



Better Market Street

SFMTA Board of Directors Meeting
October 15, 2019





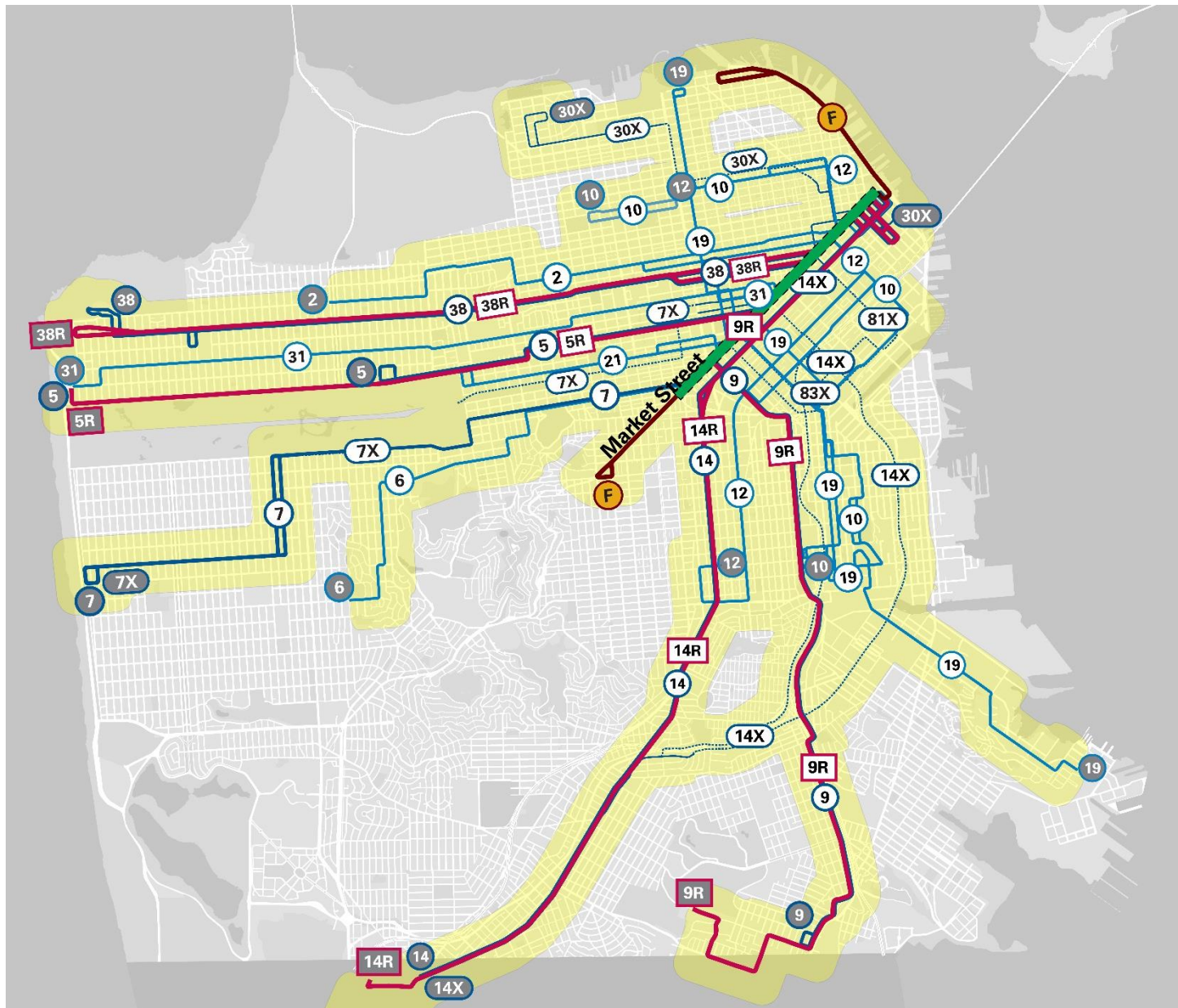
City's busiest pedestrian street

City's busiest bicycle thoroughfare

City's busiest transit corridor

City's premier cultural, civic and commercial boulevard

Market Street is Muni's backbone



- Safety challenges for all modes
- Discontinuous bike facility
- Aging infrastructure: streetlights, tracks, traffic signals, underground utilities, sidewalks
- Transit stops, curb ramps and brick paving do not meet current ADA standards
- Improving transit reliability and travel time

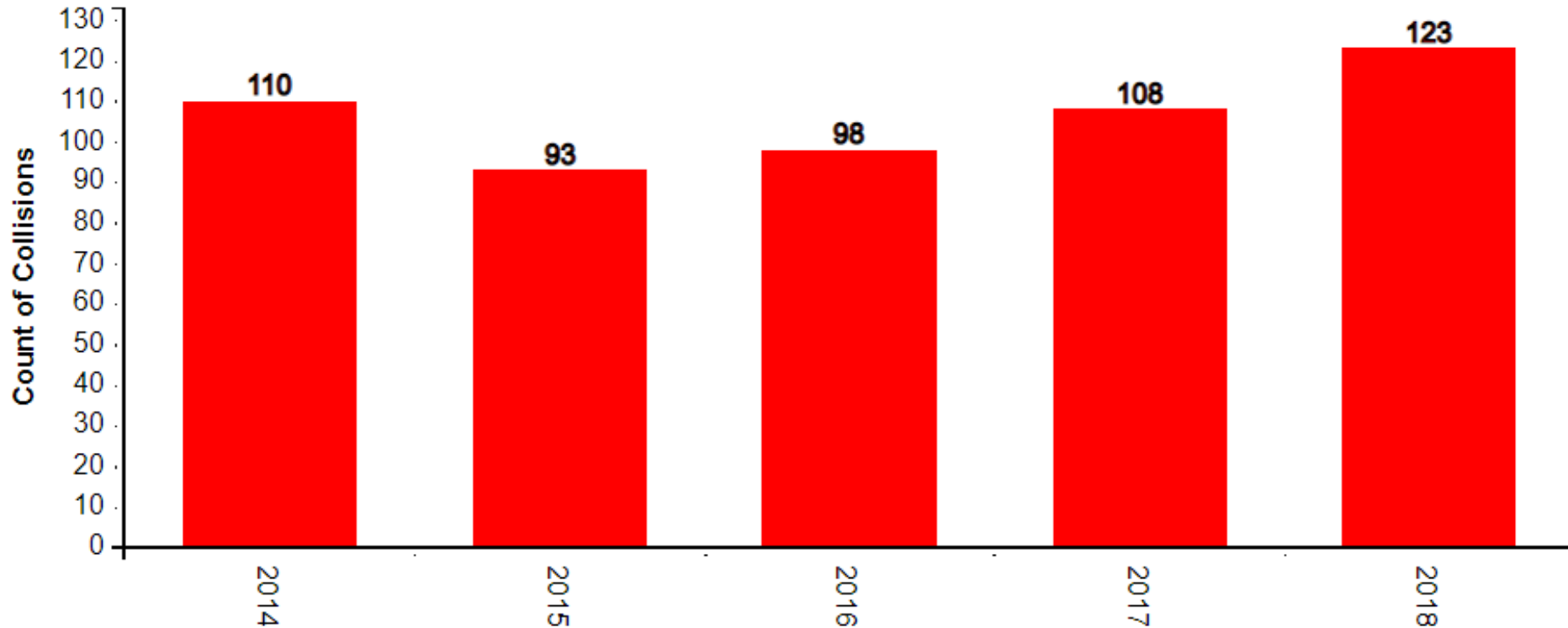




Market Street safety must be improved



Injury Collisions per year on Market Street between Octavia and Steuart

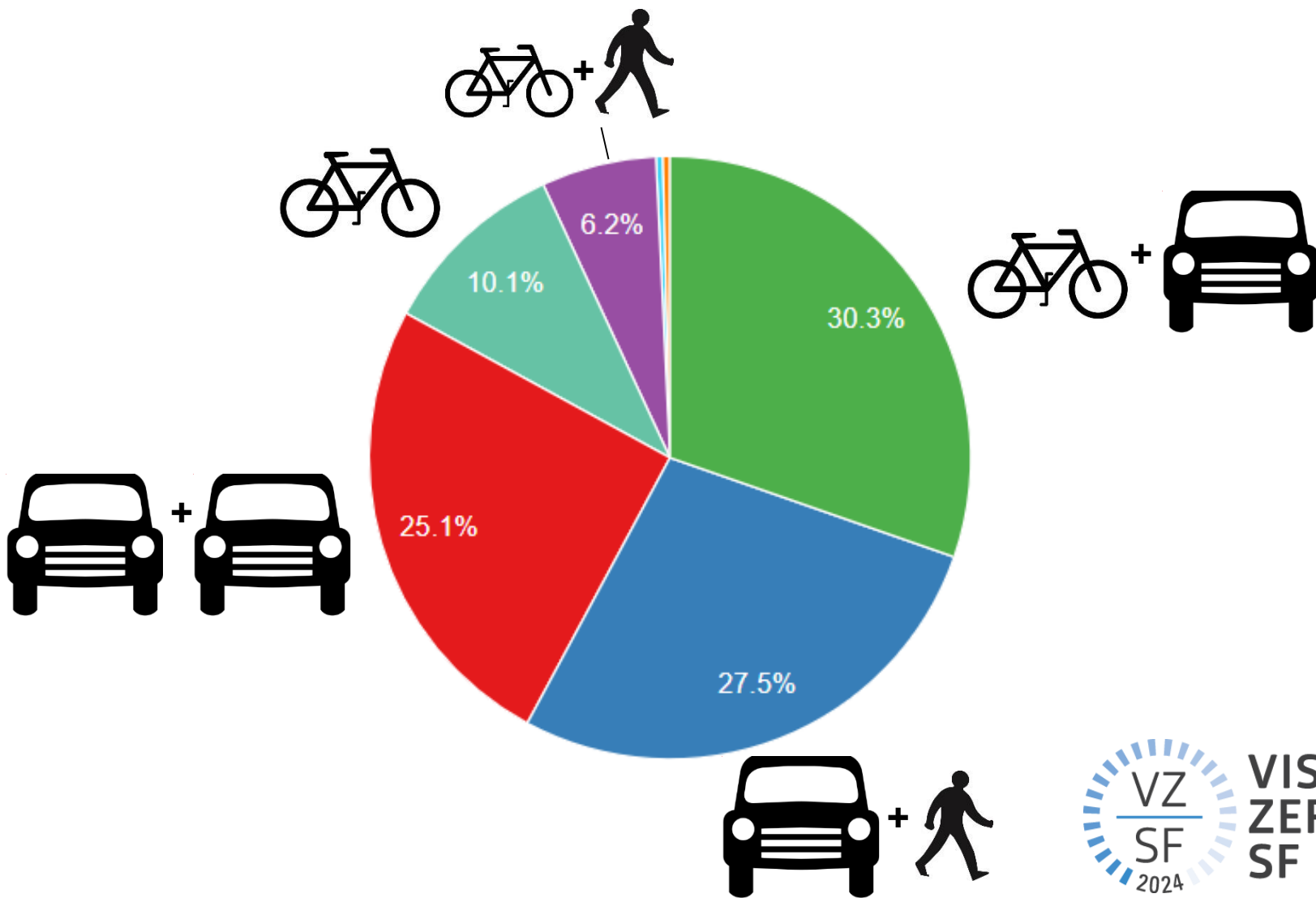




Market Street safety must be improved



75% of injury collisions involve people walking or biking





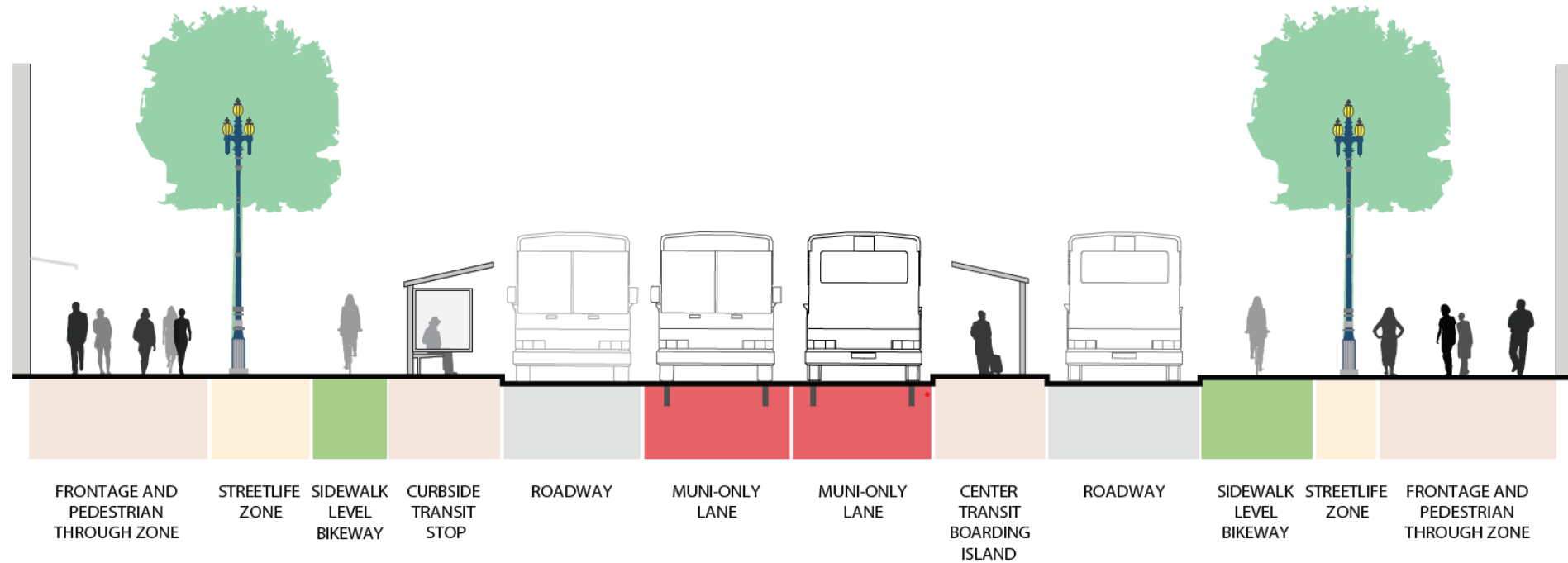
SFMTA

- Traffic Signals
- Rail
- Overhead Catenary System
- Traction Power

Partner Agencies

- Pavement
- New Sidewalks
- Curb Ramps
- Streetlights (Path of Gold)
- Sewer System
- Water System
- Communication Lines

Proposed project



- Center Muni-only lane with Rapid stop spacing
- Dedicated sidewalk-level bikeway
- Reconstructed sidewalks with street furnishing zones
- Upgraded, expanded, ADA accessible transit stops

Proposed project

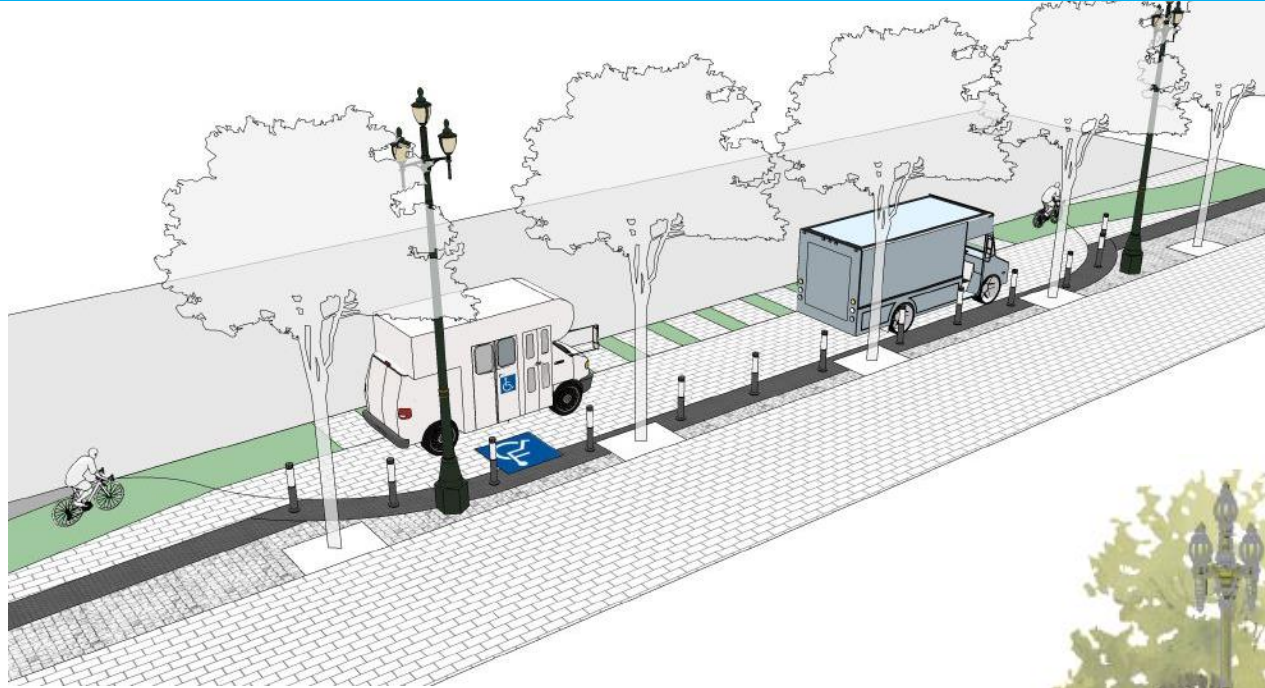


- Sidewalks rebuilt with smoother surface
- Sidewalk-level bikeway separated from pedestrians by buffer zone and tactile surface

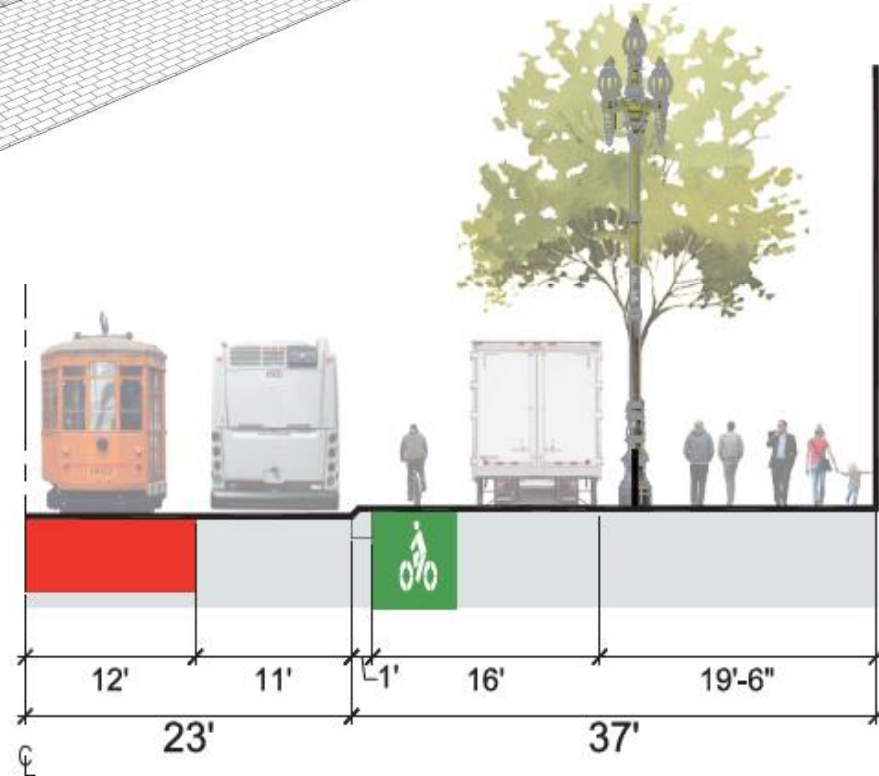




Commercial Loading & Bikeway



- Commercial loading would be allowed during non-peak hours
- Sidewalk-level flex zones allow trucks and paratransit vehicles to park on sidewalk, with space for bikes to safely maneuver around.

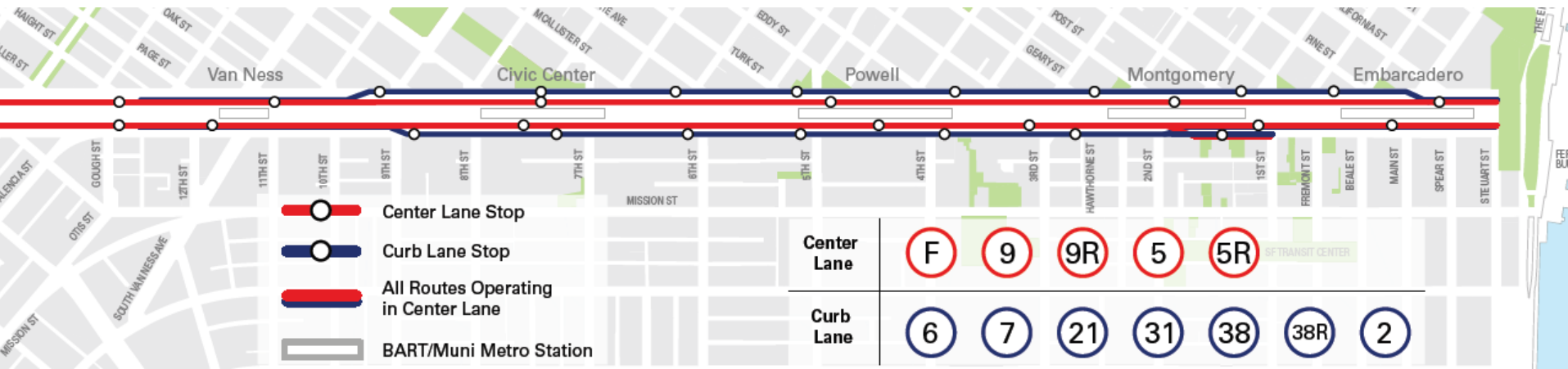




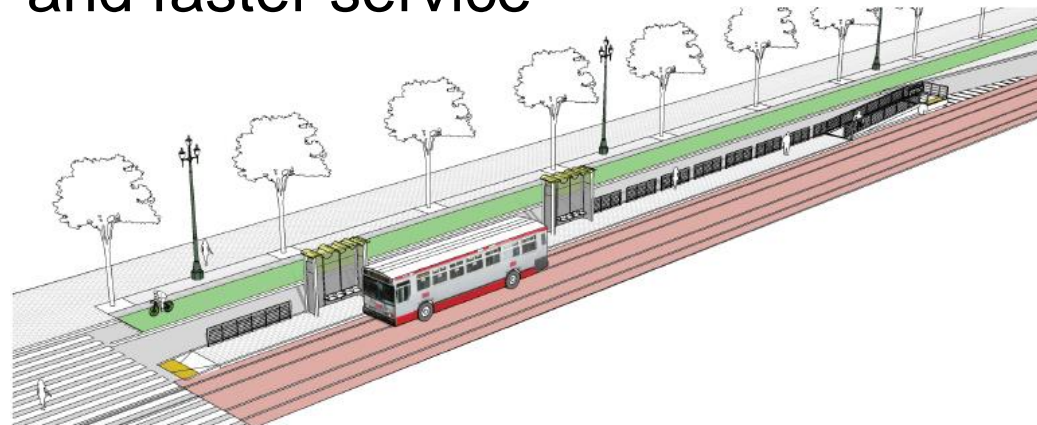
Proposed Muni Service



Travel time savings of up to 25% by changing stop spacing, reducing conflicts with bikes and extending Muni-Only Lanes.



Rapid service at BART/Muni stations provides easy transfers and faster service



Curbside local stops nearly every block minimize walking





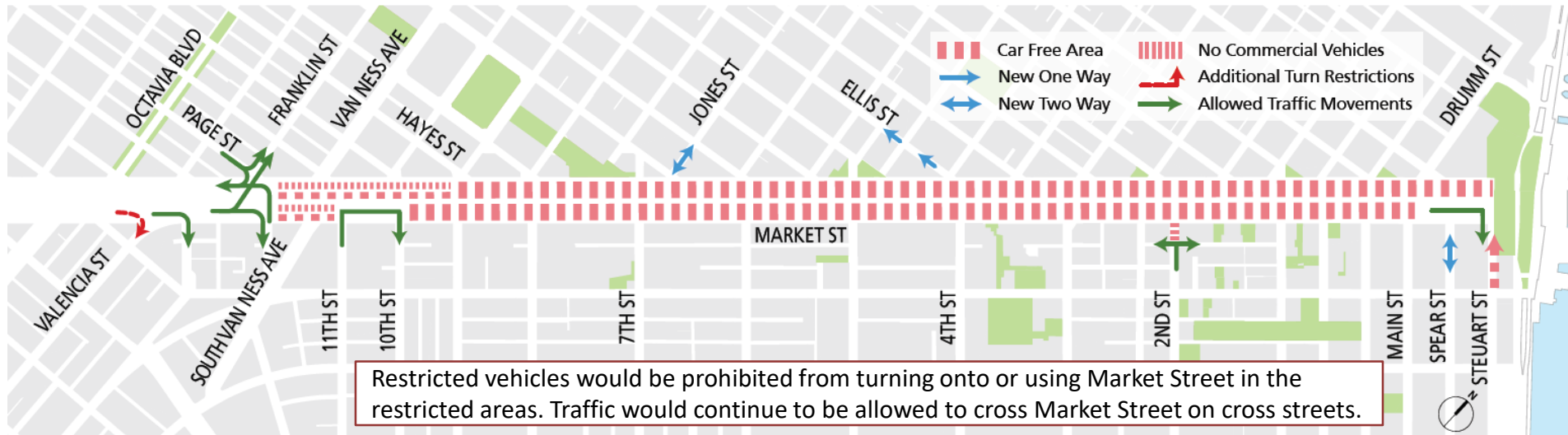
F Market Loop

New F line turn-back loop on McAllister and Charles J Brenham

- Enables short-turns for additional service where ridership is highest
- Improves transit flexibility and reliability

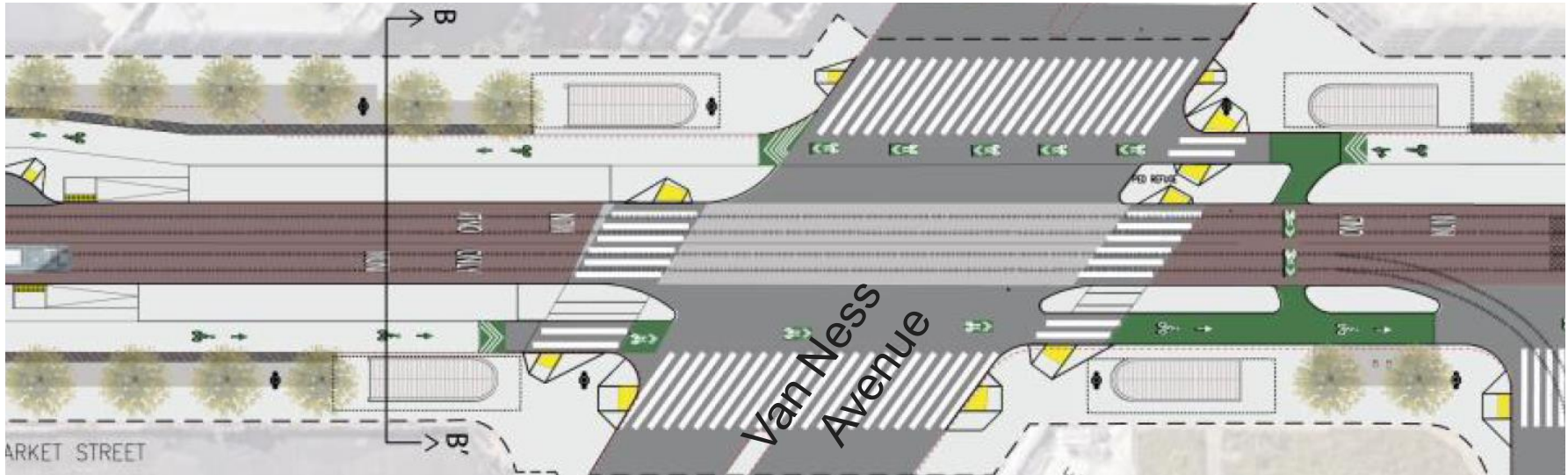


Private vehicle restrictions on Market



- Private vehicles would be prohibited from Market between 12th and Steuart, except eastbound 11th to 10th, and Drumm to Steuart
- Commercial vehicles, paratransit and taxis would continue to be allowed on Market, except for commercial vehicles near Van Ness
- Passenger and commercial loading zones would be added on cross streets for safe loading (including TNCs such as Uber, Lyft)
- Several adjacent street blocks converted to two-way (Spear, Jones) or one-way (Ellis) to facilitate circulation

- Market Street would be narrowed to only a Muni and taxi-only lane near Van Ness Avenue, removing two traffic lanes to allow for:
 - Intersection pedestrian safety improvements to narrow crossings
 - Dedicated bikeway near transit stops at Van Ness
 - Improved bike connection between Market Street and 11th Street
 - Upgraded transit stops and improved transit operations
- Private vehicles and commercial vehicles would be restricted eastbound 12th to 11th, and westbound Hayes to Franklin



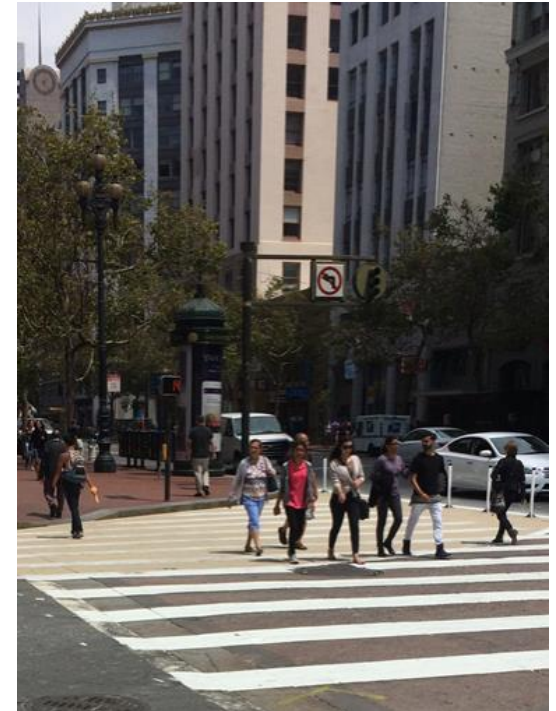
High-impact improvements to be implemented in early 2020



Car free zone east of 10th Street, with over 100 new cross-street loading zones

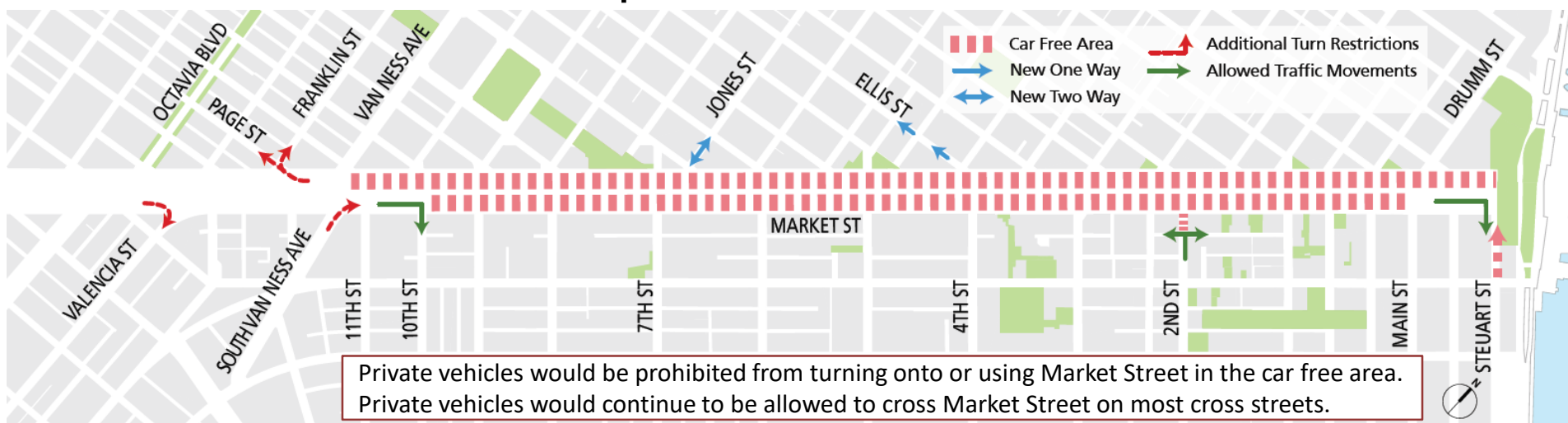


Muni-Only lane extended east to Main Street

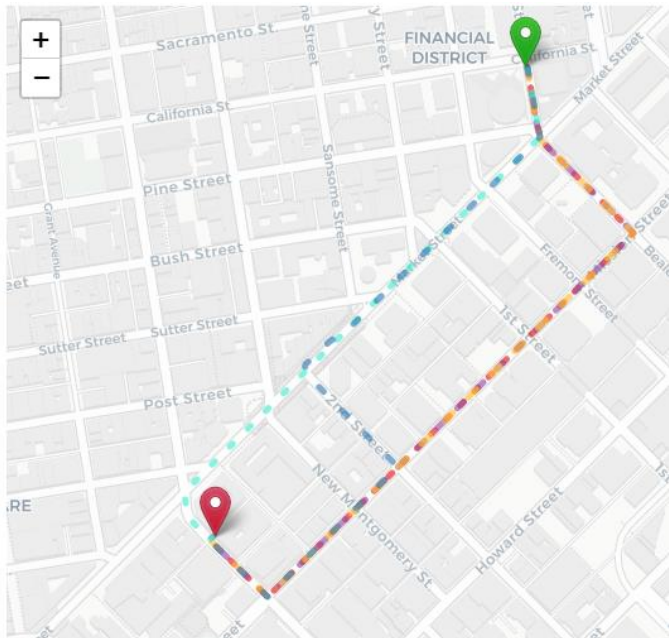


Painted safety zones at eight intersections

- Market would be car free to improve safety for people walking and biking in the highest volume areas:
 - Eastbound between 10th and Main
 - Westbound between Stuart and Van Ness
- Additional turn restrictions at Page/Franklin, Valencia and Van Ness
- Circulation changes on Jones and Ellis near Market Street also will be implemented



- 5 rounds of workshops / open houses
- Stakeholder meetings
- Community Working Group
- Turn by Turn Web Map and Plan-View Explorer
- Website
- Surveys
- Mailings
- Merchant loading survey
- Posters in corridor/on transit
- Multi-lingual outreach
- Tabling at events



Strong support for:

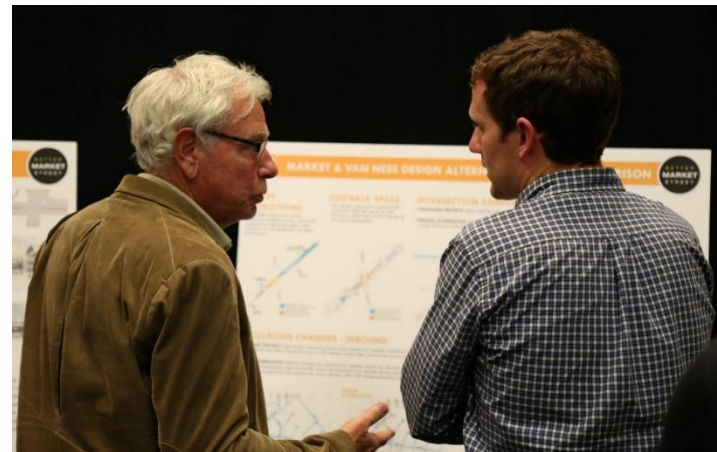
- Project vision and goals
- Separated bikeway
- Private vehicle restrictions

Recognition of multi-modal tradeoffs:

- Sidewalk widths
- Bike-truck conflicts with sidewalk-level loading zone
- Concern with more circuitous vehicle access

Design input:

- Vertical separation between sidewalk and bikeway
- Location of commercial and passenger loading zones on side streets



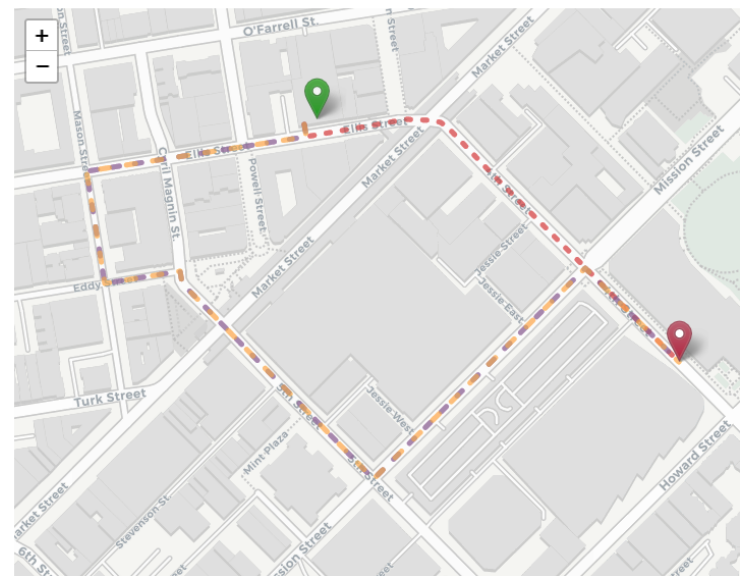
Surface transit stop spacing

- 85% of Market St transit riders would walk farther to a stop for faster transit
- Concerns with center lane Rapid stop spacing and connection to Central Subway resulted in modification to curb-lane stop at 4th Street



F Loop

- Support for operational benefits and improved access to Fisherman's Wharf
- Concerns from adjacent property owners resulted in modifying F line passenger stop location and exploring noise-reducing construction techniques



Vehicle Circulation / Western variant

- Concerns about less-direct car routes due to car-free zone and new one-way Ellis from Stockton to Cyril Magnin
- Concern about transit operations with only two lanes near Van Ness

CEQA

- January 14, 2015: Notice of Preparation to prepare EIR
- February 28-April 15, 2019: Draft EIR public comment period
- September 23, 2019: Final EIR published
- October 10, 2019: SF Planning Commission certification

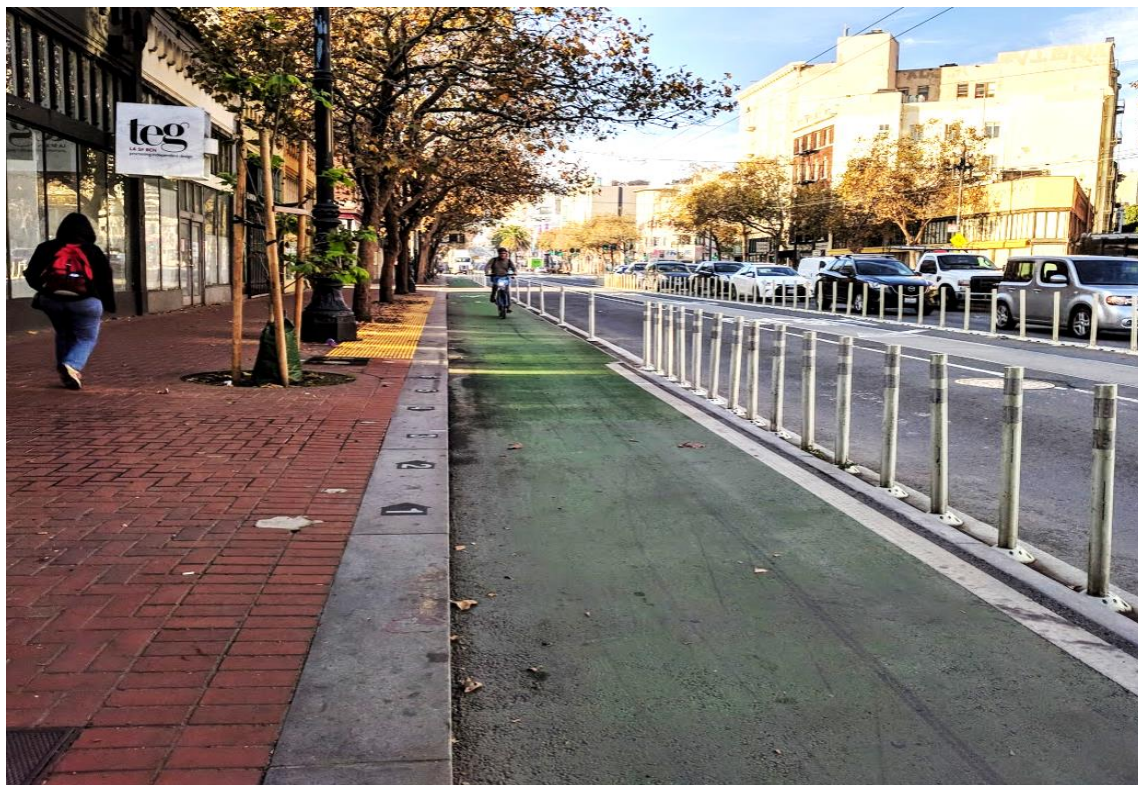
Project Approvals

- October 11, 2019: Public Works Hearing
- October 15, 2019: SFMTA Board of Directors Meeting

NEPA

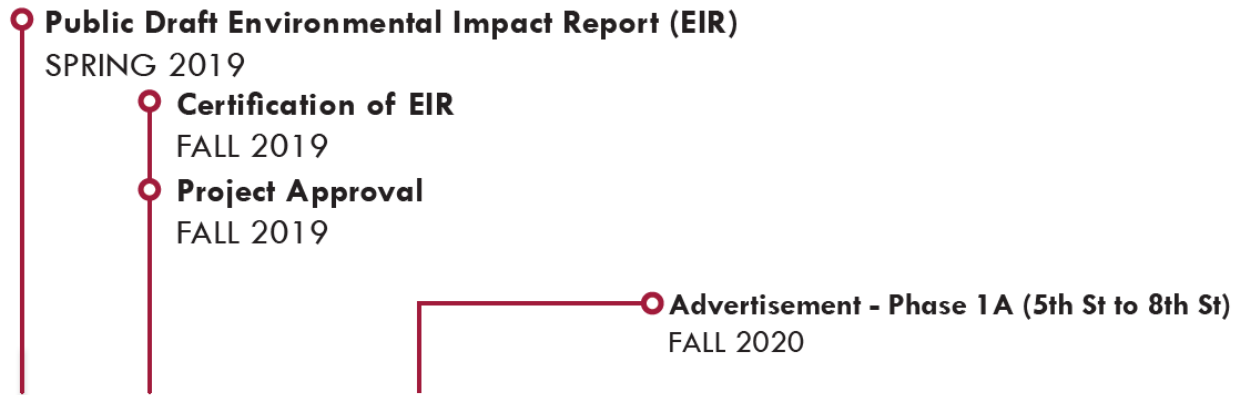
- Environmental Assessment (EA) underway
- Draft EA anticipated later this year, Final EA in 2020

Better Market Street pilot project will be constructed on the south side of Market Street between Gough and 12th streets, including:



- asphalt sidewalk-level bikeway
- detectable separation material recommended in Summer 2019 study
- proposed ADA accessible pavers on the sidewalk and granite curb

Project Schedule



2018 2019 2020 2021 2022 2023 2024

30% Design - Full Corridor
2018 - 2019

Design - Phase 1A
2018 - 2020

Early Implementation
2020

Pilot Project
2020

Construction - Phase 1A
2021 - 2023

Construction - Phase 1B
2023 - 2025

Phase 1A (5th to 8th Street) to start in 2021

Will include a construction mitigation plan, including:

- Signage/Marketing Campaign
- Ambassadors
- Contractor Incentives
- Directed Business Support
- Public Project Office Hours
- Strategically-placed staging areas
- Muni transit service plan, including F line bus replacement and transit improvements on Mission Street



Funding plan (\$M)



Fund Source	Status	PLAN	ENV	PS&E	CON Phase 1 A (5 th -8 th St)	Total by Fund Source	
General Fund	Allocated	2.5	2.6			5.1	
Octavia Land Sales	Allocated		3.1			3.1	
Market Octavia Impact Fees	Allocated		1.0			1.0	
Transit Center Impact Fees	Programmed			2.0		2.0	
Prop A GO Bond	Programmed	10.3	2.8	8.0	75.7	96.7	
MTA Operating Fund	Programmed	3.0				3.0	
BART (8th/Grove/Hyde/Market)	Programmed			.2	.4	.6	
Prop K (EP 22)	Programmed			2.2	10.4	12.6	
Prop K (EP 44)	Programmed				1.3	1.3	
OBAG 2	Programmed				3.4	3.4	
BUILD	Awarded				15.0	15.0	
SFPUC (Sewer, Water, Power, Streetlights)	Not reflected in current funding plan						
Total Identified Funding		15.8	9.5	12.5	106.1	143.8	
Total Unfunded							460.0
Total Project Cost							603.7

Fund Source

Federal	FTA 5309 (New Starts, Small Starts, Core Capacity)
Federal	FTA 5337 Fixed Guideway
Federal	OBAG 3 [FYs 2022/23-2026-27]
State	Senate Bill 1 Programs, Cap and Trade (e.g. ATP, LPP)
Regional	Regional Measure 3 (bridge tolls)
Local	MTA Prop B General Fund set-aside
Local	New Funding (vehicle license fee, bonds, sales tax, TNC tax)
Local	Impact Fees (Transit Center, etc.)

Thank you!

BETTER
MARKET
STREET

