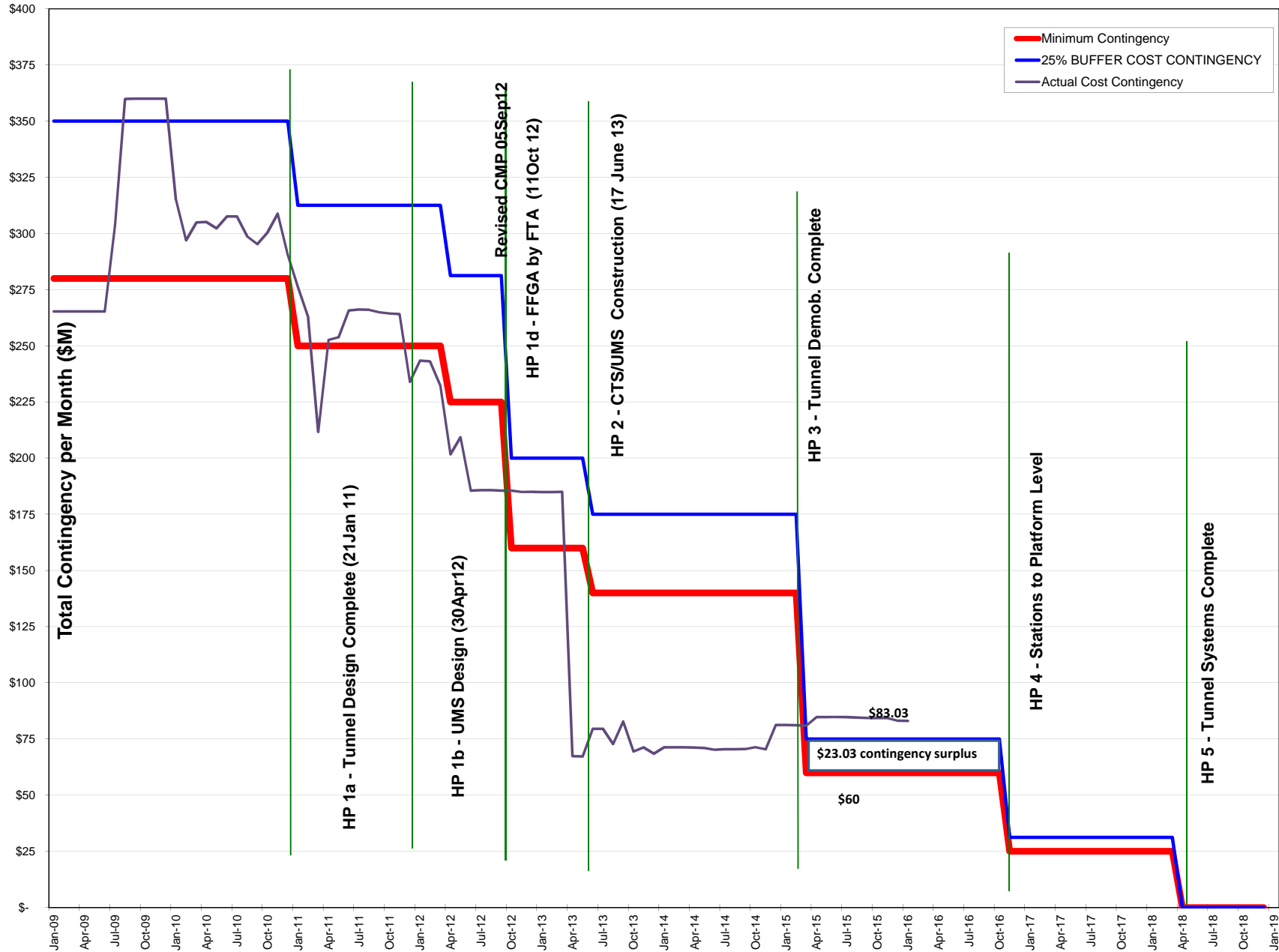
Earned Value Cash Flow Curve



Funding to Date = 1,179.79 Million

PV = 1,175 Million
 EV = 936 Million
 AC = 908 Million
 SPI = 0.80%
 CPI = 1.03%

Cost Contingency Drawdown



COST ELEMENT	CONTRACT COST					CONTINGENCY					BUDGET	VARIANCE	Cost Report Notes	
	ORIGINAL CONTRACT VALUE / September 2013 SUPPLEMENTAL BUDGET	APPROVED CHANGES	CURRENT CONTRACT VALUE	POTENTIAL CHANGES	ESTIMATE AT COMPLETION (EAC)	ORIGINAL CONTINGENCY / Sep 2013 SUPPLEMENTAL CONTINGENCY (Exclude CN 1250 & CN1251)	CONTINGENCY ADJUSTMENT TRANSFERS	REVISED AUTHORIZED CONTINGENCY (Exclude CN1250 & CN1251)	REMAINING CONTINGENCY AFTER APPROVED CHANGES DEDUCTED [h - b]	REMAINING CONTINGENCY AFTER POTENTIAL CHANGES DEDUCTED [i - d]	ORIGINAL CONTRACT VALUE + REVISED AUTHORIZED CONTINGENCY [a + h]	BUDGET - ESTIMATE AT COMPLETE [j - e]		
	a	b	[a + b]	d	[c + d]	f	g	h	i	j	j	k		
SCC 10-50 ONSTRUCTION CONTRACT PACKAGES														
1250	UTILITY RELOCATION PACKAGE #1	9,273,939	2,694,211	11,968,150		11,968,150	1,953,377	740,834	2,694,211		11,968,150		14	
	<i>Contract 1250 Department of Technology</i>	166,756		166,756		166,756					166,756			
1251	UTILITY RELOCATION PACKAGE #2	16,832,550	3,962,032	20,794,582		20,794,582	5,367,297	(1,405,265)	3,962,032		20,794,582		15	
	<i>Contract 1251 Department of Technology</i>	75,615		75,615		75,615					75,615			
1252	GUIDEWAY TUNNEL	233,584,015	1,421,807	235,005,822	(34,969)	234,970,853	23,658,464	(21,173,511)	2,484,953	1,063,145	1,098,114	236,068,968	1,098,114	16
1300	STATIONS	839,676,400	270,956	839,947,356	23,832,796	863,780,152	20,000,000		20,000,000	19,729,044	(4,103,752)	859,676,400	(4,103,752)	17
	<i>1253 UNION SQUARE/MARKET ST STATION [UMS]</i>	294,030,590	111,258	294,141,848	14,507,909	308,649,757	5,000,000		5,000,000	4,888,742	(9,619,167)	299,030,590	(9,619,167)	
	<i>1254 CHINA TOWN STATION [CTS]</i>	247,567,810	403,233	247,971,043	6,120,199	254,091,242	5,000,000		5,000,000	4,596,767	(1,523,432)	252,567,810	(1,523,432)	
	<i>1255 YERBA BUENA/ MOSCONE STATION [YBM]</i>	158,089,000	(1,581,215)	156,507,785	4,244,283	160,752,068	5,000,000		5,000,000	6,581,215	2,336,932	163,089,000	2,336,932	
	<i>1256 SURFACE TRACKWORK & SYSTEMS [STS]</i>	139,989,000	1,337,680	141,326,680	(1,039,595)	140,287,085	5,000,000		5,000,000	3,662,320	4,701,915	144,989,000	4,701,915	
OTHER		31,233,501		31,233,501	0	31,233,501	1,160,000		1,160,000	1,160,000	1,160,000	32,393,501	1,160,000	
	SCC 10 - 50 Construction Sub-total	1,130,842,776	8,349,006	1,139,191,782	23,797,827	1,162,989,609	44,818,464	(21,173,511)	23,644,953	21,952,189	(1,845,638)	1,161,143,971	(1,845,638)	
SCC 60 - 80 SOFT COSTS PACKAGES														
60	ROW, LAND, EXISTING IMPROVEMENTS	36,511,799	(4,265,478)	32,246,321		32,246,321	1,000,000	4,265,478	1,000,000	5,265,478	5,265,478	37,511,799	5,265,478	18
70	VEHICLES	24,108,712	(10,799,712)	13,309,000		13,309,000	2,276,941	10,799,712	13,076,653	13,076,653	13,076,653	26,385,653	13,076,653	19
80	PROFESSIONAL SERVICES	310,518,041		310,518,041		310,518,041	18,221,079		18,221,079	18,221,079	18,221,079	328,739,120	18,221,079	
	SCC 60 - 80 Construction Sub-total	371,138,552	(15,065,190)	356,073,362		356,073,362	21,498,020	15,065,190	32,297,732	36,563,210	36,563,210	392,636,572	36,563,210	
SCC 90	UNALLOCATED CONTINGENCY						3,845,945	20,673,511	24,519,456	24,519,456	24,519,456	24,519,456	24,519,456	20
TOTAL		1,501,981,328	(6,716,184)	1,495,265,144	23,797,827	1,519,062,971	70,162,429	14,565,190	80,462,141	83,034,855	59,237,028	1,578,299,999	59,237,028	
												Total Project Budget	1,578,300,000	21
												Estimate At Completion	1,519,062,971	22
												Variance	59,237,028	23
Note #15 - Adjusted Contract 1252 Guideway Tunnel contingency "column g" to reflect construction contract modifications #20, #40, #41 and \$48 were funded by other funding sources.														

Contract Modification/Trend Log - Contract 1300 Stations

Awarded NTE Amount	\$839,676,400	\$839,947,356
Substantial Completion	2/10/2018	2/10/2018

	UMS	CTS	YBM	STS	COST REPORT NOTES
Potential Changes	\$14,507,909	\$6,120,199	\$4,244,283	(\$1,039,595)	24
Forecasted - Trends	\$4,716,511	\$3,406,406	\$953,192	\$141,130	
Negotiation					
Change of date range to receive art	(\$10,001)				
UMS -Remove AWSS Hydrant at OFarrel	\$8,421				
UMS Powell St. Elevator Site Hazmat	\$26,041				
STS Traffic Signal and SL Changes				\$298,307	
STS Comm and Elec Cabinets Relocati				\$67,221	
CTS-PCC#17 FH at Jade Galore		\$106,350			
YBM Delete Instrument & Monitoring			(\$50,195)		
STS - OCS Pole Changes				\$12,706	
CTS-FACO#39- Soil Testing cmply OAB		\$5,478			
UMS-FACO #32 8" Waterline Conf NDSC	\$73,811				
UMS FACO #31 NDSC Incomplete PGE DB	\$36,980				
UMS-FACO #25 OCS Pole@ Market/Ellis	\$3,349				
UMS-FACO #19 Street Light at Stock.	\$2,361				
STS-FACO #48 Work Related St Lght				\$5,000	
UMS FACO #34 Becho Obstruct J Piles	\$7,435				
UMS FACO #45 Blocked Stubs PG&E	\$7,500				
STS Traction Power Cable Downsizing				(\$117)	
UMS Transfer Instru BART Facilities	\$30,001				
CTS-PCC40 Plaza Surface Slab Pntrtn		\$5,525			
YBM PCC 41 Install #7 Box Clementin			\$29,001		
UMS Sewer Line Conflict	\$744,465				
STS Existing Fuel & Transite in MRY				\$10,000	
YBM COR 64 Buried sheet pile P-7			\$15,001		
YBM COR 116 Archaeological Discover			\$100,001		
USG COR 222 Shaft Wall Connections	\$33,272				
USG COR 223 Wall Footing Clarif.	\$75,001				
USG COR 224 (N) Concrete Wall Conn.	\$76,609				
USG COR 228 Tiebacks at Level X	\$75,001				
USG COR 227 Plaza Lvl. Sequence	\$200,001				
USG COR 225 SOG/Wall Connection	\$50,001				
USG COR 226 SOG/Column Connection	\$20,001				
USG COR 261 8" Wall at Grid 11B	\$15,001				
USG COR 263 12" Shear Walls	\$50,001				
USG COR 262 Maintenance Hatch Walls	\$15,001				

Contract Modification/Trend Log - Contract 1300 Stations

Awarded NTE Amount	\$839,676,400	\$839,947,356
Substantial Completion	2/10/2018	2/10/2018

	UMS	CTS	YBM	STS	COST REPORT NOTES
USG COR 264 Str. Steel Shop Dwgs.	\$5,001				
UMS COR 250 Macy's DSC	\$75,001				
USG COR 280 Strut Wall Grid 16 - 17	\$50,001				
USG COR 283 Slab Cantilver Detail	\$10,001				
USG COR 284 New Beam at Grid Line C	\$50,001				
USG COR 285 Elev.Edge of Slab Dtl.	\$50,001				
USG COR 286 Shear Wall GL 13/B-C	\$25,001				
USG COR 273 E.Slab/Shear Wall Conn.	\$100,001				
USG COR 274 Shear Wall at Grid 10	\$10,001				
USG COR 275 Conn. Plaza Grid B	\$50,001				
USG COR 276 Escalator #1	\$10,001				
STS - Deletion of ARS (Revision 1)				(\$4,689,000)	
YBM COR 63 Buried objects in P-86			\$24,648		
STS COR 100 DSC Zayo PVC Conf w SW				\$80,082	
STS COR 210 WD pit size increase				\$21,988	
CTS COR 085 Addl FACO 10 Costs		\$13,267			
CTS PCC 001 Delete DB on Stockton		(\$84,018)			
STS COR #92 PG&E Vault Conf 12 AWSS				\$78,074	
STS COR #118 78" Sewer Excess Debris				\$54,455	
UMS - PCC #29 (Concrete Wale Suppt)	\$9,239				
CTS COR 041 FACO #4 JT Unid. Cond		\$28,026			
CTS COR 040 FACO 002 Connect (E) DB		\$16,936			
YBM COR 49 Buried timber piles			\$88,230		
STS 78 Sewer Existing Concrete Crow				\$1,666	
YBM COR 182 Cap (E) 16" AWSS Tee			\$18,859		
CTS-COR#201 Swr Line & Station Roof		\$55,000			
YBM COR 115 C1250 unencased JT			\$9,843		
STS-COR #84 Sewer Cleaning and Dewa				\$3,598	
STS COR #236 E WD Config at Freelon				\$57,283	
STS COR 272 Pier AWSS Conflict				\$24,287	
STS COR 258 WD/PVC Conflict				\$87,511	
STS COR 211 SW conf AWSS 4th/Freelo				\$4,561	
STS COR #229 Multi E Util Conf w N				\$8,284	
UMS COR 232 S.Walk Hatches RFI 240	\$10,141				
STS COR #233 18" VCP in 3'x5' Sewer				\$1,764	
STS COR #88 Modify CBs and Culverts				\$4,395	
STS COR 101 Cleaning for non-78" SW				\$58,906	

Contract Modification/Trend Log - Contract 1300 Stations

Awarded NTE Amount
Substantial Completion

\$839,676,400
2/10/2018

\$839,947,356
2/10/2018

	UMS	CTS	YBM	STS	COST REPORT NOTES
STS COR #187 DB Conf w N 12 Water				\$37,012	
CTS COR 231 Sidewalk Hatches		\$19,249			
YBM COR 240 Headhouse Contam. Mtl.			\$126,229		
STS COR #241 2-In WD Line w Offset				\$5,386	
STS COR #296 Mult E Util Confl AWSS				\$130,001	
UMS COR 301 Concr. Overpour and PVC	\$6,642				
YBM PCC 062 Delete SW MH FOL 10+35			(\$21,453)		
STS COR 073 AWSS Incorrectly Shown				\$35,134	
STS COR 067 FACO #41 GW Lead Filter				\$22,695	
STS COR 037 FACO 037 30" FM Removal				\$63,972	
STS COR 090 Subsurface Obstrc				\$20,452	
YBM COR 310 12" WL Conf w/ 36" FM			\$355,711		
CTS COR 353 AWSS confl w HDPE in SW		\$3,203			
YBM COR 86 Side sewer conflict AT&T			\$75,001		
STS COR 091 PG&E Vault Conf 16" Wtr				\$20,001	
STS COR 093 12" Water Conf 12" Tee				\$5,001	
STS COR 094 Unknown DB Conf 12" Wtr				\$20,001	
USG COR 238 Grid 17 & Vent Conn.	\$50,001				
USG COR 237 Elevator Sump Pits	\$20,001				
STS COR 074 AWSS Offset/Sewer Demo				\$108,384	
USG COR 315 Beam Conflict at C Line	\$50,001				
USG COR 281 8in Slab Conn.at B Line	\$25,001				
USG COR 345 8 inch Slab Support	\$15,001				
USG COR 343 Edge of Slab Detail	\$5,001				
USG COR 360 Column on Grid Line B	\$5,001				
USG COR 363 Grid Line B Footing	\$10,001				
USG COR 358 Elev. Machine Rooms	\$5,001				
USG COR 359 Escalator #1 Truss Span	\$2,001				
USG COR 361 Mud Slab Clarification	\$50,001				
USG COR 374 Add Built Up Columns	\$25,001				
STS COR 198 NDSC Out of Spec Cover				\$8,123	
UMS COR 300 14 HDPE at Geary	\$2,069				
YBM COR 362 Temp Pavement Section			\$139,802		
STS COR 367 DSC Conf w/ CP and FM				\$36,256	
YBM COR 373 DSC NoD Contaminated Ma			\$47,144		
UMS Roof Deck - Schedule Recovery	\$78,765				
STS PCC 066 Add CS ATCS Emer Stop B				\$315,001	

Contract Modification/Trend Log - Contract 1300 Stations

Awarded NTE Amount	\$839,676,400	\$839,947,356
Substantial Completion	2/10/2018	2/10/2018

	UMS	CTS	YBM	STS	COST REPORT NOTES
STS COR 387 Oil Line Confl SW MH				\$6,124	
USG COR 386 Built up Column	\$75,001				
USG COR 388 Grid Line C Beam Slab	\$10,001				
UMS COR 391 AT&T Duct Bank Conflict	\$25,001				
USG COR 365 Elevator Hoist Beam	\$50,001				
UMS COR 389 4" Steel line asbestos	\$20,001				
YBM COR 390 RFI 1654 Chip Headwall			\$10,001		
USG COR 394 Ramp Conflict with HVAC	\$75,001				
USG COR 395 Rebar Detail over HVAC	\$5,001				
USG COR 396 HVAC Wall Opening Rebar	\$5,001				
USG COR 398 Reinforcing at Columns	\$20,001				
USG - PCC #030 (US Garage Underpin)	\$474,470				
USG COR 397 Duct Trench at Level 4	\$5,001				
USG COR 384 Slab on Grade Depressio	\$100,001				
USG COR 414 SOG Underneath the Ramp	\$100,001				
USG COR 435 Work Stoppage	\$50,001				
USG Hydrant Relocation on Geary St.	\$64,162				
USG COR 415 Glass Roof Steel Elev.	\$10,001				
USG COR 438 Shear Wall at GL 10B	\$2,001				
USG COR 439 Top of Built-Up Columns	\$20,001				
USG COR 444 Mech. Opening in Wall	\$10,001				
CTS COR 453 Steel Line Confl w/ SS		\$5,070			
UMS PG&E Additional Conduits	\$36,657				
STS PCC 077 Tunnel Sump Pump Upgrad				\$11,147	
UMS Preload Rqmt for Concourse	\$2,700,001				
USG Door Openings in Shear Walls	\$35,001				
YBM COR 457 Modify Beams in Conc.			\$15,001		
UMS COR 466 Asbestos Conduit	\$7,501				
YBM COR 477 Issues with Headwalls			\$20,001		
YBM COR 485 Issues with N S Headwal			\$30,001		
YBM COR 54 Conc obst 20" cut & cap			\$56,654		
YBM COR 72 Concrete, jet grout 1252			\$84,509		
YBM COR 230 Change load Bilco hatch			\$31,474		
YBM COR 249 Utility conflicts Folso			\$91,877		
UMS FACO #29 BART Hazmat Abatement	\$22,884				
STS COR 089 Extra Sewer Work				\$29,468	
YBM COR 45 Abandoned sewer not slurr			\$44,645		

Contract Modification/Trend Log - Contract 1300 Stations

Awarded NTE Amount	\$839,676,400	\$839,947,356
Substantial Completion	2/10/2018	2/10/2018

	UMS	CTS	YBM	STS	COST REPORT NOTES
YBM COR 458 TPC site support Zayo			\$4,460		
USG PCC 73 Acceleration of Work	\$102,203				
YBM COR 50 Contam soil along H Line			\$257,594		
YBM COR 243 Culvert Conflict NE Cor			\$11,334		
STS COR 480 Conc confl w (N) FM				\$886	
YBM COR 248 Transite pipe Folsom			\$1,501		
YBM COR 46 Add analy tests by AEW			\$5,001		
YBM COR 267 Design SW hatch Stair 5			\$11,059		
USG COR 490 Wall Opening for HVAC	\$15,001				
YBM COR 525 Defective Documents			\$50,001		
USG COR 532 Wall Connection	\$5,001				
USG COR 534 Existing Column Tie-In	\$5,001				
USG COR 538 B Line Ramp Wall Rebar	\$5,001				
USG COR 539 Elev. 3/4 Machine Pocke	\$10,001				
UMS COR 441 Support of E Utilities	\$20,001				
USG COR 344 Unknown Grade Beams	\$10,001				
USG COR 541 Plaza Level Lighting	\$0				
USG COR 544 Vent Shaft	\$0				
USG COR 546 Tiebacks at Level X	\$0				
UMS COR 543 Ellis Seismic Joint Dtl	\$0				
USG COR 551 Additional Waterstops	\$0				
USG COR 552 Plumbing	\$0				
UMS COR 550 Grout in Arthur Beren	\$0				
USG COR 553 Details at Glass Roof	\$0				
USG COR 555 Stair #1	\$0				
UMS COR 493 Steel Shape Inside Pile	\$25,001				
UMS COR 516 Utilities and Slab at G	\$10,001				
Proposed Contract Change (PCC)					
UMS - Geoprobe Credit	(\$30,320)				
UMS - Relocation of TS Cabinet	\$23,275				
STS - Tunnel Lowering				\$100,001	
CTS - Stairs 5, 6, 7 Mods		\$25,000			
UMS - Escalator Barricade	(\$122,511)				
UMS - USG HVAC Trench Modifications	\$75,001				
CTS - CMU Wall Bracing		\$10,200			
UMS MRY Duct Bank-East Conflict	\$59,251				
YBM PCC 32 Delete Post-Grout of TB-2			(\$10,001)		

Contract Modification/Trend Log - Contract 1300 Stations

Awarded NTE Amount	\$839,676,400	\$839,947,356
Substantial Completion	2/10/2018	2/10/2018

	UMS	CTS	YBM	STS	COST REPORT NOTES
YBM_CTS PCC 33 Platform Gate Revisio			\$100,001		
CTS - Removal of Bus Bulb		\$15,997			
CTS Delete PGE Work at Vault 732		(\$50,000)			
UMS Remove Fire Hose Valve	\$20,000				
UMS MRY Duct Bank-West	\$54,981				
STS PCC #51 Inventory Temp Crossover				\$20,001	
Deletion of ARS (Part II)				(\$600,001)	
YBM PCC 56 OCS Pole Foundations			\$551		
UMS - DCW and Hose Bibbs	\$30,000				
STS Track Switch Mach Change (Rev 1)				\$50,001	
STS PCC 063 Del ATT/TSIC/PGE on 4th				(\$100,000)	
CTS PCC 061 (Rev1) Rev Escalator Pit		\$50,001			
CTS PCC 067 Wall Shift North Access		\$25,001			
USG Ramp Barriers and Handrail	\$75,001				
CTS PCC 050 Chinatown Plaza		\$0			
YBM PCC 69 Sidewalk Bulb-out Stair#1			\$62,508		
USG Removal of Existing Column	(\$101)				
YBM PCC 76 AWSS, SSFM, 20"WM Changes			\$150,001		
UMS Jet Grout at O'Farrell	\$40,001				
YBM PCC 79 Install 12in WM to Howar			\$169,450		
YBM PCC 82 Delete Scope Due to Hotel			(\$50,000)		
YBM PCC 37 SFAC Node Sculpture			\$100,001		
YBM PCC 59 Pavers Basis of Design			\$20,001		
UMS Locate PG&E Conduits	\$20,001				
CTS PCC 083 Switchgear Breakers		\$30,001			
STS PCC 084 Removal of ATT DB & Vau				\$50,001	
YBM PCC 85 Ticketing Hall Changes			\$100,001		
UMS PCC 86 Headwall Soffit Pile	\$0				
USG PCC 87 Tiebacks, L2, GL 14-15	\$40,001				
YBM PCC 88 Extend 24" WM to Howard			\$250,001		
USG PCC 89 E. Light Pole Foundation	\$2,501				
YBM PCC 90 Replace Bollards on 4th			\$15,001		
YBM PCC 91 Concourse Deck Conflict			\$0		
Change Order Request (COR)					
STS COR 17 Changes to Sewer Structu				\$20,000	
STS-FACO #47 Sewer Related Work Pt2				\$33,898	
STS- AT&T Conduit Changes				\$22,290	

Contract Modification/Trend Log - Contract 1300 Stations

Awarded NTE Amount
Substantial Completion

\$839,676,400
2/10/2018

\$839,947,356
2/10/2018

	UMS	CTS	YBM	STS	COST REPORT NOTES
UMS COR 212 Comp. Grout PSI Drop	\$155,001				
UMS COR 235 DSC Transite Pipe	\$10,000				
STS COR 113 DSC PVC Conduit/WL Conf				\$11,997	
STS COR 145 DSC PVC Conduits Conf				\$1,276	
STS COR 164 DSC 8" AWSS Lat Conf 78				\$5,750	
STS COR 254 DSC 4" Confl w 36" FM				\$25,197	
CTS COR 255 Additional Instruments		\$429,777			
STS COR 270 SW AWSS Conflict				\$8,280	
STS COR 271 DB AWSS Conflict				\$891	
UMS COR 277 8" Steel line Asbestos	\$4,056				
STS COR 290 E CB PVC Confl WD AWSS				\$10,000	
STS COR 297 TC for Track Work at 4t				\$150,001	
UMS COR 110 DSC Obst. at JG Columns	\$1,197,010				
USG COR 246 Tieback Access RFI 1050	\$160,933				
CTS COR 305 Abandon OBW-CTS-03		\$1,790			
STS COR 371 Conflicts w/ 12" AWSS				\$30,001	
CTS COR 372 DSC Potholing @ N Acces		\$9,846			
STS COR 392 Util at 4th-Town SW MH				\$25,001	
STS COR 402 3x5 SW Confl w/ VCP SW				\$10,001	
UMS COR 403 Waterproofing at BART	\$20,001				
STS COR 404 Contam Soil in MRY DB				\$12,303	
UMS COR 407 8 inch SS Line Conflict	\$4,956				
STS COR 014 Addtl MNHS for 78" SW				\$20,217	
STS COR 401 AWSS Layout 4th/King				\$295,269	
STS COR 406 Addtl TC at 4th/King				\$675,001	
STS COR 409 Conc in confl w/ (N) FM				\$3,722	
STS COR 411 Delay 8" WL miss serv				\$7,501	
STS COR 416 Conc DB/wall/lines conf				\$30,001	
CTS COR 430 Unknown Concrete		\$3,754			
CTS COR 429 Thickened Street Base		\$2,618			
CTS COR 408 MSX Termination		\$10,001			
CTS COR 436 WD confl with SW MH A		\$20,731			
CTS COR 437 Unanticipated Elec @ Sh		\$50,001			
STS COR 443 AWSS Procurement Delay				\$25,001	
CTS COR 445 3x5 w/ HDPE/PVC Inside		\$10,001			
CTS COR 450 Switchgear Cubicles		\$100,001			
STS COR 451 PGE Vlt WD Line Confl				\$30,001	

Contract Modification/Trend Log - Contract 1300 Stations

Awarded NTE Amount
Substantial Completion

\$839,676,400
2/10/2018

\$839,947,356
2/10/2018

	UMS	CTS	YBM	STS	COST REPORT NOTES
CTS COR 452 ATT Vlt Confl w/ SS Lat		\$10,001			
STS COR 454 Addtl Conflicts w/ 8" W				\$120,001	
STS COR 455 Conflicts w/ 8" WD Line				\$10,001	
STS COR 456 Conflicts w/ MRV Poles				\$20,001	
UMS COR 393 Wrap 14" Sewer Line	\$4,131				
UMS COR 417 Redesigned Manhole	\$22,192				
STS COR 461 WD Confl w/ Mult Utilit				\$100,001	
STS COR 469 SW Cleaning 4th/King				\$1,836	
CTS COR 470 Steel Pipe/SW MNH Confl		\$2,186			
USG COR 471 Tieback Install Delays	\$46,250				
STS COR 475 Removal of Fiber Optic				\$1,101	
STS COR 476 Conc DB Conf w/ FH				\$25,001	
STS COR 129 Debris (N) 10" culvert				\$11,569	
YBM COR 459 AWSS / WD Conflict			\$83,484		
YBM COR 474 Folsom Culvert at PG&E			\$24,661		
USG COR 486 Level X Test Tieback	\$0				
STS COR 500 Tunnel Monuments				\$5,001	
USG COR 501 Tieback X-16	\$0				
UMS COR 510 6" VCP Side Sewer	\$24,484				
USG COR 512 Plaza Slab Conflict	\$50,001				
USG COR 514 Added Structural Dtls.	\$0				
STS COR 519 Re-pressure test WD				\$2,739	
CTS COR 521 Accomodations for Chine		\$11,848			
STS COR 523 AV & GV on 8" WD Main				\$1,601	
CTS COR 526 Connection b/t Wall & D		\$25,001			
CTS COR 527 Connection b/t Wall/Dec		\$25,001			
CTS COR 528 AWSS in 3x5 Brk SW		\$21,049			
STS COR 530 Tunnel Track Machine				\$20,064	
STS COR 531 ATT Confl w/ 36" FM				\$25,001	
STS COR 536 Util in Conf w 36" FM				\$30,001	
STS COR 542 Tunnel Invert Slab Mix				\$1,001	
STS COR 545 Traffic Control Costs				\$200,001	
STS COR 557 ATCS Revisions				\$50,001	
STS COR 558 CB in conf w AWSS				\$3,001	
STS COR 561 ATCS Mitigation NCT				\$1	
STS COR 563 Utils conf w new AWSS				\$5,001	

Contract Modification/Trend Log - Contract 1300 Stations

Awarded NTE Amount	\$839,676,400	\$839,947,356
Substantial Completion	2/10/2018	2/10/2018

	UMS	CTS	YBM	STS	COST REPORT NOTES
STS COR 491 Util in conf w/ 10" VCP				\$10,001	
Change Order - Pending					
CTS-Plaza Surface Level Struct Mods		(\$10,337)			
STS Additional AT&T Ductbank at 4th				\$108,053	
UMS FACO #53 NDSC Transite Pipe	\$61,055				
STS COR 052 Fire Hyd NE 4th&Bryant				\$5,346	
CTS COR 007 (E) Soil Characteristics		\$1,714,205			
UMS Install Deluge Main	\$63,831				
UMS Install Floor Drain	\$5,804				
STS COR #114 Conc Obs 4th-Bryant SE				\$1,262	
STS COR #170 Cores in 78" RC Crown				\$1,666	
STS COR #252 Thick Pavemnt 4th/Kin				\$905	
YBM COR 295 DSC AT&T Vault Conflict			\$2,563		
STS COR 048 Add'l Soil Testing Req				\$9,226	
YBM COR 117 Additional Signage			\$3,902		
YBM COR 268 Repair PG&E Sink Hole			\$8,956		
YBM COR 418 Force Main Vault Size			\$3,033		
UMS Protection for in-slab piping	\$10,394				
YBM COR 448 Elbow at Clementina			\$723		
UMS COR 079 DSC Storage Tanks	\$97,817				
YBM COR 253 DSC Pipe Conflict			\$6,504		
STS COR 468 Buried Debris in Subgra				\$2,027	
YBM COR 95 UST Removal Folsom			\$156,733		
YBM PCC 47 Add PG&E ductbank			\$133,784		
YBM PCC 55 Add 24" water main			\$230,778		
YBM COR 87 Transite pipe water insta			\$12,732		
Approved	\$111,258	\$403,233	<b style="color: red;">(\$1,581,215)	\$1,337,680	
Contract Modification					
CMod #1 BART Elevator Option 1 @ Pow	\$90,000				
CMod #3 CTS Work Safely Ard Power Po		\$25,956			
CMod #5 YBM Deletion of Comp Groutin			(\$1,833,869)		
CMod #6 CTS Plaza Constr Supt Servi		\$75,000			
CMod#7 STS FACOs 016, 017 &COR 009				\$80,170	
CMod #4 CTS-Force Account Change Or		\$130,000			
Cmod #10 YBM PCC 042			\$64,287		
CMod#8 STS PCC 006 ATT MH, PB&Trench				\$225,208	
CMod #9 YBM COR 10,15,16,18,20,25			\$126,663		

Contract Modification/Trend Log - Contract 1300 Stations

Awarded NTE Amount	\$839,676,400	\$839,947,356
Substantial Completion	2/10/2018	2/10/2018

	UMS	CTS	YBM	STS	COST REPORT NOTES
CMod #11 UMS PCC 002	\$12,997				
CMod #12 STS Traffic Control				\$1,032,302	
CMod # 14 YBM COR 036, 078			\$58,526		
CMod #15 YBM COR 196			\$3,178		
CMod #16 UMS COR 184	\$8,261				
CMod #017 CTS CORs 001 053 & 069		\$54,322			
CMod #018 CTS PCC 012		\$60,248			
CMod #13 CTS COR 006		\$57,707			
Grand Total	14,619,167	6,523,432	2,663,068	\$298,085	

7.6 BUDGET REVISIONS: REPORT SORTED BY CONSTRUCTION PACKAGES & SOFT COSTS

Report Period: Feb 2016

Group by Contract & SCC	CATEGORY ITEM	Jan 2016			Feb 2016			BUDGET TRANSFERS [Feb 2016] vs. [Jan 2016]	Cost Report Notes
		Jan 2016 Base	Jan 2016 Allocated Contingency	Jan 2016 Base + Allocated Contingency (YOE)	Feb 2016 Base	Feb 2016 Allocated Contingency	Feb 2016 Base + Allocated Contingency (YOE)		
10-50	CONSTRUCTION CONTRACT PACKAGES	1,139,191,782	21,952,189	1,161,143,971	1,139,191,782	21,952,189	1,161,143,971	0	
1250	UTILITY RELOCATION PACKAGE #1	12,134,906		12,134,906	12,134,906		12,134,906	0	
	Contract 1250 Form B Credit	(2,275,419)		(2,275,419)	(2,275,419)		(2,275,419)	0	
1251	UTILITY RELOCATION PACKAGE #2	20,870,197		20,870,197	20,870,197		20,870,197	0	
	Contract 1251 Form B Credit	(7,618,412)		(7,618,412)	(7,618,412)		(7,618,412)	0	
1252	GUIDEWAY TUNNEL	235,005,822	1,063,145	236,068,968	235,005,822	1,063,145	236,068,968	0	25
	Contract 1252 Form B Credit	(254,050)		(254,050)	(254,050)		(254,050)	0	
1300	CN1300 STATIONS TOTAL	839,947,356	19,729,044	859,676,400	839,947,356	19,729,044	859,676,400	0	
1253: UMS	UNION SQUARE/MARKET STREET STATION [UMS]	294,141,848	4,888,742	299,030,590	294,141,848	4,888,742	299,030,590	0	
	UMS 1253 Form B Credit	(528,370)		(528,370)	(528,370)		(528,370)	0	
1254: CTS	CHINA TOWN STATION [CTS]	247,971,043	4,596,767	252,567,810	247,971,043	4,596,767	252,567,810	0	
	CTS 1254 Form B Credit	(451,703)		(451,703)	(451,703)		(451,703)	0	
1255: YBM	YERBA BUENA/ MOSCONE STATION [YBM]	156,507,785	6,581,216	163,089,001	156,507,785	6,581,216	163,089,001	0	
	YBM 1255 Form B Credit	(100,000)		(100,000)	(100,000)		(100,000)	0	
1256: STS	SURFACE TRACKWORK & SYSTEMS [STS]	141,326,680	3,662,319	144,988,999	141,326,680	3,662,319	144,988,999	0	
	STS 1256 SFPUC SEWER MAIN CREDIT	(2,925,296)		(2,925,296)	(2,925,296)		(2,925,296)	0	
	STS 1256 Form B Credit	(1,000,000)		(1,000,000)	(1,000,000)		(1,000,000)	0	
OTHER	OTHER CONSTRUCTION TOTAL	46,386,751	1,160,000	47,546,751	46,386,751	1,160,000	47,546,751	0	
40.06	PUBLIC ART PROGRAM	8,175,555	1,160,000	9,335,555	8,175,555	1,160,000	9,335,555	0	
40.02	MISC. CONSTR CONTRCT WK (TRACTION POWER FOR 1251)	258,202		258,202	258,202		258,202	0	
40.01	CONTRACT 1300 SOIL PROCESS	500,000		500,000	500,000		500,000	0	26
50.01	TEMPORARY LICENSE AGREEMENT (ATCS CENTRAL CONTROL)	487,972		487,972	487,972		487,972	0	
50.06	MTA FARE COLLECTION EQUIPMENT	5,400,000		5,400,000	5,400,000		5,400,000	0	
50.06	BART FARE COLLECTION EQUIPMENT	700,000		700,000	700,000		700,000	0	
40.02	JOB ORDER CONTRACTS (JOCS) - CONSTRUCTION	117,255		117,255	117,255		117,255	0	
40.08	AON RISK INSURANCE	18,088,750		18,088,750	18,088,750		18,088,750	0	

7.6 BUDGET REVISIONS: REPORT SORTED BY CONSTRUCTION PACKAGES & SOFT COSTS

Report Period: Feb 2016

Group by Contract & SCC	CATEGORY ITEM	Jan 2016 Base			Feb 2016 Base			BUDGET TRANSFERS [Feb 2016] vs. [Jan 2016]	Cost Report Notes
		Jan 2016 Base	Jan 2016 Allocated Contingency	Jan 2016 Base + Allocated Contingency (YOE)	Feb 2016 Base	Feb 2016 Allocated Contingency	Feb 2016 Base + Allocated Contingency (YOE)		
40.02 40.08	PUBLIC AGENCIES UTILITY COORDINATION	3,713,215		3,713,215	3,713,215		3,713,215	0	
40.02	DEPARTMENT OF PARKING AND TRAFFIC (DPT)	1,200,000		1,200,000	1,200,000		1,200,000	0	
50.03	UNION SQUARE/ MARKET STREET STATION POWER FEED	2,959,826		2,959,826	2,959,826		2,959,826	0	
50.03	UNION SQUARE/ MARKET STREET STATIONS PERMANENT POWER	(2,350,000)		(2,350,000)	(2,350,000)		(2,350,000)	0	
50.03	CHINATOWN STATION POWER FEED	2,959,826		2,959,826	2,959,826		2,959,826	0	
50.03	CHINATOWN STATION PERMANENT POWER	(2,350,000)		(2,350,000)	(2,350,000)		(2,350,000)	0	
50.03	YERBA BUENA/ MOSCONE STATION [YBM] POWER FEED	3,125,222		3,125,222	3,125,222		3,125,222	0	
50.03	YERBA BUENA/ MOSCONE STATION [YBM] PERMANENT POWER	(2,368,540)		(2,368,540)	(2,368,540)		(2,368,540)	0	
50.03	SURFACE STATION POWER FEED	11,839		11,839	11,839		11,839	0	
50.04	COMMUNICATION CONNECTION COSTS	5,757,629		5,757,629	5,757,629		5,757,629	0	
60	ROW, LAND, EXISTING IMPROVEMENTS	32,246,321	5,265,478	37,511,799	32,246,321	5,265,478	37,511,799	0	
60.01	PURCHASE OR LEASE OF REAL ESTATE	30,065,810	5,265,478	35,331,288	30,065,810	5,265,478	35,331,288	0	27
60.02	RELOCATION OF EXISTING HOUSEHOLDS	2,180,511		2,180,511	2,180,511		2,180,511	0	
70	VEHICLES	13,309,000	13,076,653	26,385,653	13,309,000	13,076,653	26,385,653	0	
70.01	LIGHT RAIL	13,309,000	13,076,653	26,385,653	13,309,000	13,076,653	26,385,653	0	28
70.07	SPARE PARTS								
80	PROFESSIONAL SERVICES	310,518,041	18,221,079	328,739,120	310,518,041	18,221,079	328,739,120	0	
80.01	PRELIMINARY ENGINEERING	46,202,674		46,202,674	46,202,674		46,202,674	0	
80.02 80.06	FINAL DESIGN	61,322,751		61,322,751	61,322,751		61,322,751	0	
80.03	PROJECT MANAGEMENT FOR DESIGN & CONSTRUCTION	89,021,634	13,905,845	102,927,479	89,021,634	13,905,845	102,927,479	0	
80.04	CONSTRUCTION ADMINISTRATION & MANAGEMENT	91,037,791	2,956,812	93,994,603	91,037,791	2,956,812	93,994,603	0	

7.6 BUDGET REVISIONS: REPORT SORTED BY CONSTRUCTION PACKAGES & SOFT COSTS

Report Period: Feb 2016

Group by Contract & SCC	CATEGORY ITEM	Jan 2016			Feb 2016			BUDGET TRANSFERS [Feb 2016] vs. [Jan 2016]	Cost Report Notes
		Jan 2016 Base	Jan 2016 Allocated Contingency	Jan 2016 Base + Allocated Contingency (YOE)	Feb 2016 Base	Feb 2016 Allocated Contingency	Feb 2016 Base + Allocated Contingency (YOE)		
80.05	INSURANCES	6,800,000		6,800,000	6,800,000		6,800,000	0	
80.06	LEGAL: PERMITS. REVIEW FEES BY OTHER AGENCIES	8,258,184		8,258,184	8,258,184		8,258,184	0	
80.07	SURVEYS, TESTING, INVESTIGATION. INSPECTION	883,100		883,100	883,100		883,100	0	
80.08	START-UP	6,991,907	1,358,422	8,350,329	6,991,907	1,358,422	8,350,329	0	
	ALL SCC CATEGORIES 10 TO 80	1,495,265,145	58,515,399	1,553,780,544	1,495,265,145	58,515,399	1,553,780,544	0	
90	UNALLOCATED CONTINGENCIES			24,519,459			24,519,459		29
	TOTAL PROJECT COST 10 TO 100			1,578,300,002			1,578,300,002		
	TOTAL CONTINGENCY			83,034,858			83,034,858		
	CONTINGENCY MINIMUM			60,000,000			60,000,000		
	BELOW OR ABOVE MINIMUM			23,034,858			23,034,858		

COST STATUS BY CATEGORY	SCC CODES	BUDGET Jan 2016	BUDGET TRANSFERS	BUDGET Feb 2016	Feb 2016 CTD	Feb 2016 EAC
CONSTRUCTION	SCC 010 - 050	1,161,143,972	0	1,161,143,972	647,373,432	1,162,989,609
REAL ESTATE	SCC 060	37,511,799	0	37,511,799	30,646,005	32,246,321
VEHICLES	SCC 070	26,385,653	0	26,385,653	2,147,204	13,309,000
PRELIM ENGINEERING	SCC 080.01	46,202,674	0	46,202,674	46,202,675	46,202,674
FINAL DESIGN	SCC 080.02	61,318,331	0	61,318,331	61,576,939	61,322,751
PM FOR DESIGN & CONSTRUCTION	SCC 080.03 - 080.04	196,922,082	0	196,922,082	107,703,545	180,059,425
OTHER PROF SRVCS	SCC 080.05 - 080.08	24,296,033	0	24,296,033	12,301,963	22,933,191
UNALLOC CONTINGENCY	SCC 090	24,519,456	0	24,519,456		
Grand Total		1,578,300,000	0	1,578,300,000	907,951,763	1,519,062,971

SCC DESCRIPTION	Feb 2016 BUDGET	Feb 2016 CTD
010 - GUIDEWAY & TRACK ELEMENTS	285,468,439	183,624,767
020 - STATIONS, STOPS, TERMINALS, INTERMODAL	576,722,832	268,425,632
040 - SITEWORK & SPECIAL CONDITIONS	203,581,602	177,916,157
050 - SYSTEMS	95,371,099	17,406,875
060 - ROW, LAND, EXISTING IMPROVEMENTS	37,511,799	30,646,005
070 - VEHICLES (number)	26,385,653	2,147,204
080 - PROFESSIONAL SERVICES (applies to Cats. 10-50)	328,739,120	227,785,122
090 - UNALLOCATED CONTINGENCY	24,519,456	
Grand Total	1,578,300,000	907,951,763

SCC DESCRIPTION	Feb 2016 BUDGET	Feb 2016 CTD
010.02-Guideway: At grade semi-exclusive (allows cross-traffic)	2,860,000	145,000
010.06-Guideway: Underground cut & cover	70,802,812	61,872,316
010.07-Guideway: Underground tunnel	200,594,901	116,531,981
010.09-Track: Direct fixation	6,761,089	2,456,871
010.12-Track: Special (switches, turnouts)	4,449,637	2,618,600
020.01-At-grade station, stop, shelter, mall, terminal, platform	6,673,138	1,308,634
020.02-Aerial station, stop, shelter, mall, terminal, platform	3,662,319	0
020.03-Underground station, stop, shelter, mall, terminal, platform	544,685,538	263,886,505
020.07-Elevators, escalators	21,701,837	3,230,493
040.01-Demolition, Clearing, Earthwork	11,144,242	9,342,944
040.02-Site Utilities, Utility Relocation	56,846,731	54,287,564
040.03-Haz. mat'l, contam'd soil removal/mitigation, ground water treatments	4,957,405	3,041,552
040.04-Environmental mitigation, e.g. wetlands, historic/archeologic, parks	1,020,165	465,707
040.05-Site structures including retaining walls, sound walls	2,706,431	2,706,431
040.06-Pedestrian / bike access and accommodation, landscaping	9,755,506	2,110,364
040.07-Automobile, bus, van accessways including roads, parking lots	6,967,874	2,107,265
040.08-Temporary Facilities and other indirect costs during construction	110,183,249	103,854,330
050.01-Train control and signals	28,031,423	6,000,203
050.02-Traffic signals and crossing protection	12,584,529	6,464,418
050.03-Traction power supply: substations	21,487,073	3,808,853
050.04-Traction power distribution: catenary and third rail	12,441,113	1,084,397
050.05-Communications	12,062,374	49,003
050.06-Fare collection system and equipment	6,100,000	0
050.07-Central Control	2,664,586	1
060.01-Purchase or lease of real estate	35,331,288	28,236,576
060.02-Relocation of existing households and businesses	2,180,511	2,409,430
070.01-Light Rail	26,385,653	2,147,204
080.01-Preliminary Engineering	46,202,674	46,202,675
080.02-Final Design	61,318,331	61,576,939
080.03-Project Management for Design and Construction	102,927,479	59,983,555
080.04-Construction Administration & Management	93,994,603	47,719,990

SCC DESCRIPTION	Feb 2016 BUDGET	Feb 2016 CTD
080.05-Professional Liability and other Non-Construction Insurance	6,800,000	6,340,196
080.06-Legal; Permits; Review Fees by other agencies, cities, etc.	8,262,604	4,569,839
080.07-Surveys, Testing, Investigation, Inspection	883,100	13,831
Grand Total	1,578,300,000	907,951,763

[A] Cost Account Description	BUDGET	ACTUAL COSTS				[G] VARIANCE (B - F)	COST REPORT NOTES
	[B] February 2016 Budget (YOE)	[C] PRIOR MONTH Total	[D] PRIOR MONTH Monthly	[E] CURRENT Monthly	[F] CURRENT Total		
TOTAL PRELIMINARY ENGINEERING	46,542,061	46,542,061	0	0	46,542,061	0	30
11 - SFMTA PROJECT MANAGEMENT	8,828,359	8,253,957	0	0	8,253,957	574,403	31
12 - SFMTA ENGINEERING SERVICES	11,425,594	11,425,594	0	0	11,425,594	0	32
16 - DEPARTMENT OF PARKING AND TRAFFIC (DPT)	935,451	802,883	0	0	802,883	132,568	
21 - ARTS COMMISSION	1,500,570	1,500,570	0	0	1,500,570	1	33
22 - FIRE DEPARTMENT	33,825	33,825	0	0	33,825	0	
23 - CITY ATTORNEY'S OFFICE	1,234,754	1,234,754	0	0	1,234,754	0	
24 - RISK MANAGEMENT	0	0	0	0	0	0	
26 - PLANNING	99,604	99,604	0	0	99,604	0	
27 - DEPARTMENT OF PUBLIC HEALTH (DPH)	4,420	4,420	0	0	4,420	0	
29 - CITY AUDITOR	308,540	315,616	0	0	315,616	(7,076)	
32 - DPW - IDC ENGINEERING (HYDRAULIC)	3,322,887	3,714,064	377,632	0	3,714,064	(391,177)	
34 - DPW - IDC CONSTRUCTION (CAPITAL)	17,462	17,462	0	0	17,462	0	
36 - DPW - BSM INFRASTRUCTURE (MAPPING)	76,549	76,549	0	0	76,549	0	
39 - DPW - PCS SITE ASSESSMENT & REMEDIATION (SAR)	13,993	13,993	0	0	13,993	0	
51 - 821 HOWARD STREET	1,005,653	1,005,653	0	0	1,005,653	0	
55 - 651 BRANNAN	2,294,910	2,294,910	0	0	2,294,910	0	34
63 - CENTRAL SUBWAY PARTNERSHIP - AECOM-EPC JV CONTRACT 149	26,793,234	26,793,234	0	0	26,793,234	0	35
66 - ANIL VERMA	395,204	395,204	0	0	395,204	0	36
67 - HILL INTERNATIONAL CONTRACT 156	6,716,294	6,716,294	0	0	6,716,294	0	
68 - ARTHUR GALLAGER & CO. CS 164	6,800,000	6,340,196	0	0	6,340,196	459,804	
71 - TUNNEL/UTILITIES - CONTRACT # CONTRACT 155-1	5,469,336	5,469,336	0	0	5,469,336	0	37
72 - STATIONS - CONTRACT # CONTRACT 155-2	26,220,609	26,220,609	0	0	26,220,609	0	38
73 - SYSTEMS/INTEGRATION - CONTRACT 155-3	11,432,312	11,432,312	0	0	11,432,312	(0)	39
331 - BAY AREA RAPID TRANSIT (BART)	146,427	146,427	0	0	146,427	0	
332 - SAN FRANCISCO COUNTY TRANSPORTATION AUTHORITY (SFCTA)	0	0	0	0	0	0	
TOTAL FINAL DESIGN	115,075,987	114,307,465	377,632	0	114,307,465	768,523	
11 - SFMTA PROJECT MANAGEMENT	15,589,933	6,086,261	181,367	155,518	6,241,779	9,348,154	
1.3.011.01.080.03 - CM:SFMTA LABOR-PROJECT MANAGEMENT	15,589,933	6,086,261	181,367	155,518	6,241,779	9,348,154	
12 - SFMTA ENGINEERING SERVICES	905,264	2,117,489	68,739	47,287	2,164,776	(241,194)	
1.3.012.02.080.04 - CM: SFMTA LABOR-ENGINEERING CONTRACT 1252	123,582	57,648	0	0	57,648	65,934	
1.3.012.06.080.04 - CM: SFMTA LABOR-ENGINEERING CONTRACT 1300	1,800,000	2,059,841	68,739	47,287	2,107,128	(307,128)	
13 - SFMTA CONSTRUCTION MANAGEMENT	44,075,375	9,396,171	201,728	194,603	9,590,774	33,466,283	
1.3.013.01.080.04 - CM:SFMTA LABOR-CONSTR. MANAGEM	43,057,057	9,396,171	201,728	194,603	9,590,774	33,466,283	

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16 - DEPARTMENT OF PARKING AND TRAFFIC (DPT)	3,588,074	1,522,484	49,627	7,494	1,529,978	2,067,855	
1.3.016.01.080.04 - DPT CONTRACT 1300 SUPPORT UMS	299,600	80,010	5,362	1,253	81,262	218,338	
1.3.016.01.080.04 - DPT CONTRACT 1300 SUPPORT CTS	274,900	65,939	2,497	837	66,776	208,124	
1.3.016.01.080.04 - DPT CONTRACT 1300 SUPPORT YBM	238,400	100,974	1,300	226	101,200	137,201	
1.3.016.01.080.04 - DPT CONTRACT 1300 SUPPORT STS	876,876	160,504	40,469	5,178	165,682	711,194	
1.3.016.02.040.08 - DPT: FIELD OPS TUNNEL [B84]	0	1,464	0	0	1,464	(1,464)	
1.3.016.02.040.08 - DPT: FIELD OPS TUNNEL [B86]	0	204,261	0	0	204,261	(204,261)	
1.3.016.06.040.02 - DPT:DPT TRAFFIC SHOP CONTRACT 1300	1,200,000	0	0	0	0	1,200,000	
1.3.016.07.080.04 - DPT:SSD DS/CN: 1UTL	38,450	0	0	0	0	38,450	
1.3.016.08.040.08 - DPT:PCOS:2UTL [68A]	400,728	400,728	0	0	400,728	0	
1.3.016.08.040.08 - DPT:SSD CN:2UTL	0	108,020	0	0	108,020	(108,020)	
1.3.016.08.080.04 - DPT:SSD [1326]	259,120	274,552	0	0	274,552	(15,432)	
1.3.016.08.080.04 - DPT:SSD [13BN]	0	23,302	0	0	23,302	(23,302)	
1.3.016.08.080.04 - DPT:SSD [13CN]	0	963	0	0	963	(963)	
1.3.016.08.080.04 - DPT:SSD [B85]	0	92,008	0	0	92,008	(92,008)	
1.3.016.09.040.08 - PCOS:1300/STS [68CPT544132Z.CPT544132Z]	0	9,759	0	0	9,759	(9,759)	
17 - MOTIVE POWER	2,195	0	0	0	0	2,195	
1.3.017.07.040.02 - PWR:SFMTA-MOTIVE POWER-UTL.REL	2,195	0	0	0	0	2,195	
18 - SFMTA OPERATIONS	400,000	39,586	0	0	39,586	286,883	
1.3.018.04.040.02 - OPS:SUPPORT TO CONTRACT 1300/CTS	100,000	26,469	0	0	26,469	73,531	
1.3.018.06.080.07 - OPS:SUPPORT TO CONTRACT 1300/UMS	300,000	13,117	0	0	13,117	286,883	
19 - OTHER SFMTA	700,000	1,504,961	(7,178)	0	1,504,961	(804,961)	
1.3.019.01.080.07 - OTH.MTA SURVEY AND TESTING	500,000	0	0	0	0	500,000	
1.3.019.07.080.07 - OTH.MTA SFMTA-SURVEY; TSTG [6840]	0	714	0	0	714	(714)	
1.3.019.08.040.08 - OTH.MTA 1251 MATERIALS	150,000	126,149	0	0	126,149	23,851	
1.3.019.08.080.08 - OTH.MTA OPERATION SUPPORT DUR	50,000	1,378,098	(7,178)	0	1,378,098	(1,328,098)	
21 - ARTS COMMISSION	12,010,885	2,395,173	27,392	61,365	2,456,538	9,554,347	
1.3.021.01.040.06 - ARTS:CTYCO-ARTS COMMISSION CONSTRUCTION COSTS	4,772,555	0	0	0	0	4,772,555	
1.3.021.01.080.03 - ARTS:CTYCO-ARTS COMMISSION [1227]	2,030,147	388,167	0	0	388,167	1,641,980	40
1.3.021.01.080.04 - ARTS:CTYCO-ARTS COMMISSION [PWE335MPFUNA.CPT5441227]	21,000	11,386	0	0	11,386	9,614	
1.3.021.06.080.03 - ARTS:CTYCO-ARTS COMMISSION PM [285MC.132J]	158,970	311,667	17,438	15,508	327,175	(168,205)	
1.3.021.01.080.03 - ARTS:CTYCO-ARTS COMMISSION [PWE335MPFUNA.CPT5441327]	0	1,265	0	0	1,265	(1,265)	
1.3.021.06.040.06 - ARTS:CTYCO-ARTS COMMISSION [68CPT5441327.CPT5441327]	1,500,000	1,393,660	0	0	1,393,660	106,340	
1.3.021.06.040.06 - ARTS:CTYCO-ARTS COMMISSION [285MCPFUNA.CPT5441327]	1,903,000	289,028	9,954	45,857	334,886	1,568,114	
1.3.021.01.080.03 - ARTS:CTYCO-ARTS COMMISSION [132J]	465,213	0	0	0	0	465,213	
1.3.021.97.040.06 - ARTS:ARTS COMMISSION ALLOC CO	1,160,000	0			0	1,160,000	
23 - CITY ATTORNEY'S OFFICE	2,171,781	1,555,745	0	0	1,555,745	616,036	
1.3.023.01.080.06 - ATTY:CN LEGAL-CITY ATTORNEY OF	2,171,781	1,555,745	0	0	1,555,745	616,036	
25 - PUBLIC UTILITIES COMMISSION SEWER	(2,925,296)	0	0	0	0	(2,925,296)	

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1.3.025.09.040.02 - STS.1256: SITE UTILITIES SFPUC SEWER MAIN	(2,925,296)					(2,925,296)	
26 - PLANNING	137,062	19,493	0	0	19,493	117,569	
1.3.026.01.080.06 - CM:CTYCO-PLANNING DEPARTMENT	137,062	19,493	0	0	19,493	117,569	
28 - PUBLIC UTILITIES COMMISSION WATER	4,242,012	2,371,600	41,937	5,893	2,377,493	1,864,519	
1.3.028.02.040.02 - CM:CTYCO-PUBLIC UTIL COMM. (PUC)	0	4,745	0	0	4,745	(4,745)	
1.3.028.02.040.08 - PUC: FIELD OPERATIONS TUNNEL	398,400	538,067	0	0	538,067	(139,667)	
1.3.028.02.080.04 - PUC:MTA CSP CN1252 [470465]	105,000	91,587	0	0	91,587	13,413	
1.3.028.03.040.02 - PUC:CDD CONTRACT 1300/UMS SUPPORT	606,354	101,365	9,422	0	101,365	504,989	
1.3.028.03.080.04 - PUC:CMB CONTRACT 1300/UMS INSPECTION	230,000	30,363	0	0	30,363	199,637	
1.3.028.04.040.02 - PUC:CDD CONTRACT 1300/CTS SUPPORT	271,755	159,610	6,669	0	159,610	112,145	
1.3.028.04.080.04 - PUC:CMB CONTRACT 1300/CTS INSPECTION	115,000	6,956	0	0	6,956	108,044	
1.3.028.05.040.02 - PUC:CDD CONTRACT 1300/YBM SUPPORT	450,282	182,520	8,665	0	182,520	267,762	
1.3.028.05.080.04 - PUC:CMB CONTRACT 1300/YBM INSPECTION	184,000	4,008	0	0	4,008	179,992	
1.3.028.06.040.02 - PUC:CMB CONTRACT 1300/SFWD AWSS MATERIAL	0	13,472	0	0	13,472	(13,472)	
1.3.028.07.040.02 - PUC:PUC CDD WATER CONNECTION CONTRACT 1250	248,035	291,501	0	0	291,501	(43,466)	
1.3.028.07.080.04 - PUC:PUC CMB INSPECTION CONTRACT 1250	74,468	113,844	0	0	113,844	(39,376)	
1.3.028.08.040.02 - PUC:PUC CDD WATER CONNECTION CONTRACT 1251 [445]	565,389	318,130	0	0	318,130	247,259	
1.3.028.08.080.04 - PUC:PUC CMB INSPECTION CONTRACT 1251	266,252	289,424	0	0	289,424	(23,172)	
1.3.028.09.040.02 - PUC:CMB CONTRACT 1300/STS SUPPORT	520,077	161,272	17,182	5,893	167,165	352,912	
1.3.028.09.080.04 - PUC:CMB CONTRACT 1300/STS INSPECTION	207,000	64,737	0	0	64,737	142,263	
32 - DPW - IDC ENGINEERING (HYDRAULIC)	1,150,459	(224,386)	(374,866)	1,592	(222,794)	1,223,719	
1.3.032.01.080.04 - CM:DPW:1424J-BUREAU OF ENGINEERING (BOE) [AB12]	60,000	(285,405)	(380,803)	0	(285,405)	345,405	
1.3.032.03.080.04 - DPW IDC HYDRAULIC CN1300 UMS SUPPORT	297,938	1,187	175	180	1,367	296,571	
1.3.032.04.080.04 - DPW IDC HYDRAULIC CN1300 CTS SUPPORT	295,639	8,890	2,133	0	8,890	286,749	
1.3.032.05.080.04 - DPW IDC HYDRAULIC CN1300 YBM SUPPORT	301,882	20,475	1,049	1,412	21,887	279,995	
1.3.032.06.080.04 - DPW: BOE: 1300 DSDC	6,000	0	0	0	0	6,000	
1.3.032.08.080.04 - DPW.HYRDDPW-BOE IDC ENG SVC DC	9,000	0	0	0	0	9,000	
1.3.032.09.080.04 - DPW IDC HYDRAULIC CN1300 STS SUPPOR	180,000	30,467	2,580	0	30,467	149,533	
34 - DPW - IDC CONSTRUCTION (CAPITAL)	6,695,348	4,562,530	59,559	42,536	4,605,066	2,090,282	
1.3.034.02.080.04 - DPW:CONSTR:1252 CM [CD12]	730,000	1,257,166	24,198	17,279	1,274,445	(544,445)	
1.3.034.02.080.04 - DPW:CONSTR:1252 CM [13AC12]	206,000	138,397	0	0	138,397	67,603	
1.3.034.01.080.04 - DPW:BCM LABOR [2113]	2,140,142	2,140,142	0	0	2,140,142	0	
1.3.034.06.080.04 - DPW:CONSTR:1300 CM [13CP12]	3,619,206	1,026,825	35,361	25,256	1,052,081	2,567,125	
36 - DPW - BSM INFRASTRUCTURE (MAPPING)	465,562	110,294	0	0	110,294	436,268	
1.3.036.01.080.04 - DPW:MPPG:DPW-BUREAU OF ST USE	417,129	32,680	0	0	32,680	384,449	
1.3.036.02.080.04 - DPW:MPPG:1300-DPW-BUREAU OF ST USE	0	33,084	0	0	33,084	(33,084)	
1.3.036.02.080.06 - DPW:MPPG:DPW-BUREAU OF ST USE	90,000	44,530	0	0	44,530	45,470	
1.3.036.06.080.06 - DPW:MPPG:BSM PERMIT REVIEW	39,433	0	0	0	0	39,433	
37 - DPW - PCS MATERIAL TESTING LABORATORY	83,100	0	0	0	0	83,100	

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1.3.037.01.080.07 - DPW.MTL.LABDPW-MATERIAL TESTIN	83,100	0	0	0	0	83,100	
39 - DPW - PCS SITE ASSESSMENT & REMEDIATION (SAR)	622,474	939,922	2,868	65	939,987	(317,513)	
1.3.039.01.080.04 - DPW:SITE ASSESSMENT & REMEDIATION (SAR) [132112]	8,621	506,858	0	0	506,858	(498,237)	
1.3.039.01.080.04 - DPW:SITE ASSESSMENT & REMEDIATION (SAR) [2213]	92,459	92,459	0	0	92,459	0	
1.3.039.01.080.04 - DPW:SITE ASSESSMENT & REMEDIATION (SAR) [2250]	78,400	78,400	0	0	78,400	0	
1.3.039.01.080.04 -DPW:SITE ASSESSMENT & REMEDIATION (SAR) [2257]	151,515	151,515	0	0	151,515	0	
1.3.039.01.080.04 - DPW:SITE ASSESSMENT & REMEDIATION (SAR) [2313]	24,343	24,343	0	0	24,343	0	
1.3.039.01.080.04 - DPW:SITE ASSESSMENT & REMEDIATION	58,757	10,043	0	65	10,109	48,648	
1.3.039.01.080.04 - DPW:SITE ASSESSMENT & REMEDIATION (SAR) [CE13]	31,367	31,367	0	0	31,367	0	
1.3.039.01.080.04 - DPW:SITE ASSESSMENT & REMEDIATION (SAR) [CH13]	100,000	8,621	0	0	8,621	91,379	
1.3.039.01.080.04 - DPW:SITE ASSESSMENT & REMEDIATION (SAR)	17,000	0	0	0	0	17,000	
1.3.039.02.080.04 - DPW: SITE ASSESSMENT & REMEDIATION (SAR) – CN1252 [13CE11]	18,632	16,880	0	0	16,880	1,753	
1.3.039.02.080.04 - DPW: SITE ASSESSMENT & REMEDIATION (SAR) – CN1300 [13CH11]	41,379	19,435	2,868	0	19,435	21,944	
46 - MACY'S WEST - SFPUC SEWER WORK	258,202	258,202	0	0	258,202	0	
1.3.046.08.040.02 - MCY.SWRC. CONTRACT: MACY'S-SEW	258,202	258,202	0	0	258,202	0	
51 - 821 HOWARD STREET	4,690,481	520,030	15,513	13,691	533,721	4,156,760	
1.3.051.01.080.03 - ODC.HWRD:ODCs - 821 HOWARD STR	4,625,481	507,598	14,339	13,650	521,248	4,104,233	
1.3.051.02.080.04 - ODC.HWRD:ODCs - TUNNEL CONTRACT 1252	10,000	1,056	0	0	1,056	8,944	
1.3.051.06.080.04 - ODC.HWRD:ODCs - STATION CONTRACT 1300	55,000	11,376	1,174	41	11,417	43,583	
55 - 651 BRANNAN	0	10,348	0	0	10,348	(10,348)	
1.3.055.01.080.03 - CM:ODCs - 651 BRANNAN STREET	0	10,348	0	0	10,348	(10,348)	41
63 - CENTRAL SUBWAY PARTNERSHIP - AECOM-EPC JV CONTRACT 149	42,373,401	26,326,349	600,000	340,576	26,666,925	15,706,476	
1.3.063.01.080.03 - CM:PM:AECOM.CS149 OM-EPC JV CS149-PM	9,507,939	5,877,903	7,542	(85,265)	5,792,638	3,715,301	42
1.3.063.01.080.04 - CM:AECOM.CS149OM-EPC JV CS-149 [3B]	5,218,630	3,259,362	11,313	(127,898)	3,131,464	2,087,166	
1.3.063.01.080.04 - CM:AECOM.CS149OM-EPC JV CS-149 [3E]	7,000,000	7,982,813	0	0	7,982,813	(982,813)	
1.3.063.01.080.04 - CM:AECOM.CS149OM-EPC JV CS-149 [3F]	0	2,420,811	525,791	429,288	2,850,099	(2,850,099)	
1.3.063.01.080.04 - CM:AECOM.CS149OM-EPC JV CS-149 [9B]	11,042	11,042	0	0	11,042	(0)	
1.3.063.01.080.04 - CM:AECOM.CS149OM-EPC JV CS-149 [9D]	550,000	515,694	0	0	515,694	34,306	
1.3.063.01.080.04 - CM:AECOM.CS149OM-EPC JV CS-149 [9E]	600,000	523,943	0	0	523,943	76,057	
1.3.063.01.080.04 - CM:AECOM.CS149OM-EPC JV CS-149 [9F]	0	154,836	55,354	124,452	279,288	(279,288)	
1.3.063.01.080.04 - FD:CM:EPC JV CS49-PM [123A]	5,579,945	5,579,945	0	0	5,579,945	0	
1.3.063.97.080.03 - AECOM.CS149 ALLOCAT CONTING	13,905,845					13,905,845	
67 - HILL INTERNATIONAL CONTRACT 156	11,000,000	1,655,550	71,750	60,649	1,716,199	9,283,801	
1.3.067.01.080.03 - HILL.CS156:HILL INTL. CS-156 [1336]	600,000	617,212	0	0	617,212	(17,212)	
1.3.067.01.080.03 - HILL.CS156:HILL INTL. CS-156 [1337]	9,400,000	911,078	71,750	60,649	971,726	8,428,274	
1.3.067.01.080.03 - HILL.CS156:HILL INTL. [1330]	1,000,000	127,261	0	0	127,261	872,739	
69 - BAYLAND SOIL PROCESS CONTRACT 175	500,000	121,445	26,444	0	121,445	378,555	43
1.3.069.06.040.01 - BAYLAND.CS175:BAYLAND SOIL PROCESS [133K]	500,000	121,445	26,444	0	121,445	378,555	
71 - TUNNEL/UTILITIES - CONTRACT # CONTRACT 155-1	1,358,950	1,904,919	12,080	12,080	1,916,999	(558,049)	

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1.3.071.01.080.04 - CM: CS155.1 DESIGN SUPPORT DURING CM [1232]	0	(87,201)	0	0	(87,201)	87,201	44
1.3.071.02.080.04 - CM: CS155.1 DESIGN SUPPORT DURING CM [1332]	1,358,950	1,992,120	12,080	12,080	2,004,200	(645,250)	
72 - STATIONS - CONTRACT # CONTRACT 155-2	8,752,240	4,506,204	186,177	175,376	4,681,580	4,070,660	
1.3.072.01.080.04 - CM: CS155.2 DESIGN SUPPORT DURING CM [1233]	50,000	51,351	0	0	51,351	(1,351)	45
1.3.072.01.080.04 - CM: CS155.2 DESIGN SUPPORT DURING CM [1333]	8,702,240	4,454,854	186,177	175,376	4,630,230	4,072,010	
73 - SYSTEMS/INTEGRATION - CONTRACT 155-3	4,828,269	1,510,676	62,107	111,599	1,622,275	3,205,994	
1.3.073.01.080.04 - CM: CS155.3 DESIGN SUPPORT DURING CM [1236]	90,000	89,791	0	0	89,791	209	
1.3.073.01.080.04 - CM: CS155.3 DESIGN SUPPORT DURING CM [1334]	4,738,269	1,420,886	62,107	111,599	1,532,485	3,205,784	
81 - UTILITIES RELOCATION #1 (PORTAL & MOS) - CONTRACT 1250	11,968,150	11,968,150	0	0	11,968,150	0	
1.3.081.07.040.01 - UR1.CONTRACT 1250:SITWORK: DEMOLIT	167,458	167,458	0	0	167,458	0	
1.3.081.07.040.02 - UR1.CONTRACT 1250:SITWORK: UTILITI	10,099,341	10,099,341	0	0	10,099,341	0	
1.3.081.07.040.03 - UR1.CONTRACT 1250:SITWORK:HAZMAT	453,321	453,321	0	0	453,321	0	
1.3.081.07.040.08 - UR1.CONTRACT 1250:SITWORK:TEMPORAR	1,248,030	1,248,030	0	0	1,248,030	0	
82 - UTILITIES RELOCATION #2 (UMS) - CONTRACT 1251	20,794,582	20,794,582	0	0	20,794,582	0	46
1.3.082.08.040.01 - UR2.CONTRACT 1251:SITWORK: DEMOLIT	752,240	752,240	0	0	752,240	0	
1.3.082.08.040.02 - UR2.CONTRACT 1251:SITWORK:UTILITI	10,328,044	10,328,044	0	0	10,328,044	0	
1.3.082.08.040.03 - UR2.CONTRACT 1251:SITWORK:HAZMAT	172,712	172,712	0	0	172,712	0	
1.3.082.08.040.05 - UR2.CONTRACT 1251:SITWORK: STRUCTU	2,706,431	2,706,431	0	0	2,706,431	0	
1.3.082.08.040.06 - UR2.CONTRACT 1251:SITWORK:PEDESTRA	319,317	319,317	0	0	319,317	0	
1.3.082.08.040.07 - UR2.CONTRACT 1251:SITWORK:AUTO/BUS	190,362	190,362	0	0	190,362	0	
1.3.082.08.040.08 - UR2.CONTRACT 1251:SITWORK:TEMP FAC	6,325,476	6,325,476	0	0	6,325,476	0	
GUIDEWAY TUNNELS TOTAL	236,068,968	234,616,308	0	0	234,616,308	1,452,660	
83 - GUIDEWAY TUNNELS - CONTRACT # 1252 BASE	233,584,015	232,689,302	0	0	232,689,302	894,713	
1.3.083.02.010.06 - CONTRACT 1252:GUIDEWAY:UNDERGRN'D CUT	60,446,425	61,377,878	0	0	61,377,878	(931,453)	
1.3.083.02.010.07 - CONTRACT 1252:GUIDEWAY:UNDERGROUND	105,423,090	105,097,157	0	0	105,097,157	325,933	
1.3.083.02.020.03 - CONTRACT 1252: STATIONS: UNDERGROUND	21,685,000	21,685,000	0	0	21,685,000	0	
1.3.083.02.040.01 - CONTRACT 1252:SITWORK:DEMO CLEARING	2,440,000	2,440,000	0	0	2,440,000	0	
1.3.083.02.040.02 - CONTRACT 1252:SITWORK:UTILITIES & RE	10,895,000	10,087,676	0	0	10,087,676	807,324	
1.3.083.02.040.03 - CONTRACT 1252:SITWORK:HAZMAT&MITIGAT	200,000	0	0	0	0	200,000	
1.3.083.02.040.04 - CONTRACT 1252:SITWORK:ENVIRON. MITIG	300,000	54,292	0	0	54,292	245,708	
1.3.083.02.040.06 - CONTRACT 1252:SITWORK:PED/BIKE ACCES	50,000	50,000	0	0	50,000	0	
1.3.083.02.040.07 - CONTRACT 1252:SITWORK:AUTO/BUS ACCES	1,345,000	1,345,000	0	0	1,345,000	0	
1.3.083.02.040.08 - CONTRACT 1252:SITWORK:TEMP FACILITIE	30,799,500	30,552,299	0	0	30,552,299	247,201	
83 - GUIDEWAY TUNNELS - CONTRACT # 1252 CMODs	1,421,807	1,927,006	0	0	1,927,006	(505,199)	
1.3.083.83.010.06 - CONTRACT 1252: CONTRACT MOD	81,937	81,937	0	0	81,937	0	
1.3.083.83.010.07 - CONTRACT 1252: CONTRACT MOD	1,180,322	1,280,322	0	0	1,280,322	(100,000)	
1.3.083.83.020.03 - CONTRACT 1252: CONTRACT MOD	965,121	1,077,223	0	0	1,077,223	(112,102)	
1.3.083.83.040.02 - CONTRACT 1252: CONTRACT MOD	1,665,207	1,665,208	0	0	1,665,208	(1)	
1.3.083.83.040.03 - CONTRACT 1252: CONTRACT MOD	401,933	401,933	0	0	401,933	0	

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	[B] February 2016 Budget (YOE)	[C] PRIOR MONTH Total	[D] PRIOR MONTH Monthly	[E] CURRENT Monthly	[F] CURRENT Total		
1.3.083.83.040.08 - CONTRACT 1252: CONTRACT MOD	(2,872,713)	(2,579,617)	0	0	(2,579,617)	(293,096)	
1.3.083.93.010.07 - CONTRACT 1252: TUNNEL ALLOC CONTING	1,063,146	0	0	0	0	1,063,146	47
CONTRACT 1300 - STATIONS, TRACKWORK AND SYSTEMS TOTAL	859,676,400	340,399,276	8,789,129	13,022,920	353,422,196	506,254,204	47a
84 - UNION SQUARE/MARKET STREET STATION (UMS) - WORK PACKAGE 1253	294,030,590	138,634,002	2,203,682	4,622,390	143,256,392	150,774,198	17
1.3.084.03.020.03 - UMS.1253: UNDERGROUD STATION	253,822,452	118,786,206	1,611,155	3,909,617	122,695,823	131,126,629	
1.3.084.03.020.07 - UMS.1253: ELEVATORS ESCALATOR	9,465,694	1,183,381	0	152,500	1,335,881	8,129,813	
1.3.084.03.040.01 - UMS.1253: DEMOLITION CLEARING	6,071,588	4,236,588	372,500	420,000	4,656,588	1,415,000	
1.3.084.03.040.02 - UMS.1253: SITE UTILITIES UTIL	3,971,620	1,908,101	18,750	8,250	1,916,351	2,055,269	
1.3.084.03.040.03 - UMS.1253: HAZARDOUS MATERIALS	550,000	295,865	33,745	22,023	317,888	232,112	
1.3.084.03.040.04 - UMS.1253: ENVIRONMENTAL MITIGA	244,500	145,875	0	0	145,875	98,625	
1.3.084.03.040.06 - UMS.1253: PEDESTRIAN/BIKE	18,969	12,501	0	0	12,501	6,468	
1.3.084.03.040.07 - UMS.1253: AUTOMOBILE BUS ACCE	1,547,185	27,001	0	0	27,001	1,520,184	
1.3.084.03.040.08 - UMS.1253: TEMPORARY FACILITIES	10,398,701	9,560,944	117,532	60,000	9,620,944	777,757	
1.3.084.03.050.02 - UMS.1253: TRAFFIC SIGNALS AND	4,773,076	2,400,000	50,000	50,000	2,450,000	2,323,076	
1.3.084.03.050.03 - UMS.1253: TRACTION POWER SUPPL	1,815,534	9,001	0	0	9,001	1,806,533	
1.3.084.03.050.04 - UMS.1253: TRACTION POWER DISTR	216,957	66,038	0	0	66,038	150,919	
1.3.084.03.050.05 - UMS.1253: COMMUNICATIONS	1,134,314	2,501	0	0	2,501	1,131,813	
84 - UNION SQUARE/MARKET STREET STATION (UMS) CMODs	111,258	98,261	8,261	12,997	111,258	0	
1.3.084.84.020.03 - CMOD:UMS.1253: UNDERGROUD STATION	21,258	8,261	8,261	12,997	21,258	0	
1.3.084.84.020.07 - CMOD:UMS.1253: ELEVATORS, ESCALATORS	90,000	90,000	0	0	90,000	0	
1.3.084.94.020.03 - UMS.1253: AC: ALLOC CONTING	4,888,742	0	0	0	0	4,888,742	48
85 - CHINATOWN STATION (CTS) - WORK PACKAGE 1254	247,567,810	85,460,524	4,321,510	5,587,868	91,048,392	156,519,418	
1.3.085.04.010.07 - CTS.1254: GUIDEWAY: UNDERGROUND TUNNEL	76,417,579	5,778,769	1,749,000	3,204,000	8,982,769	67,434,810	
1.3.085.04.020.03 - CTS.1254: UNDERGROUND STATION	133,001,053	56,836,707	2,430,575	2,366,177	59,202,884	73,798,169	
1.3.085.04.020.07 - CTS.1254: ELEVATORS ESCALATOR	6,812,856	1,000,001	0	0	1,000,001	5,812,855	
1.3.085.04.040.01 - CTS.1254: DEMOLITION CLEARING	400,000	400,000	0	0	400,000	0	
1.3.085.04.040.02 - CTS.1254: SITE UTILITIES UTIL	6,001,718	4,281,586	81,135	0	4,281,586	1,720,132	
1.3.085.04.040.03 - CTS.1254: HAZARDOUS MATERIALS	350,000	12,500	0	0	12,500	337,500	
1.3.085.04.040.04 - CTS.1254: ENVIRONMENTAL MITIGA	325,665	206,064	0	0	206,064	119,601	
1.3.085.04.040.06 - CTS.1254: PEDESTRIAN/BIKE	15,000	0	0	0	0	15,000	
1.3.085.04.040.07 - CTS.1254: AUTOMOBILE BUS ACCE	225,677	14,500	0	0	14,500	211,177	
1.3.085.04.040.08 - CTS.1254: TEMPORARY FACILITIES	16,571,322	15,804,762	0	0	15,804,762	766,560	
1.3.085.04.050.02 - CTS.1254: TRAFFIC SIGNALS AND	1,599,593	868,134	33,300	17,691	885,825	713,768	
1.3.085.04.050.03 - CTS.1254: TRACTION POWER SUPPL	4,063,927	227,500	0	0	227,500	3,836,427	
1.3.085.04.050.04 - CTS.1254: TRACTION POWER DISTRIBUTION	124,481	30,000	27,500	0	30,000	94,481	
1.3.085.04.050.05 - CTS.1254: COMMUNICATIONS	1,658,938	0	0	0	0	1,658,938	
85 - CHINATOWN STATION (CTS) CMODs	403,233	374,977	172,277	0	374,977	28,256	
1.3.085.85.020.03 - CMOD:CTS.1254: UNDERGROUND STATION	75,000	54,488	0	0	54,488	20,512	
1.3.085.85.040.01 - CMOD:CTS.1254: POWER POLE	155,956	148,212	0	0	148,212	7,744	

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	[B] February 2016 Budget (YOE)	[C] PRIOR MONTH Total	[D] PRIOR MONTH Monthly	[E] CURRENT Monthly	[F] CURRENT Total		
1.3.085.85.040.02 - CMOD:CTS.1254: SITE UTILITIES UTIL	114,570	114,570	114,570	0	114,570	0	
1.3.085.85.040.08 - CMOD:CTS.1254: TEMPORARY FACILITIES	57,707	57,707	57,707	0	57,707	0	
1.3.085.95.020.03 - CTS.1254: AC: ALLOC CONTING	4,596,767	0	0	0	0	4,596,767	49
86 - YERBA BUENA MOSCONE STATION (YBM) - WORK PACKAGE 1255	158,089,000	80,429,070	1,057,525	1,222,253	81,651,323	76,437,677	
1.3.086.05.020.03 - YBM.1255: UNDERGROUND STATION	119,048,929	58,217,620	823,800	932,208	59,149,828	59,899,100	
1.3.086.05.020.07 - YBM.1255: ELEVATORS ESCALATOR	5,333,287	639,239	0	165,372	804,611	4,528,676	
1.3.086.05.040.01 - YBM.1255: DEMOLITION CLEARING	657,000	657,000	0	0	657,000	0	
1.3.086.05.040.02 - YBM.1255: SITE UTILITIES UTIL	6,520,189	5,680,219	0	25,000	5,705,219	814,971	
1.3.086.05.040.03 - YBM.1255: HAZARDOUS MATERIALS	2,629,439	1,499,581	254,396	(9)	1,499,572	1,129,867	
1.3.086.05.040.04 - YBM.1255: ENVIRONMENTAL MITIGA	100,000	59,476	0	0	59,476	40,524	
1.3.086.05.040.06 - YBM.1255: PEDESTRIAN/BIKE	16,665	1	0	0	1	16,664	
1.3.086.05.040.07 - YBM.1255: AUTOMOBILE BUS ACCE	1,542,725	491,401	0	0	491,401	1,051,324	
1.3.086.05.040.08 - YBM.1255: TEMPORARY FACILITIES	15,564,753	12,258,798	(53,671)	82,000	12,340,798	3,223,955	
1.3.086.05.050.02 - YBM.1255: TRAFFIC SIGNALS AND	1,726,492	863,434	33,000	17,682	881,116	845,376	
1.3.086.05.050.03 - YBM.1255: TRACTION POWER SUPPL	3,708,425	59,800	0	0	59,800	3,648,625	
1.3.086.05.050.05 - YBM.1255: COMMUNICATIONS	1,241,096	2,501	0	0	2,501	1,238,595	
86 - YERBA BUENA MOSCONE STATION (YBM) CMODs	(1,581,215)	252,654	61,704	0	252,654	(1,833,869)	
1.3.086.86.040.02 - CMOD:YBM.1255: SITE UTILITIES UTIL	249,476	249,476	58,526	0	249,476	0	
1.3.086.86.040.08 - CMOD:YBM.1255: TEMPORARY FACILITIES	(1,830,691)	3,178	3,178	0	3,178	(1,833,869)	
1.3.086.96.020.03 - YBM.1255: AC: ALLOC CONTING	6,581,216	0	0	0	0	6,581,216	50
87 - SURFACE TRACKWORK AND SYSTEMS -WORK PACKAGE 1256	139,989,000	34,844,410	964,170	1,344,063	36,188,473	103,800,527	
1.3.087.09.010.02 - STS.1256: GUIDEWAY: AT-GRADE SEMI-EXCLUSIVE (ALLOWS CROSS TRAFFIC)	2,860,000	145,000	0	0	145,000	2,715,000	
1.3.087.09.010.06 - STS.1256: GUIDEWAY: UNDERGROUND CUT & CVR	10,274,450	315,001	219,000	97,500	412,501	9,861,949	
1.3.087.09.010.07 - STS.1256: GUIDEWAY: UNDERGROUN	16,510,764	1,160,933	9,600	10,800	1,171,733	15,339,031	
1.3.087.09.010.09 - STS.1256: TRACK DIRECT FIXATION	6,761,089	2,446,871	10,000	10,000	2,456,871	4,304,219	
1.3.087.09.010.12 - STS.1256: TRACK: SPECIAL	4,449,637	2,618,600	12,500	0	2,618,600	1,831,037	
1.3.087.09.020.01 - STS.1256: AT-GRADE STATION	6,673,138	1,251,134	0	57,500	1,308,634	5,364,504	
1.3.087.09.040.02 - STS.1256: SITE UTILITIES, UTILITY RELOCA	17,509,893	7,588,839	307,655	422,699	8,011,538	9,498,355	
1.3.087.09.040.03 - STS.1256: HAZARDOUS MATERIALS	200,000	183,626	0	0	183,626	16,374	
1.3.087.09.040.04 - STS.1256: ENVIRONMENTAL MITIGA	50,000	0	0	0	0	50,000	
1.3.087.09.040.07 - STS.1256: AUTOMOBILE BUS ACCE	2,116,925	39,001	0	0	39,001	2,077,924	
1.3.087.09.040.08 - STS.1256: TEMPORARY FACILITIES	13,850,984	10,085,823	30,400	0	10,085,823	3,765,161	
1.3.087.09.050.01 - STS.1256: TRAIN CONTROL AND SIGNALS	27,543,451	5,481,543	204,999	468,660	5,950,203	21,593,248	
1.3.087.09.050.02 - STS.1256: TRAFFIC SIGNALS AND	4,485,368	2,149,873	31,241	97,604	2,247,477	2,237,891	
1.3.087.09.050.03 - STS.1256: TRACTION POWER SUPPL	9,911,014	762,640	0	0	762,640	9,148,374	
1.3.087.09.050.04 - STS.1256: TRACTION POWER DISTRIBUTION	6,099,675	615,525	138,775	135,300	750,825	5,348,850	
1.3.087.09.050.05 - STS.1256: COMMUNICATIONS	8,028,025	0	0	44,000	44,000	7,984,025	
1.3.087.09.050.07 - STS.1256: CENTRAL CONTROL	2,664,586	1	0	0	1	2,664,585	
87 - SURFACE TRACKWORK AND SYSTEMS (STS) CMODs	1,337,680	305,378	0	233,349	538,727	798,953	

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1.3.087.89.040.02 - CMOD:STS.1256: SITE UTILITIES, UTILITY RELOCA	305,378	305,378	0	0	305,378	0	
1.3.087.89.040.08 - CMOD:STS.1256: TEMPORARY FACILITIES	1,032,302	0	0	233,349	233,349	798,953	
1.3.087.99.020.01 - STS.1256: AC: ALLOC CONTING	3,662,319	0	0	0	0	3,662,319	51
141 - CONSTRUCTION ADMINISTRATION	2,956,812	0	0	0	0	2,956,812	
1.3.141.97.080.04 - CONSTR.ADMIN:ALLOC CONTING	2,956,812					2,956,812	
142 - LEGAL/PERMITS	2,014,204	0	0	0	0	2,014,204	
1.3.142.01.080.06 - LGL.PRMTSF:LEGAL; PERMITS	2,014,204	0	0	0	0	2,014,204	
144 - STARTUP	8,300,329	0	0	0	0	8,300,329	
1.3.144.01.080.08 - STRT: STARTUP	6,941,907	0	0	0	0	6,941,907	
1.3.144.97.080.08 - STRTA: AC STARTUP ALLOC CONTIN	1,358,422					1,358,422	
151 - TEMPORARY LICENSE AGREEMENT	17,000	0	0	0	0	17,000	
1.3.151.01.080.06 - TEMP.LICPORARY LICENSE AGREEME	17,000	0	0	0	0	17,000	
170 - COMMUNICATIONS CONNECTIONS	5,757,629	0	0	0	0	5,757,629	
1.3.170.01.050.04 - COMM.CONNN:COMMUNICATION CONN	5,757,629	0	0	0	0	5,757,629	
181 - AON RISK INSURANCE CS 163	18,113,750	18,798,132	0	0	18,798,132	(684,382)	
1.3.181.01.040.08 - AON.CS163 AON RISK INS.	18,088,750	18,773,132	0	0	18,773,132	(684,382)	
1.3.181.01.080.03 - AON.CS171 AON RISK INS. STUDY	25,000	25,000	0	0	25,000	0	
191 - FARE COLLECTION CONTRACTOR	5,400,000	0	0	0	0	5,400,000	
1.3.191.01.050.06 - FARE.CONSUM:FARE COLLECTION	5,400,000	0	0	0	0	5,400,000	
192 - THALES T&S CENTRAL CONTROL	487,972	50,000	0	0	50,000	437,972	
1.3.192.01.050.01 - THALES T&S ATCS	487,972	50,000	0	0	50,000	437,972	
202 - JOC2-022.0	63,938	0	0	0	0	63,938	
1.3.202.01.040.02 - JOC2-022:15&22 POTHOLING UTILI LGHT FNDS	63,938	0	0	0	0	63,938	
203 - JOC2-029.0	53,317	0	0	0	0	53,317	
1.3.203.07.040.02 - JOC0292-029: RELOCATE VAULTS-S	53,317	0	0	0	0	53,317	
302 - PG&E	1,988,173	2,749,912	0	0	2,749,912	(761,739)	
1.3.302.03.050.03 - PGE PERMANENT POWER UMS	(2,350,000)	0	0	0	0	(2,350,000)	
1.3.302.03.050.03 - PGE POWER FEED UMS	2,959,826	115,690	0	0	115,690	2,844,136	
1.3.302.04.050.03 - PGE PERMANENT POWER CTS	(2,350,000)	0	0	0	0	(2,350,000)	
1.3.302.04.050.03 - PGE POWER FEED CTS	2,959,826	0	0	0	0	2,959,826	
1.3.302.05.050.03 - PGE PERMANENT POWER YBM	(2,368,540)	0	0	0	0	(2,368,540)	
1.3.302.05.050.03 - PGE POWER FEED YBM	3,125,222	2,634,222	0	0	2,634,222	491,000	
1.3.302.09.050.03 - PGE POWER FEED STS	11,839	0	0	0	0	11,839	
331 - BAY AREA RAPID TRANSIT (BART)	951,356	60,455	0	0	60,455	890,901	
1.3.331.01.080.04 - CM:SFMTA LABOR-ENG SVCS-IRP/BART/SF	0	33,152	0	0	33,152	(33,152)	
1.3.331.01.080.06 - CM: BAY AREA RAPID TRANSIT (BART) [122A]	951,356	27,304	0	0	27,304	924,052	
333 - AMERICAN PUBLIC TRANSP. ASSOCIATION (APTA) CS-APTA	146,500	54,612	0	0	54,612	91,888	
1.3.333.01.080.03 - APTA:APTA - IRP [2G]	46,500	27,304	0	0	27,304	19,196	

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1.3.333.01.080.03 - APTA:APTA - IRP [2C]	100,000	27,308	0	0	27,308	72,692	
334 - BART FARE COLLECTION SYSTEM	700,000	0	0	0	0	700,000	
1.3.334.01.050.06 - BART:BART FARE COLLECTION EQP	700,000	0	0	0	0	700,000	
401 - ECONOMIC AND WORKFORCE DEVELOPMENT (EWD)	17,600	17,600	0	0	17,600	0	
1.3.401.01.080.04 - EWD: MAYORS OFFICE ECON DEV	17,600	17,600	0	0	17,600	0	
402 - DEPARTMENT OF TECHNOLOGY	242,371	237,534	0	0	237,534	4,837	
1.3.402.07.050.04 - DT:1UTL:COMM. CONNECTIONS	166,756	166,179	0	0	166,179	577	
1.3.402.08.050.04 - DT:2UTL:COMM.CONNECTIONS	75,615	71,354	0	0	71,354	4,261	
404 - DEPARTMENT OF BUILDING INSPECTION (DBI)	1,204,081	1,204,081	0	0	1,204,081	0	
1.3.404.01.080.06 - DPT OF BUILDING INSPECTION	1,204,081	1,204,081	0	0	1,204,081	0	
491 - FORM B - REIMBURSEMENT	(12,227,954)	0	0	0	0	(12,227,954)	
1.3.491.02.040.02 - FORMB - CONTRACT 1252 UTILITY REIMBUR	(254,050)	0			0	(254,050)	52
1.3.491.03.040.02 - FORMB - UMS:CONTRACT 1300 UTILITY REIMBURSEMENT	(528,370)	0			0	(528,370)	53
1.3.491.04.040.02 - FORMB - CTS:CONTRACT 1300 UTILITY REIMBURSEMENT	(451,703)	0			0	(451,703)	54
1.3.491.05.040.02 - FORMB - YBM:CONTRACT 1300 UTILITY REIMBURSEMENT	(100,000)	0			0	(100,000)	55
1.3.491.07.040.02 - FORMB - CONTRACT 1250 UTILITY REIMBUR	(2,275,419)	0			0	(2,275,419)	57
1.3.491.08.040.02 - FORMB - CONTRACT 1251 UTILITY REIMBUR	(7,618,412)	0			0	(7,618,412)	58
1.3.491.09.040.02 - FORMB - STS:CONTRACT 1300 UTILITY REIMBURSEMENT	(1,000,000)	0			0	(1,000,000)	59
TOTAL CONSTRUCTION PHASE	1,328,370,949	700,161,688	10,014,374	14,253,244	714,414,932	613,823,711	
1.4.091.01.070.01 - LRVS: LIGHT RAIL VEHICLES RFP [34B]	1,325,000	1,319,773	0	0	1,319,773	5,227	
1.4.091.01.070.01 - LRVS: LIGHT RAIL VEHICLES PROJECT MGT [68E]	827,132	827,431	0	0	827,431	(299)	
1.4.091.01.070.01 - LRVS: LRV PROCUREMENT ODC	25,000	0	0	0	0	25,000	
1.4.091.01.070.01 - LRVS: LRV PROCUREMENT	11,131,868	0	0	0	0	11,131,868	
1.4.091.97.070.01 - LRVA:AC: VEHICLES ALLOC CONTI	13,076,653					13,076,653	19
TOTAL VEHICLES	26,385,653	2,147,204	0	0	2,147,204	24,238,450	
1.5.015.01.060.01 - RE: EASEMENT ACQUISIT	400,000	322,939	0	0	322,939	77,061	
1.5.015.01.060.01 - RE: REAL EST SITE ACQ	16,523,400	14,307,667	0	0	14,307,667	2,215,733	
1.5.015.01.060.01 - RE: REAL ESTATE	750,000	766,272	0	0	766,272	(16,272)	
1.5.015.01.060.01 - RE: REC & PARK MOU	6,987,624	6,987,624	0	0	6,987,624	0	
1.5.015.01.060.01 - RE:-DEPT OF TRANSPOR	2,686,000	2,686,000	0	0	2,686,000	0	
1.5.015.01.060.01 - RE:-LICENSES FEES	400,000	381,311	0	0	381,311	18,689	
1.5.015.97.060.01 - RE:A:AC: RE: REAL ESTATE ALLO	5,265,478	0			0	5,265,478	
1.5.023.01.060.01 - ATTY:REAL ES	2,212,882	2,678,858	0	0	2,678,858	(465,976)	
1.5.101.01.060.02 - RES.RELO: RELOCATION COST	1,275,200	1,289,701	0	0	1,289,701	(14,501)	
1.5.102.01.060.02 - COMM.RELO-RELOC COMMERCIAL	905,311	1,119,729	179,000	0	1,119,729	(214,418)	
TOTAL ROW, LAND, EXISTING IMPROVEMENTS	37,405,895	30,540,101	179,000	0	30,540,101	6,865,794	
90 - CONTINGENCY	83,034,856					83,034,856	

[A] Cost Account Description	BUDGET	ACTUAL COSTS					
	[B] February 2016 Budget (YOE)	[C] PRIOR MONTH Total	[D] PRIOR MONTH Monthly	[E] CURRENT Monthly	[F] CURRENT Total	[G] VARIANCE (B - F)	COST REPORT NOTES
1.7.500.91.090.00 - UNALLOCATED CONTINGENCY	24,519,456					24,519,456	60
TOTAL ALLOCATED CONTINGENCY	58,515,400					58,515,400	
TOTAL PROJECT COST							
	1,578,300,001	893,698,519	10,571,006	14,253,244	907,951,763	670,215,933	

7.1 Program Project Budget	
1	The Central Subway Project (CSP) (SFMTA Capital Program CPT 544) is defined in the FTA-SFMTA October 2012 Full Funding Grant Agreement with a budget of \$1.578 billion.
2	The TBM Retrieval Shaft Relocation (SFMTA Capital Program CPT 690) is one of four capital projects that is related to CSP. These projects are reported for background information as needed outside of the main body of the Project Monthly Progress Report.
3	The Chinatown Plaza (CPT 718) is for Chinatown Station enhancement capital project. The project has funding outside of the Central Subway Project. The construction is carried out in Contract 1300 Contract Modifications.
4	CPT 665 is a Real Estate project to relocation in compliance with California regulations for business relocations but outside of the Central Subway Project as defined by the FTA FFGA.
5	CPT 705 is an SFMTA capital improvement between the Agency and community stakeholders outside of the Central Subway Project.
6	Utility company reimbursements (Form B) result in funds received for work carried out on behalf of utilities concurrent to CSP work to achieve efficiencies.
7	PG&E Power Feed reimbursement funds are the refunds from PG&E when completion of Stations construction and switch to permanent power.
8	BART Elevator funds are reimbursements for work carried out on behalf of BART to install BART Powell Street Station elevator.
9	The Tutor Perini - CAD Files funds are the result of payments by the contractor for project documentation not included in the contract.
10	SFPUC Sewer Main funds are reimbursements for work carried out on behalf of San Francisco PUC (includes 10% construction contingency).
11	SFPUC 24" Water Main funds are reimbursements for work carried out in Contract 1252 Contract Modification #41 on behalf of San Francisco PUC (includes construction management cost).
12	Traffic Effectiveness Project funded Contract Modification #40 for Culvert, Street & Sidewalk Restoration in North Beach are reimbursements for work carried out in Contract 1252 on behalf of SFMTA SSD.
13	SFPUC North Beach 24" Water Main Additional Work funds are reimbursements for work carried out in Contract 1252 Contract Modification #48 on behalf of San Francisco PUC (includes construction management cost).

7.4 Contingency Management Trend Report	
14	In Oct 2014 Report, updated Contract 1250 contract cost to segregate contract amount and contract modification amount. Note that September 2013 Supplemental Authorized Contingency "column f" did not include completed contract.
15	In Oct 2014 Report, updated Contract 1251 contract cost to segregate contract amount and contract modification amount. Note that September 2013 Supplemental Authorized Contingency "column f" did not include completed contract.

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16	Contract 1252 Original Contract Value "column a" and Original Congency "column f" did not match September 2013 Supplemental due to Supplemental were used the revised value to reflet Contract Modifications #3-#18. Reduced Contract 1252 contingency to reflect CMod #20 for retrieval shaft relocation cost \$5.15M funded by CPT690, CMod #40 for Culvert, Street & Sidewalk Restoration cost \$694,651 funded by Traffic Effectiveness Project, and CMod #41 for install 24" Water Main in North Beach cost \$328,860 funded by SFPUC. In August 2015 report, release \$15M CN1252 Tunnel assigned contingency to program unallocated contingency.
17	BART Elevator scope and SFPUC Sewer Main scope is in Contract 1300; effort will be funded by BART. In January 2015 Report, corrected Station Contract value to match awarded amount.
18	In April 2015 report, real estate budget stated in RAMP Rev5 is \$36.7M, including \$1M contingency. The cost workbook ROW & contingency budget reflects this with \$36,511,799 and \$1,000,000 respectively. Revised costbook ROW budget & contingency to be \$37,511,799. The \$4,265,478 Caltrans lease savings is allocated to ROW allocated contingency.
19	In Dec 2014 Report, redistributed LRV budget to reflect recent firm bid cost per vehicle (\$3,327,250/unit) from vehicle procurement contract award. (SFMTA Board meeting 15JUL14, calendar item #11). Vehicle line item total budget remains unchanged, redistributed fund by reducing base amount to \$13,309,000, column "c" and increased allocated contingency column "h", by same amount.
20	In Oct 2014 Report, made two corrections: i) revised Professional Services, Original Contract Value "column a" from \$310,518,041 to \$310,618,041, ii) revised Original Congency. "column f" unallocated contingency from \$3,883,481 to \$3,845,945. In April 2015 report, used \$500K program contingency for CS-175 Bayland Soil Process contract. In August 2015 report, added \$15M from Contract 1252.
21	The total Central Subway Project budget of \$1.578 billion, based on the October 2012 FFGA with the FTA, is the primary MPR report reference.
22	Estimate at Completion is shown at Column "e".
23	Estimate at Completion vs. Budget variance is shown at Column "k".

7.5 Contract Modification/Trend Log - Contract 1300 Stations

24	Reported all trend cost for Contractor Change Order Requests and Proposed Contract Change and applied probability to forecasted trends.
----	---

7.6 Budget Revisions: Report Sorted by Construction Packages

25	In Dec 2014 Report, reduced CN1252 allocated contingency by \$28K to excute Contract Modification #46, see Report 7.5 Detail Contingency Usage Report. In August 2015 report, release \$15M CN1252 Tunnel assigned contingency to program unallocated contingency.
26	Program contingency increased by \$500,000. Refer to Report Notes #20.
27	Program contingency increased by \$5,265,478. Refer to Report Notes #18.

28	In Dec 2014 Report, redistributed LRV budget to reflect recent firm bid cost per vehicle (\$3,327,250/unit) from vehicle procurement contract award. (SFMTA Board meeting 15JUL14, calendar item #11). Vehicle line item total budget remains unchanged, redistributed fund by reducing base amount to \$13,309,000 and increased allocated contingency by same amount.
29	In April 2015 report, program contingency decreased by \$500,000. Refer to Reports #20. In August 2015 report, release \$15M CN1252 Tunnel assigned contingency to program unallocated contingency.

7.9 Detail Monthly Expenditure Report

Phase 1 Preliminary Engineering

30	Famis cost for Preliminary Engineering (PE) is \$48,210,903.71. Cost Report for Preliminary Engineering (PE) is \$46,542,060. Some Design cost reported in Famis were moved to Design Phase, refer to Notes #19 and #20.
----	--

Phase 2 Design Phase

31	Famis cost adjustment to transfer Project Management cost from July 2013 to Phase 3 Construction Phase.
32	Famis Phase 1 PE Index Code: 357906.CPT5441112 cost is \$10,222,939 \$8,949,300 is reported in Cost Report Phase 1 PE and the balance of \$1,273,639 is reported in Phase 2 Design.
33	1.2.021.01.080.03 - FD:CTYCO-ARTS COMMISSION [357909ART001.CPT5441227]: FAMIS: \$1,425,167 Cost Report: \$1,425,167 cost is reported in Phase 2 Design, 1.2.021.01.080.03 Cost Transfer: Remaining cost is reported in Phase 3 Construction, 1.3.021.01.080.03 - ARTS:CTYCO-ARTS COMMISSION [357909ART001.CPT5441227]
34	1.2.055.01.080.02 - FD:ODCs - 651 BRANNAN STREET [35CPT5441241.CPT5441241]: FAMIS: \$2,294,910 Cost Report: \$2,294,910 1.2.055.01.080.02 Cost Transfer: Future costs to be allocated to 1.3.055.01.080.02 - FD:ODCs - 651 BRANNAN STREET [35CPT5441241.CPT5441241]
35	1.2.063.01.080.03 - AECOM.CS149 OM-EPC JV CS149-PM [68CPT544133D.CPT544133D]: FAMIS: \$4,698,167 Cost Report: \$4,698,167 on 1.2.063.01.080.03 Cost Transfer: Future costs to 1.3.063.01.080.03 - AECOM.CS149 OM-EPC JV CS149-PM [68CPT544133D.CPT544133D]
36	AVA Cost \$395,204 is reported in Phase 2 Final Design 1.2.066.01.080.03
37	1.2.071.01.080.02 - FD:FINAL DESIGN-DP1 [35CPT5441232.CPT5441232]: FAMIS: \$5,608,147 Cost Report: \$5,469,336 Cost Transfer: \$138,811 to 1.3.071.01.080.04 - FD:FINAL DESIGN-DP1 [35CPT5441232.CPT5441232]

38	1.2.072.01.080.02 - FD:FINAL DESIGN-DP2 [35CPT5441233.CPT5441233]: FAMIS: \$26,268,511 COST REPORT: \$26,220,609 COST TRANSFER: \$47,902 to 1.3.072.01.080.04 - FD:FINAL DESIGN-DP2 [35CPT5441233.CPT5441233]
39	1.2.073.01.080.02 - FD:FINAL DESIGN-DP3 [35CPT5441236.CPT5441236]: FAMIS: \$11,502,372 COST REPORT: \$11,432,312 COST TRANSFER: \$70,060 to 1.3.073.01.080.04 - CM: DP3 [35CPT5441236.CPT5441236]

Phase 3 CONSTRUCTION PHASE	
40	1.3.021.01.080.03 - ARTS:CTYCO-ARTS COMMISSION [357909ART001.CPT5441227]: FAMIS: \$1,525,982 Cost Report: \$1,425,167 1.2.021.01.080.03 Cost Transfer: any future costs to 1.3.021.01.080.03
41	1.3.055.01.080.02 - FD:ODCs - 651 BRANNAN STREET [35CPT5441241.CPT5441241]: FAMIS: \$2,294,910 Cost Report: \$2,294,910 1.2.055.01.080.02 - FD:ODCs - 651 BRANNAN STREET [35CPT5441241.CPT5441241] Cost Transfer: Future costs to be allocated to 1.3.055.01.080.02
42	1.3.063.01.080.03 - AECOM.CS149 OM-EPC JV CS149-PM [68CPT544133D.CPT544133D]: FAMIS: \$4,698,167 Cost Report: \$4,698,167 on 1.2.063.01.080.03 Cost Transfer: Future costs to 1.3.063.01.080.03 - AECOM.CS149 OM-EPC JV CS149-PM [68CPT544133D.CPT544133D]
43	Used \$500K program contingency for CS-175 Bayland Soil Process contract. Refer to Report Notes #20.
44	1.3.071.01.080.04 - FD:FINAL DESIGN-DP1 [35CPT5441232.CPT5441232]: FAMIS: \$5,608,147 Cost Report: \$5,469,336 Cost Transfer: \$138,811 to 1.3.071.01.080.04 - FD:FINAL DESIGN-DP1 [35CPT5441232.CPT5441232]
45	1.3.072.01.080.04 - FD:FINAL DESIGN-DP2 [35CPT5441233.CPT5441233]: FAMIS: \$26,268,511 COST REPORT: \$26,220,609 COST TRANSFER: \$47,902 to 1.3.072.01.080.04 - FD:FINAL DESIGN-DP2 [35CPT5441233.CPT5441233]
46	Contract 1251 Final cost is \$20,794,582.
47a	In July 2015 Report, used Contract 1300 Contractor schedule to report budget and actual cost. The Standard Cost Categories (SCC) allocation changed from previous reports. In August 2015 Report, adjusted some of Contract 1300 Contractor SCC assignment to match most of previous SCC assignment.
47	Revised Contract 1252 allocated contingency SCC code from 040.08 to 010.07.

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48	Revised Contract 1300/UMS allocated contingency SCC code from 040.08 to 020.03.
49	Revised Contract 1300/CTS allocated contingency SCC code from 040.08 to 020.03.
50	Revised Contract 1300/YBM allocated contingency SCC code from 040.08 to 020.03.
51	Revised Contract 1300/STS allocated contingency SCC code from 040.08 to 020.01.
52	Revised Form B Reimbursements SCC code from 900.01 to 040.02
53	Revised Form B Reimbursements SCC code from 900.01 to 040.02
54	Revised Form B Reimbursements SCC code from 900.01 to 040.02
55	Revised Form B Reimbursements SCC code from 900.01 to 040.02
56	Revised Form B Reimbursements SCC code from 900.01 to 040.02
57	Revised Form B Reimbursements SCC code from 900.01 to 040.02
58	Revised Form B Reimbursements SCC code from 900.01 to 040.02
59	Revised Form B Reimbursements SCC code from 900.01 to 040.02
60	Increase Program contingency \$1,023,508. Refer to Report Notes #11 and #12. In April 2015 report, program contingency decreased by \$500,000. Refer to Report Notes #20. In August 2015 report, release \$15M CN1252 Tunnel assigned contingency to program unallocated contingency.

Appendix B

DETAIL SCHEDULE REPORTS

Revision 1: revised work packages work perform next month

SCHEDULE HIGHLIGHTS

The Master Project Schedule (MPS) below includes progress through December 2015. The February 2016 Schedule Update submittal from Contract 1300 Contractor is rejected based on erroneous logic changes. The Contractor's February 2016 Schedule Update shows a 4-day improvement on the Critical Path. The Contract 1300 schedule represented in this report is based on the December 2015 Update. The Program is continuing to working on a properly updated Contract 1300 schedule.

The MPS shows a forecast Revenue Service Date of May 24, 2019.

The controlling critical (longest) path of the MPS runs through CTS Excavation succeeded by STS Startup & Testing, Commissioning and Pre-Revenue Activities to the Baseline Finish and Revenue Service Date. See Appendix B – Longest Path. The latest schedule shows the longest path running through the Chinatown Station (CTS). The CN-1300 Contractor continues to fall behind schedule by not following their current plan and by not working on critical activities to meet schedule goals and Contract Milestones. Contractor is required to implement a Recovery Schedule to put the Project back on schedule.

Schedule Contingency is fully utilized on the critical path of the MPS, which is below the Minimum Schedule Contingency level of 6 months. A schedule re-evaluation will be performed, utilizing the updated Contract 1300 Schedule. Recovery options are being implemented in key areas as work proceeds. SFMTA continues to meet with Contractor to discuss all schedule concerns and comments. The Contractor, Tutor Perini Corporation's (TPC) schedule continues to show schedule slippage due to lack of schedule progress on Critical Path activities.

The 1300 Contractor submitted fifteen (15) Schedule Updates from December 2014 to February 2016. SFMTA rejected six (6) Schedule Updates from September 2015 to February 2016. Contractor has been directed to develop a Recovery Schedule as required by Contract and correct out-of-sequence and Retained Logic driving many of the forecast dates. Review of schedule updates as well as identifying recovery options is ongoing.

Contract 1300 - WP1253 UMS / WP1254R CTS / WP1255 YBM / WP1256 STS:

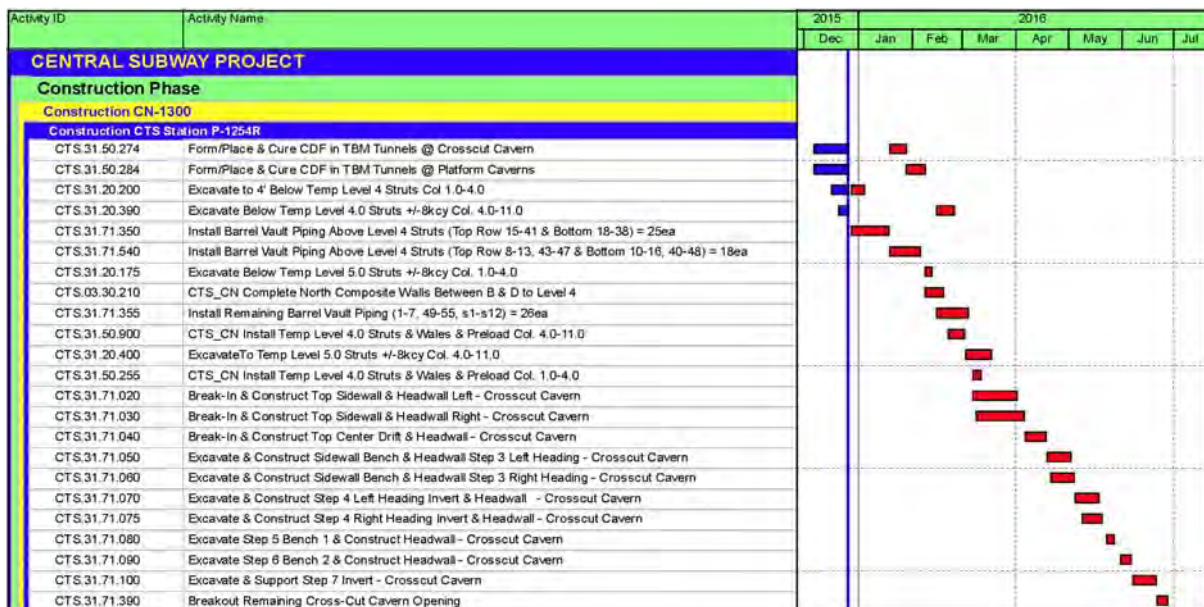
The Contractor, Tutor Perini Corporation's (TPC) baseline schedule is incorporated into the master program. The preliminary Tutor Perini Corporation's (TPC) December schedule is used in February Report.

Work Package P-1254R (CTS) has performed the following work this month:

- Crosscut Cavern: continue coring barrel vaults, started drilling and installing Barrel Vaults.
- Excavation to elevation – 50'-60' level.
- Connections for the brick sewer
- Completed CDF backfill from Headhouse, SB Tunnel platform south
- Headhouse: Install rebar and form and pour west wall
- Chinatown Moratorium in February, before Chinese New Year to accommodate street fair

Work Package P-1254R (CTS) will perform the following work next month

- Utility demolition
- Headhouse: Install Temp level 4.0 Struts and excavate to Temp level 5.0 Struts
- Crosscut Cavern: complete barrel vault piping
- Install spilling over South Egress Exit
- Complete water distribution



Schedule: Contract 1300 December 2015 Update

Work Package P-1253 (UMS) has performed the following work this month:

- Traffic Handling – Stockton Street closure between Geary and Ellis, no southbound lane on Stockton between Post and Geary.
- South Concourse and Platform Station: continued mobilizing jet grout operations

- Platform Station: Resumed jet grouting installation. Excavated and chipped piles for roof deck sections 1A and 4A-9, installed temporary bridge at 1A, installed metal decking, studs, and rebar for roof deck pour 9-4A
- North Concourse: Set up compensation grout plant and drill and install compensation grout pipes; perform utility work on Geary Street and Stockton Streets; chip piles in preparation for wall construction
- UMS Garage: Continued demolition on the plaza level
- UMS Garage: Continued installation of tiebacks
- UMS Garage: Continue work on shear walls between new ramp and Stockton Street

Work Package P-1253 (UMS) will perform the following work next month:

- North Concourse: Complete compensation grouting; continue chip, pile, install, rebar mesh & shotcrete leveling
- Platform Station: Continue jet grout installation; deck, form, rebar, pour station box;
- Start backfill Roof Deck 4A at Platform Station
- Access Shaft: Excavate, lag and support; install temporary lid
- Ellis Street: Install water main on south side
- UMS Garage: continue demolition; installing bracing & shoring walls & slabs



Schedule: Contract 1300 December 2015 Update

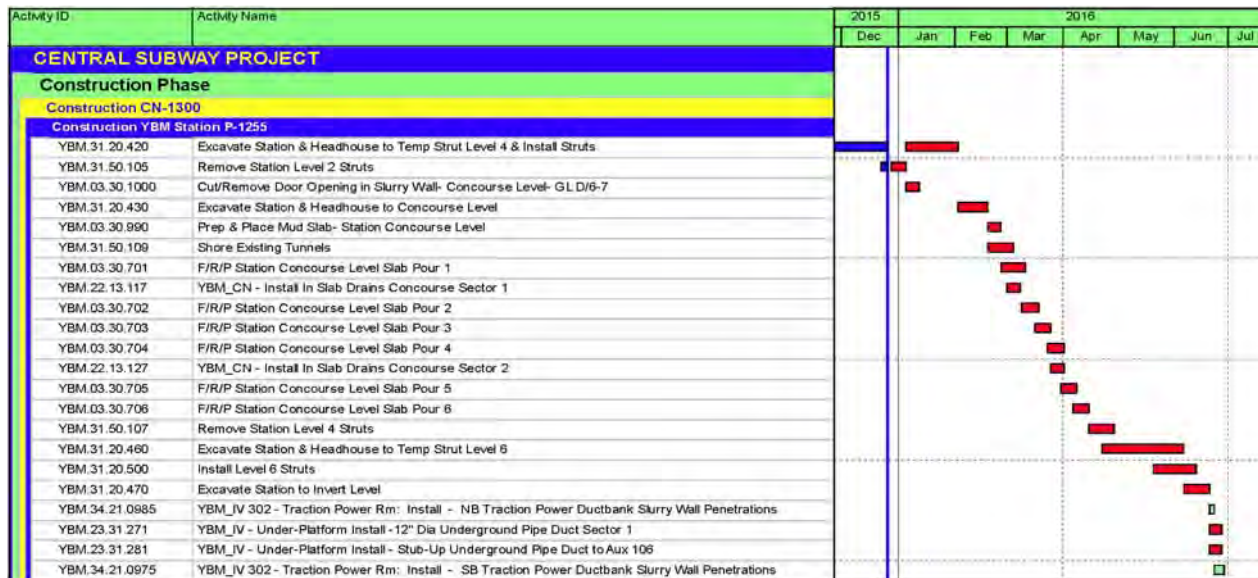
Work Package P-1255 (YBM) has performed the following work this month:

- Traffic flowing on east side of 4th Street with west side work area closed for construction

- Station box level 4 temporary bracing installation is in progress beneath Mezzanine level; excavation for Concourse level slab is in progress below level 4 bracing.
- Temporary bracing is being installed within tunnel to provide support during installation of Concourse level beams and slab.
- Headhouse level 4 temporary bracing installation has begun.
- Archaeological monitoring by spot checking is in progress.

Work Package P-1255 (YBM) will perform the following work next month:

- Continue excavation and temporary bracing installation in headhouse and station box, including beginning excavation to allow Concourse level slab installation
- Station box interior walls: rebar for type C wall on Mezzanine level GL 05-08
- Install in slab drains Concourse sector 1
- Continue utility installation on 4th Street north of the north headwall and on Folsom Street



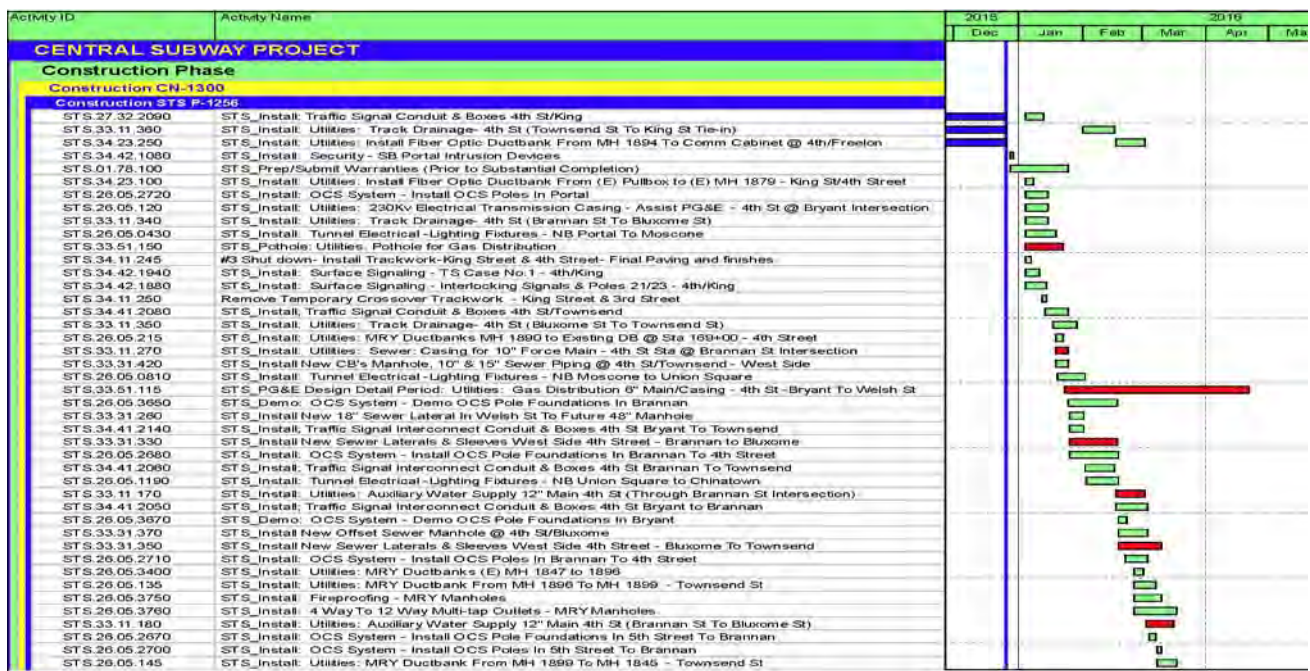
Schedule: Contract 1300 December 2015 Update

Work Package P-1256 (STS) has performed the following work this month:

- Continue Muni ductbank installation
- Continue 78" sewer rehabilitation
- Continue 36" sewer force main
- Continue Auxiliary Water Supply System (AWSS) installation
- Continue water line work
- Continue AT&T cutover
- Continue tunnel prep work
- Continue OCS pole foundation installation

Work Package P-1256 (STS) will perform the following work next month:

- Continue 78" sewer rehabilitation on west side of 4th St.
- Continue MRY ductbank and vault installation at 4th/Freelon
- Completed 36" force main at intersection 4th and Townsend and continue work north of Townsend
- Continue 4th/King sewer structure modification
- Continue tunnel prep work
- Continue OCS pole foundation installation
- AT&T on site working on cutover



Schedule: Contract 1300 December 2015 Update

SCHEDULE REVISIONS

No Contract 1300 schedule update was added this period to the Central Subway Project Master Schedule.

LIST OF SCHEDULE REPORTS

No detail reports provided in this monthly period update.

Appendix C

PROJECT SCOPE AND FUNDING OVERVIEW

Project Overview

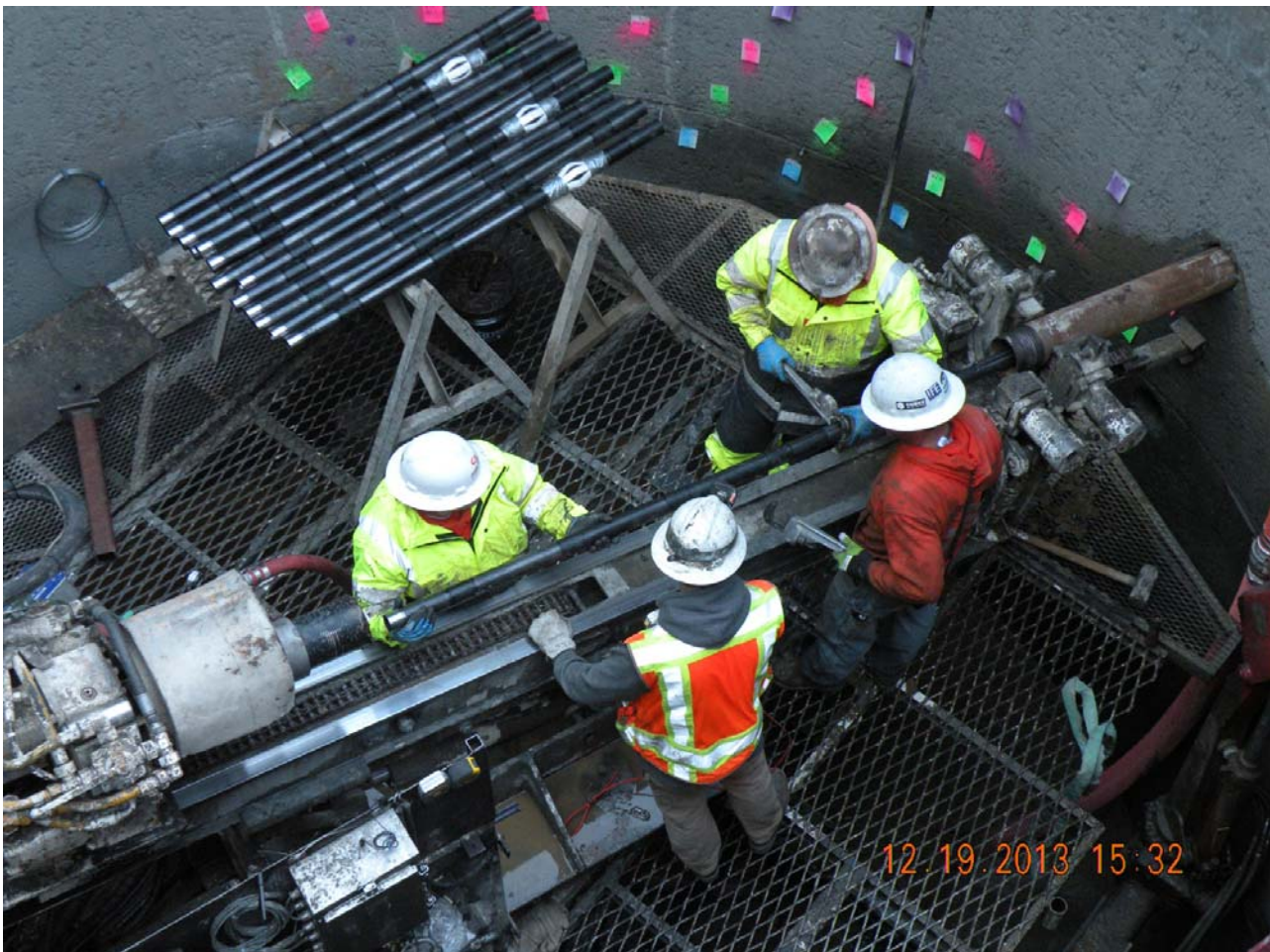
The Central Subway Project will construct a modern, efficient light-rail line that will improve public transit in San Francisco. This new 1.7-mile extension of Muni's T Third Line will provide direct connections to major retail, sporting and cultural venues while efficiently transporting people to jobs, educational opportunities and other amenities throughout the city.

The Central Subway Project is Phase 2 of the San Francisco Municipal Transportation Agency's (SFMTA) Third Street Light Rail Transit Project. Phase 1 of the project constructed a 5.1-mile light-rail line along the densely populated 3rd Street corridor. It began revenue service in April 2007, restoring light-rail service to a high transit-ridership area of San Francisco for the first time in 50 years.

The Central Subway Project will extend the T Third Line from the 4th Street Caltrain Station to Chinatown, providing a direct, rapid transit link from the Bayshore and Mission Bay areas to SoMa, Union Square and downtown.

Four new stations will be built along the 1.7-mile project alignment—an above-ground station at 4th and Brannan streets and three underground stations at Moscone Center, Union Square and Chinatown.

The Central Subway will run through the burgeoning technology and digital-media hub in SoMa, where dozens of companies have taken up residence along the 4th Street corridor. Increased



Project Overview - continued

transit options will attract new employers – the Central Subway makes travel more convenient throughout the corridor and improves connections to downtown, local and regional rail and the Muni bus system.

The Central Subway Project will contribute to San Francisco’s economic competitiveness and help secure the city’s status of a regional, national and global hub. It will provide a pollution-free transit option that will reduce the environmental impact of transportation in the city, save natural resources, reduce traffic congestion and improve public transit for thousands of San Franciscans.

Funding Overview

The Central Subway Project is funded by the federal government, the State of California, the Metropolitan Transportation Commission, the San Francisco County Transportation Authority (SFCTA) and the City and County of San Francisco.

The majority of funding for the Central Subway Project is expected to be provided by the Federal Transit Administration’s (FTA) New Starts program, with a total commitment over the life of the project of \$942.2 million. To date, \$41 million in Department of Transportation Congestion Mitigation and Air Quality Improvement Program funds have been committed and expended.

With the addition in the December 2013 MPR of work to relocate the retrieval site for two tunnel boring machines (TBMs), the SFMTA’s baseline budget for the Central Subway Project is \$1.588 billion. In total, about half of the Third Street Light Rail Transit Project’s funding is from federal sources, with the remaining half from state and local sources. This is in line with the expectations of the FTA for New Starts-financed programs.

The table below summarizes the local, state and federal fund sources for both phases of the T Third Line including with the addition of the retrieval shaft to the Phase 2 totals.

	T Third (Phase 1)	Central Subway (Phase 2 + Retrieval Shaft Relocation)	Total (Phase 1 + Phase 2 + Retrieval Shaft Relocation)	Percentage of Total
Federal	\$123.380	\$983.225	\$1,106.605	49.5%
State	\$160.700	\$471.100	\$631.800	28.2%
Local	\$364.380	\$133.675	\$498.055	22.3%
Total	\$648.460	\$1,588.000	\$2,236.460	100.0%

All amounts in millions of dollars

The six charts that follow summarize use of fund sources by phase and with the addition of the retrieval shaft relocation additional budget and funding:

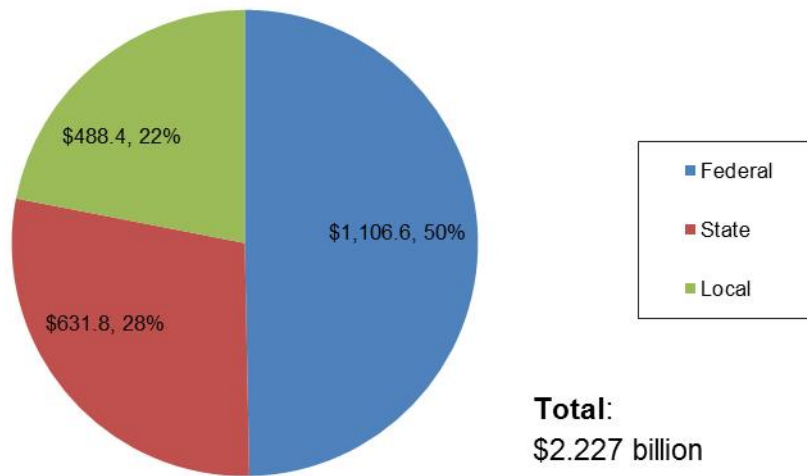
- Phase 1 + Phase 2 of the T Third Line federal, state and local funding percentages previous to the addition of the retrieval shaft relocation budget and funding in December 2013.

Funding Overview - continued

- Phase 2 Central Subway Project only total funding source percentages previous to the addition of the retrieval shaft relocation budget and funding.
- Phase 2 Central Subway Project only detail of the six State and Local funding sources previous to the addition of the retrieval shaft relocation.
- The next three charts that follow are the above three data sets above with the retrieval shaft relocation budget and funding added to the overall presentation.

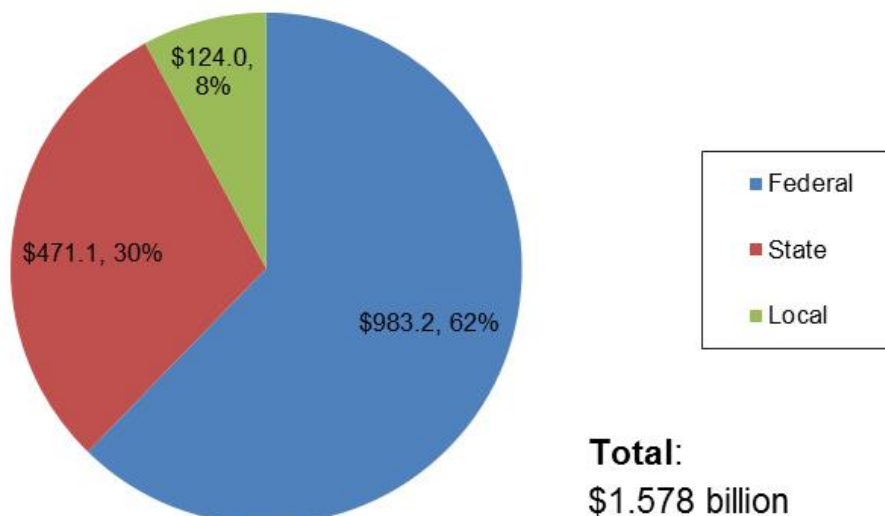
Third Street Light Rail Transit Project Funding

Phase 1 + Phase 2
(\$ in millions)



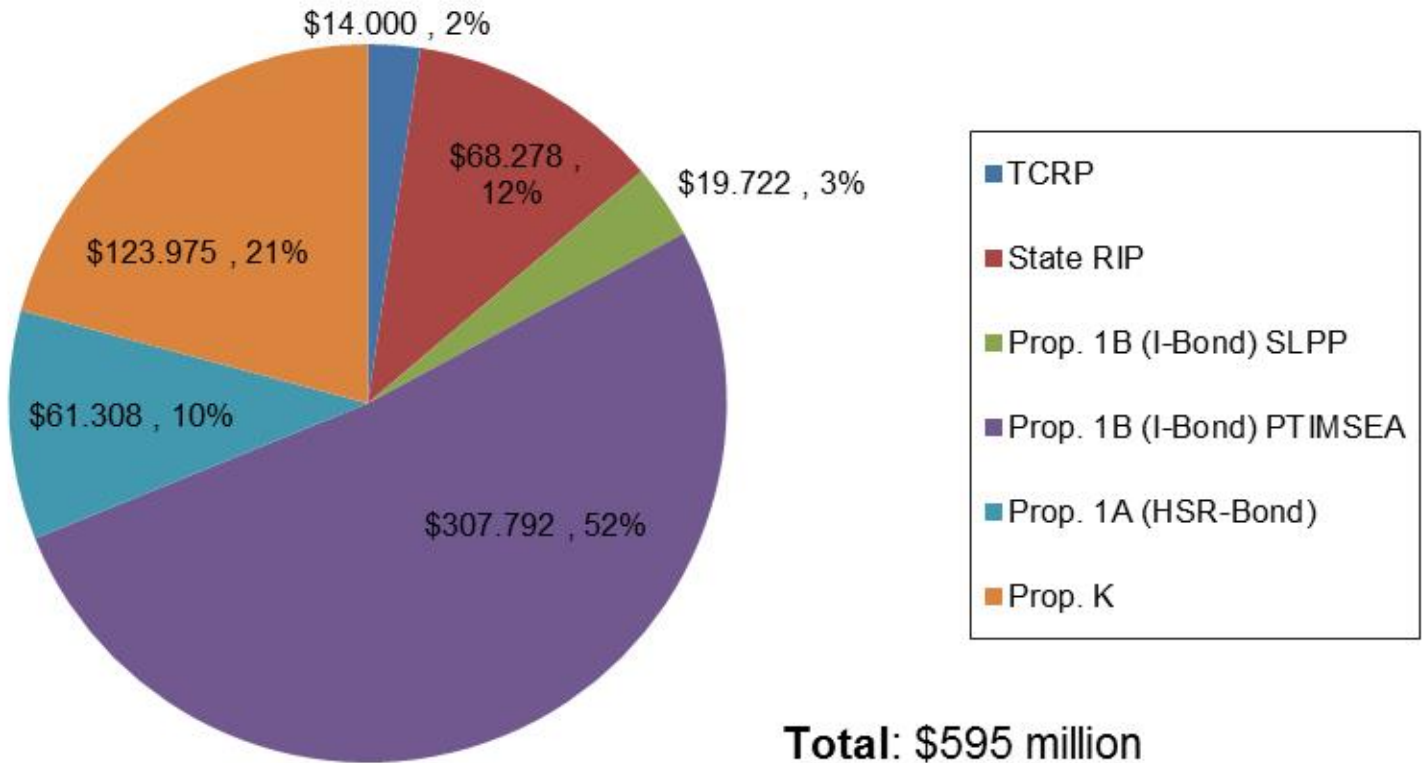
Central Subway Project Funding

Phase 2
(\$ in millions)



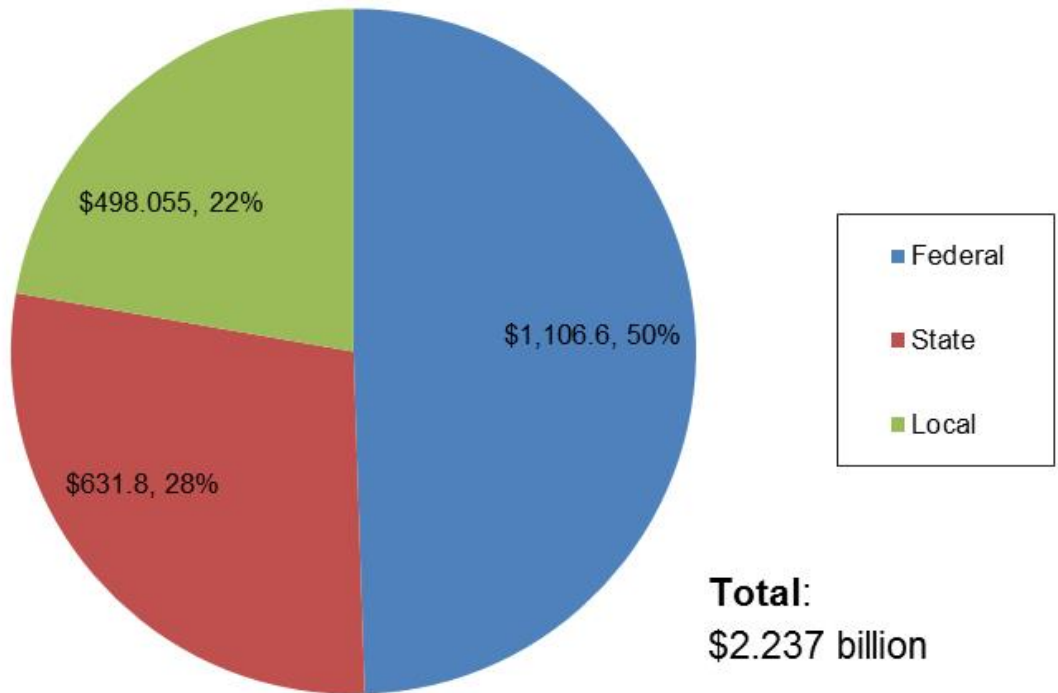
State and Local Funding

Phase 2
(\$ in millions)



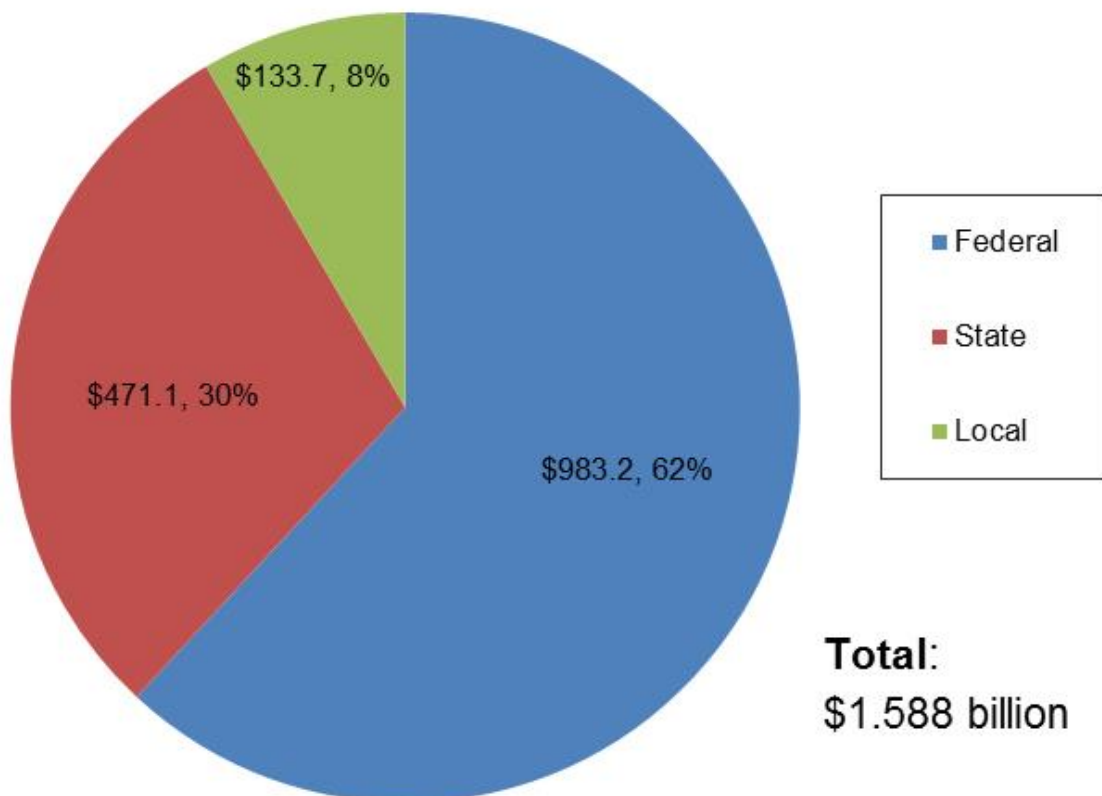
Third Street Light Rail Transit Project Funding

Phase 1 + Phase 2 + Retrieval Shaft Relocation
(\$ in millions)

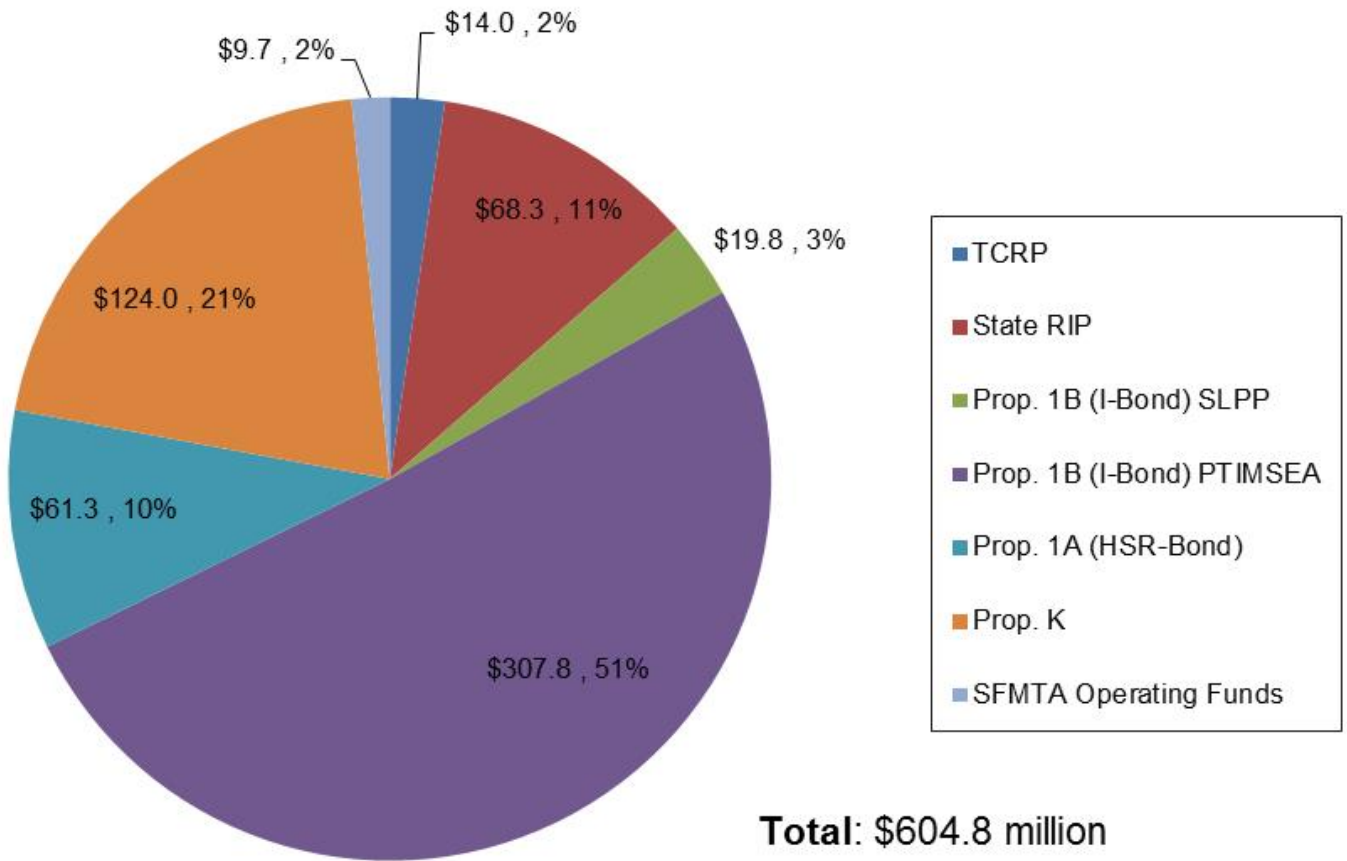


Central Subway Project Funding

Phase 2 + Retrieval Shaft Relocation
(\$ in millions)



State and Local Funding Phase 2 + Retrieval Shaft Relocation (\$ in millions)



Appendix D

COMPLETED CONTRACTS

Moscone Station and Portal Utility Relocation

Contract 1250

Contractor: Synergy Project Management, Inc.

Budget/Expenditures

Category	Amount
Original Budget	\$11,227,316
Expenditures Final	\$11,968,150
Utility Reimbursements	(\$2,275,419)
Final Program Cost	\$9,692,731
Budget Impact (Underrun)	(\$1,534,585)

Contract Details

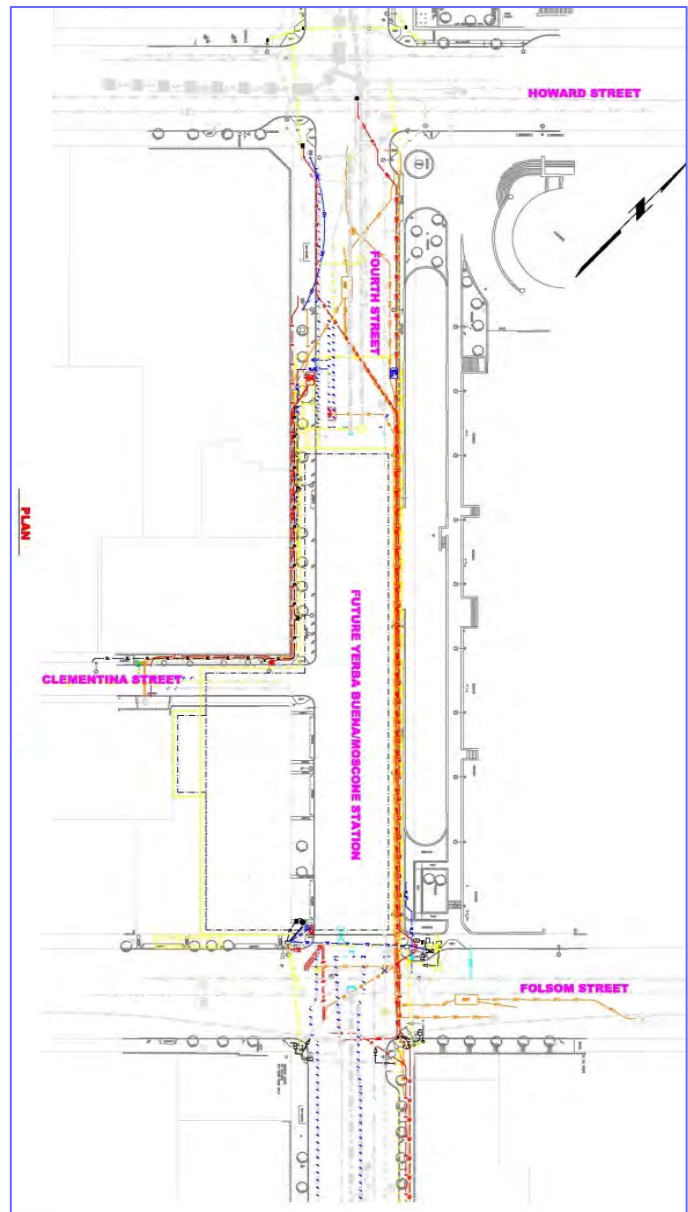
Contract Awarded:	November 17, 2009
Notice to Proceed:	January 4, 2010
Substantial Completion:	June 23, 2011
Contract Award Value:	\$ 9,273,939
Modifications Final :	\$ 2,694,211
Final Contract Value:	\$11,968,150

Status

- Work complete
- Project closeout administration and documentation
- Final Completion Date: June 23, 2011

Description

This project relocates utilities within the footprint of the proposed Yerba Buena/Moscone Station and the 4th Street Portal where the tunnel boring machines will descend underground. Also included is installation of building protections and monitoring of buildings adjacent to utility trenches.



Union Square/Market Street Station Utility Relocation

Contract 1251

Contractor: Synergy Project Management, Inc.

Budget/Expenditures

Category	Amount
Original Budget	\$22,199,847
Expenditures Final	\$20,794,581
Utility Reimbursements	(7,413,510)
Final Program Costs	\$13,176,169
Budget Impact (Underrun)	(\$9,023,678)

Contract Details

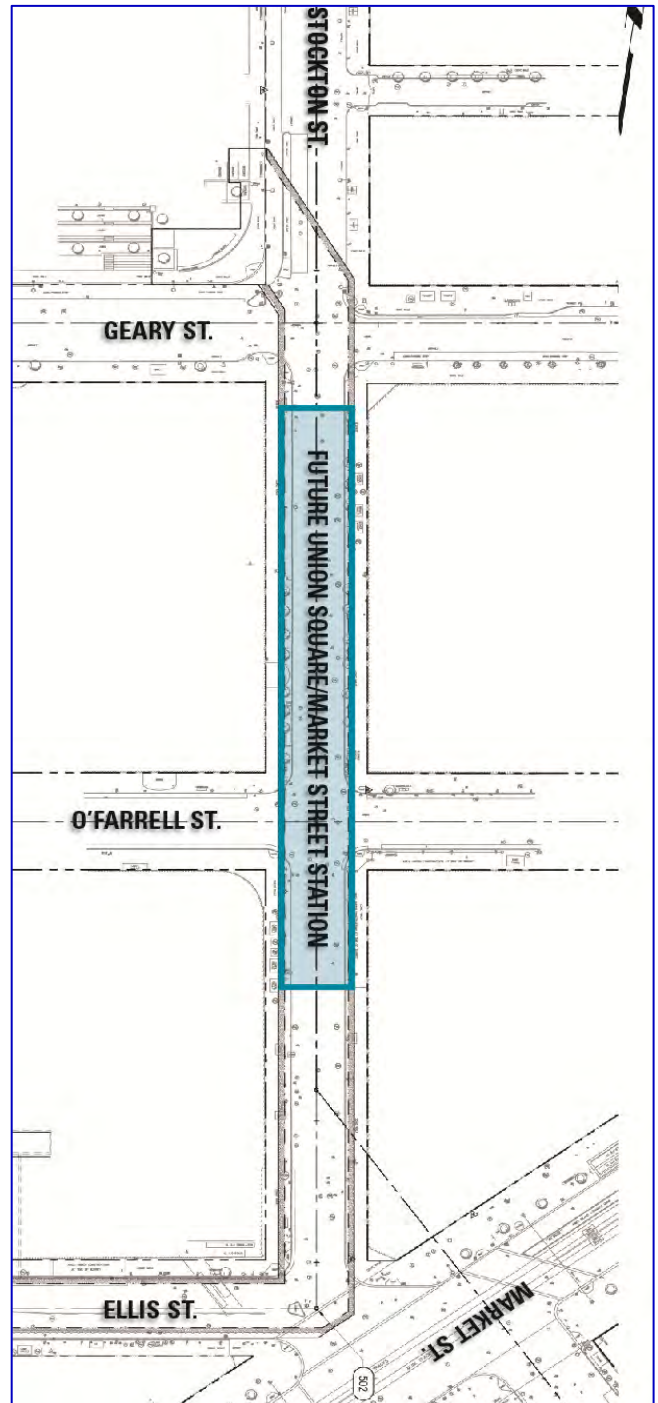
Contract Awarded:	December 7, 2010
Notice to Proceed:	January 12, 2011
Substantial Completion:	August 16, 2012
Contract Award Value:	\$16,832,550
Modifications Final:	\$3,962,031
Final Contract Value:	\$20,794,581

Status

- Final completion date October 15, 2012.
- Completed punch list work
- Project Final Acceptance by the SFMTA Board of Directors

Description

This project relocates utilities for the Union Square/Market Street Station and temporarily reroutes existing trolley coach lines around the construction.



Central Subway Pagoda Palace Demolition

Contract 1277 Contractor: MH Construction

Work Description

Demolish and clear the former Pagoda Theater for use the site to recover the tunnel boring machines when tunnels are completed in 2015. Locate and supply contractor facilities and installations. Obtain permits and approvals and coordinate work with City agencies and utility companies. Furnish and install signs and distribute notices to the local community prior to commencing with construction, cleanup and remove of debris from the site.

- Work was substantially completed Sept. 24, 2013.
- Administrative closeout in progress.

Budget/Expenditures	
Category	Amount
Current Budget	\$722,592
Expenditures to Date	\$638,278

Contract Details

Contract Awarded:	June 12, 2013
Notice to Proceed:	July 15, 2013
Substantial Completion:	Sept. 24, 2013
Contract Award Value:	\$498,995
Modifications to Date:	\$108,228
Current Contract Value:	\$678,134



Central Subway Tunneling

Contract 1252 Contractor: Barnard Impregilo Healy Joint Venture

Description of Work

1.5-mile twin bore tunnels from Hwy I-80 to North Beach using two tunnel boring machines (TBMs). Contractor procurement and installation of the TBMs; construction of the TBM launch box and retrieval shaft excavation support; Yerba Buena/Moscone Station and Union Square/Market Street Station end walls; tunnel excavation and installation of precast segmental lining, the 4th Street portal transition to the surface and cross passages. Throughout, settlement monitoring and protection of existing utilities, buildings and BART tunnels.

Status

- Final Completion Date: May 15, 2015
- Administrative closeout in progress

Budget/Expenditures	
Category	Amount
Current Budget	\$251,068,968
Other Project Budget	\$5,150,000
Other Offset Credits	\$1,135,610
Expenditures to Date	\$234,616,104

Contract Details	
Contract Awarded:	June 28, 2011
Notice to Proceed 1:	January 27, 2012
Notice to Proceed 2:	March 14, 2012
Partial NTP 3:	April 12, 2012
Notice to Proceed 3:	October 15, 2012
Substantial Completion:	April 15, 2015
Contract Award Value:	\$233,584,015
Modifications to Date:	\$7,707,417
Current Contract Value:	\$241,291,432



Appendix E

SBE PARTICIPATION

Quarterly Report

Current Report: October 2015 - December 2015

PROGRAM SUPPORT CONTRACTS – SBE PARTICIPATION

Appendix E presents the Central Subway Program Small Business Enterprise or SBE goals and the actual SBE participation achieved to date – as of December 31, 2015.¹

CS Program SBE Summary Table for Professional Services and Construction Contracts

The summary compares the dollar value of the Base Contracts, the SBE Contract Goals, the percent and dollar value expended to date and the SBE actual participation to date.

Contract No.	Contractor	Services/Segment	A	B	C	D	E	F	G	
			Contract Amount	SFMTA SBE Contract Goal	Contract Expenditure to Date (Est.)	SBE Actual to Date	SBE Contract \$s = A * B	SBE Amount to Date = C * D	Contractor's SBE Goal (in Bid)	
A Project Professional Services Contracts			<i>millions</i>			<i>millions</i>				
1	149	CS Partnership	Project Management	\$85.14	30%	\$52.46	35%	\$25.54	\$18.57	31.4%
2	156	Hill International	Project Controls Task 1	\$17.11	26%	\$8.80	29%	\$4.45	\$2.55	26.0%
3	155-1	PB Telemon	Tunnels Design	\$7.94	30%	\$7.74	30%	\$2.38	\$2.30	31.6%
4	155-2	CS Design Group	Stations Design	\$36.52	30%	\$30.85	42%	\$10.96	\$13.00	36.4%
5	155-3	HNTB, Inc.- B&C	Systems, Track & Surface Station Design	\$17.23	30%	\$24.34	28%	\$5.17	\$6.82	30.0%
Subtotal Professional Services				\$163.94		\$124.18		\$48.50	\$43.24	
B Project Construction Contracts			<i>millions</i>			<i>millions</i>				
1	1250	Synergy Inc	Utility Relocation 1	\$11.97	20%	\$11.97	97.2%	\$2.39	\$11.63	96.4%
2	1251	Synergy Inc	Utility Relocation 2	\$20.79	20%	\$20.79	87.4%	\$4.16	\$18.18	94.9%
3	1252	BIH	Tunnels and Portal - in Construction	\$241.29	6%	234.62	5.8%	\$14.48	\$13.71	6.1%
4	1277	MH Construction	Pagoda Demolition	\$0.68	100%	\$0.64	100.0%	\$0.68	\$0.64	100.0%
5	1300	Tutor-Perini	Stations/Track/Systems - in Construction	\$839.82	20%	\$331.61	17.2%	\$167.96	\$57.16	25.5%
Subtotal Construction Contracts				\$1,114.55		\$599.63		\$189.67	\$101.32	
Contract	Contractor	Services/Segment	Base Contract	SFMTA Goal	Expenditures	SBE Actual	= A * B	= C * D	Bid Goal	
			A	B	C	D	E	F	G	

SBE Summary Table Notes and Sources:

- a) Column A is the base contract amount awarded. Column B is the Agency SBE goal percent for each contract awarded.

The SFMTA SBE Contract Goals are also on the Central Subway web site under the listing of on-going contracts – see “**Closed and Awarded Contracts**” at this link: <http://centralsubwaysf.com/content/closed-and-awarded-contracts>

- b) Column C shows each contract’s current amount expended to date (estimated) including accruals. Column D is the actual SBE percent level of each contract based on payments to date. Column E is the expected SBE dollar amount when the contract amount is completed and the SFMTA SBE goal achieved using this calculation: Columns A * B = Column E, the SBE Expected \$ Amount.

Column F is the actual SBE dollar amount out of the total contract expenditure to date:

Columns C * D = Column F, the SBE Expended \$ Amount.

The source of the SBE Actual percent to date and dollar amounts are Progress Payment Applications and Contractor’s monthly submittals that may include the current estimated accruals. The BIH SBE percent is from the contractor’s progress payment #40, Form 6.

¹ An SBE is a for-profit, small business concern with a three (3) year average gross revenue not exceeding \$14 million or \$12 million, depending on the scope of work to be performed, that is certified under any of the following programs: the State of California’s Small Business Program with the Department of General Services (“State Program”), the City and County of San Francisco’s LBE Program (“City Program”), or the California Unified Certification Program (“Federal DBE program”).

- c) Column G, the Contractor's SBE Goal in the submitted bid, is background information that is not calculated in the table. The table source of the Contractor's SBE Goals is from the SFMTA Contract Compliance Office. A Contractor's SBE goal in the bid is one source used by SFMTA Contract Compliance to assess and propose the Agency's SBE goal for a contract.
- d) The three construction contracts shown in **bold type, 1250, 1251 and 1277**, with gray background, are completed contracts. Little to no changes will be shown in future reports.
- e) The SBE Hill International Actual to Date SBE participation is 28.9% for the overall SFMTA contract. The Hill International data is for the Central Subway Task 1 portion of the Hill International contract to provide SFMTA Project Controls services and systems.
- f) The SBE SFMTA goal for Contract 1300 Tutor-Perini is 20% SBE with a provision of 50% for trucking.
The 1300 Tutor-Perini SBE percent Actual is based on the SBE data provided in Progress Payment #24, December 2015, SFMTA SBE FORM No. 6.
- g) The SBE SFMTA goal for Contract 1277 MH Construction was based on an SBE set-aside.

SBE Participation Details

The two tables that follow present the Central Subway's professional services and construction contract amounts, expenditures and SBE levels with additional details.

Active Professional Services Contracts - SBE Participation Details

As of: 12/31/2015

Contract:	Project Management and Construction management	
Contract No.	CS-149 Central Subway Partnership	
Status:	On-going	
	Base Contract Value	\$85,139,092
	Approved Change Orders	-0-
	Current Contract Value	\$85,139,092
	Expended to Date (est.)	\$52,457,677
	% Expended	61.6%
	SBE Participation	35.4%
Contract:	Project Controls Cost and Schedule Support	
Contract No.	CS 156 Hill International Task 1*	
Status:	On-going	
	Base Contract Value	\$17,112,873
	Approved Change Orders	-0-
	Current Contract Value	\$17,112,873
	Expended to Date (est.)	\$8,796,914
	% Expended	51.4%
	SBE Participation	29.0%
Contract:	Design Package 1 for CNs 1250, 1251 and 1252 Tunnels*	
Contract No.	CS-155-1 PB / Telemon	
Status:	Design is completed. Construction support ongoing	
	Base Contract Value	\$5,795,000
	Approved Change Orders (7)	\$2,145,159
	Current Contract Value	\$7,940,159
	Expended to Date (est.)	\$7,741,568
	% Expended	97.5%
	SBE Participation	29.7%
Contract:	Design Package 2 for 1253 UMS, 1254 CTS, 1255 YBM Stations.	
Contract No.	CS-155-2 Central Subway Design Group	
Status:	Design is completed. Construction support ongoing	
	Base Contract Value	\$35,059,252
	Approved Change Orders (1)	\$1,460,360
	Current Contract Value	\$36,519,612
	Expended to Date (est.)	\$30,845,986
	% Expended	84.5%
	SBE Participation	42.2%
Contract:	DP 3 Systems, Track work, Surface station.	
Contract No.	CS-155-3 HNTB-B&C	
Status:	Design is completed. Construction support ongoing	
	Base Contract Value	\$16,822,238
	Approved Change Orders (5)	\$312,814
	Current Contract Value	\$17,232,252
	Expended to Date (est.)	\$24,341,480
	% Expended	141.3%
	SBE Participation	28.0%

Active and Completed Construction Contracts - SBE Participation Details

Data as of: 12/31/2015

Contract:	Synergy Inc Utility Relocation 1 YBM & Launch Box	
Contract No.	1250	
Status:	Contract is completed and closed out	
	Base Contract Value	\$9,273,939
	Approved Change Orders	\$2,694,211
	Final Contract Value	\$11,968,150
	% Expended	100%
	SBE SFMTA Goal	20%
	SBE Participation To Date	97.2%

Contract:	Synergy Inc Utility Relocation 2 UMS	
Contract No.	1251	
Status:	Contract is completed and closed out	
	Base Contract Value	\$16,832,550
	Approved Change Orders	3,962,031
	Final Contract Value	\$20,794,581
	% Expended	100%
	SBE SFMTA Goal	20.0%
	SBE Participation To Date	87.4%

Contract:	Pagoda Palace Demolition / MH Construction	
Contract No.	1277	
Status:	Construction is complete. Contract in Close Out	
	Base Contract Value	\$498,995
	Approved Change Orders	\$179,139
	Current Contract Value	\$678,134
	Expended to Date (est.)	\$638,278
	% Expended	94.12%
	SBE SFMTA Goal	100.0%
	SBE Participation To Date	100.0%

Contract:	Tunnels Barnard/Impregilo/Haley	
Contract No.	1252*	
Status:	Construction is underway and ongoing	
	Base Contract Value	\$233,584,015
	Approved Change Orders	\$7,707,417
	Current Contract Value	\$241,291,432
	Expended to Date (est.)	\$234,616,104
	% Expended	97.2%
	SBE SFMTA Goal	6.0%
	SBE Participation To Date	5.8%

Contract:	Stations and Systems / Tutor Perini	
Contract No.	1300*	
Status:	Construction is underway and ongoing	
	Base Contract Value	\$839,676,400
	Approved Change Orders	\$144,947
	Current Contract Value	\$839,821,347
	Expended to Date (est.)	\$331,610,147
	% Expended	39.5%
	SBE SFMTA Goal	20.0%
	SBE Participation To Date	17.2%

Notes:

* Contract 1252 and Contract 1300 Dec cost are accrual.

Photos on the next page:

Following the tremendous success of Winter Walk 2014, the green turf, vintage-inspired lamp posts, and nightly light show were set up once more for the 2015 holiday season. Off the Grid food trucks were present for four days a week, offering a wide variety of culinary delights. Winter Walk's pedestrian plaza was set up on Stockton between Geary and Ellis.

central subway

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This document is published by the SFMTA and the City and County of San Francisco as a service to individuals and agencies interested in the Central Subway Project. Funding for the Central Subway is made possible through funds provided by the Federal Transit Administration, the State of California, the Metropolitan Transportation Commission and the San Francisco County Transportation Authority.



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